

ARKANSAS STATE HIGHWAY COMMISSION

CONTRACT

FOR THE CONSTRUCTION OF

STATE JOB NO. C23006

STATE AID CITY JOB (CODE 9971)

CONWAY DAVIS ST. OVERLAY (S)

CITY STREET DAVIS STREET

IN FAULKNER COUNTY

Bound herein are the Supplemental Specifications, Special Provisions, Proposal Form and Schedule of Items applicable to this proposed construction contract.

Applicable to this proposed construction contract, but not bound herein, are the Arkansas State Highway Commission Standard Specifications for Highway Construction, Edition of 2014, and the Construction Plans.

1-17-08

Revised: 6-1-09

Revised: 2-15-12

Revised: 1-15-15

Revised: 5-26-16

Revised: 11-17-17

ARKANSAS DEPARTMENT OF TRANSPORTATION

NOTICE OF NONDISCRIMINATION

The Arkansas Department of Transportation (Department) complies with all civil rights provisions of federal statutes and related authorities that prohibit discrimination in programs and activities receiving federal financial assistance. Therefore, the Department does not discriminate on the basis of race, sex, color, age, national origin, religion (not applicable as a protected group under the Federal Motor Carrier Safety Administration Title VI Program), disability, Limited English Proficiency (LEP), or low-income status in the admission, access to and treatment in the Department's programs and activities, as well as the Department's hiring or employment practices. Complaints of alleged discrimination and inquiries regarding the Department's nondiscrimination policies may be directed to Joanna P. McFadden Section Head – EEO/DBE (ADA/504/Title VI Coordinator), P. O. Box 2261, Little Rock, AR 72203, (501)569- 2298, (Voice/TTY 711), or the following email address: joanna.mcfadden@ardot.gov

Free language assistance for Limited English Proficient individuals is available upon request.

This notice is available from the ADA/504/Title VI Coordinator in large print, on audiotape and in Braille.

Arkansas Department of Transportation
Supplemental Specifications and Special Provisions Listing
State Job Number C23006

The following supplemental specifications and special provisions for this project supplement the standard specifications, edition of 2014. In case of conflict, the supplemental specifications and special provisions shall govern.

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JOB SP	MANDATORY ELECTRONIC CONTRACT
JOB SP	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB SP	ADJUSTMENT OF QUANTITIES FOR STATE-AID STREET PROJECTS
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JOB SP	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
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SS 400-4	DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES
SS 400-5	PERCENT AIR VOIDS FOR ACHM MIX DESIGNS
SS 400-6	LIQUID ANTI-STRIP ADDITIVE
SS 410-1	CONSTRUCTION REQUIREMENTS AND ACCEPTANCE OF ASPHALT CONCRETE PLANT MIX COURSES
SS 410-2	DEVICES FOR MEASURING DENSITY FOR ROLLING PATTERNS
SS 603-1	LANE CLOSURE NOTIFICATION
SS 604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES

ARKANSAS DEPARTMENT OF TRANSPORTATION

SUPPLEMENTAL SPECIFICATION

ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS

Errors noted in the printed book of Standard Specifications for Highway Construction, Edition of 2014, are listed below and this publication is hereby revised as follows:

- Page 124: The third sentence of the first paragraph of Subsection 110.03(c) should read: The Engineer will make a decision within 10 business days concerning the necessity or practicability of the request.
- Page 195: The sixth paragraph of subsection 303.02 should read: For Classes 1 through 8 materials, the fraction passing the #200 (0.075 mm) sieve shall not be greater than three-fourths of the fraction passing the #40 (0.0425 mm) sieve. For Classes 3 through 8, the fraction passing the #40 (0.425 mm) sieve shall have a liquid limit not greater than 25.
- Page 363: In the second paragraph of Subsection 502.02, the reference to ASTM 775 should be replaced by "ASTM A 775".
- Page 636: In the second paragraph of Subsection 730.02, the references to AASHTO M 183 should be replaced with ASTM A36.
- Page 637: The last sentence of the second paragraph of Subsection 730.03 should read: All bolts, nuts, and washers shall be galvanized according to AASHTO M 232 or ASTM B 695, Class 40 or 50.
- Page 767: In the fourth paragraph of Subsection 807.06(a), the reference to ASTM B595 should be replaced by "ASTM B695".
- Page 841: Subsection 817.04(a) should read: The treatment of lumber and timber shall meet the applicable requirements of the current edition of the AWPA, Standards U1, Commodity Specification E, Use Category UC4C.

ARKANSAS DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION

JOB NO. C23006

BIDDING REQUIREMENTS AND CONDITIONS

Section 102 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

The fourth sentence of the second paragraph of **Subsection 102.01** is hereby deleted, and the following substituted therefore:

Prospective bidders may file a questionnaire at any time; however, prospective bidders will not be given authorization to submit a proposal unless a rating has been extended based on an acceptable questionnaire.

The last paragraph of **Subsection 102.01** is hereby deleted.

The second sentence of **Subsection 102.02** is hereby deleted, and the following substituted therefore:

The Notice to Contractors will contain a description of the proposed work, and information regarding access to proposal documents, plans, specifications, and the amount and nature of the proposal guaranty.

Subsection 102.03 is hereby deleted, renamed **Contents of Proposal Documents**, and the following substituted therefore:

The proposal documents will state the location and description of the contemplated construction and will show the estimate of the various quantities and kinds of work to be performed or materials to be furnished, and will have a schedule of items. The proposal documents will state the time in which the work must be completed, the amount of the proposal guaranty, and the date and time of the letting of work. The documents will also include any special provisions or requirements that vary from or are not contained in the standard specifications.

All forms included in the proposal documents are considered a part thereof. The plans, specifications, and other documents designated in the proposal documents will be considered a part of the proposal whether included or not.

The first through fourth paragraphs of **Subsection 102.04** are hereby deleted, and the following substituted therefore:

To become an eligible bidder, prospective bidders must be registered to bid in Arkansas with Bid Express. Prospective bidders must also contact the Program Management Division at (501) 569-2261 during regular business hours between the date the project is advertised and 4:30 p.m. on the day prior to the scheduled bid opening to request to become eligible to bid specific projects. Only prequalified contractors or their authorized representative may request to become an eligible bidder.

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SPECIAL PROVISION

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BIDDING REQUIREMENTS AND CONDITIONS

If the prospective bidder's prequalification rating is not "unlimited", the bidder shall file a certification with the Department citing all contracts in force and the unfinished value of such work. A prospective bidder will not be allowed to submit a proposal until a certification for the current bidding period is on file and the amount of work the contractor may be allowed to undertake is determined. The contractor's prequalification rating, less the unfinished value of all contracts in force, will determine the amount of additional work that the contractor may be allowed to undertake. A contractor will not be allowed to submit a proposal on an individual project for which the estimated cost is more than the amount that the contractor may be allowed to undertake, but the contractor will be allowed to submit a proposal on more than one project, providing that the estimated cost of each project is not more than the amount that the contractor may be allowed to undertake. In the event a contractor submits a low bid on more than one project and the aggregate amount is greater than the amount the contractor may be allowed to undertake, the Commission will exercise its discretion in the award of a particular project or projects.

A charge will be assessed for authorization to submit a proposal, paper copies of the proposal documents, and plans issued. These services are provided during regular business hours until 4:30 p.m. on the day prior to the scheduled bid opening at the Arkansas Department of Transportation, 10324 Interstate 30, Little Rock, Arkansas 72209, (501) 569-2261. Payment shall be made at the time services are provided or upon receipt of statement therefore. No refund will be allowed for bids not submitted or for plans or proposal documents returned.

The second sentence of the first paragraph of **Subsection 102.06** is hereby deleted, and the following substituted therefore:

The bidder is expected to examine carefully the site of the proposed work, the proposal documents, plans, specifications, supplemental specifications, and special provisions before submitting a proposal.

The first paragraph of **Subsection 102.07** is hereby deleted, and the following substituted therefore:

The proposal shall only be submitted through the internet bidding service, Bid Express. The bidder shall specify a unit price in figures for each pay item for which a quantity is given. A unit price of "zero" (\$0.00) is a valid price and will be considered. A blank unit price is not considered valid. The unit bid price should not be carried beyond 1 cent (\$0.01). Any figures on the unit bid price beyond 1 cent will be dropped.

The second and third paragraphs of **Subsection 102.07** are hereby deleted.

ARKANSAS DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION

JOB NO. C23006

BIDDING REQUIREMENTS AND CONDITIONS

The fifth paragraph of **Subsection 102.07** is hereby deleted, and the following substituted therefore:

The bidder's proposal must be submitted with a digital signature containing the name of the individual, one or more members of the partnership, one or more members or officers of each firm representing a joint venture, or one or more officers of a corporation, or by an agent of the Contractor legally qualified and acceptable to the Department.

The sixth paragraph of **Subsection 102.07** is hereby deleted, and the following substituted therefore:

If the proposal is submitted with a digital signature of any person who is not listed in the bidder's Prequalification Questionnaire (Questionnaire Form) as the individual, as a partner of a partnership, or as an officer of a corporation, authorization for such submittal should be on file with the Department prior to the download of bids. This authorization shall be made before the downloading of bids and be in the form of a Power of Attorney duly executed and signed by an official with power to constitute such authority.

The last sentence of the seventh paragraph of **Subsection 102.07** is hereby deleted, and the following substituted therefore:

Those items of Asphalt Binder that are subject to a minimum bid price will bear the note "(Minimum bid price is \$120.00 per ton)" within the Schedule of Items of the proposal documents.

The first sentence of the ninth paragraph of **Subsection 102.07** is hereby deleted, and the following substituted therefore:

The proposal documents for all federal aid projects will contain a bidders list.

The last sentence of the ninth paragraph of **Subsection 102.07** is hereby deleted, and the following substituted therefore:

The information provided will not be used for contract awarding purposes but must be provided before the Contractor will be given authorization to submit proposals for future lettings.

Subsection 102.08 Irregular Proposals is hereby deleted, and the following substituted therefore:

- (a) Proposals will be considered irregular and will be rejected for the following reasons:
- (1) If the proposal does not contain a unit price for each pay item listed except in the case of authorized alternate pay items.
 - (2) If the proposal is not digitally signed by an authorized representative of the firm.

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BIDDING REQUIREMENTS AND CONDITIONS

- (3) If the proposal is not accompanied by the proper proposal guaranty.
 - (4) If a proposal is received from an individual, firm, partnership, or corporation with an interest, as principal, in another proposal for the same project.
 - (5) If the proposal is not accompanied by the Certification to Submit DBE Participation.
- (b) Proposals will be considered irregular and may be rejected for the following reasons:
- (1) If the proposal is not accompanied by a bid schedule and bid schedule narrative as required in the proposal documents.
 - (2) Unbalanced proposals in which the prices for some items are out of proportion to the reasonable costs representative of those items.
 - (3) If there are irregularities of any kind that may tend to make the proposal incomplete, indefinite, or ambiguous as to its meaning.

The first sentence of **Subsection 102.09** is hereby deleted and the following substituted therefore:

No proposal will be considered by the Commission unless a guaranty in the form of a bank draft, certified check, or cashier's check drawn on a solvent bank or trust company, or a bidder's paper bond executed by an approved surety company has been received by the Program Management Division prior to the download of bids.

The following paragraph is hereby added after the first paragraph of **Subsection 102.09**:

Electronic bid bonds are allowed. The prospective bidder should verify their bid bond in their proposal prior to submission.

Subsection 102.10 is hereby deleted and the following substituted therefore:

The proposal shall only be submitted through the internet bidding service, Bid Express.

Subsection 102.11 is hereby deleted, and the following substituted therefore:

A bidder may withdraw or modify a proposal after it has been submitted to Bid Express, up to the time set for the deadline for proposals to be received. A proposal may also be withdrawn if the Commission fails to make an award within 40 calendar days after the date of downloading.

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BIDDING REQUIREMENTS AND CONDITIONS

Subsection 102.12 is hereby deleted, renamed **Downloading of Proposals**, and the following substituted therefore:

Proposals will be downloaded and then posted on the Department's website at the time and place indicated in the Notice to Contractors.

The last sentence of **Subsection 102.15** is hereby deleted, and the following substituted therefore:

In any case, the prospective bidders will be contacted prior to the download of bids.

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MANDATORY ELECTRONIC CONTRACT

Paper Contract Documents and Forms will not be accepted.

The Department will only accept and execute an electronic contract for this project through Doc Express, a paperless contracting system. Prospective bidders will need to contact Doc Express to set up an account prior to the bid opening date. The toll-free phone number for Doc Express is 1-888-352-2439 and their website address is www.docexpress.com.

Section 103 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows.

The first sentence of **Subsection 103.06(a)** is hereby deleted, and the following substituted therefore:

The Contract shall be electronically signed by the successful bidder and electronically submitted to the Program Management Division, Construction Contract Procurement Section, together with the required bonds and proof of liability insurance, within 10 business days after the notice of award has been issued.

Subsection 103.08(d)(3)d. is hereby deleted, and the following substituted therefore:

Documentation of the bidder's commitment to use a DBE subcontractor whose participation it submits to meet a contract goal; and

Subsection 103.08(d)(3)e. is hereby deleted, and the following substituted therefore:

Document confirmation from the DBE that it is participating in the contract as provided in the Contractor's commitment.

Subsection 103.08(d)(5) is hereby deleted, and the following substituted therefore:

The preceding information shall be submitted directly to the Arkansas Department of Transportation, Program Management Division, via Doc Express.

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MANDATORY ELECTRONIC DOCUMENT SUBMITTAL

Paper Document Submittals will not be accepted.

The Department will only accept electronically-submitted documents for consideration on this project. All correspondence and submittals to the Department shall be submitted through Doc Express, a paperless contracting system. When signed originals are required, the original shall be the document uploaded to Doc Express and the signature shall be the electronic signature applied through Doc Express. The Contractor shall use the same organizational account for project documentation as used to fulfill the requirements of the Mandatory Electronic Contract Special Provision. The toll-free phone number for Doc Express is 1-888-352-2439 and their website address is www.docexpress.com.

Any reference in the Standard Specifications to document submittal in writing or by U.S. Mail, facsimile, or in person is hereby amended to require that such documents be submitted using Doc Express with the following exceptions:

- Material delivery tickets which are used for payment or for field verification shall be submitted on paper as required by the Standard Specifications for Highway Construction, Edition of 2014.
- Any document with specific submittal requirements in state and/or federal law or federal regulations that conflict with the requirements of this Special Provision shall be submitted in accordance with such state and/or federal law or federal regulations.

A user guide is available on the Department's web page to assist Contractors with the use of Doc Express. The "Contractor Guide to Using Doc Express" is available on the Department's web page at this link:

http://ardot.gov/construction_division/Contractor_Guide_DocExpress.pdf.

The Contractor may provide access for subcontractors to view and submit items in Doc Express by following the instructions provided in the "Contractor Guide to Using Doc Express". Once an organizational account is activated and the Contractor provides access to the contract, a subcontractor may submit documents to the Contractor in Doc Express by uploading the electronic documents as directed in the User Guide. Any documents uploaded by the subcontractor must be then retrieved and published by the Contractor within Doc Express for further action by the Engineer. The Engineer will not review or take any actions on any documents submitted by the subcontractor until the document has been appropriately submitted by the Contractor.

Any submittals, documents, subcontracts, proposals, working drawings, or any other items submitted by the Contractor within Doc Express are not considered approved by the Engineer until written notification of the approval is published by the Engineer in the "CON-Correspondence-From Department to Contractor" drawer in Doc Express. Any action taken by the Contractor prior to this notification is taken at the Contractor's own risk.

The Department's System Administration team has no authority to take action on any documents submitted to the system. Access for this team is for management of the application only. Knowledge of any document submitted is not imputed to the Department by the knowledge of Systems Administration.

The requirements of this Special Provision shall supersede the requirements of all other Special Provisions unless such Special Provision includes a stated exception to this Special Provision.

ARKANSAS DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION

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ADJUSTMENT OF QUANTITIES FOR STATE-AID STREET PROJECTS

Section 104 of the Standard Specifications for Highways Construction, Edition of 2014, is hereby amended as follows:

The following is added as the last paragraph of **Subsection 104.02**:

On State-Aid Street Projects, funding amounts are limited for each city. The Commission reserves the right to increase or decrease the quantities on a project at the time of the award in order to utilize all funds that are available or to ensure that funds are available to pay for the work performed. For projects which include more than one city, each city may have a different funding limit. On those projects the Commission reserves the right to increase or decrease the quantities placed within each city in order to utilize all funds that are available or to ensure that funds are available to pay for the work performed. The Contractor shall perform the work as increased or decreased in each city and no allowance will be made for anticipated profits on increases or decreases so incurred.

ARKANSAS DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION

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**FLEXIBLE BEGINNING OF WORK
(PAVEMENT PRESERVATION, CITY AID, AND STATE AID PROJECTS)**

Section 108, Prosecution and Progress, of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

Subsection 108.02 (b) Work Order.

(2) The last bullet stating "Erection of advance warning signs." is hereby deleted and the following substituted therefor:

- The Contractor may elect to erect advance warning signs for their convenience after the issuance of the work order but prior to the date that work will begin, provided that:
 - i. The signs are covered and kept properly covered until the work begins. Failure to cover the signs after initial installation will constitute the Contractor's intent to begin work and time charges will be assessed starting that date. The sign covering shall cover the face completely and prevent the message from being read during both day and night conditions and shall not damage the sheeting face. The covering materials shall be heavy duty, opaque and either dark green, brown, or black in color.
 - ii. No progress payments will be made for the Signs or Mobilization until after the work begins (for the purpose of this special provision, beginning of work is defined as the beginning of contract work other than the installation of signs and other traffic control devices, such as paving operations, shoulder construction operations, cold milling, etc.)

(4) **b. Working Day Contract.** is hereby deleted and the following substituted:

b. Working Day Contract. Unless the Contractor is otherwise advised in writing, the Work Order for a working day contract shall become effective on the fifteenth calendar day following the execution of the Contract by the Department. Should the effective date fall on Saturday, Sunday, a legal holiday designated in Subsection 101.01(c), Monday following a holiday on Sunday, or Friday preceding a holiday on Saturday, the effective date shall be the next work day. The written Work Order from the Engineer will follow with the effective date being as specified. The assessment of contract time, according to Subsection 108.06, will commence when the Contractor begins work or on July 6, 2020 if the Contractor has not commenced work. The Contractor will submit written notification to the Engineer five days prior to commencing work.

(4) **Subsection 108.02(c)** is hereby deleted.

ARKANSAS DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION

JOB NO. C23006

SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS

Division 106 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

The following is hereby added to **Subsection 106.04, Acceptance of Materials**:

All ACHM Contractor Acceptance Tests shall be submitted electronically by use of the ACHM Microsoft Excel Spreadsheet for Contractors/Suppliers and on paper.

The ACHM Microsoft Excel Spreadsheet for Contractors/Suppliers can be downloaded from the following website: http://www.ardot.gov/contracts/contractor_information/contractor.aspx.

To download this file and the supporting documentation, click on the link labeled Contractor_ACHM.exe which is listed under User Help File and Utilities on the website noted above.

Use of this file requires Microsoft Excel 2000, 2003, or 2007.

The preferred method of transmitting the file is to e-mail the completed ACHM Microsoft Excel Spreadsheet for Contractors/Suppliers to the Department's ACHM Plant Inspector assigned to the project. It is also acceptable to transmit the file by Compact Disk (CD) or other electronic device. Regardless of the method of transmission used, the signed paper acceptance tests must be provided to the Resident Engineer.

Any questions or issues arising from the use of this file should be referred to the Resident Engineer.

ARKANSAS DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION

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BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT

Section 409.03(h) of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

The following bullet is added under detailed requirements:

- Broadband Internet Service shall be provided.

The Broadband Internet Service shall be provided with an Internet Protocol (IP) address which is reachable on the global Internet (public) and which is permanently assigned (static). The Contractor is not required to provide this service if an IP address which is both static and public is not available.

If this service is not available at the beginning of a project but becomes available during the life of the project, the Contractor shall provide the service immediately from the date of availability.

The data transfer rate shall be 3 megabits per second (Mbps) download and 500 kilobits per second (kbps) upload, or higher, with latency not to exceed 150 milliseconds. If the Broadband Internet Service meets all of the requirements of this specification except for the data transfer rate and/or latency, then the best performing available connection shall be provided.

Prior to the selection of the Broadband Internet Service provider, the Contractor shall submit to the Resident Engineer, in writing, the proposed method for providing Broadband Internet Service. The Resident Engineer shall review this submittal and respond in writing regarding the acceptability of the proposed method.

The Broadband Internet Service shall be provided with equipment providing one Ethernet port.

ARKANSAS DEPARTMENT OF TRANSPORTATION

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WARM MIX ASPHALT

DESCRIPTION: The Department will allow the use of Warm Mix Asphalt (WMA). All provisions for the production and placement of conventional HMA mixtures as stipulated in Section 410 Construction Requirements and Acceptance of Asphalt Concrete Plant Mix Courses of the Standard Specifications for Highway Construction, Edition 2014, are applicable except as noted below.

Section 410 Construction Requirements and Acceptance of Asphalt Concrete Plant Mix Courses of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

Section 410.03: Replace the third sentence with "WMA production temperatures at the plant shall be according to the Contractor's approved mix design but may be adjusted based on recommendations of the WMA additive/process manufacturer."

Add the following paragraph: "Implementation of best management practices in the control of aggregate moisture content prior to introduction to the drying or mixing drum is highly recommended in order to achieve the maximum benefit of WMA technology."

Section 410.07: Replace the last sentence of the first paragraph with "Spreading and finishing temperatures shall be according to the Contractor's approved mix design, but in no case shall the WMA be placed at a temperature less than 220° F."

ARKANSAS DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION

JOB NO. C23006

DENSITIES FOR ACHM SURFACE COURSE

Division 400 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

The following is hereby added as the second paragraph of **Subsection 407.04, Construction Requirements and Acceptance:**

On city street projects, the Engineer may reduce the required minimum density of the ACHM Surface Course to 90% of the maximum theoretical density based on actual field conditions which would either prevent obtaining 92.0% of the maximum theoretical density or when damage to underlying utilities and/or subgrade is imminent. However, no reduction from the specified density of 92.0% will be permitted on ACHM placed on a subgrade, an underlying base course, or layers of ACHM constructed by the Contractor under this same contract.

ARKANSAS DEPARTMENT OF TRANSPORTATION

SPECIAL PROVISION

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RECYCLED ASPHALT SHINGLES

DESCRIPTION: This specification covers the use of Recycled Asphalt Shingles (RAS) in Asphalt Concrete Hot Mix (ACHM).

MATERIALS: ACHM utilizing RAS shall meet all of the requirements of Sections 404, 409, 410, and 416 and the requirements of Section 405, 406, or 407 for the mixture type specified on the plans. The mix design will follow AASHTO MP 23-14 and AASHTO PP 78-14 except as follows:

- (1) A maximum of three (3) percent RAS by total mix weight will be allowed in any mix design. The amount of RAS will be included in the total Recycled Asphalt Pavement (RAP) percentage if RAP is used.
- (2) Shingle material shall be processed so that 100 percent passes the 3/8" (9.5 mm) sieve and ninety-five (95) percent passes the No. 4 (4.75 mm) sieve.
- (3) Pre-consumer, or manufacturer waste, shingles may be used in mix designs requiring PG 64-22 binder only.
- (4) Post-consumer, or tear-off, shingles may be used in mix designs requiring PG 64-22 binder only.
- (5) Pre- and post-consumer shingles may not be combined in mix designs.
- (6) Tear-off shingles shall be acquired from residential sources only and shall have been produced after 1980. In no case shall a roofing material containing rubber or rubber-like polymer components be used as RAS in asphalt mixes.
- (7) When using tear-off shingles, one stockpile test per lot of mix shall be conducted to confirm the binder content and gradation of the RAS product, and the sample shall be representative of the RAS material entering the production process for the lot tested. Binder content and gradation shall be determined using ARDOT 450 or AASHTO T 308. If AASHTO T 308 is used, the sample size shall be 400 grams, and the sample shall be oven dried to a constant mass (at a temperature not exceeding 140°F) prior to testing. Shingle fibers shall be removed from the extracted sample prior to gradation testing.

ARKANSAS DEPARTMENT OF TRANSPORTATION**SPECIAL PROVISION****JOB NO. C23006****TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES (MASH)**

Section 604 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

The first paragraph of subsection **604.02 Materials (a) General** is hereby deleted and the following substituted therefore:

All work zone traffic control devices used on the project, including sign supports, barricades, traffic drums equipped with flashing lights, crash cushions, and impact attenuators, manufactured after December 31, 2019, shall comply with the requirements of the Manual for Assessing Safety Hardware (MASH). Such devices manufactured on or before December 31, 2019, and successfully tested to the requirements of National Cooperative Highway Research Program (NCHRP) Report 350 or the 2009 edition of MASH, may continue to be used throughout their normal service lives. The Contractor shall furnish a certification of such compliance from the manufacturer or supplier of all work zone traffic control devices prior to using the devices on the project. The certification shall state the device meets the requirements of MASH, or in the case that the device was manufactured on or before December 31, 2019, the certification shall state the device meets the requirements of NCHRP 350 or MASH. The certification shall include a copy of the Federal Highway Administration's (FHWA) approval letter with all attachments for each device. Devices shall be fabricated and installed in accordance with the plans and with the crash testing documentation provided in the FHWA approval letter which is available at:

http://safety.fhwa.dot.gov/roadway_dept/policy_guide/road_hardware/

ARKANSAS DEPARTMENT OF TRANSPORTATION
SPECIAL PROVISION
LIQUIDATED DAMAGES

As specified in the Contract, liquidated damages for this project will be as shown in the following tables:

WORKING DAY PROJECTS

ORIGINAL CONTRACT AMOUNT		RATE
FROM MORE THAN	TO AND INCLUDING	-----
\$ 0	\$ 50,000	\$ 400
50,000	100,000	700
100,000	500,000	800
500,000	1,000,000	1,100
1,000,000	2,000,000	1,300
2,000,000	5,000,000	1,500
5,000,000	10,000,000	1,900
10,000,000	15,000,000	2,000
15,000,000	20,000,000	2,100
20,000,000	-----	2,500

FIXED DATE PROJECTS

ORIGINAL CONTRACT AMOUNT		RATE
FROM MORE THAN	TO AND INCLUDING	-----
\$ 0	\$ 50,000	\$ 90
50,000	100,000	100
100,000	500,000	200
500,000	1,000,000	250
1,000,000	2,000,000	320
2,000,000	5,000,000	400
5,000,000	10,000,000	600
10,000,000	-----	750

**ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
CONTRACTOR'S LICENSE**

Section 102 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

The third paragraph of **Subsection 102.01, Prequalification of Bidders** is hereby deleted and the following substituted thereof:

The attention of prospective bidders is directed to Ark. Code Ann. §17-25-101 et seq., Act 150 of the 1965 Acts of Arkansas, being an "Act Regulating the Practice of Contracting in the State of Arkansas", and any subsequent amendments made thereto. When the work offered is financed in whole with State funds and is estimated to cost \$50,000 or more, the prospective bidder must show evidence of license with the Contractors Licensing Board for the State of Arkansas before being furnished with a proposal form.

The third paragraph of **Subsection 108.01, Subletting of Contract** is hereby deleted and the following substituted thereof:

It shall be the responsibility of the Contractor to determine that all parties performing work amounting to \$50,000 or more are currently licensed by the Contractors Licensing Board for the State of Arkansas.

**ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
DEPARTMENT NAME CHANGE**

All references to the Arkansas State Highway and Transportation Department contained within the Standard Specifications for Highway Construction (Edition of 2014), the Qualified Products List, the Manual of Field Sampling and Testing Procedures, plan sheets, Supplemental Specifications, and all Special Provisions contained in this proposal are hereby deleted and replaced with the title of Arkansas Department of Transportation.

All references to AHTD contained within the Standard Specifications for Highway Construction (Edition of 2014), the Qualified Products List, the Manual of Field Sampling and Testing Procedures, plan sheets, Supplemental Specifications, and all Special Provisions contained in this proposal are hereby deleted and replaced with the abbreviation ARDOT.

All references to the Arkansas State Highway Commission contained within the Standard Specifications for Highway Construction (Edition of 2014), the Qualified Products List, the Manual of Field Sampling and Testing Procedures, the Standard Drawings, plan sheets, Supplemental Specifications, and all Special Provisions contained in this proposal remain in effect.

**ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
ISSUANCE OF PROPOSALS**

Section 102 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

Subsection 102.04(j) is hereby deleted and the following is substituted therefore:

(j) If the prospective bidder is the Contractor on a current Contract with the Commission on which Liquidated Damages are being assessed, and there are no pending time extensions warranted to remove the project from Liquidated Damages.

Subsection 102.04(k) is hereby deleted and the following is substituted therefore:

(k) If the prospective bidder has a current Contract in default.

ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER

Section 108 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

Subsection 108.02(b)(2) is hereby deleted and the following is substituted therefore:

(2) The delivery to the Department for execution of the Contract and bonds properly executed on behalf of the Contractor and surety and the minimum 72 hours advance notice as required above shall constitute the Contractor's authority to begin the following items of work:

- Mobilization;
- Preparation of shop drawings and other required submissions;
- Ordering, fabrication, assembly, and/or stockpiling of materials;
- Driving Test Piling; and
- Contract surveying, when Roadway and/or Bridge Construction Control is included in the Contract.
- Erection of advance warning signs.
- Installation of netting on structures to prevent nesting of migratory birds in accordance with applicable Special Provisions (if included in the Contract).
- Set up, installation, and testing of Automated Work Zone Information Systems (if included in the Contract).
- Off-site area approval process per Section 107.10(c).

Such advance work shall be subject to the Contractor's assumption of the risk of cancellation of the award and the following:

- The Contractor shall, on commencing such operations, take all precautions required for public safety and shall observe all the provisions in the Contract;
- In the event of cancellation of the award, the Contractor shall at Contractor expense do such work as necessary to leave the site in a neat condition to the satisfaction of the Engineer;
- In the event of cancellation of the award, all work performed shall be deemed to be at the Contractor's expense; and
- All work done under this subsection in accordance with the Contract before its execution by the Commission will, when the Contract is executed, be considered authorized work and will be paid for as provided in the Contract.

Unless otherwise notified in writing, no time will be assessed for work performed prior to the effective date of a Work Order.

No payments will be made prior to the date established by the Engineer under Subsection 109.07, which date will be after the effective date of a Work Order.

The Contractor shall not be entitled to any additional compensation or an extension of time for any delay, hindrance, or interference caused by or attributable to commencement of work before the effective date of a Work Order.

ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
QUALITY CONTROL AND ACCEPTANCE

Division 300 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

The first sentence of the third paragraph **Subsection 306.03 Acceptance Testing** is hereby deleted and the following substituted therefor:

If the material being furnished is crushed stone the Department will furnish the PL, LL, and PI for the material, further tests for PL, LL, and PI are waived.

ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
TACK COATS

Division 400 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

Section 401, Prime and Tack Coats and Emulsified Asphalt in Base Course, is hereby modified as follows:

The first sentence of **Subsection 401.03(a)** is hereby deleted and the following substituted therefore:

The surface to be treated with prime or tack coat shall be cleaned of dust, dirt, and loose or foreign material by sweeping with mechanical brooms immediately preceding the application of the prime or tack coat.

Third sentence of **Subsection 401.03(c)** is hereby deleted and the following is substituted therefore:

No dilution beyond that which is part of the emulsification process is permitted. The tack coat shall not be diluted, cut, or otherwise thinned after receipt from the manufacturer's facility.

The fifth sentence of **Subsection 401.03(c)** is hereby deleted and the following substituted therefore:

The rate of application shall be from 0.03 gallon to 0.10 gallon per square yard (0.1 L/sq m to 0.5 L/sq m) of residual asphalt as designated by the Engineer.

Section 410, Construction Requirements and Acceptance of Asphalt Concrete Plant Mix Courses, is hereby modified as follows:

The sixth paragraph of **Subsection 410.05** is hereby deleted and the following substituted therefore:

For foreign material, or when the time lapse between courses is more than 8 hours, the earlier course shall be cleaned and given a tack coat before placing the succeeding course. When directed, the tack coat shall be applied and paid for under Section 401. If directed by the Engineer, a tack coat shall be used even though the elapsed time has been less than 8 hours.

ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES

Division 400 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

Section 404, QUALITY CONTROL OF ASPHALT MIXTURES, is hereby modified as follows:

The third paragraph **Subsection 404.04** is hereby deleted and the following substituted therefore:

The accepted mix design shall be field verified by the Contractor at the start of mix production or after an interruption of more than 120 calendar days. The asphalt mixture shall be verified by testing mix that has been produced through the plant using the aggregate proportions shown on the accepted mix design. Production of Department approved mix designs for placement on non-ARDOT projects may be used for mix verification. The Contractor shall notify the Engineer sufficiently in advance for Department personnel to witness all testing of this production and shall provide copies of all test results to the Department.

Section 410, Construction Requirements and Acceptance of Asphalt Concrete Plant Mix Courses is hereby modified as follows:

The first and second sentence of **Subsection 410.09, Acceptance of the Payment and Adjustments in Payment**, is hereby deleted and the following is substituted therefore:

- (a) General. The accepted mix design shall be verified by the Contractor at the start of mix production for that design or after an interruption of more than 120 calendar days. A maximum of 200 tons (200 metric tons) of materials may be placed on the roadway during the verification process.

Section 411, Asphalt Concrete Plant Mix is hereby modified as follows:

The third sentence of Subsection 411.05 (B), Acceptance is hereby amended and the following is substituted therefore:

- (b) Acceptance. The accepted mix design shall be field verified by the Contractor at the start of mix production or after an interruption of more than 120 calendar days.

ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
PERCENT AIR VOIDS FOR ACHM MIX DESIGNS

Division 400 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

The fourth sentence of Paragraph 1 of **Subsection 404.01(b), Design Requirements**, is hereby deleted and the following substituted therefor:

The optimum asphalt content is the asphalt binder content at 4% Air Voids (AV).

The first bullet of Paragraph 1 is hereby deleted and the following substituted therefor:

- PG 64-22 and PG 70-22 mixes will be designed using 4% air voids;

The second sentence of Paragraph 2 of **Subsection 404.04, Quality Control of Asphalt Mixtures**, is hereby deleted and the following substituted therefor:

Adjustments to the accepted mix design to conform to actual production values without re-design of the mixture shall be based on production of the mixture at a target value of 4.0% Air Voids (AV) in specimens and an asphalt binder content not less than that specified in the accepted mix design.

Table 405-1 of **Subsection 405.03 Materials** is hereby deleted and the following substituted therefor:

Table 405-1
Design Requirements for Asphalt Concrete Hot Mix Base Course
(1-1/2" [37.5 mm])

Sieve (mm)	Control Points	
	Percent Passing (%)	
2" (50.0)	100	
1½" (37.5)	90 - 100	
1" (25.0)	90 max.	
No. 4 (4.75)	-	
No. 8 (2.36)	15 - 41	
No. 16 (1.18)	-	
No. 30 (0.60)	-	
No. 50 (0.30)	-	
No. 200 (0.075)	0 - 6	
Asphalt Binder Content	Design Value	
% Air Voids	4.0	
% VMA	11.5 – 13.0	
Minimum Water Sensitivity Ratio	80.0	
% Anti-strip	As Required	
Fines to Asphalt Ratio*	0.6 – 1.6	
Wheel Tracking Test (8000 cycles, 100 psi, 64°C)	<u>Design Gyration</u>	<u>Maximum Rut</u>
	75 & 115	0.315 in. (8.000 mm)
	160	0.197 in. (5.000 mm)
	205	0.197 in. (5.000 mm)

*Fines to asphalt ratio shall be defined as the percent materials passing the No. 200 (0.075 mm) sieve (expressed as a percent of total aggregate weight) divided by the effective asphalt binder content.

ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
PERCENT AIR VOIDS FOR ACHM MIX DESIGNS

Table 406-1 of **Subsection 406.04, Construction Requirements and Acceptance**, is hereby deleted and the following substituted therefor:

Table 406-1		
Design Requirements for Asphalt Concrete Hot Mix Binder Course (1" [25 mm])		
	Control Points	
Sieve (mm)	Percent Passing (%)	
1½" (37.5)	100	
1" (25.0)	90 - 100	
¾" (19.0)	90 max.	
No. 4 (4.75)	-	
No. 8 (2.36)	19 - 45	
No. 16 (1.18)	-	
No. 30 (0.60)	-	
No. 50 (0.30)	-	
No. 200 (0.075)	1 - 7	
Asphalt Binder Content	Design Value	
% Air Voids	4.0	
% VMA	12.5 – 14.0	
Minimum Water Sensitivity Ratio	80	
% Anti-strip	As Required	
Fines to Asphalt Ratio*	0.6 – 1.6	
Wheel Tracking Test (8000 cycles, 100 psi, 64°C)	<u>Design Gyration</u>	<u>Maximum Rut</u>
	75 & 115	0.315 in. (8.000 mm)
	160	0.197 in. (5.000 mm)
	205	0.197 in. (5.000 mm)

*Fines to asphalt ratio shall be defined as the percent materials passing the No. 200 (0.075 mm) sieve (expressed as a percent of total aggregate weight) divided by the effective asphalt binder content.

**ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
PERCENT AIR VOIDS FOR ACHM MIX DESIGNS**

Table 407-1 and Table 407-2 of **Subsection 407.04, Construction Requirements and Acceptance**, are hereby deleted and the following substituted therefor:

Table 407-1
Design Requirements for Asphalt Concrete Hot Mix Surface Course
(1/2" [12.5 mm])

Sieve (mm)	Control Points	
	Percent Passing (%)	
3/4" (19.0)	100	
1/2" (12.5)	90 - 100	
3/8" (9.5)	90 max.	
No. 8 (2.36)	28 - 58	
No. 16 (1.18)	-	
No. 30 (0.60)	-	
No. 50 (0.30)	-	
No. 200 (0.075)	2 - 10	
Asphalt Binder Content	Design Value	
% Air Voids	4.0	
% VMA	14.0 – 16.0	
Minimum Water Sensitivity Ratio	80.0	
% Anti-strip	As Required	
Fines to Asphalt Ratio*	0.6 – 1.6	
Wheel Tracking Test	<u>Design Gyration</u>	<u>Maximum Rut</u>
(8000 cycles, 100 psi, 64°C)	75 & 115	0.315 in. (8.000 mm)
	160	0.197 in. (5.000 mm)
	205	0.197 in. (5.000 mm)

*Fines to asphalt ratio shall be defined as the percent materials passing the No. 200 (0.075 mm) sieve (expressed as a percent of total aggregate weight) divided by the effective asphalt binder content.

ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
PERCENT AIR VOIDS FOR ACHM MIX DESIGNS

Table 407-2

Design Requirements for Asphalt Concrete Hot Mix Surface Course (3/8" [9.5 mm])

	Control Points	
Sieve (mm)	Percent Passing (%)	
½" (12.5)	100	
3/8" (9.5)	90 - 100	
No. 4 (4.75)	90 max.	
No. 8 (2.36)	32 - 67	
No. 16 (1.18)	-	
No. 30 (0.60)	-	
No. 50 (0.30)	-	
No. 200 (0.075)	2 - 10	
Asphalt Binder Content	Design Value	
% Air Voids	4.0	
% VMA	15.0 – 17.0	
Minimum Water Sensitivity Ratio	80.0	
% Anti-strip	As Required	
Fines to Asphalt Ratio*	0.6 – 1.6	
Wheel Tracking Test	<u>Design Gyration</u>	<u>Maximum Rut</u>
(8000 cycles, 100 psi, 64°C)	75 & 115	0.315 in. (8.000 mm.)
	160	0.197 in. (5.000 mm)
	205	0.197 in. (5.000 mm)

*Fines to asphalt ratio shall be defined as the percent materials passing the No. 200 (0.075 mm) sieve (expressed as a percent of total aggregate weight) divided by the effective asphalt binder content.

ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
LIQUID ANTI-STRIP ADDITIVE

Division 400 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

Section 404, DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES, is hereby modified as follows:

The following is added as the last bullet following the first paragraph of **Subsection 404.01(b), Design Requirements**:

- All ACHM mixes must contain a liquid, anti-strip additive.

Section 409, MATERIALS AND EQUIPMENT FOR ASPHALT CONCRETE PLANT MIX COURSES, is hereby modified as follows:

The second paragraph of **Subsection 409.02 Asphalt Binder** is hereby deleted and the following is substituted therefor:

The asphalt binder for all Asphalt Concrete Hot Mixes shall contain a heat-stable, liquid anti-strip additive. The additive shall be furnished from the Qualified Products List. The additive shall not harm the completed bituminous concrete mixture and must be compatible with the aggregate and asphalt binder supplied for the project. The anti-strip additive shall be added either by an in-line blending process just before introduction of the asphalt binder to the mixer or by blending with the asphalt binder at the asphalt binder terminal. If blended at the terminal, the bill of lading accompanying the load being delivered to the hot mix asphalt plant shall include the anti-strip manufacturer's name, product name, and quantity of all anti-strip additive included in the load.

The liquid anti-strip additive shall be added at rates as indicated below:

- For ACHM mixes where the use of an anti-strip additive is required as determined by the laboratory analysis and mix design procedures, the anti-strip additive shall be added at the rate of 0.50% to 0.75% by weight of asphalt binder as determined by the laboratory analysis and laboratory mix design procedures.
- For all other mixes, the manufacturer's recommended dosage of the additive shall be used, but the rate of liquid anti-strip additive shall not be less than 0.25% by weight of the asphalt binder.

ARKANSAS DEPARTMENT OF TRANSPORTATION

SUPPLEMENTAL SPECIFICATION

**CONSTRUCTION REQUIREMENTS AND ACCEPTANCE OF
ASPHALT CONCRETE PLANT MIX COURSES**

Section 410, Construction Requirements and Acceptance of Asphalt Concrete Plant Mix Courses, of the Standard Specifications for Highway Construction, Edition of 2014, is hereby modified as follows:

Subsection 410.10 Incentives is hereby deleted.

ARKANSAS DEPARTMENT OF TRANSPORTATION

SUPPLEMENTAL SPECIFICATION

DEVICES FOR MEASURING DENSITY FOR ROLLING PATTERNS

Section 410 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

The fourth sentence of the first paragraph of **Subsection 410.08, Rolling and Density Requirements and Joints**, is hereby deleted and the following substituted therefor:

The Engineer will observe the Contractor's use of an electromagnetic surface contact device that meets ASTM D7113/D7113M or the use of a nuclear density gauge to verify that the maximum densities possible are obtained.

ARKANSAS DEPARTMENT OF TRANSPORTATION**SUPPLEMENTAL SPECIFICATION****LANE CLOSURE NOTIFICATION**

Division 600 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

Section 603, Maintenance of Traffic and Temporary Structures, is hereby modified as follows:

The first sentence of the third paragraph **Subsection 603.02 (d)** is hereby deleted and the following substituted therefor:

The Contractor shall provide the Engineer with a minimum of five full business days advance, written notification of any nonemergency lane closure or lane width restriction. The first full business day shall commence at midnight on the first business day following written notification to the Engineer. This advanced notification is required to allow adequate notice for the issuance of over width load permits by the Department.

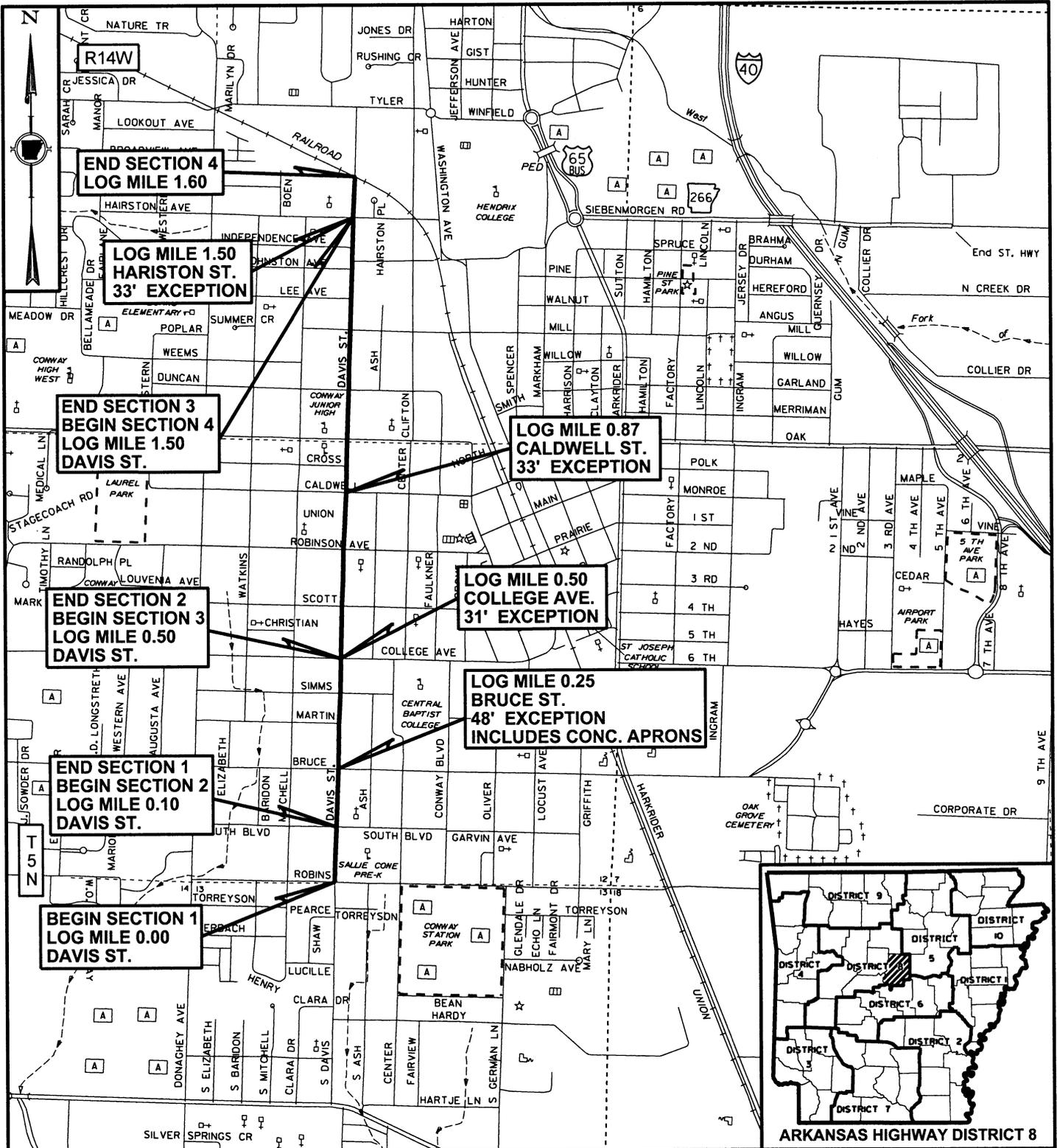
**ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENTAL SPECIFICATION
RETROREFLECTIVE SHEETING FOR
TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES**

Section 604 of the Standard Specifications for Highway Construction, Edition of 2014, is hereby amended as follows:

The following is inserted after the first paragraph of Subsection 604.02(b):

Retroreflective sheeting used on traffic drums shall meet the requirements of ASTM D4956 for Type III or IV with the additional requirements for Reboundable Sheeting. Retroreflective sheeting for delineators shall comply with section 728.

Retroreflective sheeting shall be applied to a properly treated substrate with mechanical equipment and in a manner specified by the sheeting manufacturer. Sign material (substrate) shall be of sufficient thickness and stability to maintain a substantial, effective sign for the duration of the project. One splice will be allowed in retroreflective sheeting on sign blanks. "Left", "Right", "Distances", and "Ahead" will be allowed on signs as inserts. All letters and numerals on inserts shall be of the same size and series as those on the sign face.



JOB C23006
CONWAY DAVIS ST. OVERLAY (S)
DAVIS ST.
FAULKNER COUNTY



APPROXIMATE MIDPOINT OF PROJECT
 LATITUDE: N 35° 05' 20"
 LONGITUDE: W 92° 26' 53"

INDEX OF SHEETS

TITLE

SHEET NO.

- 1 _____ TITLE SHEET
- 2 _____ INDEX OF SHEETS AND STANDARD DRAWINGS
- 3 _____ GOVERNING SPECIFICATIONS
- 4 _____ GENERAL NOTES
- 5 - 8 _____ TYPICAL SECTIONS OF IMPROVEMENT
- 9 - 13 _____ SPECIAL DETAILS
- 14 - 20 _____ QUANTITIES
- 21 _____ SUMMARY OF QUANTITIES AND REVISIONS

ROADWAY STANDARD DRAWINGS

TITLE

DRWG. NO.

- | | TITLE | DATE |
|------------|--|----------|
| PM-1 _____ | PAVEMENT MARKING DETAILS _____ | 06-01-17 |
| TC-1 _____ | STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION _____ | 11-07-19 |
| TC-2 _____ | STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION _____ | 11-07-19 |
| TC-3 _____ | STANDARD TRAFFIC CONTROLS FOR HIGHWAY CONSTRUCTION _____ | 11-07-19 |
| WR-1 _____ | WHEELCHAIR RAMPS NEW CONSTRUCTION AND ALTERATIONS _____ | 11-10-05 |

JOB C23006

REVISED 12-20-19
SHEET 2 OF 21

GOVERNING SPECIFICATIONS

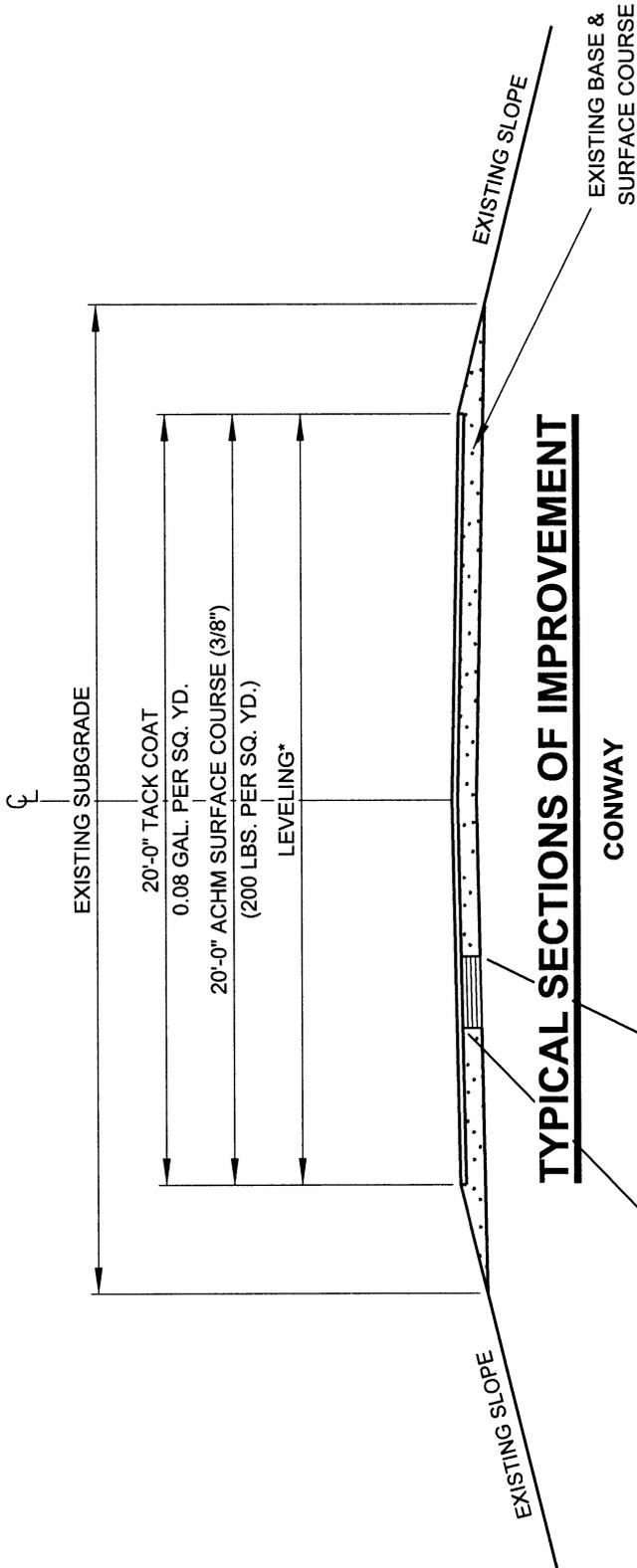
ARKANSAS STATE HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION,
EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS AND SUPPLEMENTAL SPECIFICATIONS.

NUMBER	TITLE
ERRATA	ERRATA FOR THE BOOK OF STANDARD SPECIFICATIONS
100-3	CONTRACTOR'S LICENSE
100-4	DEPARTMENT NAME CHANGE
102-2	ISSUANCE OF PROPOSALS
108-1	LIQUIDATED DAMAGES
108-2	WORK ALLOWED PRIOR TO ISSUANCE OF WORK ORDER
306-1	QUALITY CONTROL AND ACCEPTANCE
400-1	TACK COATS
400-4	DESIGN AND QUALITY CONTROL OF ASPHALT MIXTURES
400-5	PERCENT AIR VOIDS FOR ACHM MIX DESIGNS
400-6	LIQUID ANTI-STRIP ADDITIVE
410-1	CONSTRUCTION REQUIREMENTS AND ACCEPTANCE OF ASPHALT CONCRETE PLANT MIX COURSES
410-2	DEVICES FOR MEASURING DENSITY FOR ROLLING PATTERNS
603-1	LANE CLOSURE NOTIFICATION
604-1	RETROREFLECTIVE SHEETING FOR TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES
JOB C23006	ADJUSTMENT OF QUANTITIES FOR STATE-AID STREET PROJECTS
JOB C23006	BIDDING REQUIREMENTS AND CONDITIONS
JOB C23006	BROADBAND INTERNET SERVICE FOR ASPHALT CONCRETE PLANT
JOB C23006	DENSITIES FOR ACHM SURFACE COURSE
JOB C23006	FLEXIBLE BEGINNING OF WORK (PAVEMENT PRESERVATION, CITY AID, AND STATE AID PROJECTS)
JOB C23006	MANDATORY ELECTRONIC CONTRACT
JOB C23006	MANDATORY ELECTRONIC DOCUMENT SUBMITTAL
JOB C23006	RECYCLED ASPHALT SHINGLES
JOB C23006	SUBMISSION OF ASPHALT CONCRETE HOT MIX ACCEPTANCE TEST RESULTS
JOB C23006	TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES (MASH)
JOB C23006	WARM MIX ASPHALT

GENERAL NOTES

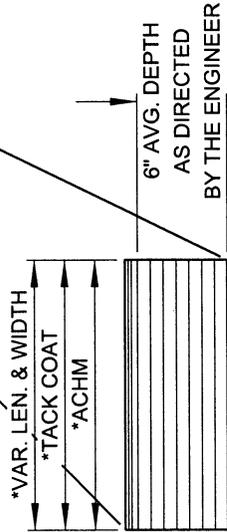
1. THE SEQUENCE AS SHOWN ON THE MAINTENANCE OF TRAFFIC PLANS IS A GENERAL OUTLINE FOR THE CONSTRUCTION OF THIS PROJECT, AND IN NO WAY IS IT INTENDED TO COVER EVERY ITEM IN THE PROJECT. ITEMS NOT CRITICAL TO THE CONSTRUCTION SEQUENCE MAY BE CONSTRUCTED IN ANY STAGE AS APPROVED BY THE RESIDENT ENGINEER.
2. SUPPLYING UTILITY RISERS AND INSTALLATION OF PERMANENT TRAFFIC SAFETY SIGNS NOT SHOWN IN THE PLANS ARE THE RESPONSIBILITIES OF THE CITY OF CONWAY.
3. THE CONTRACTOR SHALL CLIP GRASS AND DEBRIS FROM THE EDGE OF THE EXISTING ROADWAY. THESE "CLIPPINGS" SHALL BE PULLED UP TO THE EDGE OF THE NEW PAVEMENT IN SECTIONS WHERE THE SHOULDERS ARE NOT BEING REBUILT WITH AGGREGATE OR BORROW. NO DIRECT PAYMENT WILL BE MADE FOR THIS WORK.

NOTE: DETAILS MAY BE MODIFIED TO MEET LOCAL CONDITIONS AS DIRECTED BY THE ENGINEER.



TYPICAL SECTIONS OF IMPROVEMENT

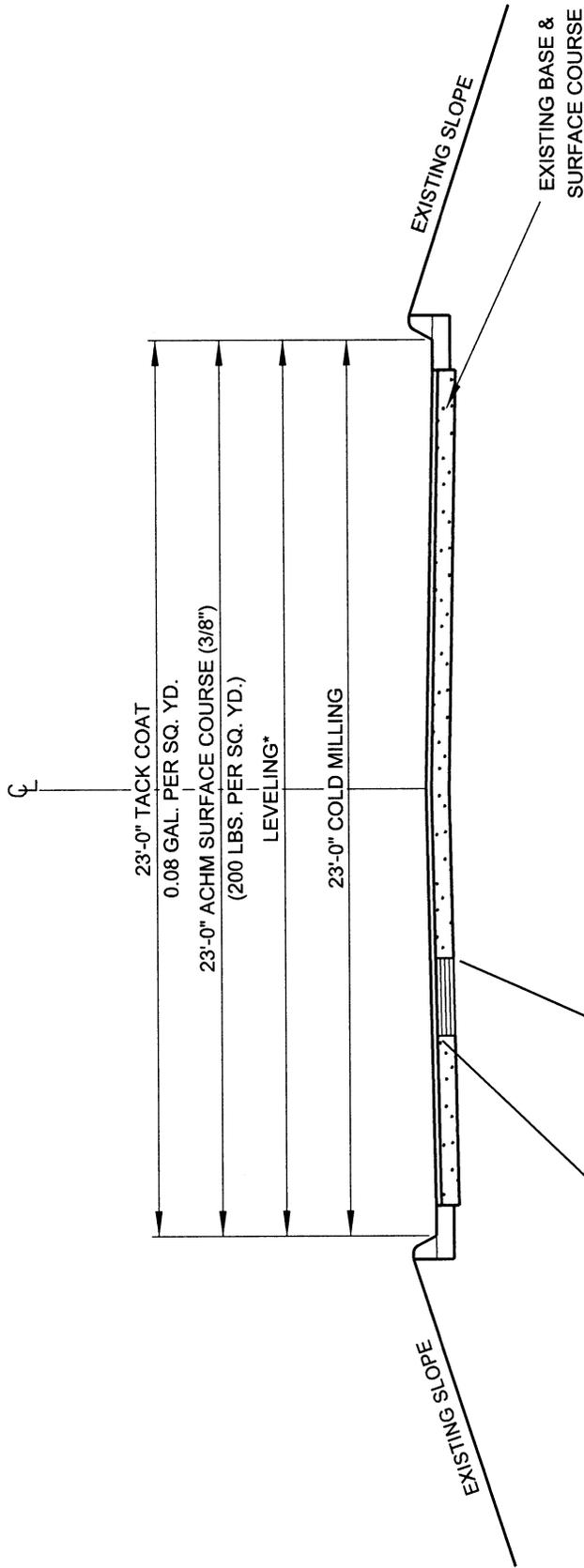
CONWAY
SECTION 1, DAVIS ST. - L.M. 0.00 TO 0.10



ACHM PATCHING OF EXISTING ROADWAY
PAY ITEMS AND CONSTRUCTION REQUIREMENTS ARE AS DEFINED IN SECTION 415 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

* LOCATION AND APPLICATION RATES ARE AT THE DIRECTION OF THE ENGINEER. SEE QUANTITY SHEET FOR ESTIMATED AMOUNTS.

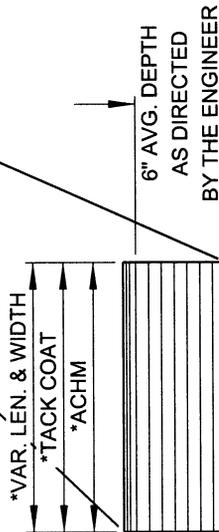
NOTE: DETAILS MAY BE MODIFIED TO MEET LOCAL CONDITIONS AS DIRECTED BY THE ENGINEER.



TYPICAL SECTIONS OF IMPROVEMENT

CONWAY

SECTION 2, DAVIS ST. - L.M. 0.10 TO 0.50



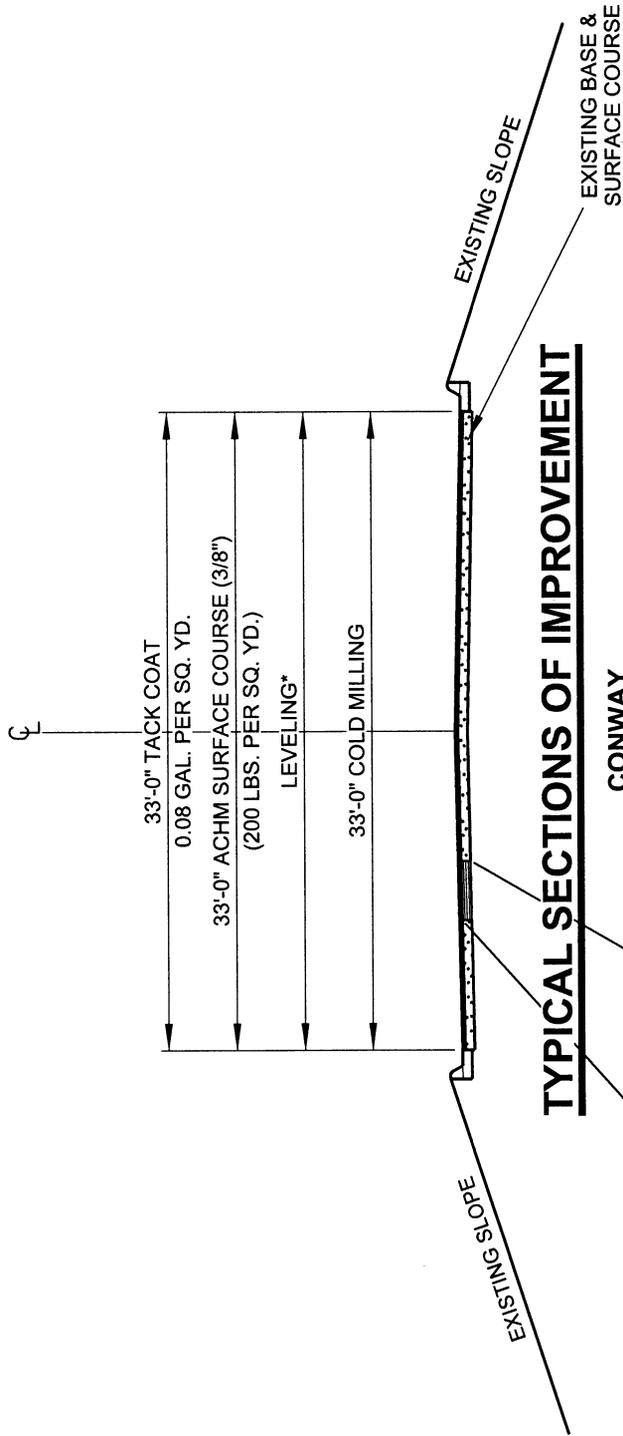
ACHM PATCHING OF EXISTING ROADWAY

PAY ITEMS AND CONSTRUCTION REQUIREMENTS ARE AS DEFINED IN SECTION 415 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

NOTE:
MATERIAL PRODUCED FROM COLD MILLING ASPHALT PAVEMENT SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

* LOCATION AND APPLICATION RATES ARE AT THE DIRECTION OF THE ENGINEER. SEE QUANTITY SHEET FOR ESTIMATED AMOUNTS.

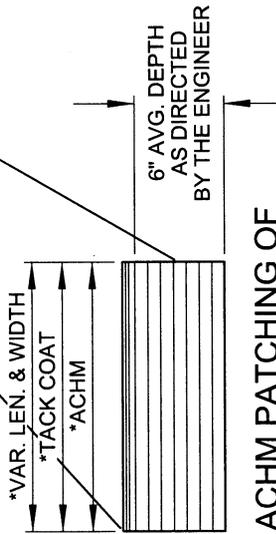
NOTE: DETAILS MAY BE MODIFIED TO MEET LOCAL CONDITIONS AS DIRECTED BY THE ENGINEER.



TYPICAL SECTIONS OF IMPROVEMENT

CONWAY

SECTION 3, DAVIS ST. - L.M. 0.50 TO 1.50

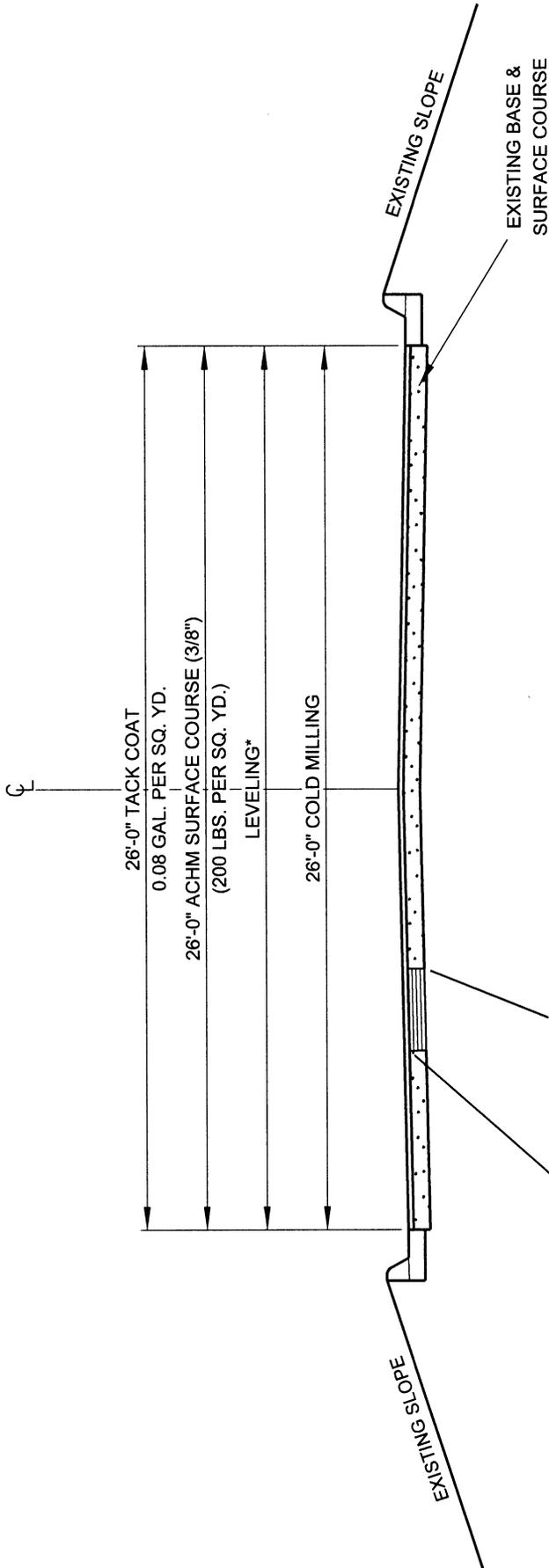


ACHM PATCHING OF EXISTING ROADWAY
PAY ITEMS AND CONSTRUCTION REQUIREMENTS ARE AS DEFINED IN SECTION 415 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

NOTE:
MATERIAL PRODUCED FROM COLD MILLING ASPHALT PAVEMENT SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

* LOCATION AND APPLICATION RATES ARE AT THE DIRECTION OF THE ENGINEER. SEE QUANTITY SHEET FOR ESTIMATED AMOUNTS.

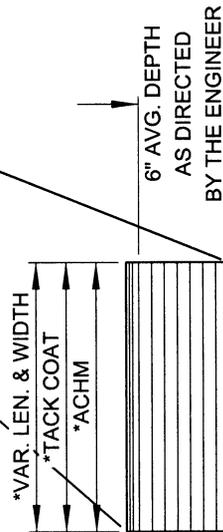
NOTE: DETAILS MAY BE MODIFIED TO MEET LOCAL CONDITIONS AS DIRECTED BY THE ENGINEER.



TYPICAL SECTIONS OF IMPROVEMENT

CONWAY

SECTION 4, DAVIS ST. - L.M. 1.50 TO 1.60

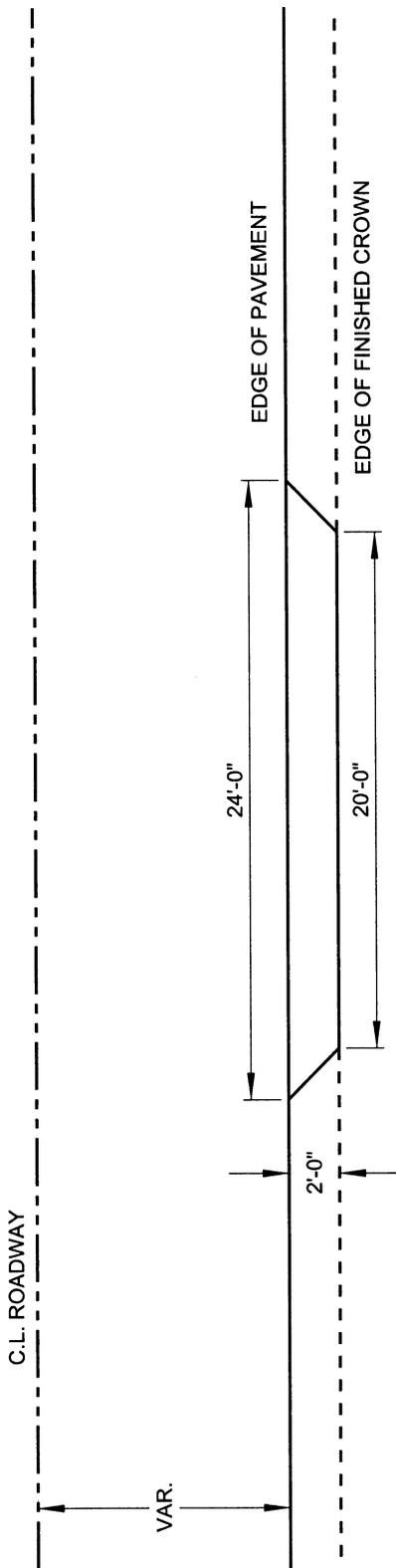


ACHM PATCHING OF EXISTING ROADWAY

PAY ITEMS AND CONSTRUCTION REQUIREMENTS ARE AS DEFINED IN SECTION 415 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

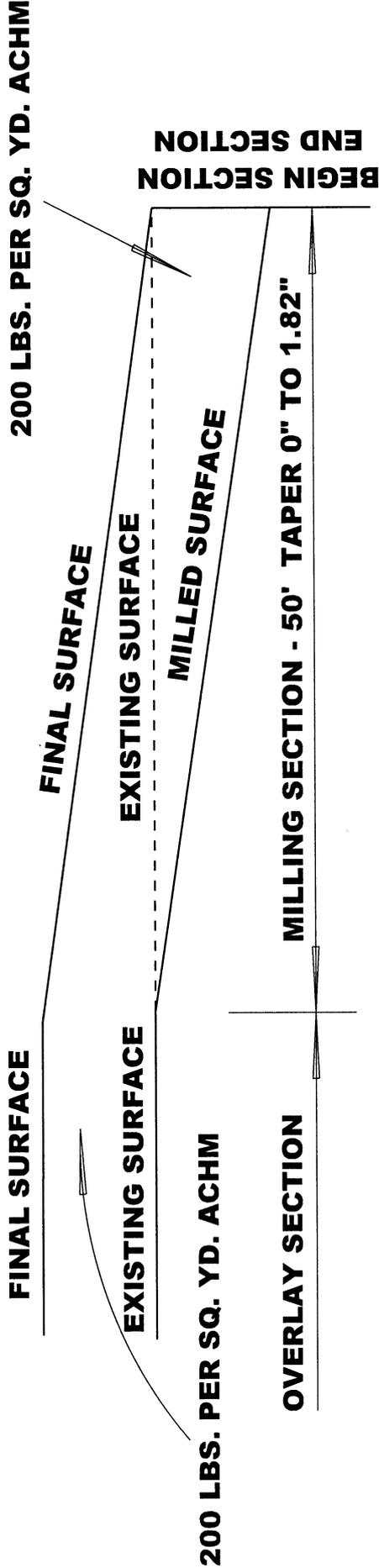
NOTE:
MATERIAL PRODUCED FROM COLD MILLING ASPHALT PAVEMENT SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

* LOCATION AND APPLICATION RATES ARE AT THE DIRECTION OF THE ENGINEER. SEE QUANTITY SHEET FOR ESTIMATED AMOUNTS.



**TYPICAL PRIVATE ENTRANCE
ADDITIONAL SURFACING AREA = 4.9 SQ. YDS.**

NOTE: DIMENSIONS MAY BE MODIFIED TO MEET
LOCAL CONDITIONS AS DIRECTED BY THE ENGINEER.



PROFILE VIEW
NOT TO SCALE

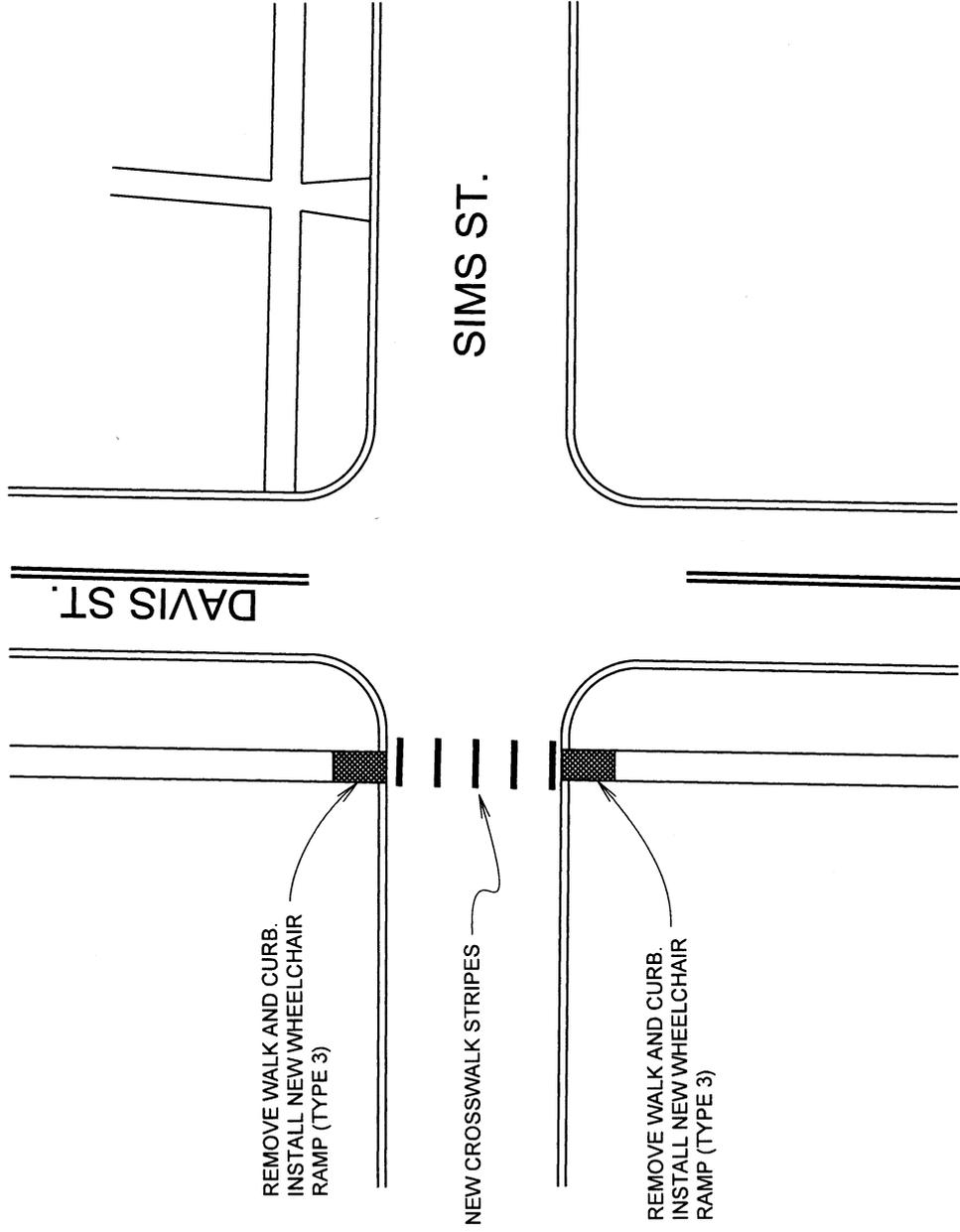
PAVEMENT TRANSITION
DETAILS FOR COLD MILLING
SECTIONS 1

LOCATIONS
 L.M. 0.00 TO 0.01, SECTION 1
 L.M. 0.09 TO 0.10, SECTION 1

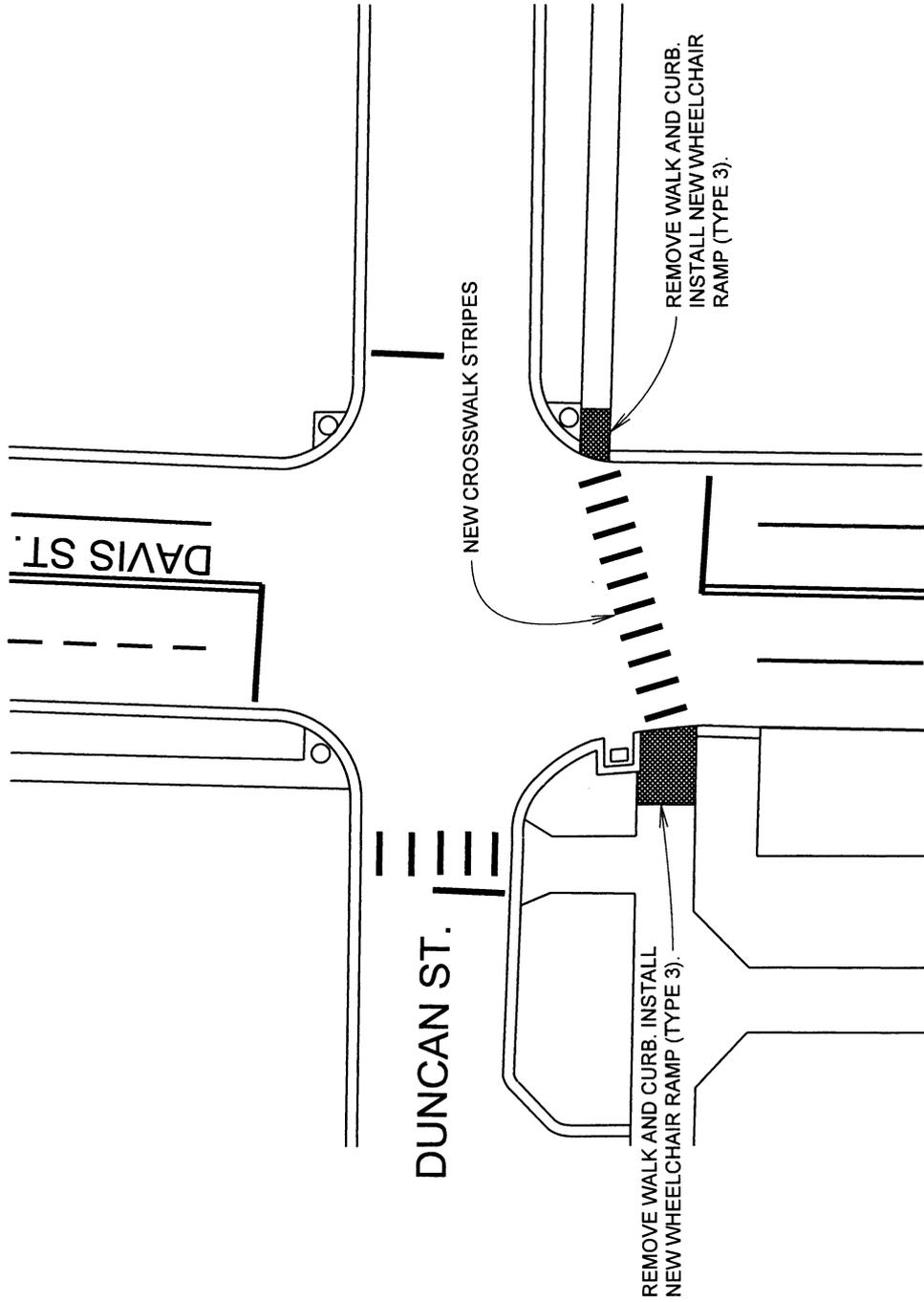
NOTE: DIMENSIONS ARE APPROXIMATED AND MAY BE MODIFIED AS DIRECTED BY THE ENGINEER.

NOTE: FOR COLD MILLING ASPHALT PAVEMENT TRANSITION, TAPER 50' OF FINAL SURFACE TO CREATE A 1.82" PAVING NOTCH, AT VARIOUS LOCATIONS, AS SHOWN ABOVE. OVERLAY MILLED AREAS USING 200 LBS. PER SQ. YD. OF ACHM.

NOTE: MATERIAL PRODUCED FROM COLD MILLING ASPHALT PAVEMENT SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.



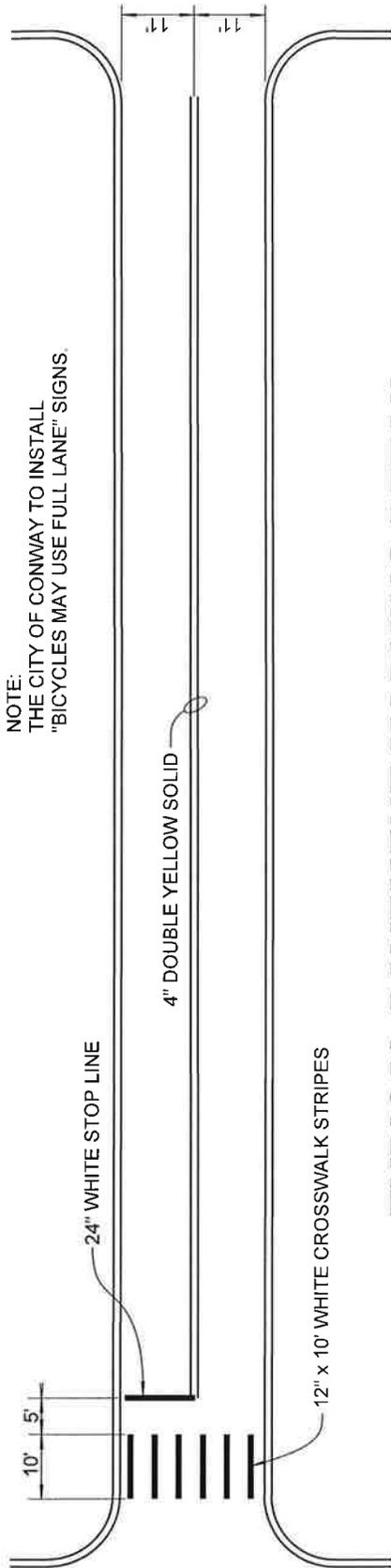
DETAILS OF WHEELCHAIR RAMP AND CROSSWALKS
L.M. 0.42 - DAVIS ST. @ SIMS ST.



DETAILS OF WHEELCHAIR RAMPS AND CROSSWALKS

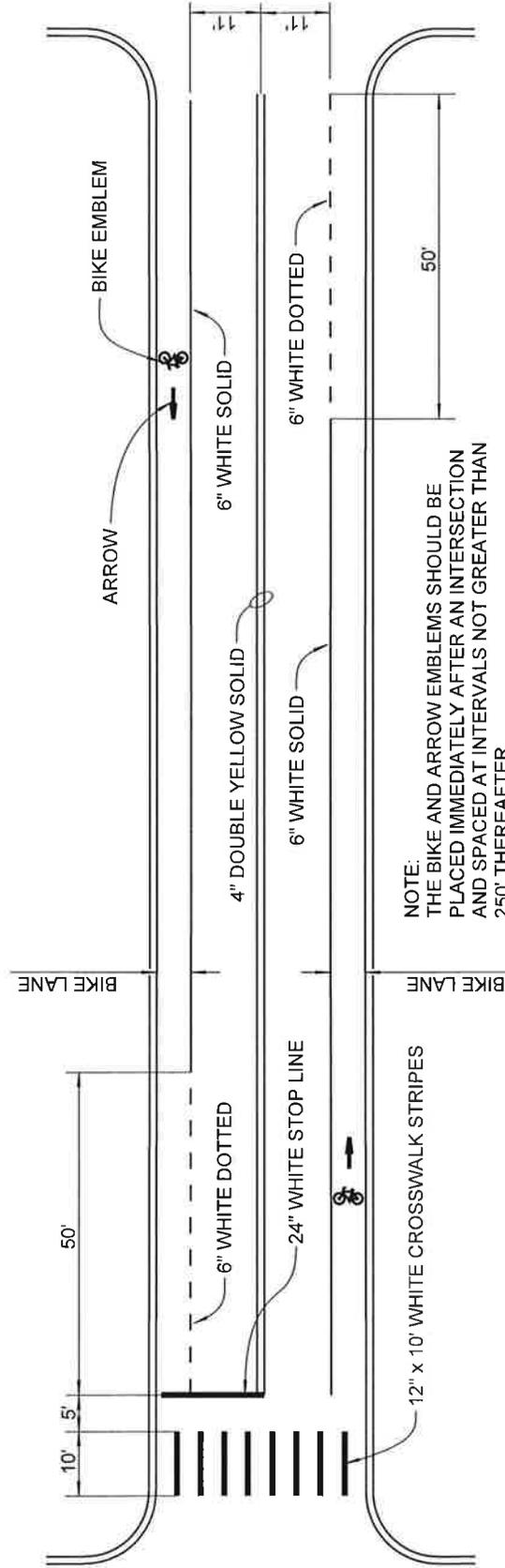
L.M. 1.11 - DAVIS ST. @ DUNCAN ST.

NOTE:
THE CITY OF CONWAY TO INSTALL
"BICYCLES MAY USE FULL LANE" SIGNS.



TYPICAL PAVEMENT MARKING DETAIL

SECTION 1 - DAVIS ST.
SECTION 2 - DAVIS ST.



NOTE:
THE BIKE AND ARROW EMBLEMS SHOULD BE
PLACED IMMEDIATELY AFTER AN INTERSECTION
AND SPACED AT INTERVALS NOT GREATER THAN
250' THEREAFTER.

TYPICAL DETAIL OF BIKE LANE MARKING, EMBLEM, AND ARROW

SECTION 3 - DAVIS ST.

NOTE:
REFER TO STANDARD DRAWING PM-1 AND THE
LATEST EDITION OF MUTCD FOR ADDITIONAL
PAVEMENT MARKING DETAILS.

PATCHING AND SURFACING

DESCRIPTION	LOG MILE		LENGTH LIN. FT.	ACHM PATCHING OF EXISTING ROADWAY** TON	TACK COAT			ACHM SURFACE COURSE (3/8")*		
	FROM	TO			WIDTH	SQ. YD.	GAL.	WIDTH	SQ. YD.	TON
SECTION 1 - DAVIS ST.										
MAIN LANES	0.00	0.10	528	30.0	20.00	1173.3	93.9	20.00	1173.3	117.3
11 PRIVATE ENTRANCES						53.9	4.3		53.9	5.4
LEVELING							37.5			25.0
SECTION 2 - DAVIS ST.										
MAIN LANES	0.10	0.50	2064	15.0	23.00	5274.7	422.0	23.00	5274.7	527.5
(48' EXCEPTION @ BRUCE ST.)										
LEVELING							82.5			55.0
SECTION 3 - DAVIS ST.										
MAIN LANES	0.50	1.50	5216	42.0	33.00	19125.3	1530.0	33.00	19125.3	1912.5
(31' EXCEPTION @ COLLEGE AVE.)										
(33' EXCEPTION @ CALDWELL ST.)										
LEVELING							285.0			190.0
SECTION 4 - DAVIS ST.										
MAIN LANES	1.50	1.60	495	14.0	26.00	1430.0	114.4	26.00	1430.0	143.0
(33' EXCEPTION @ HARISTON ST.)										
LEVELING							22.5			15.0
TOTALS:				101.0			2592.1			2990.7
USE:				101			2592			2991

BASIS OF ESTIMATE:

TACK COAT _____ 0.08 GAL. PER SQ. YD.
 ACHM SURFACE COURSE (3/8") _____ 200 POUND PER SQ. YD.

***Nmax=115**

VOLUME CONTROL:

ASPHALT BINDER (PG 64-22) IN ACHM SURFACE COURSE (3/8") _____ 6.0%
 MINERAL AGGREGATE IN ACHM SURFACE COURSE (3/8") _____ 94.0%

**QUANTITY ESTIMATED. TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER. SEE SECTION 104.03 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

REFLECTORIZED PAINT PAVEMENT MARKING (BOX 2 OF 2)

DESCRIPTION	LOG MILE		YELLOW MARKING					WHITE MARKING				
			4"	6"	12"	24"	ARROW	BIKE EMBLEM				
	FROM	TO	LIN. FT.					EACH				
SECTION 3 - DAVIS ST.												
MAIN LANES - CONTINUOUS	0.50	1.50										
(31' EXCEPTION @ COLLEGE AVE.)												
(33' EXCEPTION @ CALDWELL ST.)												
BIKE LANE	0.50	1.50		10432								
BIKE EMBLEM	0.50	1.50										20
ARROW	0.50	1.50							20			
CROSSWALK STRIPES		0.75			100							
STOP LINE		0.75								16		
STOP LINE		0.75								16		
CROSSWALK STRIPES		1.00			100							
STOP LINE		1.00								16		
STOP LINE		1.00								16		
CROSSWALK STRIPES		1.11			110							
STOP LINE		1.11								16		
STOP LINE		1.11								16		
SECTION 4 - DAVIS ST.												
MAIN LANES - CONTINUOUS	1.50	1.60										
(33' EXCEPTION @ HARISTON ST.)												
STOP LINE		1.50								13		
SUBTOTALS (BOX 2 OF 2):					11272	10432	310	109	20	20	20	20
TOTALS:					16360	10432	500	149	20	20	20	20

THIS IS A LOW VOLUME ROAD AS DEFINED IN SECTION 604.03 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

JOB C23006

REVISED 12-30-19
SHEET 19 OF 21

SUMMARY OF QUANTITIES

I T E M

ITEM NO.	DESCRIPTION	QUANTITY	UNIT
202	REMOVAL AND DISPOSAL OF CURB	37	LIN. FT.
202	REMOVAL AND DISPOSAL OF WALKS	20	SQ. YD.
SS & 401	TACK COAT	2592	GAL.
SP, SS, & 407	MINERAL AGGREGATE IN ACHM SURFACE COURSE (3/8")	2812	TON
SP, SS, & 407	ASPHALT BINDER (PG 64-22) IN ACHM SURFACE COURSE (3/8")	179	TON
412	COLD MILLING ASPHALT PAVEMENT	26505	SQ. YD.
SP, SS, & 415	ACHM PATCHING OF EXISTING ROADWAY	101	TON
601	MOBILIZATION	1.00	LUMP SUM
SS & 603	MAINTENANCE OF TRAFFIC	1.00	LUMP SUM
SP, SS, & 604	SIGNS	72	SQ. FT.
641	WHEELCHAIR RAMPS (TYPE 3)	25	SQ. YD.
718	REFLECTORIZED PAINT PAVEMENT MARKING WHITE (6")	10432	LIN. FT.
718	REFLECTORIZED PAINT PAVEMENT MARKING WHITE (12")	500	LIN. FT.
718	REFLECTORIZED PAINT PAVEMENT MARKING WHITE (24")	149	LIN. FT.
718	REFLECTORIZED PAINT PAVEMENT MARKING YELLOW (4")	16360	LIN. FT.
718	REFLECTORIZED PAINT PAVEMENT MARKING (ARROWS)	20	EACH
718	REFLECTORIZED PAINT PAVEMENT MARKING (BIKE EMBLEMS)	20	EACH

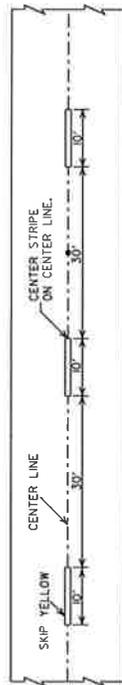
REVISIONS

DATE	REVISION	SHEET NO.
12/20/2019	REVISED ROADWAY STANDARD DRAWINGS AND ADDED SPECIAL PROVISION "TRAFFIC CONTROL DEVICES IN CONSTRUCTION ZONES (MASH)"	2, 3, & 21
12/30/2019	REVISED TYPICAL PAVEMENT MARKING DETAILS, PAVEMENT MARKING QUANTITIES, AND SUMMARY OF QUANTITIES.	13, 18, 19, & 21

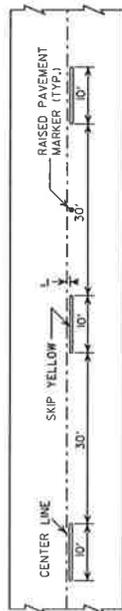
REVISED 12-30-19
REVISED 12-20-19
SHEET 21 OF 21

JOB C23006

NOTES:
 1. REFER TO "THE STRIPING DETAILS FOR PAVEMENT MARKING LINE WIDTHS."
 2. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
 3. RAISED PAVEMENT MARKERS SHALL BE PLACED ON AN 80 FEET \pm SPACING UNLESS OTHERWISE SHOWN IN THE PLANS.

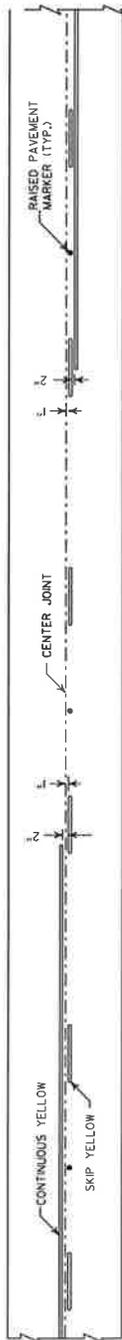


ASPHALT PAVEMENT



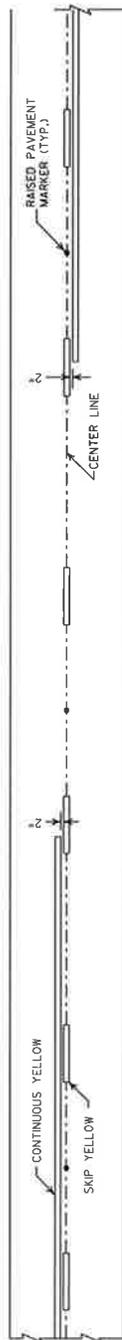
CONCRETE PAVEMENT

BROKEN LINE STRIPING

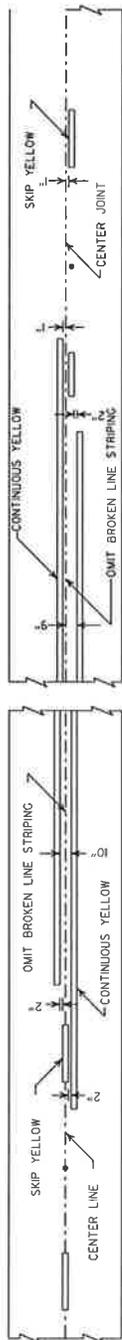


CONCRETE PAVEMENT

SOLID LINE STRIPING ON CONCRETE PAVEMENT



SOLID LINE STRIPING ON ASPHALT PAVEMENT



ASPHALT PAVEMENT

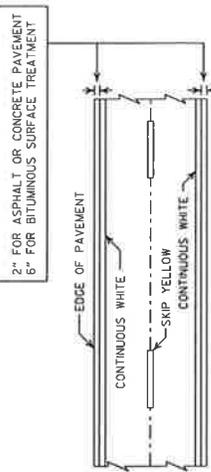
CONCRETE PAVEMENT

STRIPING AT ADJACENT NO PASSING LANES

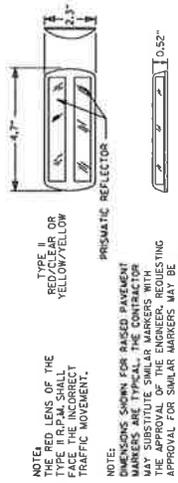


YIELD LINE DETAIL

CROSSWALK AND STOPBAR DETAILS



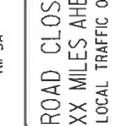
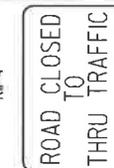
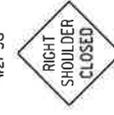
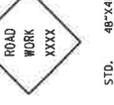
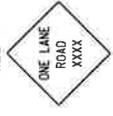
PAVEMENT EDGE LINE MARKING



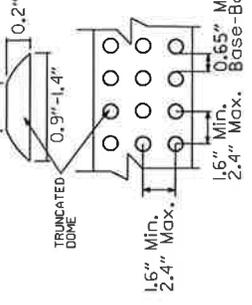
DETAIL OF STANDARD RAISED PAVEMENT MARKERS

DATE	REVISION
8-11-77	REVISED LINE WIDTHS
5-12-76	REVISED LINE WIDTHS, SPACING, & NOTES
9-12-73	REVISED DETAIL OF CROSSWALK
11-17-70	REVISED GENERAL NOTES & REMOVED ALLOWABLE PAV. MARKS
8-18-64	REVISED NOTE 2 & GENERAL
8-22-62	ADDED CROSSWALK & STOP BAR DETAILS
7-02-58	ADDED PAV. MARKERS
5-18-56	REVISED LINE WIDTHS
5-18-56	REVISED LINE WIDTHS
12-16-50	FILED

ARKANSAS STATE HIGHWAY COMMISSION
PAVEMENT MARKING DETAILS
 STANDARD DRAWING PM-1

 R-H STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"	 R-I-2 STD. 36"x36"x36" EXPY. 48"x48"x48" FWY. 60"x60"x60"	 R-2-1 STD. 24"x30" EXPY. 36"x48" FWY. 48"x60"	 W-3-5 STD. 36"x36" EXPY. 48"x48" FWY. 48"x60"	 W-3-50 STD. 36"x36" EXPY. 48"x48" FWY. 48"x60"	 R-4-1 STD. 24"x30" EXPY. 48"x48" FWY. 48"x60"	 R-4-2 STD. 24"x30" EXPY. 48"x48" FWY. 48"x60"	<p>ADVANCE DISTANCES (XXX) 500 FT 1000 FT 1500 FT 1/2 MILE 3/4 MILE 1 MILE AHEAD</p> <p>GENERAL NOTES: 1. ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION. 2. TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER. 3. EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED. 4. SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OTHER THAN 10 TO 50 FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE II BARRICADE. 5. SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 1/2" MINIMUM CHANNEL POST OR 4"x4" WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED AND SHALL BE REPAIRED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 7 POSTS IN A PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING T-1-3. 6. ROAD MOUNTED SIGNS SHALL BE CENTERED WITH THE MAIN EDGE OF THE ROADWAY. SIGNS SHALL BE PLACED FROM THE MAIN EDGE OF THE ROADWAY TO THE BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE. 7. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A MOUNTED SIGN. SIGNS MOUNTED ON PORTABLE SUPPORTS SHALL BE MOUNTED AT A HEIGHT SHALL BE 5' RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS. 8. FLAGGERS SHALL USE REINFORCED FIBER-GLASS PLACED SIGNS ONLY FOR EMERGENCY SITUATIONS. 9. MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE ROADWAY IS TO THE LEFT OF THE MOTORIST'S POSITION. 10. REFLECTIVE SIGNS SHALL BE PLACED AT LEAST 600' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN. * NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND THE REQUIREMENTS FOR THE UNIFORM SIGN BUT MEET THE REQUIREMENTS OF MANUAL FOR BARRICADES. BARRICADES SHALL BE REPAIRED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE PROJECTS. ** NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND THE REQUIREMENTS FOR THE UNIFORM SIGN BUT MEET THE REQUIREMENTS OF MANUAL FOR BARRICADES. BARRICADES SHALL BE REPAIRED, CLEANED, OR REPAIRED AS NEEDED FOR THE PROJECTS.</p>
 R-5-1 STD. 30"x30" EXPY. 36"x36" SPECIAL 48"x48"	 R-I-2 STD. 48"x30" EXPY. 48"x48" FWY. 48"x60"	 R-I-3A STD. 60"x30" EXPY. 60"x30" FWY. 60"x30"	 R-I-4 STD. 60"x30" EXPY. 60"x30" FWY. 60"x30"	 W-2-50 STD. 36"x36" EXPY. 48"x48" FWY. 48"x60"	 W-1-1 STD. 36"x36" EXPY. 48"x48" FWY. 48"x60"	 W-1-2 STD. 36"x36" EXPY. 48"x48" FWY. 48"x60"	<p>8. FLAGGERS SHALL USE REINFORCED FIBER-GLASS PLACED SIGNS ONLY FOR EMERGENCY SITUATIONS. 9. MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE ROADWAY IS TO THE LEFT OF THE MOTORIST'S POSITION. 10. REFLECTIVE SIGNS SHALL BE PLACED AT LEAST 600' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN. * NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND THE REQUIREMENTS FOR THE UNIFORM SIGN BUT MEET THE REQUIREMENTS OF MANUAL FOR BARRICADES. BARRICADES SHALL BE REPAIRED, CLEANED, OR REPAIRED AS NEEDED FOR THE PROJECTS. ** NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND THE REQUIREMENTS FOR THE UNIFORM SIGN BUT MEET THE REQUIREMENTS OF MANUAL FOR BARRICADES. BARRICADES SHALL BE REPAIRED, CLEANED, OR REPAIRED AS NEEDED FOR THE PROJECTS.</p>
 W-5-1 STD. 36"x36" EXPY. 48"x48" FWY. 48"x60"	 W-1-4 STD. 48"x30" EXPY. 48"x48" FWY. 48"x60"	 W-6-7 STD. 48"x24" SPECIAL 60"x30" FWY. 36"x48"	 W-9-2 STD. 36"x36" EXPY. 48"x48" FWY. 48"x60"	 W-3-1 STD. 36"x36" EXPY. 48"x48" FWY. 48"x60"	 W-2-1 STD. 48"x48" EXPY. 48"x48" FWY. 48"x60"	 W-2-2 STD. 48"x48" EXPY. 48"x48" FWY. 48"x60"	<p>8. FLAGGERS SHALL USE REINFORCED FIBER-GLASS PLACED SIGNS ONLY FOR EMERGENCY SITUATIONS. 9. MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE ROADWAY IS TO THE LEFT OF THE MOTORIST'S POSITION. 10. REFLECTIVE SIGNS SHALL BE PLACED AT LEAST 600' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN. * NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND THE REQUIREMENTS FOR THE UNIFORM SIGN BUT MEET THE REQUIREMENTS OF MANUAL FOR BARRICADES. BARRICADES SHALL BE REPAIRED, CLEANED, OR REPAIRED AS NEEDED FOR THE PROJECTS. ** NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND THE REQUIREMENTS FOR THE UNIFORM SIGN BUT MEET THE REQUIREMENTS OF MANUAL FOR BARRICADES. BARRICADES SHALL BE REPAIRED, CLEANED, OR REPAIRED AS NEEDED FOR THE PROJECTS.</p>
 W-2-4 STD. 36"x36" EXPY. 48"x48" FWY. 48"x60"	 G-20-1 STD. 36"x36" EXPY. 48"x48" FWY. 48"x60"	 W-21-2 STD. 30"x30" SPECIAL 36"x36" FWY. 48"x60"	 W-21-5 STD. 30"x30" SPECIAL 36"x36" FWY. 48"x60"	 M-4-9 STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"	 W-1-4b STD. 48"x48" EXPY. 48"x48" FWY. 48"x60"	 R-5-1 STD. 48"x48" EXPY. 48"x48" FWY. 48"x60"	<p>8. FLAGGERS SHALL USE REINFORCED FIBER-GLASS PLACED SIGNS ONLY FOR EMERGENCY SITUATIONS. 9. MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE ROADWAY IS TO THE LEFT OF THE MOTORIST'S POSITION. 10. REFLECTIVE SIGNS SHALL BE PLACED AT LEAST 600' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN. * NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND THE REQUIREMENTS FOR THE UNIFORM SIGN BUT MEET THE REQUIREMENTS OF MANUAL FOR BARRICADES. BARRICADES SHALL BE REPAIRED, CLEANED, OR REPAIRED AS NEEDED FOR THE PROJECTS. ** NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND THE REQUIREMENTS FOR THE UNIFORM SIGN BUT MEET THE REQUIREMENTS OF MANUAL FOR BARRICADES. BARRICADES SHALL BE REPAIRED, CLEANED, OR REPAIRED AS NEEDED FOR THE PROJECTS.</p>
 W-8-II STD. 36"x36" EXPY. 48"x48" FWY. 48"x60"	 G-20-2 STD. 60"x24" EXPY. 60"x24" FWY. 60"x24"	 G-20-2 STD. 48"x24" EXPY. 48"x24" FWY. 48"x24"	 OM-3R STD. 48"x36" EXPY. 48"x36" FWY. 48"x36"	 M-4-10 STD. 48"x48" EXPY. 48"x48" FWY. 48"x60"	 R-5-1 STD. 48"x48" EXPY. 48"x48" FWY. 48"x60"	 R-5-1 STD. 36"x60" EXPY. 36"x60" FWY. 36"x60"	<p>8. FLAGGERS SHALL USE REINFORCED FIBER-GLASS PLACED SIGNS ONLY FOR EMERGENCY SITUATIONS. 9. MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE ROADWAY IS TO THE LEFT OF THE MOTORIST'S POSITION. 10. REFLECTIVE SIGNS SHALL BE PLACED AT LEAST 600' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN. * NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND THE REQUIREMENTS FOR THE UNIFORM SIGN BUT MEET THE REQUIREMENTS OF MANUAL FOR BARRICADES. BARRICADES SHALL BE REPAIRED, CLEANED, OR REPAIRED AS NEEDED FOR THE PROJECTS. ** NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND THE REQUIREMENTS FOR THE UNIFORM SIGN BUT MEET THE REQUIREMENTS OF MANUAL FOR BARRICADES. BARRICADES SHALL BE REPAIRED, CLEANED, OR REPAIRED AS NEEDED FOR THE PROJECTS.</p>

50-65% of Base Dia.



GENERAL NOTES FOR DETECTABLE WARNING DEVICES
 THE DETECTABLE WARNING DEVICE SHALL BE LOCATED 6 TO 8 INCHES FROM THE FACE OF THE CURB. TRUNCATED DOMES IN THE DETECTABLE WARNING DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE WHEELS TO ROLL BETWEEN DOMES.
 DETECTABLE WARNING DEVICE SHALL BE 24 INCHES IN THE DIRECTION OF TRAVEL AND 12 INCHES IN THE WIDTH OF THE CURB RAMP OR FLUSH SURFACE.
 DETECTABLE WARNING DEVICE SHALL BE ON THE ARDOR SIDE OF THE CURB.
 TACTILE PANELS (ADA DETECTABLE WARNING).

DETECTABLE WARNING DEVICE DETAIL

GENERAL NOTES
 IN NEW CONSTRUCTION UNLESS OTHERWISE INDICATED ON THE PLANS, WHEELCHAIR RAMPS ARE TO BE PROVIDED AT ALL CORNERS OF CURBED STREET INTERSECTIONS AND MID-BLOCK CROSSWALK LOCATIONS.
 IN ALL EXISTING INTERSECTIONS AND MID-BLOCK CROSSWALK LOCATIONS, SECTIONS WITH PEDESTRIAN TRAFFIC AND MID-BLOCK CROSSWALK LOCATIONS THE LENGTH OF THE RAMP SHALL BE SUCH THAT THE SLOPE DOES NOT EXCEED 1:24. THE SURFACE TEXTURE OF THE RAMP SHALL CONFORM TO A CLASS 6 FINISH ACCORDING TO SECTION 882.19.
 THE NORMAL OUTER GRADE SHALL BE MAINTAINED THROUGH THE AREA ALL PAVEMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD). THE MINIMUM THICKNESS OF THE RAMP WALK & LANDING SHALL BE 4". THE MINIMUM WIDTH OF THE RAMPS SHALL BE THE WALK WIDTH OR 36", WHICHEVER IS GREATER. IT IS NECESSARY TO INSURE THAT THEY ARE PARALLEL TO A LINE DRAWN FROM THE CENTER OF ONE RAMP TO THE CENTER OF THE RAMP ON THE OPPOSITE SIDE OF THE INTERSECTION.
 THE DIMENSIONS AND QUANTITIES SHOWN ON THIS DRAWING ARE FOR THE INTERSECTION ONLY. DIMENSIONS AND QUANTITIES FOR SEWER INTERSECTIONS WILL VARY AND ARE TO BE DETERMINED BY THE ENGINEER.

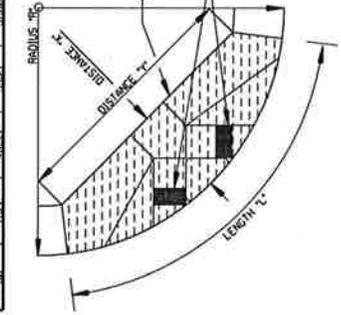
RAMP SELECTION CRITERIA

TYPE 1	CORNER LOCATIONS WITH THE WALK ADJACENT TO THE CURB (BOTH NEW CONSTRUCTION AND ALTERATIONS)
FIRST CHOICE	TYPE 2
SECOND CHOICE	TYPE 3
THIRD CHOICE	TYPE 4
FOURTH CHOICE	TYPE 5

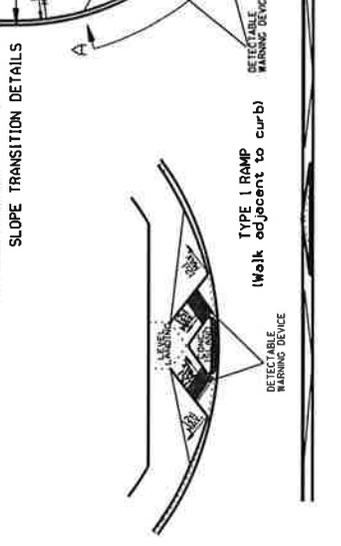
NOTE: IN ALTERATIONS, THE SELECTION OF THE TYPE OF WHEELCHAIR RAMP TO BE CONSTRUCTED SHALL BE BASED ON THE AMOUNT OF RIGHT-OF-WAY AVAILABLE AND ON THE TYPE OF TRAFFIC. THE TABLE ABOVE LISTS THE ORDER IN WHICH THE RAMPS ARE TO BE CONSIDERED. AN ALTERATION IS DEFINED AS A PROJECT THAT CHANGES OR AFFECTS THE USE OF A PEDESTRIAN PATHWAY (OVERLAYS, SIGNALIZATION PROJECTS, ETC.) BUT DOES NOT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY. VISUALLY, IT SHALL BE CONSIDERED NEW CONSTRUCTION FOR THE PURPOSES OF THE CHART ABOVE.

TYPE 1 RAMP DIMENSIONS AND QUANTITIES

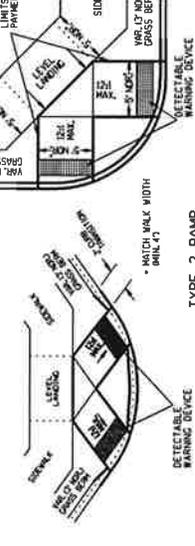
RADIUS (FEET)	DISTANCE (FEET)	LENGTH (FEET)	AREA (SQ. YD.)
15	11.62	18.82	26.21
20	15.52	26.28	39.87
25	19.42	33.75	53.53
30	23.32	41.22	67.19
35	27.22	48.69	80.85
40	31.12	56.16	94.51
45	35.02	63.63	108.17
50	38.92	71.10	121.83
55	42.82	78.57	135.49
60	46.72	86.04	149.15
65	50.62	93.51	162.81
70	54.52	100.98	176.47
75	58.42	108.45	190.13
80	62.32	115.92	203.79
85	66.22	123.39	217.45
90	70.12	130.86	231.11
95	74.02	138.33	244.77
100	77.92	145.80	258.43
105	81.82	153.27	272.09
110	85.72	160.74	285.75
115	89.62	168.21	299.41
120	93.52	175.68	313.07
125	97.42	183.15	326.73
130	101.32	190.62	340.39
135	105.22	198.09	354.05
140	109.12	205.56	367.71
145	113.02	213.03	381.37
150	116.92	220.50	395.03
155	120.82	227.97	408.69
160	124.72	235.44	422.35
165	128.62	242.91	436.01
170	132.52	250.38	449.67
175	136.42	257.85	463.33
180	140.32	265.32	476.99
185	144.22	272.79	490.65
190	148.12	280.26	504.31
195	152.02	287.73	517.97
200	155.92	295.20	531.63



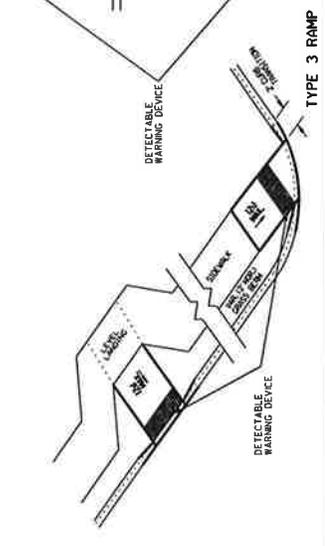
NOTE: CROSS SLOPE OF THE RAMPS, LEVEL LANDINGS, AND SIDEWALKS SHALL NOT EXCEED 2.0% UNLESS REQUIRED TO MATCH STREET LONGITUDINAL GRADE.



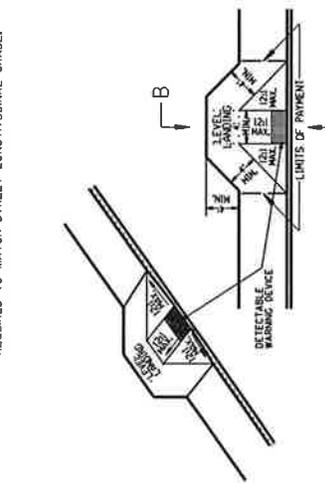
SECTION A-A



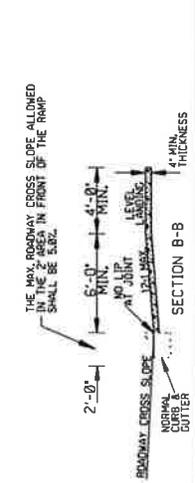
TYPE 2 RAMP



TYPE 3 RAMP



TYPE 4 RAMP (walk adjacent to curb)



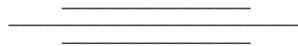
SECTION B-B

ARKANSAS STATE HIGHWAY COMMISSION



STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION

EDITION OF 2014



CONTRACT FORMS

CONTRACT

CONTRACT SCHEDULE OF PRICES

STATUTORY PERFORMANCE BOND

STATUTORY PAYMENT BOND

ARKANSAS STATE HIGHWAY COMMISSION

CONTRACT

THIS CONTRACT AND AGREEMENT, made and entered into this date _____
by and between **CK ASPHALT, LLC**

hereinafter referred to as the Contractor, and the Arkansas State Highway Commission, hereinafter referred to as the Commission, Witnesseth:

That for and in consideration of the payment to be made as set forth in the Contract Schedule of Prices, the Contractor hereby agrees to furnish all tools, labor, equipment, and materials, and to build and construct that certain project in **FAULKNER** County, designated as

Job C23006 STATE AID CITY JOB (CODE 9971)

Job Name: CONWAY DAVIS ST. OVERLAY (S)

consisting of an improvement of **City Street, DAVIS STREET**, more specifically described in the Contract Schedule of Prices attached hereto, all in exact accord with the Construction Plans on file in the Office of the Commission at Little Rock, Arkansas; and with the Arkansas Standard Specifications for Highway Construction, Edition of 2014; and with the Proposal filed with the Commission on **January 8, 2020**; and with the Supplemental Specifications and Special Provisions accompanying said Proposal, copy of said Plans, Specifications, Supplemental Specifications, and Special Provisions being attached hereto and made a part hereof as fully as though copied in full herein; under the direct supervision of the Engineer, and to the entire satisfaction of the Commission, and in accordance with the laws of the State of Arkansas; and in case the United States Government is participating in any portion of the cost of the work, the work shall also be subject to inspection and approval at all times by the appropriate Federal agency.

The Contractor agrees, for the consideration set forth in the Contract Schedule of Prices, to begin work within the time specified in the Work Order issued by the Engineer and to complete the work **within thirty (30) working days**. If the Contractor shall fail to complete the work within the time limit herein specified, he shall pay to the Commission, as liquidated damages, and not in the nature of a penalty, the sum of **within eight hundred dollars (\$800.00)** for each day delayed, it being understood and agreed between the parties hereto that the said sum fixed as liquidated damages is a reasonable sum, considering the damages that the Commission will sustain in the event of any such delay, and said amount is herein agreed upon and fixed as liquidated damages, because of the difficulty of ascertaining the exact amount of damages that may be sustained by such delay. The said sum shall be deducted from the final amount of estimate due the Contractor.

It is agreed and understood between the parties hereto that the Contractor agrees to accept and the Commission agrees to pay for the work at the prices stipulated in the Contract Schedule of Prices, such payment to be in lawful money of the United States, and the payment shall be made at the time and in the manner set forth in the Specifications.

WITNESS OUR HANDS, this date _____

CONTRACTOR

CK ASPHALT, LLC

BY: _____

PRINTED NAME: _____
(Must be Legible)

ARKANSAS STATE HIGHWAY COMMISSION

BY: _____
Director

Disclaimer:

This contract is being electronically signed. The Doc Express Document Signing History will be located on the final page of the executed contract. This document will have all dates and electronic signatures applicable to the execution of this contract.

Arkansas Department of Transportation
Contract Schedule of Prices

Letting Date: 1/8/2020

State Job No.: C23006

Federal Aid Project: 9971

Job Name: CONWAY DAVIS ST. OVERLAY (S)

Routes: DAVIS STREET

Sections:

Length: 1.570 Miles

Counties: FAULKNER

Description: THE PURPOSE OF THIS PROJECT IS TO OVERLAY AND SURFACE APPROXIMATELY 1.57 MILES OF DAVIS STREET IN THE CITY OF CONWAY IN FAULKNER COUNTY. THIS PROJECT CONSISTS OF REMOVAL AND DISPOSAL OF ITEMS, ACHM SURFACE COURSE, COLD MILLING ASPHALT PAVEMENT, ACHM PATCHING OF EXISTING ROADWAY, MAINTENANCE OF TRAFFIC, WHEELCHAIR RAMPS, PAVEMENT MARKING, AND MISC. ITEMS.

Contractor Address

CK ASPHALT, LLC

P.O. BOX 489

QUITMAN, AR 72131

Contract Time: 30 Working Days

Contract Amount: \$342,983.19

Liquidated Damages: \$800 per Day

Arkansas Department of Transportation
Contract Schedule of Prices

State Job No.: C23006

Job Name: CONWAY DAVIS ST. OVERLAY (S)

Federal Aid Project: 9971

Line Number	Item Code and Description	Estimated Quantity	Unit Bid Price	Price Extension
Section 01 - PROPOSAL ITEMS				
0001	202 - REMOVAL AND DISPOSAL OF CURB	37.000 LF	7.00	259.00
0002	202 - REMOVAL AND DISPOSAL OF WALKS	20.000 SQYD	8.35	167.00
0003	SS&401 - TACK COAT	2,592.000 GAL	2.60	6,739.20
0004	SPSS407 - MINERAL AGGREGATE IN ACHM SURFACE COURSE (3/8")	2,812.000 TON	23.00	64,676.00
0005	SPSS407 - ASPHALT BINDER (PG 64-22) IN ACHM SURFACE COURSE (3/8") (MINIMUM BID \$120.00)	179.000 TON	880.00	157,520.00
0006	412 - COLD MILLING ASPHALT PAVEMENT	26,505.000 SQYD	2.25	59,636.25
0007	SPSS415 - ACHM PATCHING OF EXISTING ROADWAY	101.000 TON	150.00	15,150.00
0008	SS&603 - MAINTENANCE OF TRAFFIC	1.000 L.S.	7,500.00	7,500.00
0009	SPSS604 - SIGNS	72.000 SQFT	4.00	288.00
0010	641 - WHEELCHAIR RAMPS (TYPE 3)	25.000 SQYD	480.00	12,000.00
0011	718 - REFLECTORIZED PAINT PAVEMENT MARKING WHITE (6")	10,432.000 LF	0.12	1,251.84
0012	718 - REFLECTORIZED PAINT PAVEMENT MARKING WHITE (12")	500.000 LF	1.00	500.00
0013	718 - REFLECTORIZED PAINT PAVEMENT MARKING WHITE (24")	149.000 LF	1.50	223.50
0014	718 - REFLECTORIZED PAINT PAVEMENT MARKING YELLOW (4")	16,360.000 LF	0.09	1,472.40
0015	718 - REFLECTORIZED PAINT PAVEMENT MARKING (ARROWS)	20.000 EACH	40.00	800.00
0016	718 - REFLECTORIZED PAINT PAVEMENT MARKING (BIKE EMBLEMS)	20.000 EACH	40.00	800.00
Section Total:				328,983.19
Section 02 - MOBILIZATION				
0017	601 - MOBILIZATION (UNIT BID AMOUNT MAY NOT EXCEED 5% OF SUBTOTAL)	1.000 L.S.	14,000.00	14,000.00
Section Total:				14,000.00
Bid Total:				342,983.19

10/9/98

**ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENT TO PROPOSAL
ANTI-COLLUSION AND DEBARMENT CERTIFICATION**

FAILURE TO EXECUTE AND SUBMIT THIS CERTIFICATION SHALL RENDER THIS BID NONRESPONSIVE AND NOT ELIGIBLE FOR AWARD CONSIDERATION.

As a condition precedent to the acceptance of the bidding document for this project, the bidder shall file this Affidavit executed by, or on behalf of the person, firm, association, or corporation submitting the bid. The original of this Affidavit shall be filed with the sponsor at the time proposals are submitted.

AFFIDAVIT

I hereby certify, under penalty of perjury under the laws of the United States and/or the State of Arkansas, that the bidder listed below has not, either directly or indirectly, entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of free competitive bidding in connection with the submitted bid for this project, is not presently barred from bidding in any other jurisdiction as a result of any collusion or any other action in restraint of free competition, and that the foregoing is true and correct.

Further, that except as noted below, the bidder, or any person associated therewith in the capacity of owner, partner, director, officer, principal investigator, project director, manager, auditor, or any position involving the administration of Federal funds:

- a. is not currently under suspension, debarment, voluntary exclusion, or determination of ineligibility by any Federal, State, or Local agency;
- b. has not been suspended, debarred, voluntarily excluded or determined ineligible by any Federal, State, or Local agency within the past 3 years;
- c. does not have a proposed debarment pending; and
- d. has not been indicted, convicted, or had an adverse civil judgment rendered by a court of competent jurisdiction in any matter involving fraud or official misconduct within the past 3 years.

FAILURE TO EXECUTE AND SUBMIT THIS CERTIFICATION SHALL RENDER THIS BID NONRESPONSIVE AND NOT ELIGIBLE FOR AWARD CONSIDERATION.

EXCEPTIONS:

**Applied To
Initiating Agency
Dates of Action**

**Applied To
Initiating Agency
Dates of Action**

**Applied To
Initiating Agency
Dates of Action**

Exceptions will not necessarily result in denial of award, but will be considered in determining bidder responsibility. Providing false information may result in criminal prosecution or administrative sanctions.

JOB NO. C23006
F.A.P. NO. 9971
Name of Bidder: CK Asphalt, LLC

**ARKANSAS DEPARTMENT OF TRANSPORTATION
SUPPLEMENT TO PROPOSAL
RESTRICTION OF BOYCOTT OF ISRAEL CERTIFICATION**

Pursuant to Arkansas Code Annotated § 25-1-503, a public entity shall not enter into a contract valued at \$1,000 or greater with a company unless the contract includes a written certification that the person or company is not currently engaged in, and agrees for the duration of the contract not to engage in, a boycott of Israel.

By agreeing below, the Contractor agrees and certifies that they do not boycott Israel and will not boycott Israel during the remaining aggregate term of the contract.

If a company does boycott Israel, see Arkansas Code Annotated § 25-1-503.

THIS CERTIFICATION SHALL BE COMPLETED BY THE BIDDER AS PART OF THIS PROPOSAL

The bidder CK Asphalt, LLC, hereby certifies that he will not boycott Israel during the term of this contract.

JOB NO. C23006
F.A.P. NO. 9971
Name of Bidder: CK Asphalt, LLC