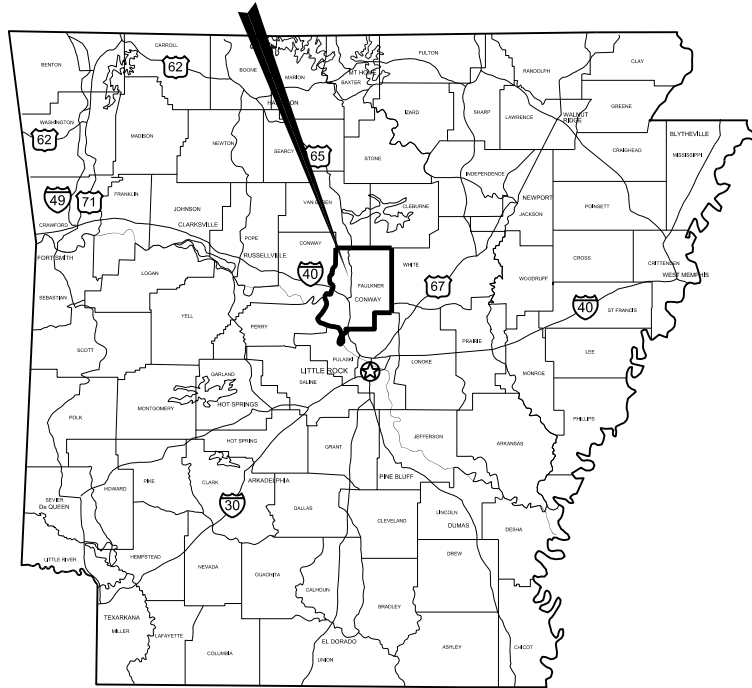


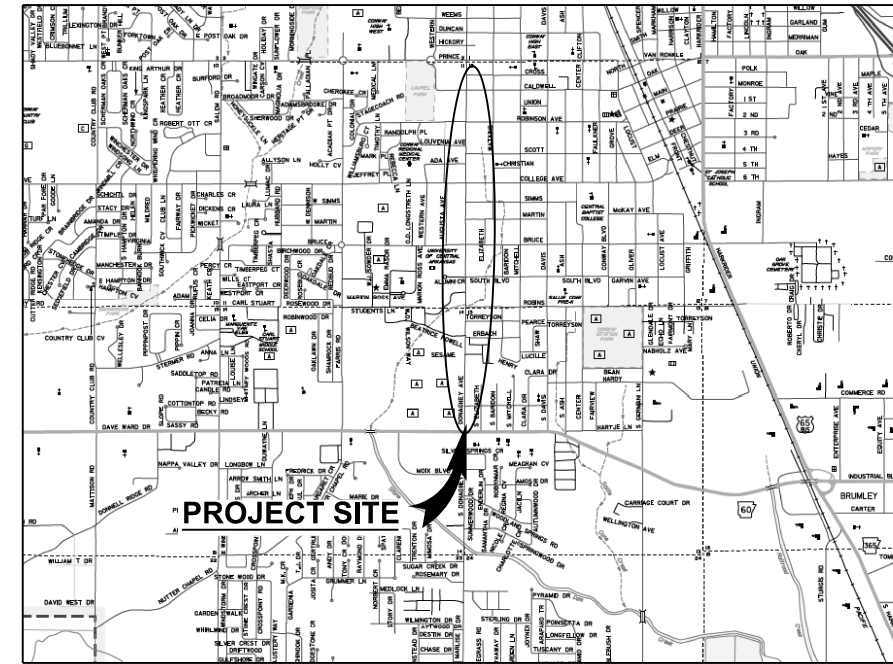
DONAGHEY AVE. IMPROVEMENTS PHASE 1 (DAVE WARD DR. TO ADA AVE.) CITY OF CONWAY CONWAY, ARKANSAS

PROJECT LOCATION



LOCATION MAP

CITY OF CONWAY JOB 18-110



VICINITY MAP

NO SCALE

**GARVER PROJECT NO. 18147010
MAY 2020**



831 Parkway
Suite C
Conway, AR 72034
(501) 537-3293

DESIGN TRAFFIC DATA

DESIGN YEAR ----- 2040
2020 ADT ----- 16,800
2040 DHV ----- -1,680
2040 ADT ----- -20,900
TRUCKS ----- -2%
DESIGN SPEED ----- -30 MPH
DIRECTIONAL DISTRIBUTION ----- -0.50



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CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

COVER SHEET

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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NUMBER **1**



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

INDEX OF SHEETS,
 GENERAL NOTES AND
 LEGEND

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
G-002
 SHEET NUMBER
2

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GENERAL NOTES:

- CAUTION: UNDERGROUND UTILITIES EXIST WITHIN AND ADJACENT TO THE LIMITS OF CONSTRUCTION. AN ATTEMPT HAS BEEN MADE TO LOCATE THESE UTILITIES ON THE PLANS; HOWEVER, ALL EXISTING UTILITIES MAY NOT BE SHOWN AND THE ACTUAL LOCATIONS OF THE UTILITIES MAY VARY FROM THE LOCATIONS SHOWN. SOME UTILITIES MAY HAVE BEEN RELOCATED SINCE THE TIME OF DESIGN AND THE CONTRACTOR'S NOTICE TO PROCEED. PRIOR TO BEGINNING ANY TYPE OF EXCAVATION, THE CONTRACTOR SHALL CONTACT THE UTILITIES INVOLVED AND MAKE ARRANGEMENTS FOR THE LOCATION OF THE UTILITIES ON THE GROUND. THE CONTRACTOR SHALL MAINTAIN THE UTILITY LOCATION MARKINGS UNTIL THEY ARE NO LONGER NECESSARY. ARKANSAS STATE LAW, THE UNDERGROUND FACILITIES DAMAGE PREVENTION ACT, REQUIRES TWO WORKING DAYS ADVANCE NOTIFICATION THROUGH THE ARKANSAS ONE-CALL SYSTEM CENTER BEFORE EXCAVATING USING MECHANIZED EQUIPMENT OR EXPLOSIVES (EXCEPT IN THE CASE OF EMERGENCY). THE ONE-CALL SYSTEM PHONE NUMBER IS 1-800-482-8998. THE CONTRACTOR IS ADVISED THAT THERE IS A SEVERE PENALTY FOR NOT MAKING THIS CALL. NOT ALL UTILITY COMPANIES ARE MEMBERS OF THE ARKANSAS ONE-CALL SYSTEM; THEREFORE, THE CONTRACTOR IS ADVISED TO CONTACT ALL NON-MEMBER UTILITIES AS WELL AS THE ONE-CALL SYSTEM. THE LOCATION OF THE EXISTING UTILITIES SHOWN IN THE PLANS ARE APPROXIMATE, AND ARE THE LOCATIONS AT THE TIME OF DESIGN.
- GRADE LINE DENOTES FINISHED GRADE WHERE SHOWN ON PLANS.
- ALL PIPE LINES, POWER, TELEPHONE AND TELEGRAPH LINES TO BE MOVED OR LOWERED BY THE RESPECTIVE OWNERS AS PER AGREEMENT WITH SUCH OWNERS.
- ANY EQUIPMENT OR APPURTENANCE THAT INTERFERES WITH THE PROPOSED CONSTRUCTION AND WHICH MAY BE THE PROPERTY OF UTILITY SERVICE ORGANIZATIONS SHALL BE MOVED BY THE OWNERS UNLESS OTHERWISE PROVIDED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING U. S. MAILBOXES WITHIN THE PROJECT LIMITS IN SUCH A MANNER THAT THE PUBLIC MAY RECEIVE CONTINUED MAIL SERVICE. PAYMENT WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS BID ITEMS.
- ALL LAND MONUMENTS LOCATED WITHIN THE CONSTRUCTION AREA SHALL BE PROTECTED IN ACCORDANCE WITH SECTION 107.12 OF THE STANDARD SPECIFICATIONS.
- ALL TREES THAT DO NOT DIRECTLY INTERFERE WITH THE PROPOSED CONSTRUCTION SHALL BE SPARED AS DIRECTED BY THE ENGINEER. CARE AND DISCRETION SHALL BE USED TO ENSURE THAT ALL TREES NOT TO BE REMOVED SHALL BE HARMED AS LITTLE AS POSSIBLE DURING THE CONSTRUCTION OPERATIONS.
- THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE OF THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- ALL FLEXIBLE BASE AND ASPHALTIC PAVEMENTS REMOVED SHALL BE PAID FOR UNDER THE ITEM - UNCLASSIFIED EXCAVATION.
- TRUNCATED DOME PAVERS SHALL BE INSTALLED IN ALL ACCESS RAMPS WITH THE EXCEPTION OF RAMPS LOCATED WITHIN ARDOT RIGHT OF WAY. THE TRUNCATED DOME PAVERS SHALL BE CONSIDERED SUBSIDIARY TO THE INDIVIDUAL ACCESS RAMP PAY ITEMS.
- BRICK PAVER ACCENTS LOCATED WITHIN THE SIDEWALK AND ROUNDABOUT SPLITTER ISLANDS REQUIRE A MINIMUM 4" CONCRETE BASE. THE CONCRETE BASE SHALL BE CONSIDERED SUBSIDIARY TO THE BRICK PAVERS.
- PIPE BEDDING SHALL BE TYPE I FOR CIRCULAR R.C. PIPE CULVERTS AND TYPE II FOR R.C. ARCH PIPE CULVERTS.

LEGEND

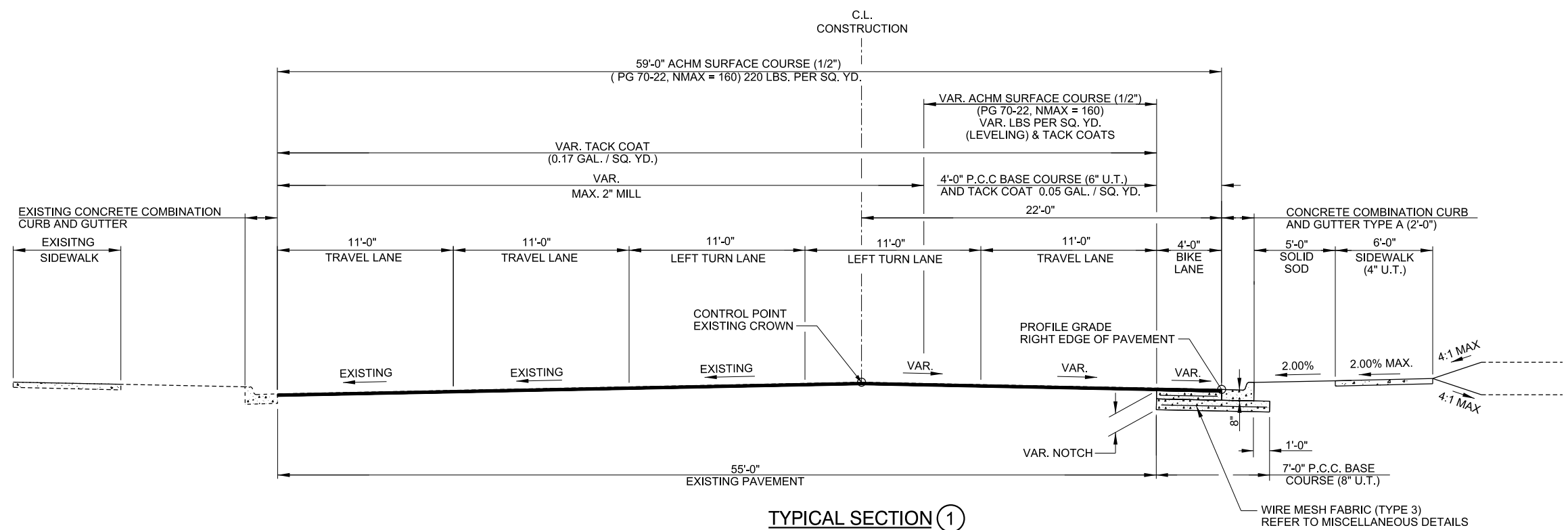
	BOREHOLE		EXISTING CENTERLINE
	CONTROL POINTS		EXISTING MAJOR CONTOUR
	SIGN		EXISTING MINOR CONTOUR
	GAS METER		EXISTING STRUCTURE
	SANITARY MANHOLE		EXISTING FENCE
	WATER VALVE		EXISTING STORM DRAIN
	WATER METER		EXISTING TREE LINE
	STORM DRAIN MANHOLE		EXISTING PROPERTY LINE
	TELEPHONE RISER		EXISTING RIGHT-OF-WAY
	ELECTRIC JUNCTION BOX		EXISTING EASEMENT
	FIBER OPTIC MANHOLE		EXISTING GAS UTILITY
	UTILITY POLE		EXISTING SANITARY UTILITY
	GUY ANCHOR		EXISTING WATER UTILITY
	LIGHT POLE		EXISTING UNDERGROUND TELEPHONE UTILITY
			EXISTING OVERHEAD ELECTRIC UTILITY
			PROPOSED TEMP. CONST. EASEMENT
			PROPOSED PERMANENT EASEMENT
			PROPOSED CENTERLINE
			PROPOSED STORM DRAIN
			PROPOSED TOP-OF-BANK
			PROPOSED TOE-OF-SLOPE
			PROPOSED SPECIAL DITCH
			PROPOSED SILT FENCE



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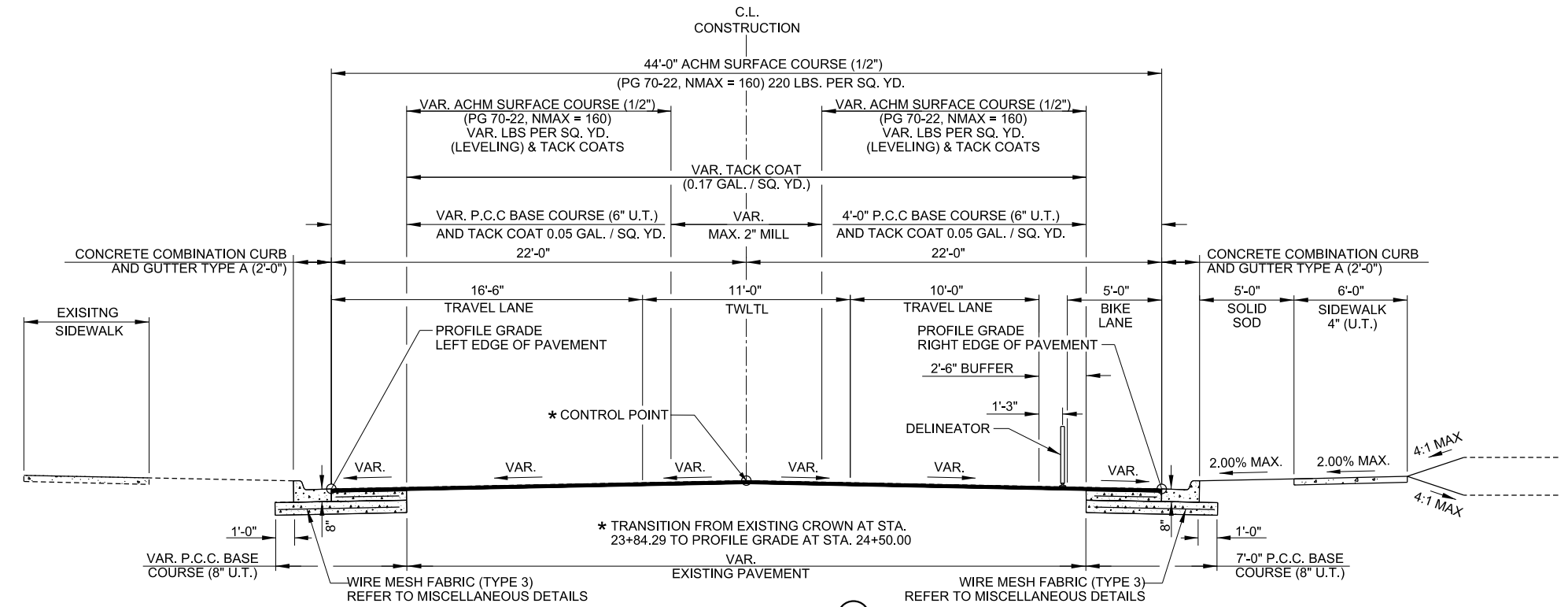


TYPICAL SECTION ①

DONAGHEY AVE. - STA. 20+70.00 TO STA. 23+84.29

TYPICAL SECTION GENERAL NOTES

1. REFER TO CROSS SECTIONS FOR DEVIATIONS FROM NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.
2. THE FINAL 2" OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT THE LANE LINES.
3. THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE TO THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
4. PRIOR TO AND DURING PLACEMENT OF PAVEMENT IN FRONT OF THE CURB AND GUTTER, THE CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE AT ALL TIMES. THE METHOD(S) USED SHALL BE APPROVED BY THE ENGINEER. PAYMENT FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS.
5. FORTA-FI, OR APPROVED EQUAL, FIBER REINFORCEMENT SHALL BE INCLUDED IN ALL SURFACE COURSE MIXES CONSTRUCTED WITHIN LIMITS OF RETAINED EXISTING ASPHALT PAVEMENT. PAYMENT FOR FIBER REINFORCEMENT SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR ACHM SURFACE COURSE.



TYPICAL SECTION ②

DONAGHEY AVE. - STA. 23+84.29 TO STA. 24+25.00

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TYPICAL SECTIONS
 (SHEET 1 OF 8)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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3

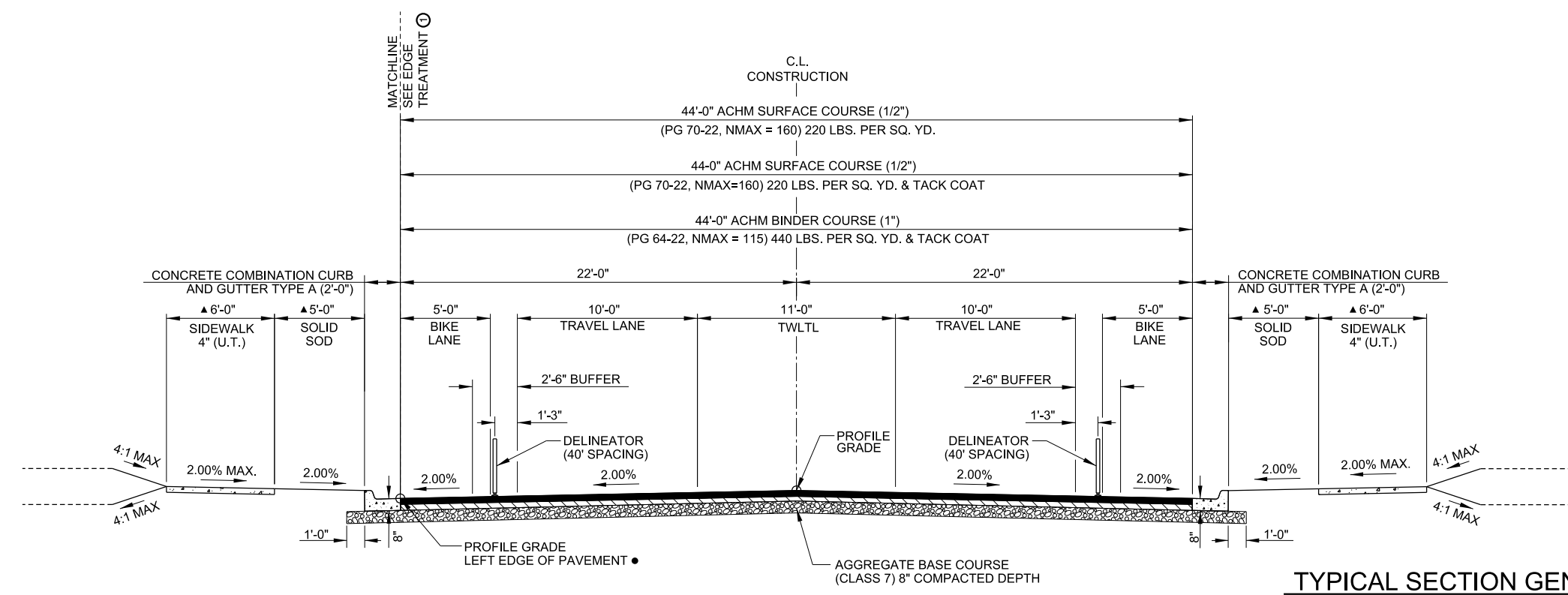
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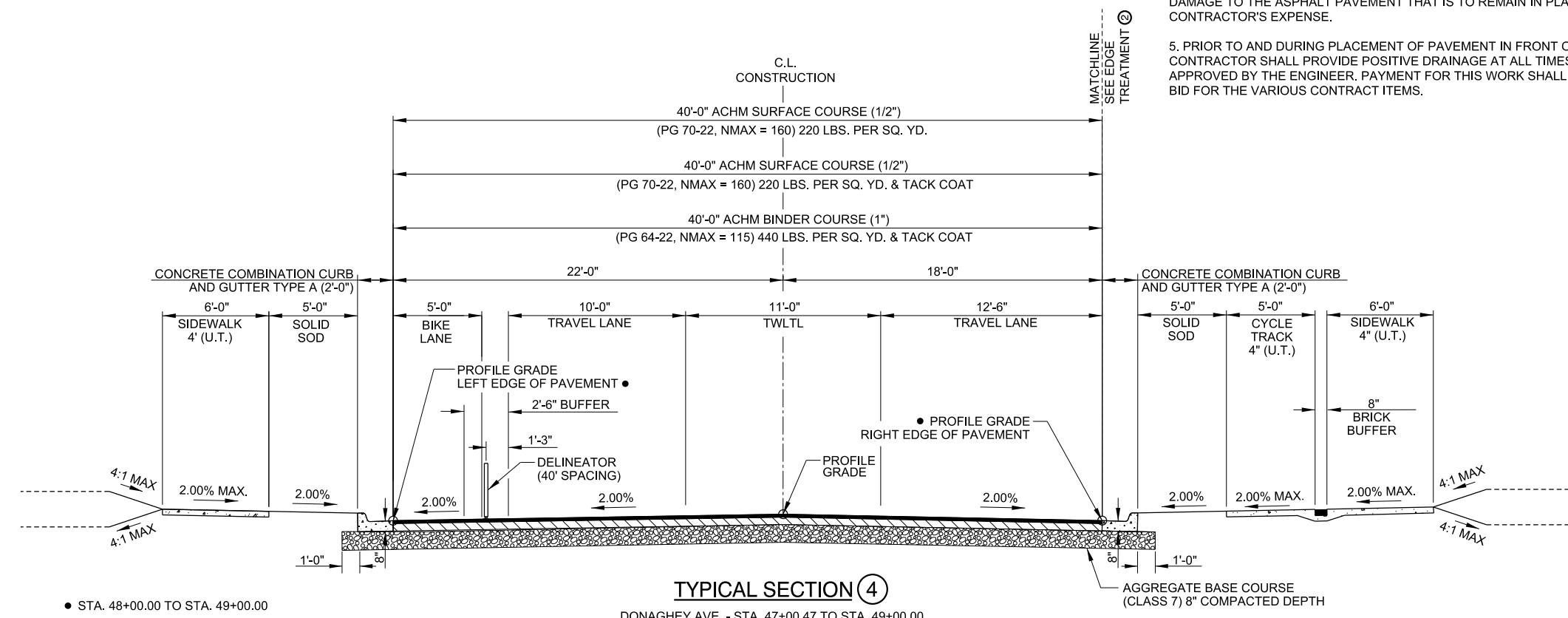
- ▲ SEE PLAN AND PROFILE SHEETS FOR VARIATIONS
- STA. 24+25.00 TO STA. 25+00.00

TYPICAL SECTION 3

DONAGHEY AVE. - STA. 24+25.00 TO STA. 47+00.47

TYPICAL SECTION GENERAL NOTES

1. REFER TO CROSS SECTIONS FOR DEVIATIONS FROM NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.
2. THE THICKNESS OF AGG. BASE COURSE SHALL BE WITHIN PLUS OR MINUS ONE INCH OF THE PLAN THICKNESS SHOWN. THE CONTRACTOR WILL CORRECT ANY DEFICIENT THICKNESS THAT DOES NOT MEET TOLERANCE INDICATED. PAYMENT WILL NOT BE MADE FOR MATERIAL PLACED IN EXCESS OF THE TOLERANCE INDICATED.
3. THE FINAL 2" OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT THE LANE LINES.
4. THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE TO THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
5. PRIOR TO AND DURING PLACEMENT OF PAVEMENT IN FRONT OF THE CURB AND GUTTER, THE CONTRACTOR SHALL PROVIDE POSITIVE DRAINAGE AT ALL TIMES. THE METHOD(S) USED SHALL BE APPROVED BY THE ENGINEER. PAYMENT FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS.



- STA. 48+00.00 TO STA. 49+00.00

TYPICAL SECTION 4

DONAGHEY AVE. - STA. 47+00.47 TO STA. 49+00.00

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TYPICAL SECTIONS
 (SHEET 2 OF 8)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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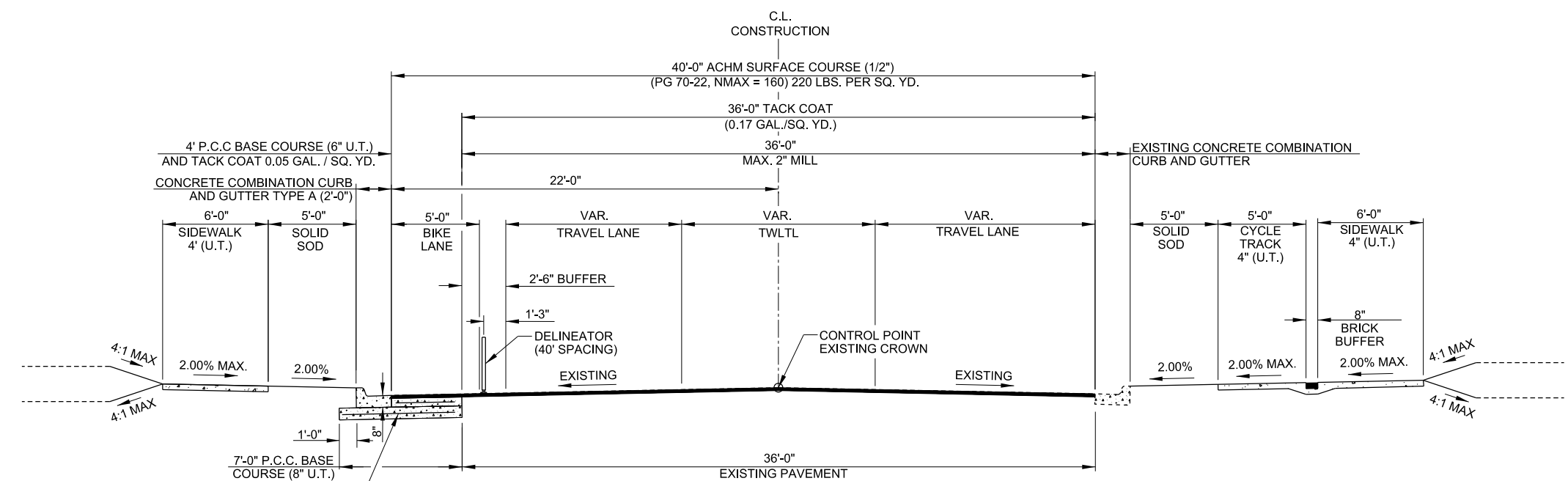
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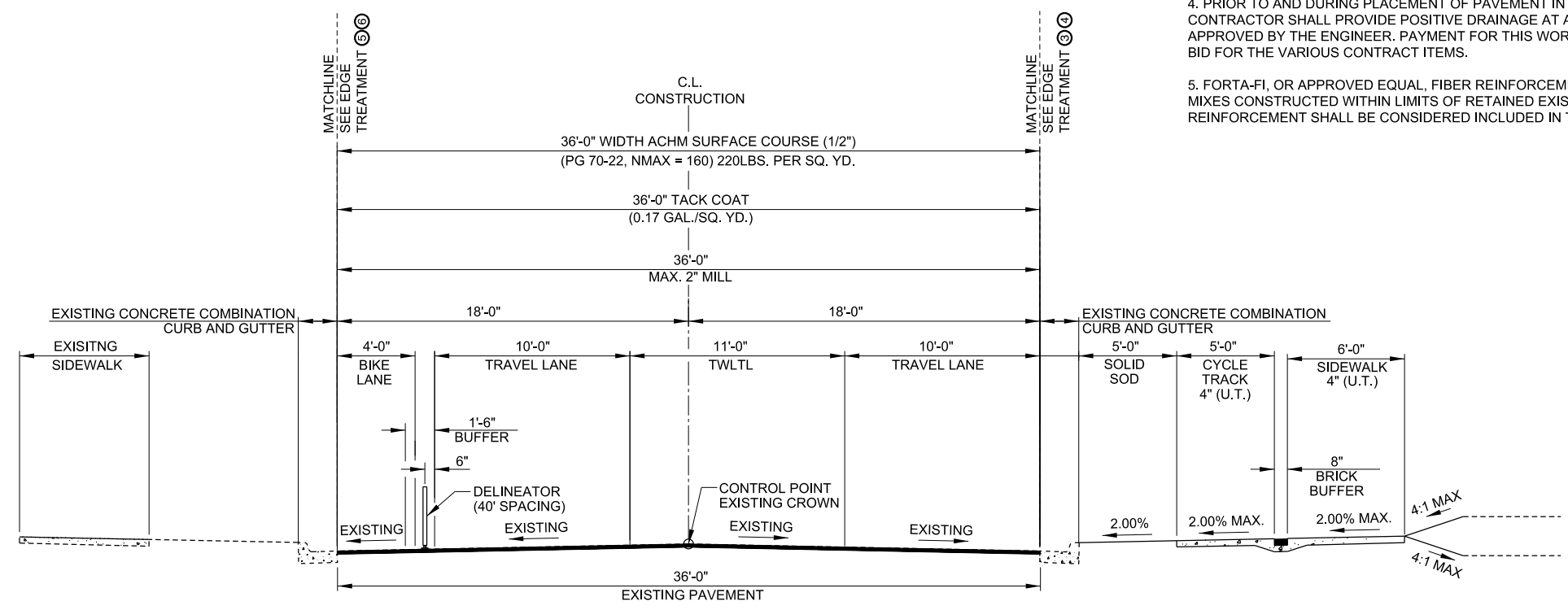
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TYPICAL SECTION 5
 DONAGHEY AVE. - STA. 49+00.00 TO STA. 49+54.19

TYPICAL SECTION GENERAL NOTES

1. REFER TO CROSS SECTIONS FOR DEVIATIONS FROM NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.
2. THE FINAL 2" OF SURFACE COURSE IS TO BE PLACED AFTER ALL OTHER COURSES HAVE BEEN LAID. LONGITUDINAL JOINTS SHALL BE AT THE LANE LINES.
3. THE EXISTING ASPHALT PAVEMENT TO BE REMOVED FROM THE REMAINING PAVEMENT SHALL BE SEPARATED BY SAWING ALONG A NEAT LINE. AFTER SAWING, THE PAVEMENT TO BE REMOVED SHALL BE CAREFULLY REMOVED IN A MANNER THAT WILL NOT DAMAGE THE PAVEMENT THAT IS TO REMAIN. ANY DAMAGE TO THE ASPHALT PAVEMENT THAT IS TO REMAIN IN PLACE SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
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5. FORTA-FI, OR APPROVED EQUAL, FIBER REINFORCEMENT SHALL BE INCLUDED IN ALL SURFACE COURSE MIXES CONSTRUCTED WITHIN LIMITS OF RETAINED EXISTING ASPHALT PAVEMENT. PAYMENT FOR FIBER REINFORCEMENT SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR ACHM SURFACE COURSE.



TYPICAL SECTION 6
 DONAGHEY AVE. - STA. 49+54.19 TO STA. 70+40.05

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TYPICAL SECTIONS
 (SHEET 3 OF 8)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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C-103

SHEET NUMBER
5



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TYPICAL SECTIONS
 (SHEET 4 OF 8)

JOB NO.: 18147010
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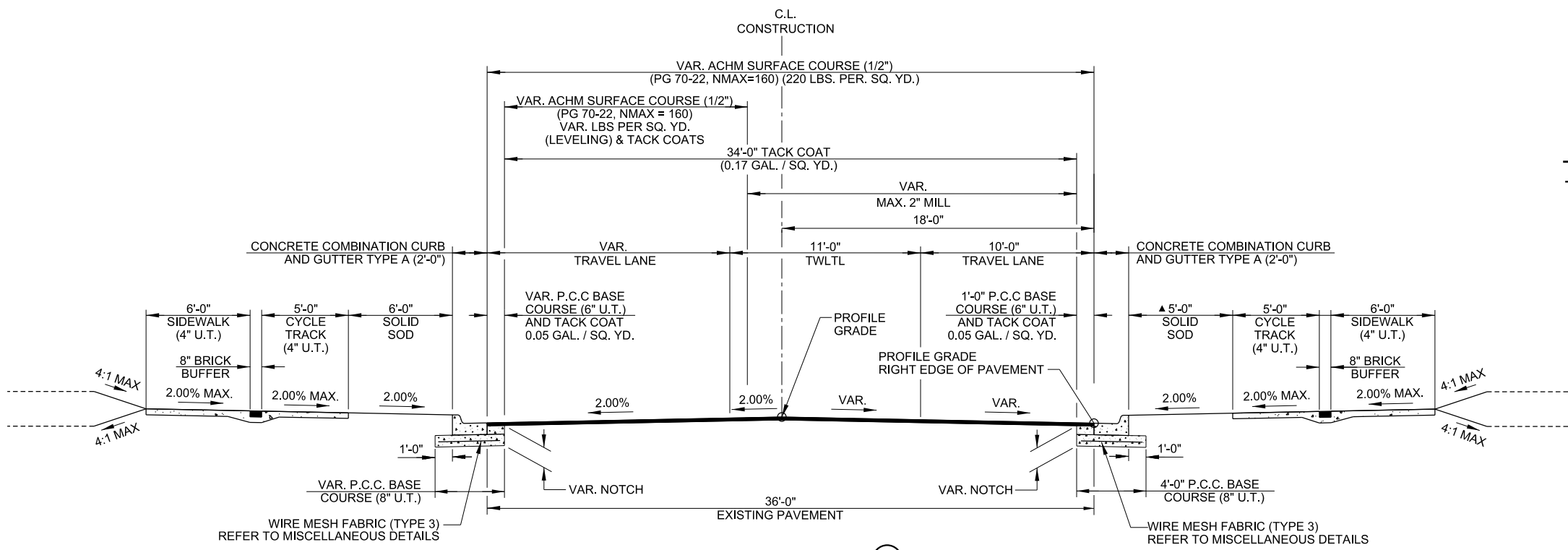
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DRAWING NUMBER
C-104

SHEET NUMBER
6

TYPICAL SECTION GENERAL NOTES

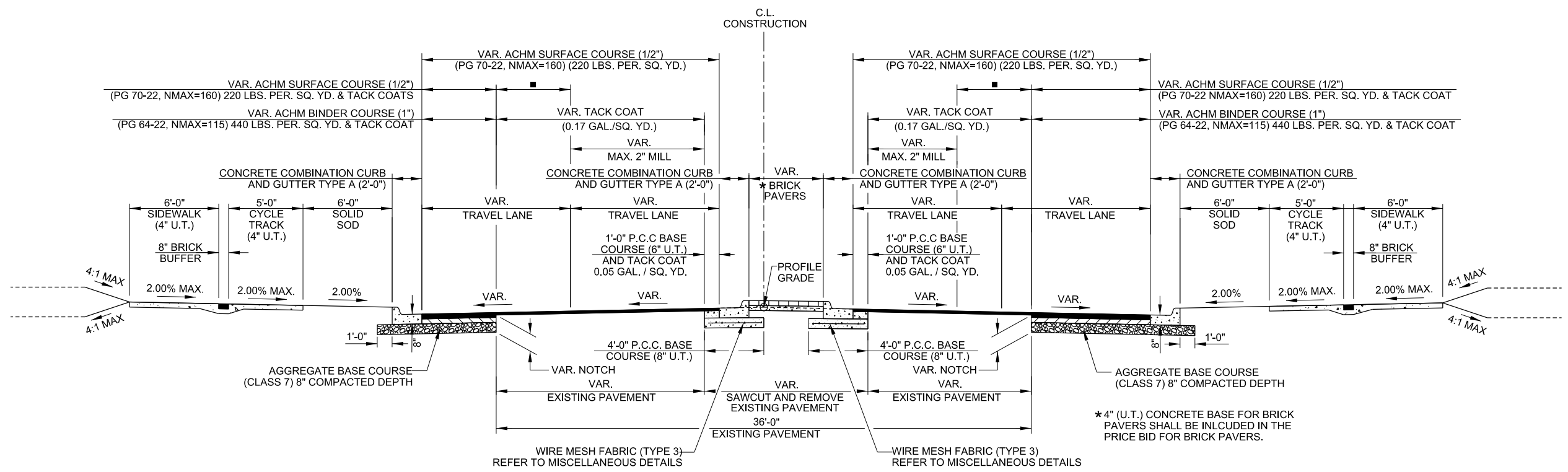
- REFER TO CROSS SECTIONS FOR DEVIATIONS FROM NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.
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- FORTA-FI, OR APPROVED EQUAL, FIBER REINFORCEMENT SHALL BE INCLUDED IN ALL SURFACE COURSE MIXES CONSTRUCTED WITHIN LIMITS OF RETAINED EXISTING ASPHALT PAVEMENT. PAYMENT FOR FIBER REINFORCEMENT SHALL BE CONSIDERED INCLUDED IN THE PRICE BID FOR ACHM SURFACE COURSE.



TYPICAL SECTION 7

DONAGHEY AVE. - STA. 70+40.05 TO STA. 71+88.53

NOTE:
 SEE INTERSECTION DETAILS FOR VARIABLE WIDTHS
 ▲ SEE PLAN AND PROFILE SHEETS FOR VARIATIONS



TYPICAL SECTION 8

DONAGHEY AVE. - STA. 71+88.53 TO STA. 72+62.67
 DONAGHEY AVE. - STA. 74+02.67 TO STA. 74+94.60
 COLLEGE AVE. (WEST) - STA. 8+50.70 TO STA. 9+30.28
 COLLEGE AVE. (EAST) - STA. 10+70.00 TO STA. 11+51.30

NOTE:
 SEE INTERSECTION DETAILS FOR VARIABLE WIDTHS

VAR. ACHM SURFACE COURSE (1/2") (PG 70-22, NMAX = 160) VAR. LBS PER SQ. YD. (LEVELING) & TACK COATS

* 4" (U.T.) CONCRETE BASE FOR BRICK PAVERS SHALL BE INCLUDED IN THE PRICE BID FOR BRICK PAVERS.

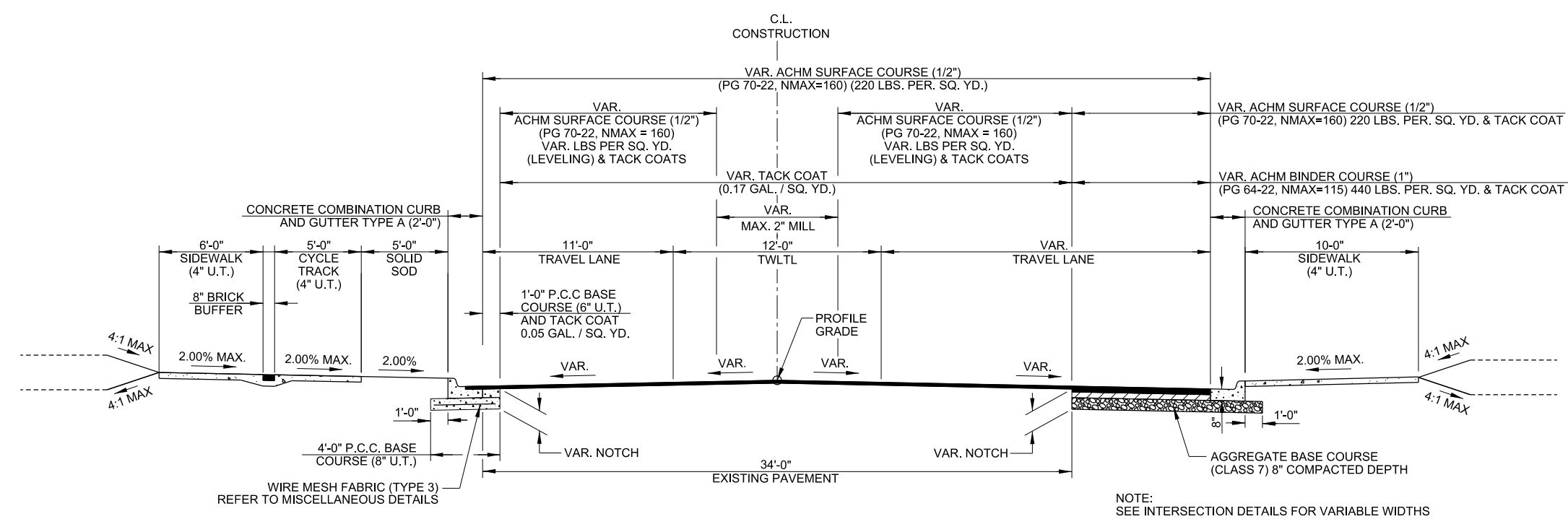
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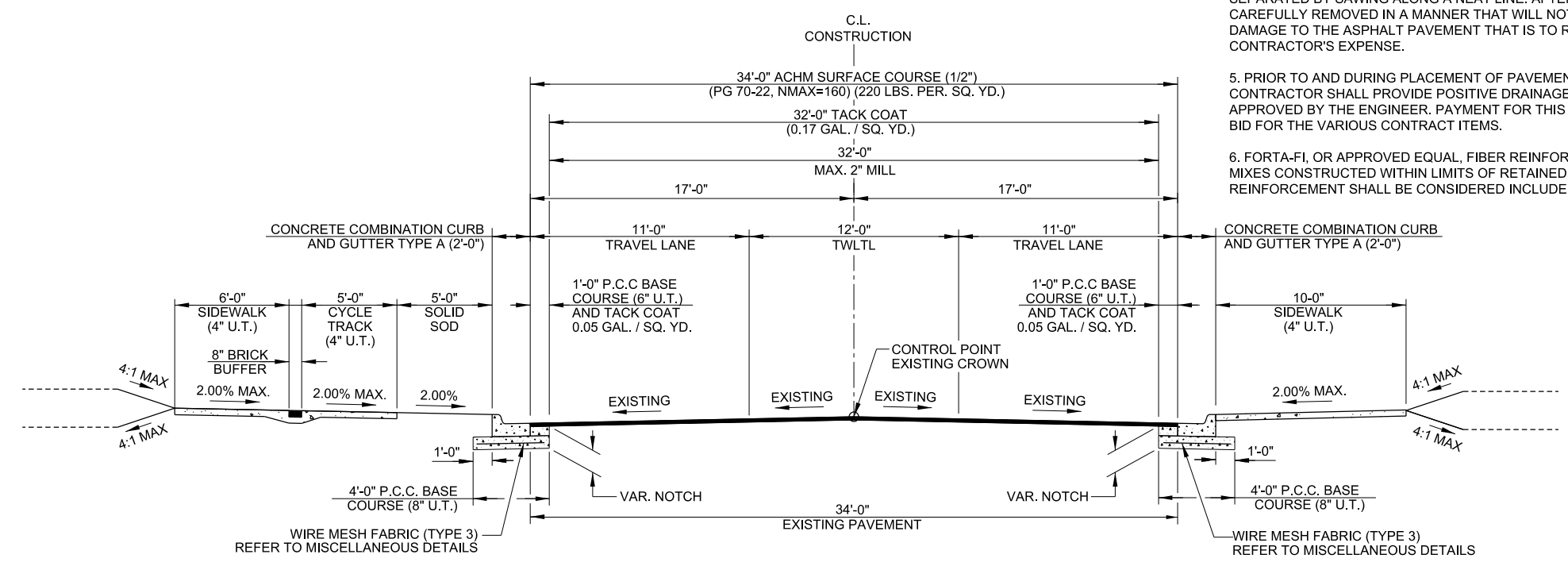
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TYPICAL SECTION 9
 DONAGHEY AVE. - STA. 74+94.60 TO STA. 76+11.43

TYPICAL SECTION GENERAL NOTES

1. REFER TO CROSS SECTIONS FOR DEVIATIONS FROM NORMAL SLOPES. NO CHANGES SHALL BE MADE FROM THE PLANNED SLOPES WITHOUT THE APPROVAL OF THE ENGINEER.
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TYPICAL SECTION 10
 DONAGHEY AVE. - STA. 76+11.43 TO STA. 77+80.00

NOTE:
 SEE INTERSECTION DETAILS FOR VARIABLE WIDTHS

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TYPICAL SECTIONS
 (SHEET 5 OF 8)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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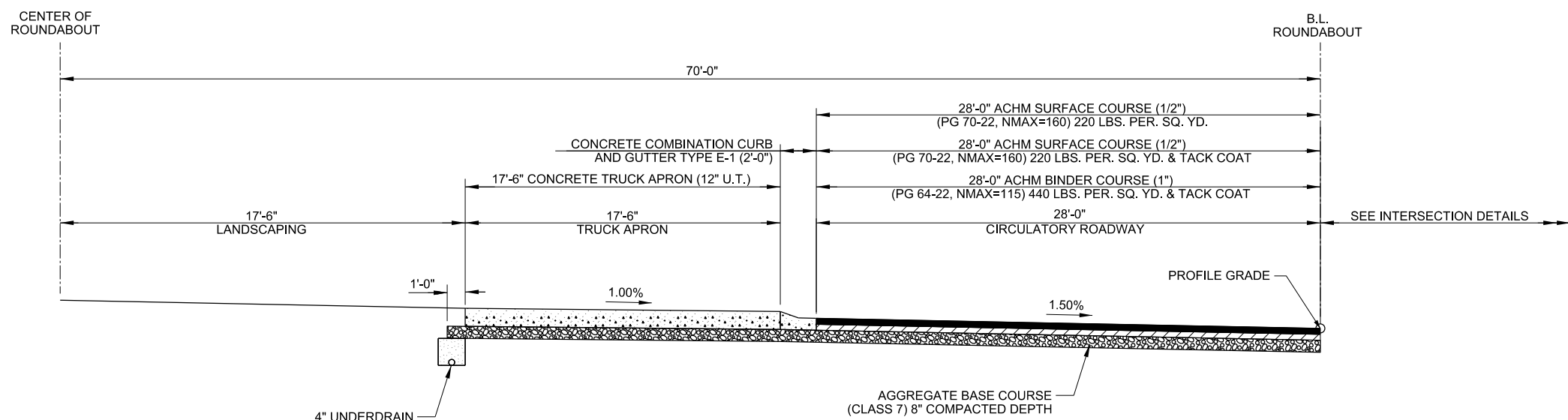
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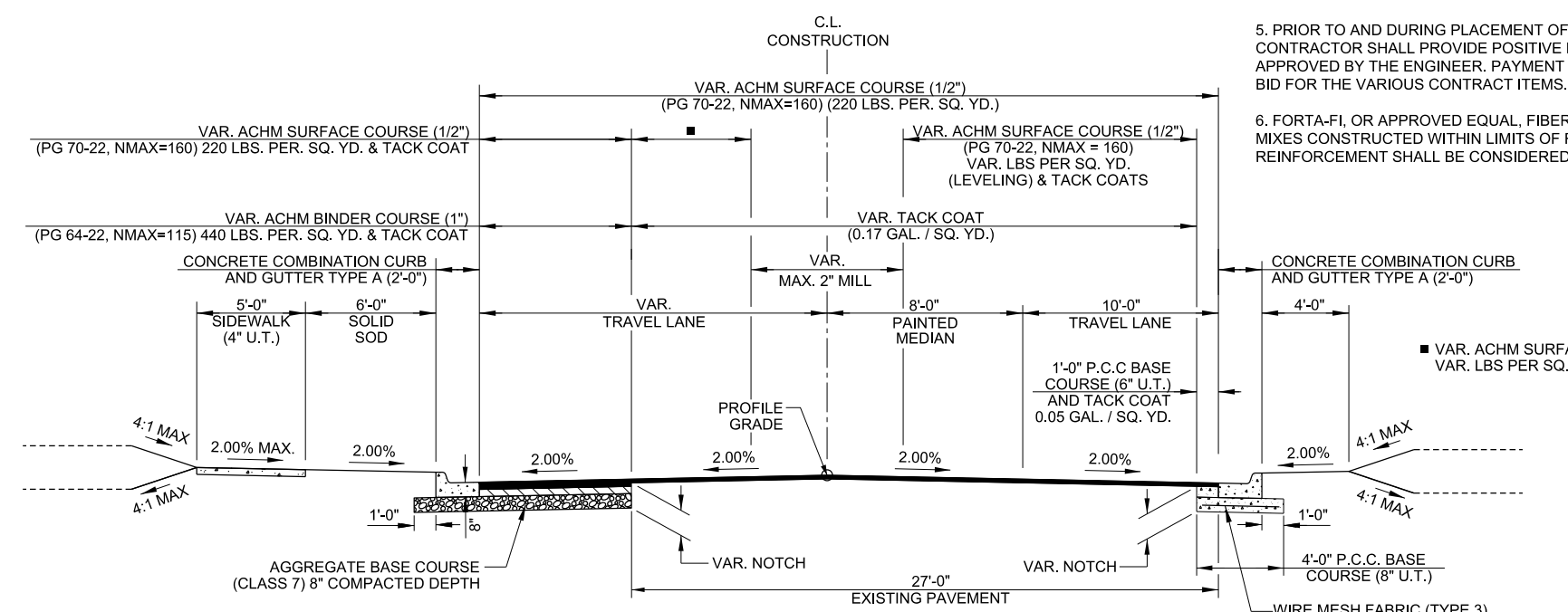
NOTE:
 CONNECT 4" PIPE UNDERDRAIN TO DROP INLET AT DONAGHEY AVE. STA. 72+80.00 RT.

TYPICAL SECTION 11

ROUNDABOUT STA. 10+00.00 TO STA. 14+39.82

TYPICAL SECTION GENERAL NOTES

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TYPICAL SECTION 12

COLLEGE AVE. (WEST) - STA. 6+71.39 TO STA. 8+50.70

NOTE:
 SEE INTERSECTION DETAILS FOR VARIABLE WIDTHS

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TYPICAL SECTIONS
 (SHEET 6 OF 8)

JOB NO.: 18147010
 DATE: MAY 2020
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 DRAWN BY: HJB

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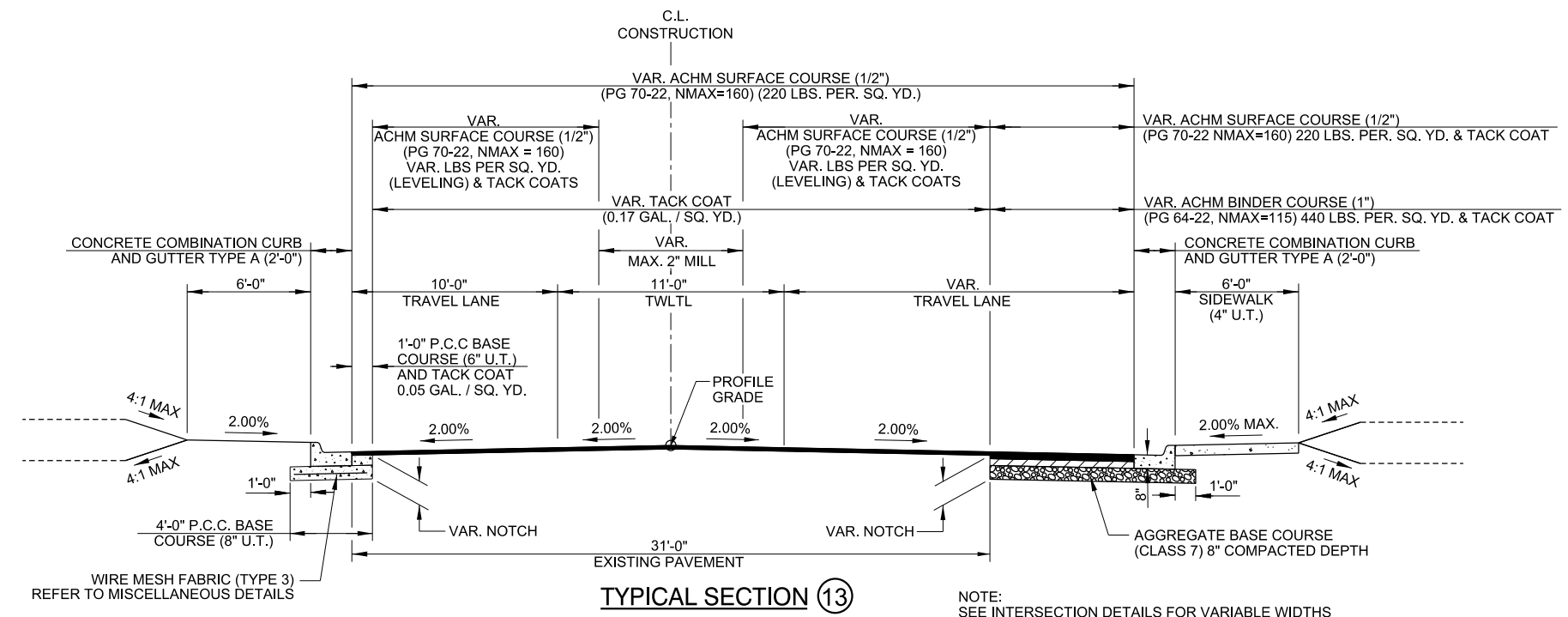
SHEET NUMBER
8



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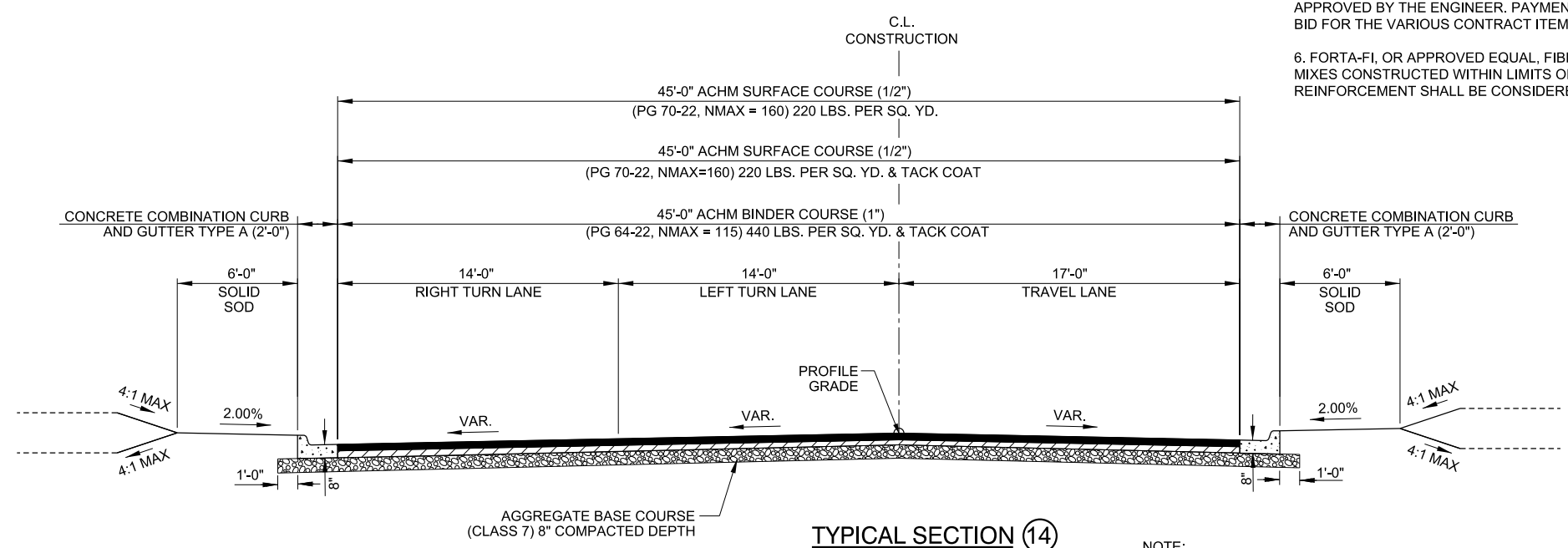


TYPICAL SECTION 13
 COLLEGE AVE. (EAST) - STA. 11+51.30 TO STA. 13+30.00

NOTE: SEE INTERSECTION DETAILS FOR VARIABLE WIDTHS

TYPICAL SECTION GENERAL NOTES

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TYPICAL SECTION 14
 ROBINS ST. - STA. 10+22.00 TO STA. 11+45.00

NOTE: SEE INTERSECTION DETAILS FOR VARIABLE WIDTHS

BY	DESCRIPTION	DATE	REV.



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TYPICAL SECTIONS
 (SHEET 7 OF 8)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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C-107

SHEET NUMBER
9

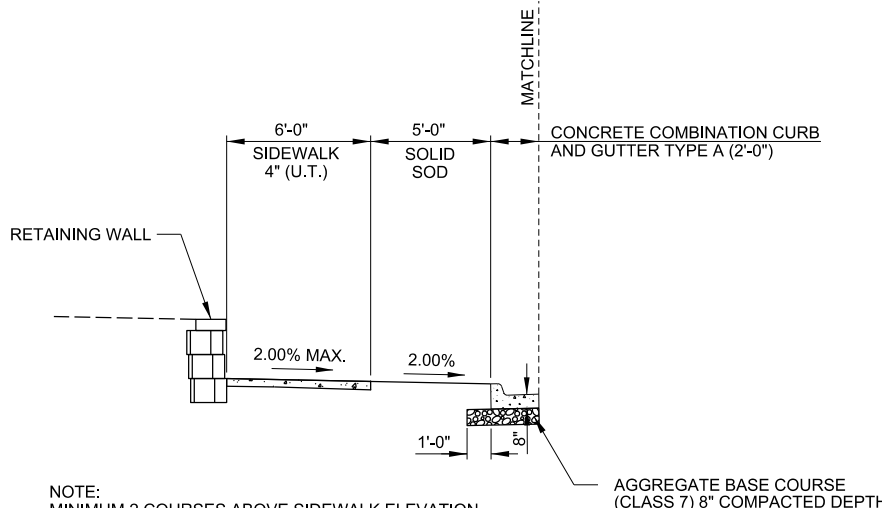
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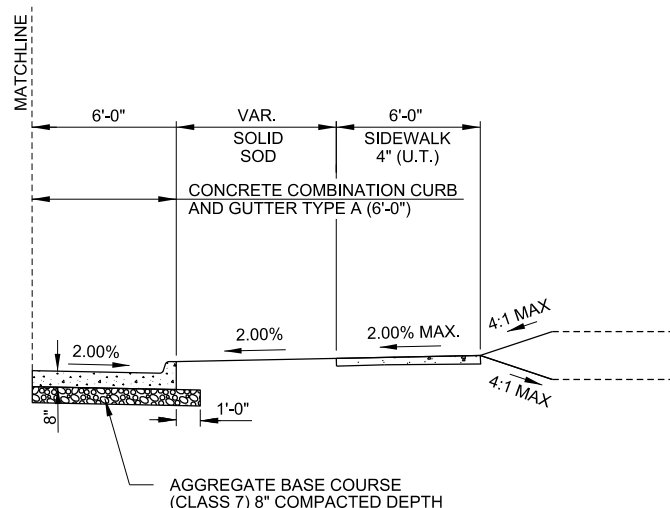
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NOTE:
 MINIMUM 2 COURSES ABOVE SIDEWALK ELEVATION WITH CAP STONE. SEE LANDSCAPE DETAILS FOR MORE INFORMATION.

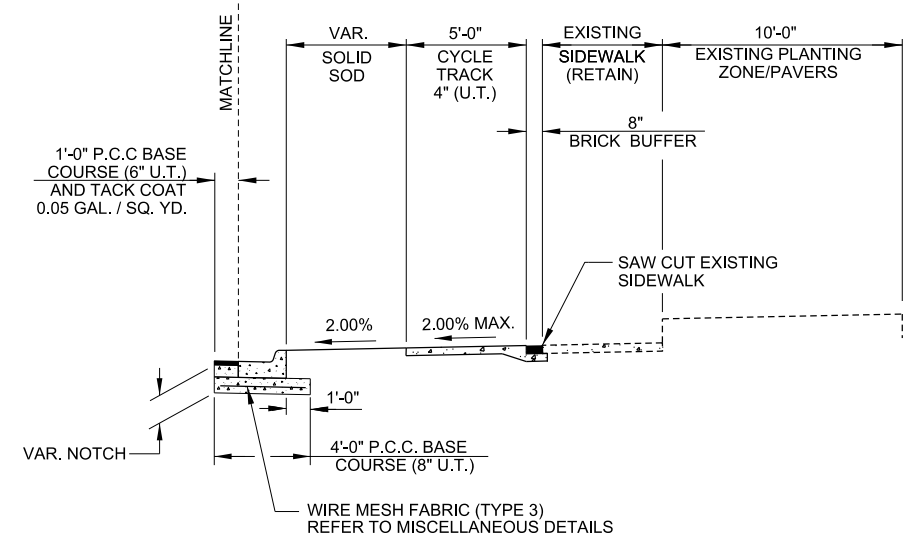
EDGE TREATMENT ①

STA. 40+60.00 TO STA. 41+75.00



EDGE TREATMENT ②

STA. 47+00.47 TO STA. 47+63.13

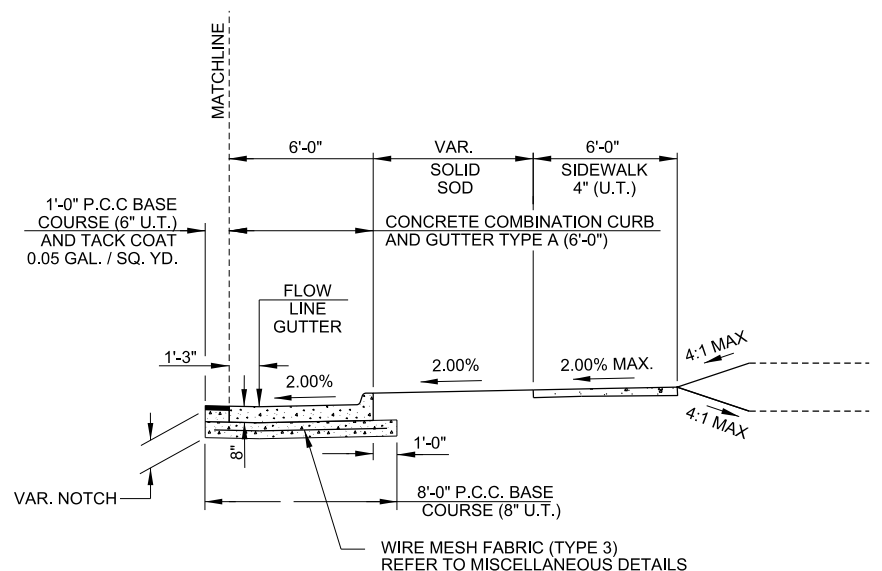


EDGE TREATMENT ③

STA. 57+46.48 TO STA. 59+12.74

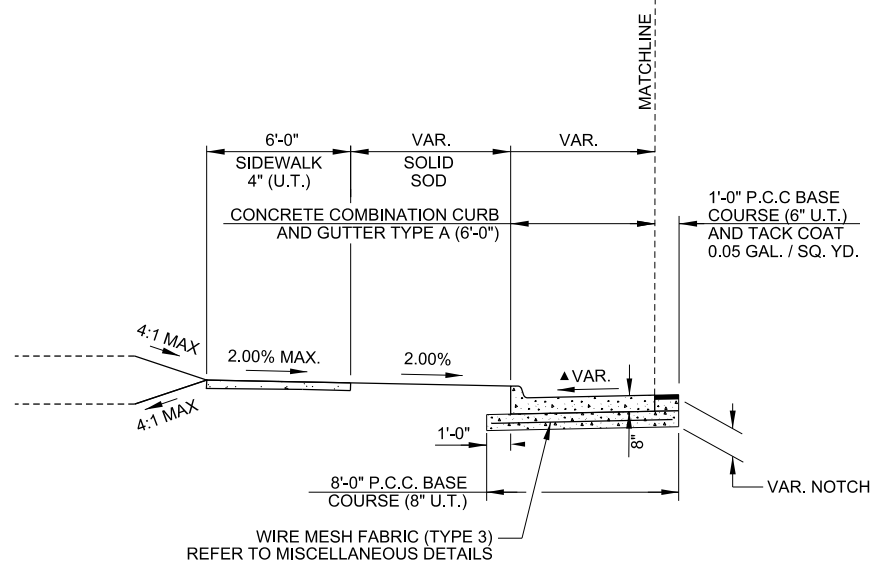
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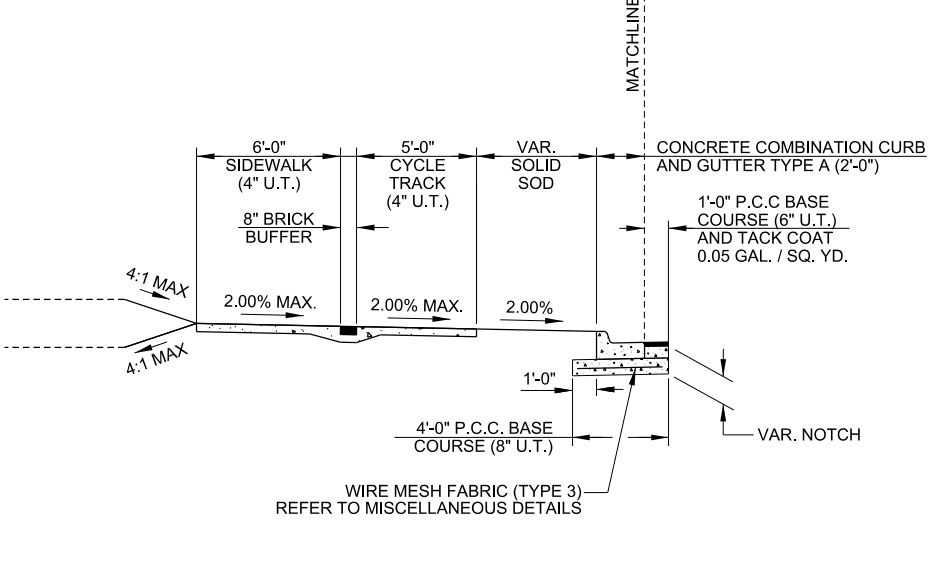
EDGE TREATMENT ④

STA. 59+12.74 TO STA. 59+89.33
 STA. 60+62.86 TO STA. 61+00.74



EDGE TREATMENT ⑤

STA. 69+80.00 TO STA. 70+00.00



EDGE TREATMENT ⑥

STA. 70+00.00 TO STA. 70+40.05

▲ MAINTAIN POSITIVE GUTTER FLOW TO SOUTH

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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TYPICAL SECTIONS
 (SHEET 8 OF 8)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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C-108
 SHEET NUMBER
10



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

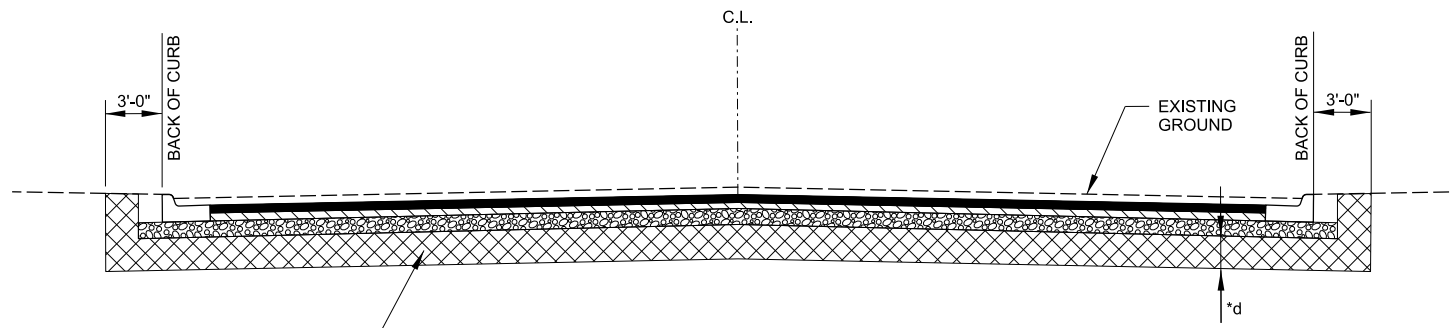
MISCELLANEOUS
 DETAILS
 (SHEET 1 OF 10)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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C-201

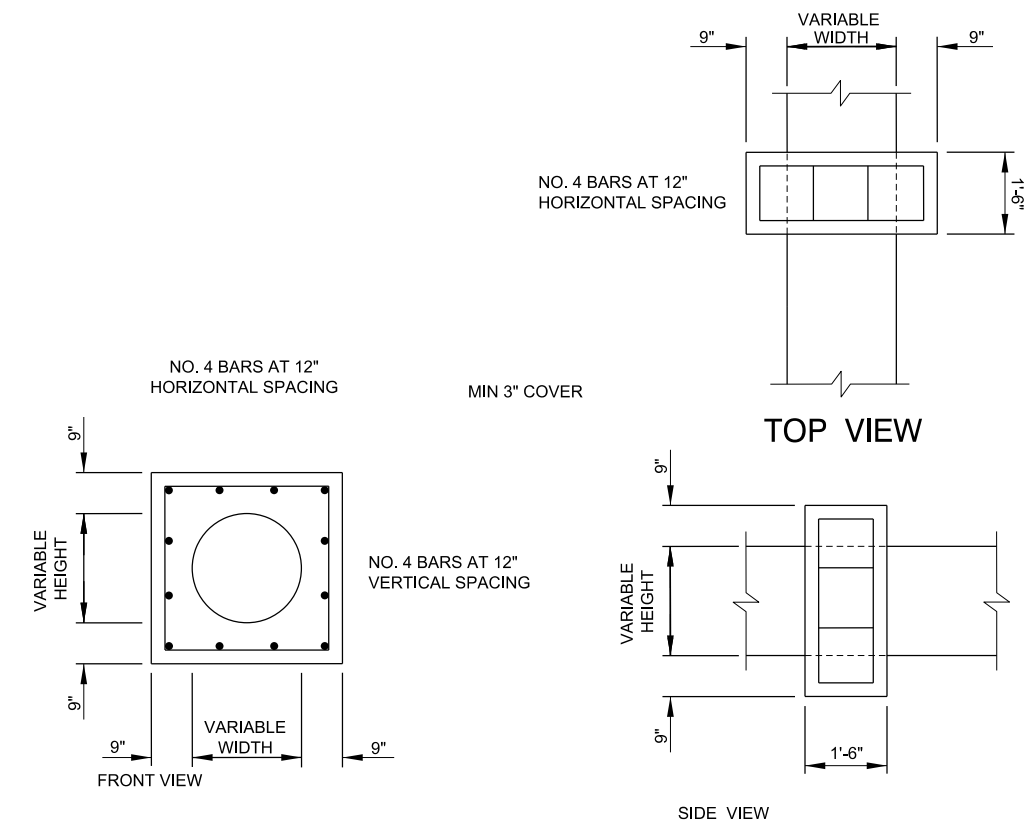
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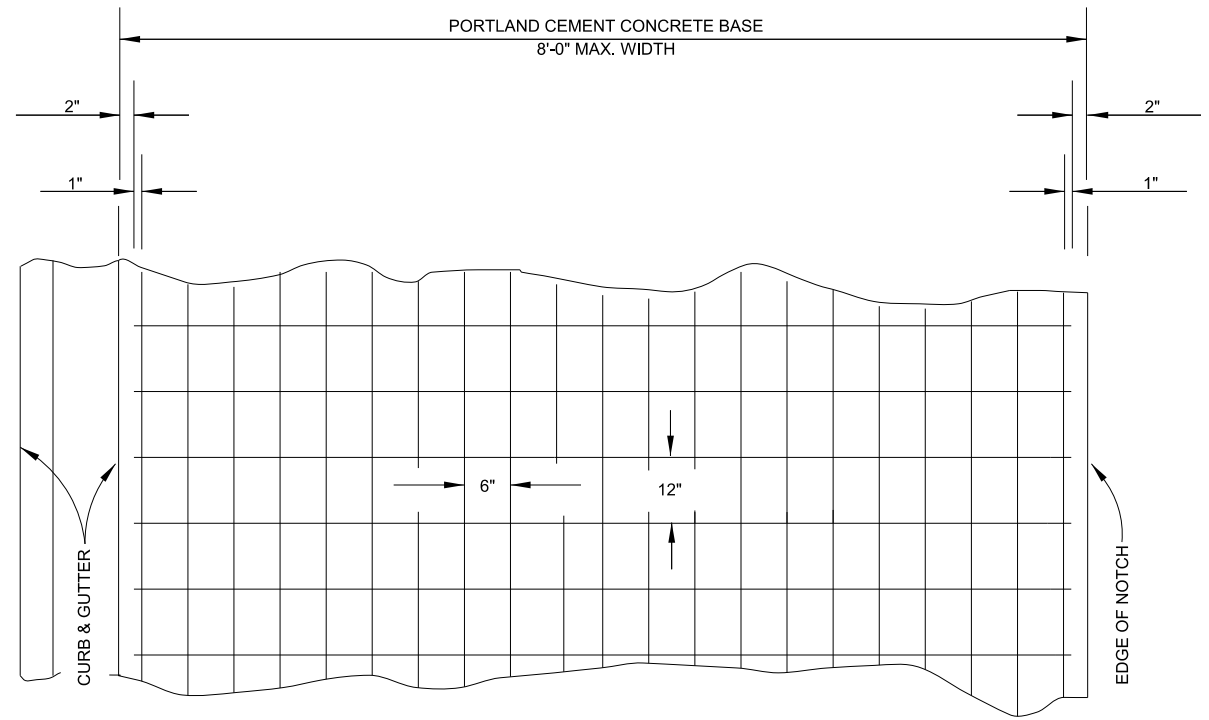
UNDERCUT AND BACKFILL AS DIRECTED BY THE ENGINEER

UNDERCUT DETAIL
 N.T.S.

* 2' - 4' ESTIMATED. TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER. 24,296 CU. YDS. HAS BEEN INCLUDED IN THE UNIT PRICE SCHEDULE.



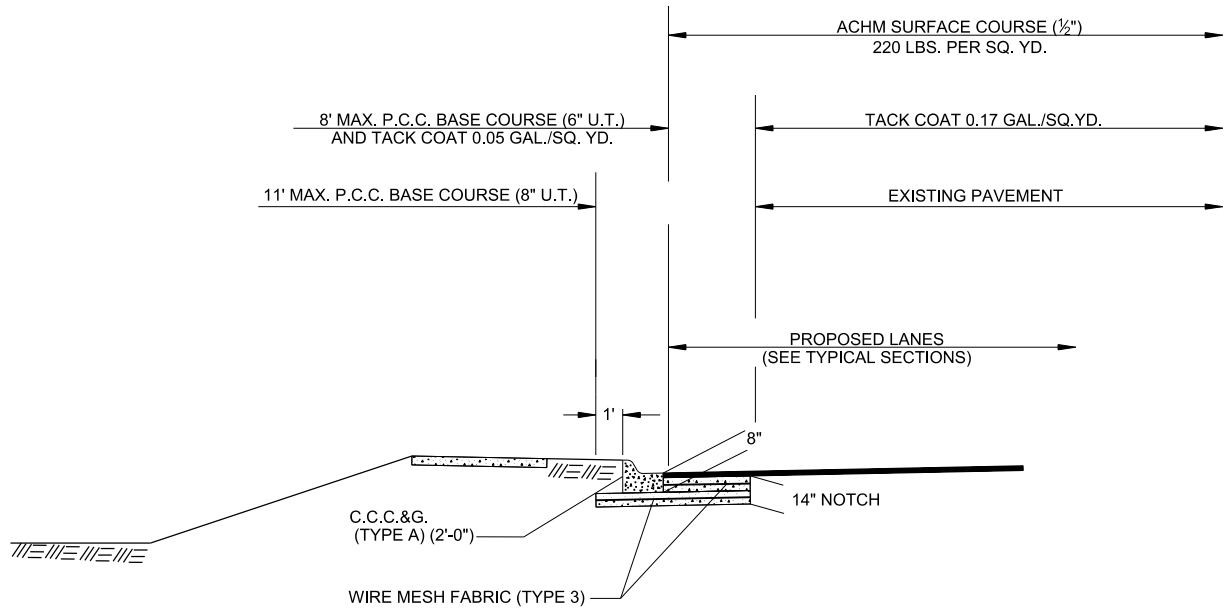
PIPE EXTENSION REINFORCED CONCRETE COLLAR DETAIL
 N.T.S.



6" X 12" MESH FABRIC (TYPE 3) (W5.5 X W2.9) = 4.26 LBS./SQ. YD.

NOTES:

- LAP MESH FABRIC MIN. 12" LONGITUDINALLY AND MIN. 6" TRANSVERSELY.
- MESH FABRIC IS NOT REQUIRED WHEN WIDTH OF PORTLAND CEMENT CONCRETE BASE IS LESS THAN 12".
- MESH FABRIC (TYPE 3) WILL NOT BE PAID FOR DIRECTLY, BUT FULL COMPENSATION THEREFORE WILL BE CONSIDERED INCLUDED IN THE CONTRACT PRICE BID PER SQ. YD. FOR PORTLAND CEMENT CONCRETE BASE (6" U.T.) AND PORTLAND CEMENT CONCRETE BASE (8" U.T.)



P.C.C. BASE WIDENING DETAIL

P.C.C. BASE WIDENING TO BE USED IF AND WHERE DIRECTED BY THE ENGINEER.

REINFORCING STEEL FOR PAVEMENT (MESH FABRIC TYPE 3) DETAIL

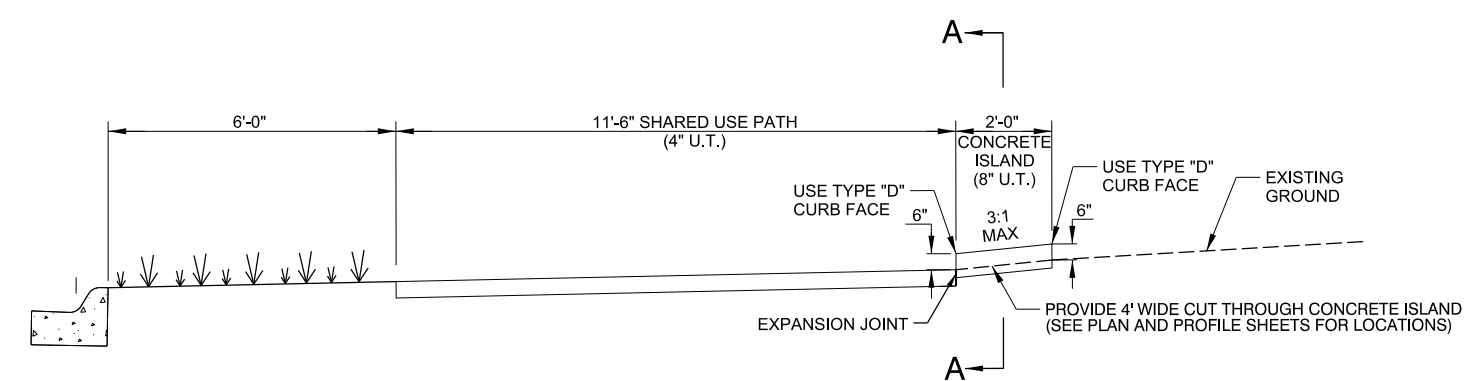
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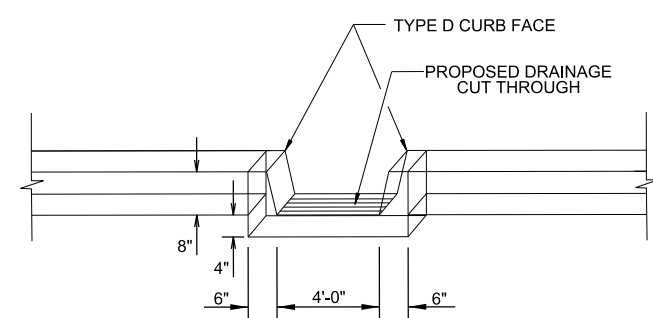


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NOTES:

1. SEE PLAN AND PROFILE SHEETS FOR ISLAND LOCATIONS.
2. THE EXISTING PAVEMENT SHALL BE SAWCUT ALONG A NEAT LINE AT THE BACK SIDE OF THE CONCRETE ISLAND.
3. A 2' RADIUS SHALL BE CONSTRUCTED AT ISLAND TERMINATION POINTS WITH DRIVEWAYS AND STRUCTURES AS SHOWN IN THE PLANS.



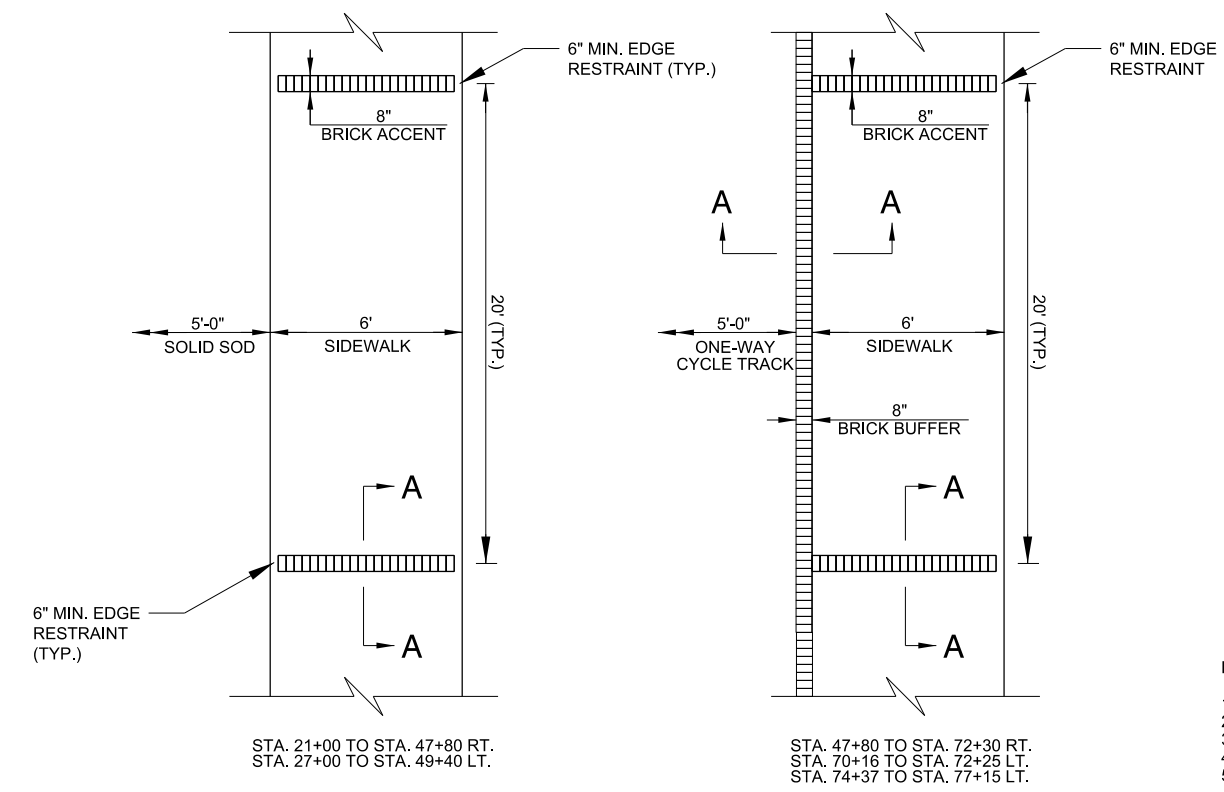
NOTE:

DRAINAGE CUT THROUGH ISLAND SHALL BE POURED MONOLITHICALLY. ALL MATERIALS REQUIRED TO CONSTRUCT DRAINAGE CUT THROUGH ISLAND SHALL BE INCLUDED IN THE PRICE BID FOR "CONCRETE ISLAND BEHIND WALK (8")"

SECTION A-A

CURBED ISLAND BEHIND WALK DETAIL

N.T.S.

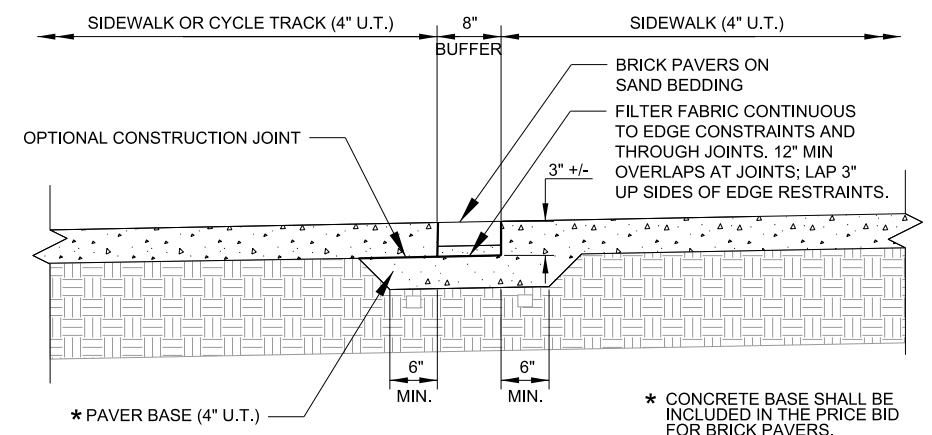


STA. 21+00 TO STA. 47+80 RT.
 STA. 27+00 TO STA. 49+40 LT.

STA. 47+80 TO STA. 72+30 RT.
 STA. 70+16 TO STA. 72+25 LT.
 STA. 74+37 TO STA. 77+15 LT.

BRICK PAVER ACCENTS

N.T.S.



SECTION A-A

NOTES:

1. PAVERS TO BE SURROUNDED WITH 6" MIN. CONCRETE BAND FOR EDGE RESTRAINT.
2. JOINT SAND SHALL BE POLYMERIC.
3. PAVER SHALL BE PINE HALL ENGLISH EDGE FULL RANGE (4"x8"x2-1/4") OR APPROVED EQUAL.
4. SAND BEDDING FOR PAVERS TO BE MAX. 1" TO MIN. 1/2" THICK SAND.
5. COORDINATE LAYOUT OF TRANSVERSE BRICK ACCENT WITH ENGINEER.

REV.	DATE	DESCRIPTION	BY



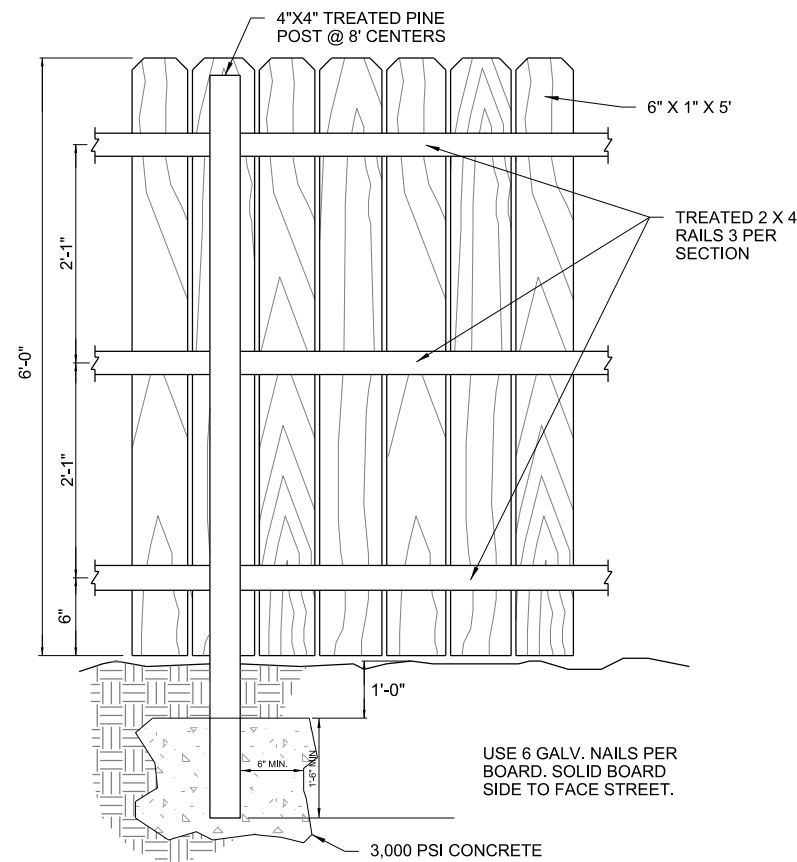
CITY OF CONWAY
 CONWAY, ARKANSAS
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 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

MISCELLANEOUS
 DETAILS
 (SHEET 2 OF 10)

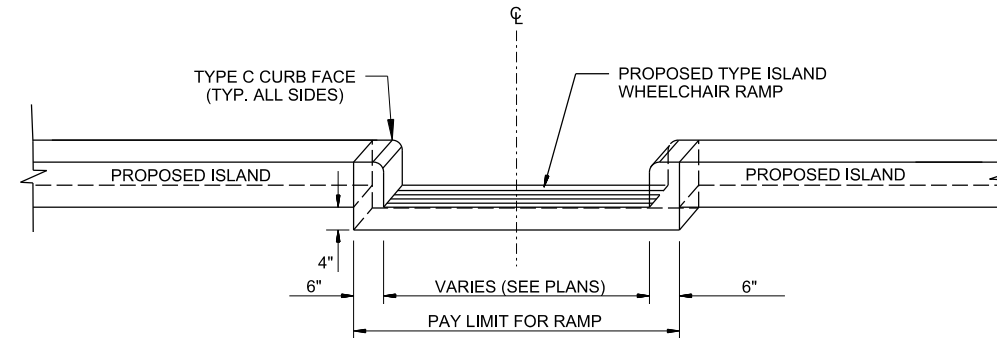
JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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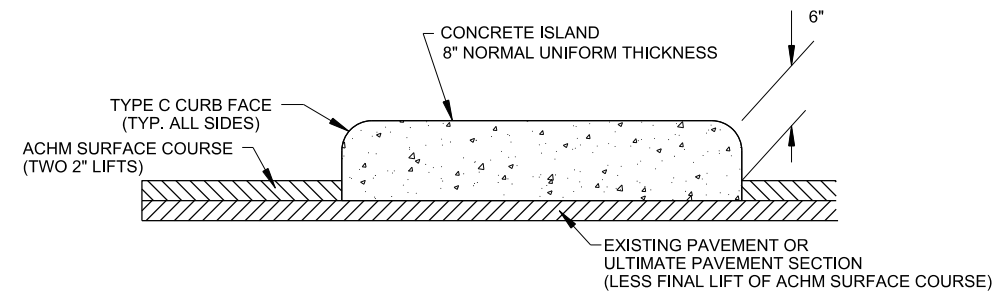
DRAWING NUMBER
C-202
 SHEET NUMBER
12



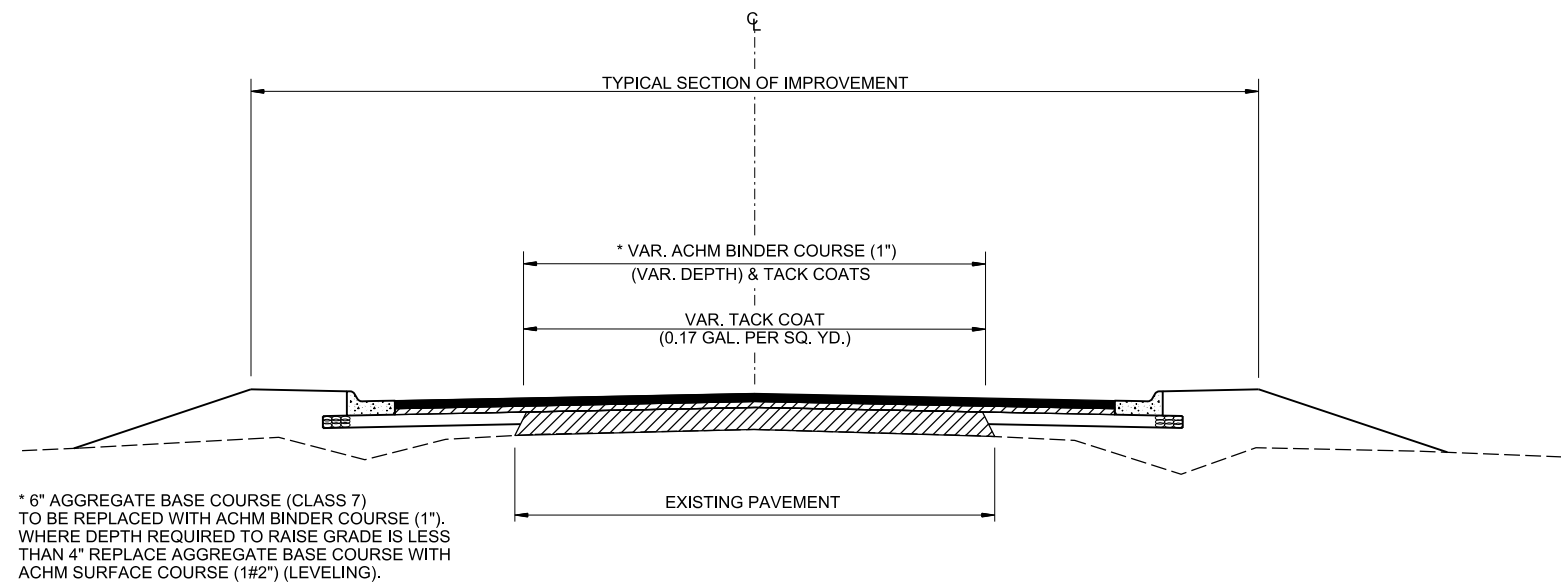
WOOD PRIVACY FENCE



NOTE: CONCRETE WALK THROUGH ISLAND SHALL BE POURED MONOLITHICALLY. ALL MATERIALS, INCLUDING TRUNCATED DOME PAVERS, REQUIRED TO CONSTRUCT CONCRETE WALK THRU ISLAND SHALL BE INCLUDED IN THE PRICE BID FOR RAMPS (TYPE ISLAND).



CONCRETE ISLAND DETAIL



NOTE:
THIS DETAIL TO BE USED ONLY IF AND WHERE DIRECTED BY THE ENGINEER FOR THE COLLEGE ROUNDABOUT APPROACHES AND CIRCULATORY ROADWAY.

METHOD OF RAISING GRADE



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

MISCELLANEOUS
DETAILS
(SHEET 3 OF 10)

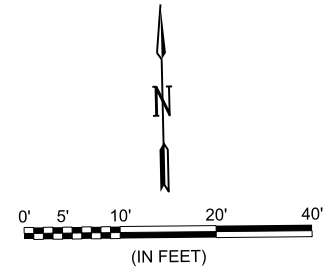
JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
C-203
SHEET NUMBER **13**

*SHORING SHALL BE CONSIDERED SUBSIDIARY TO THE PAY THE 12' x 5' REINFORCED CONCRETE BOX CULVERT.

BOX CULVERT COORDINATES			
STATION	TYPE	NORTHING	EASTING
10+00.00	POB	271159.55	1176385.74
10+65.22	PI	271167.27	1176450.51
11+22.86	PI	271165.55	1176508.11
11+54.12	POE	271179.50	1176536.09



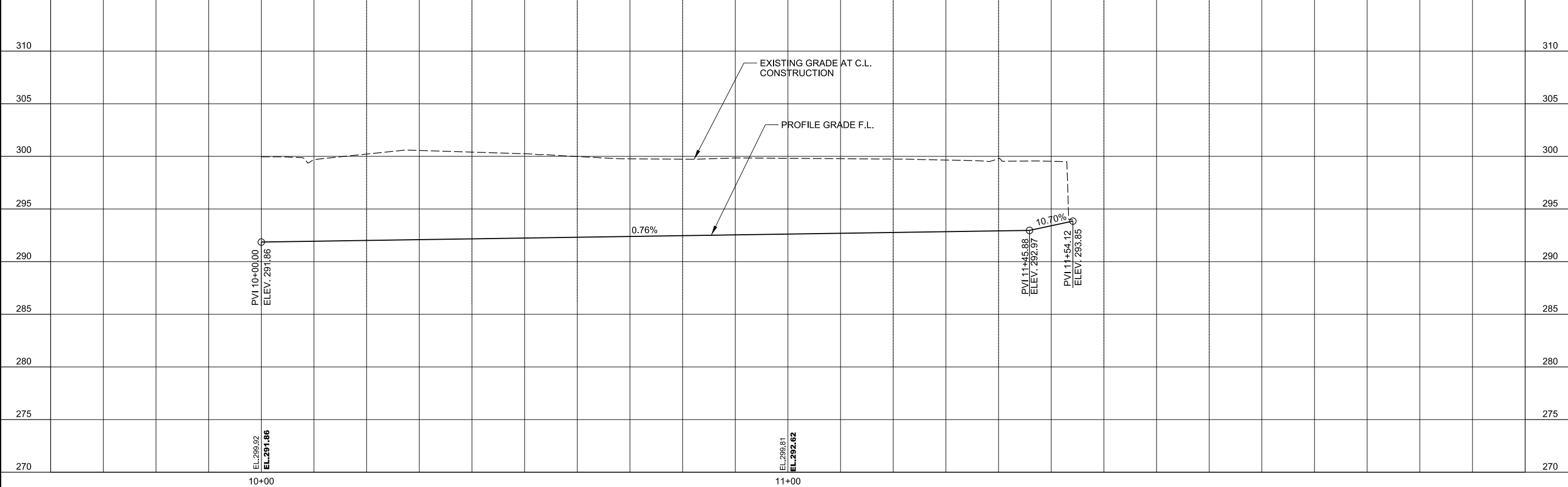
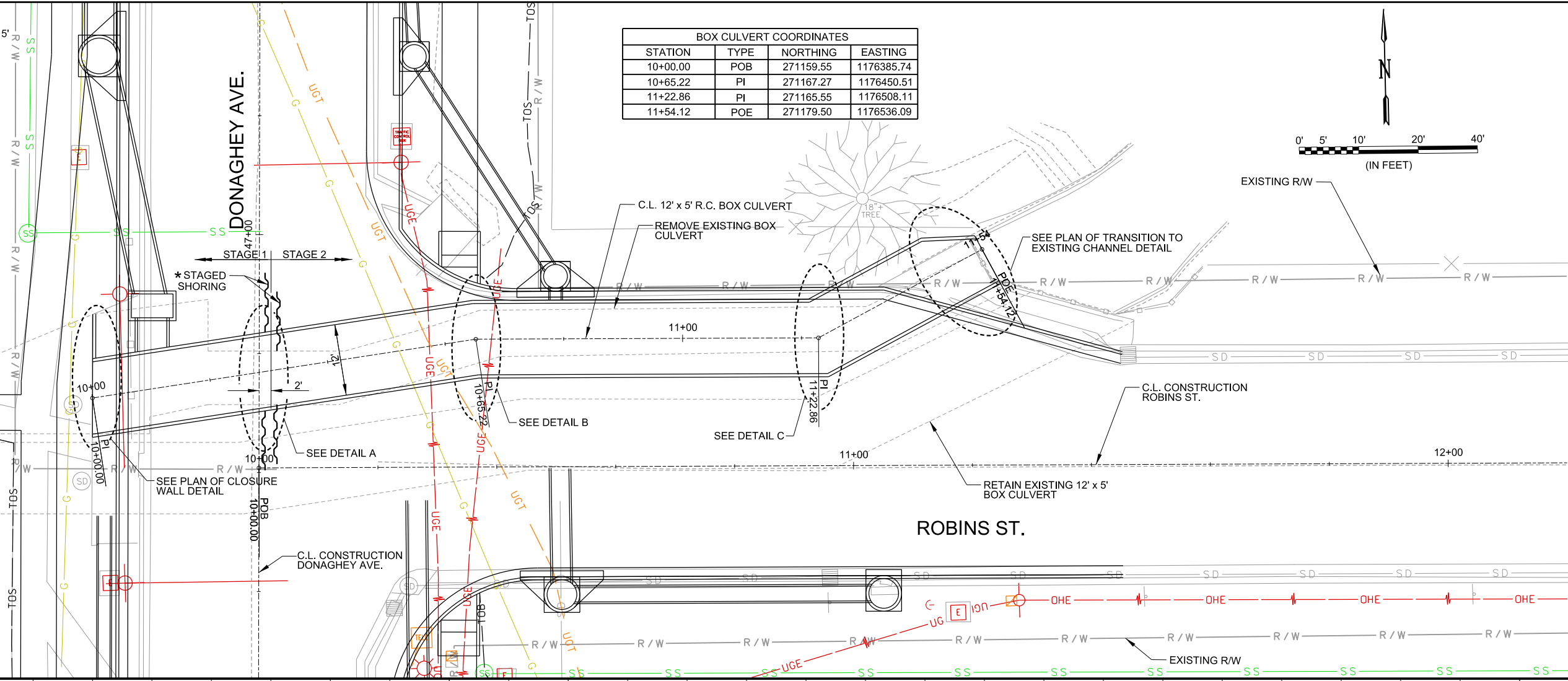
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STATE OF ARKANSAS
 GARVER LLC
 No. 766
 ARKANSAS-ENGINEER

STATE OF ARKANSAS
 Justin L. Tackett
 LICENSED PROFESSIONAL ENGINEER
 No. 14994
 JUSTIN L. TACKETT
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SEE GENERAL NOTE 1, DRAWING G-002 OF THESE PLANS.



REV.	DATE	DESCRIPTION	BY

CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

MISCELLANEOUS
 DETAILS
 SHEET (4 OF 9)

JOB NO.: 18147010
 DATE: MAY 2020
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C-204

SHEET NUMBER
14



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

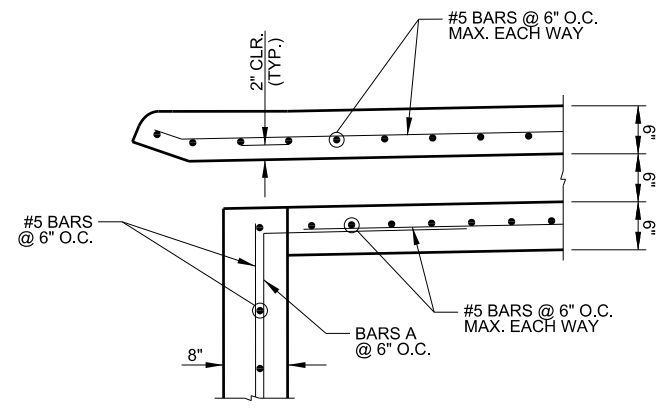
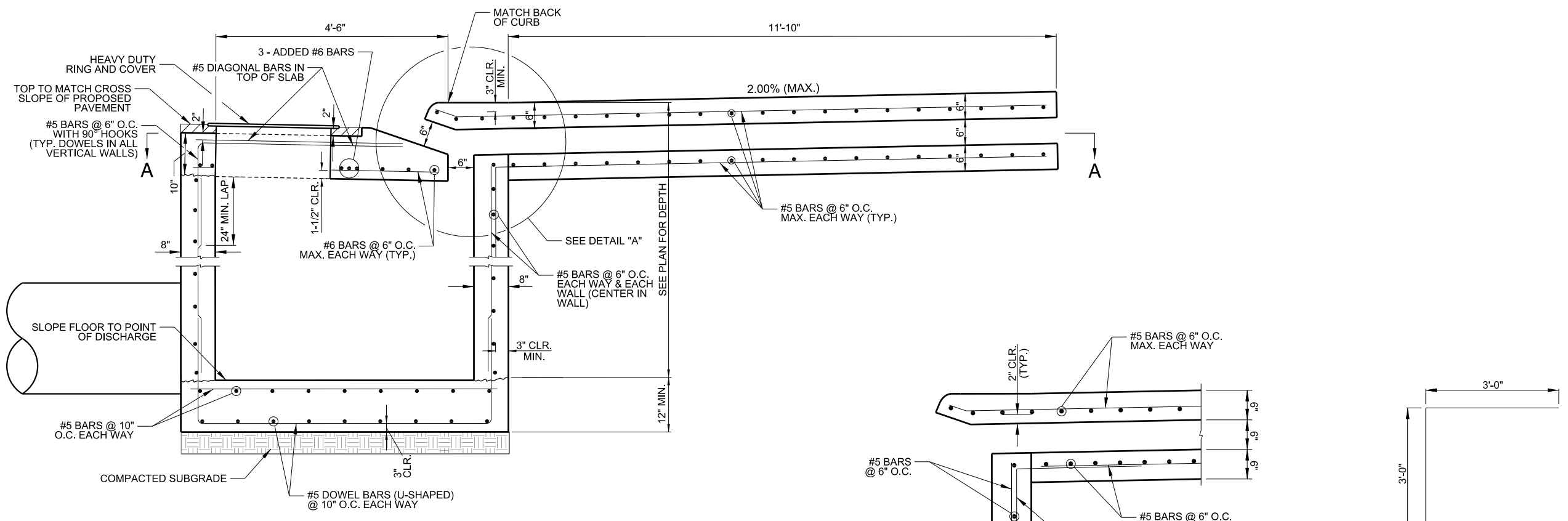
MISCELLANEOUS
 DETAILS
 (SHEET 5 OF 10)

JOB NO.: 18147010
 DATE: MAY 2020
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 DRAWN BY: HJB

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C-205

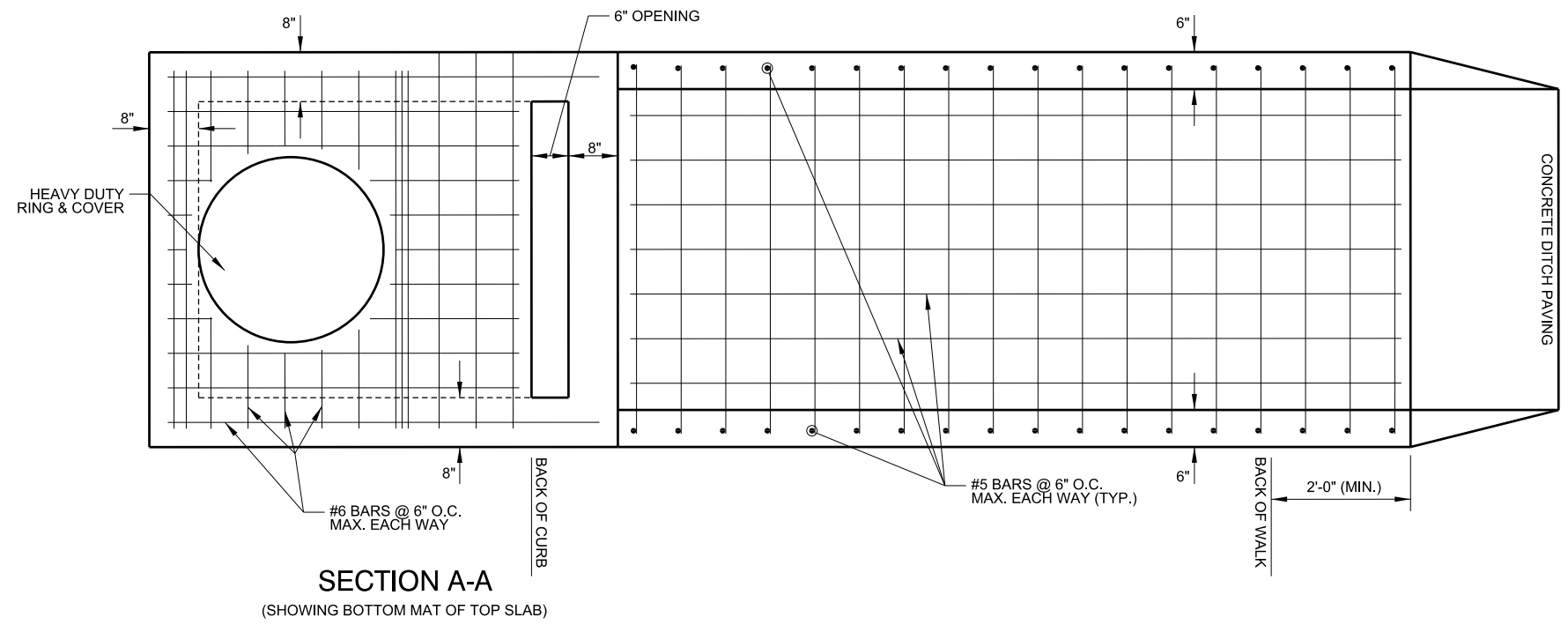
SHEET NUMBER
15



DETAIL "A"
 N.T.S.

- NOTE:
1. HEAVY DUTY RING AND COVER TO BE EAST JORDAN V-1600-2 & 1348A OR APPROVED EQUAL.
 2. ALL EXPOSED CORNERS SHALL HAVE 3/4" CHAMFER.
 3. ALL REINFORCING BARS SHALL BE GRADE 60.
 4. ALL WORK SHALL COMPLY WITH SECTION 609 OF THE ARDOT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.

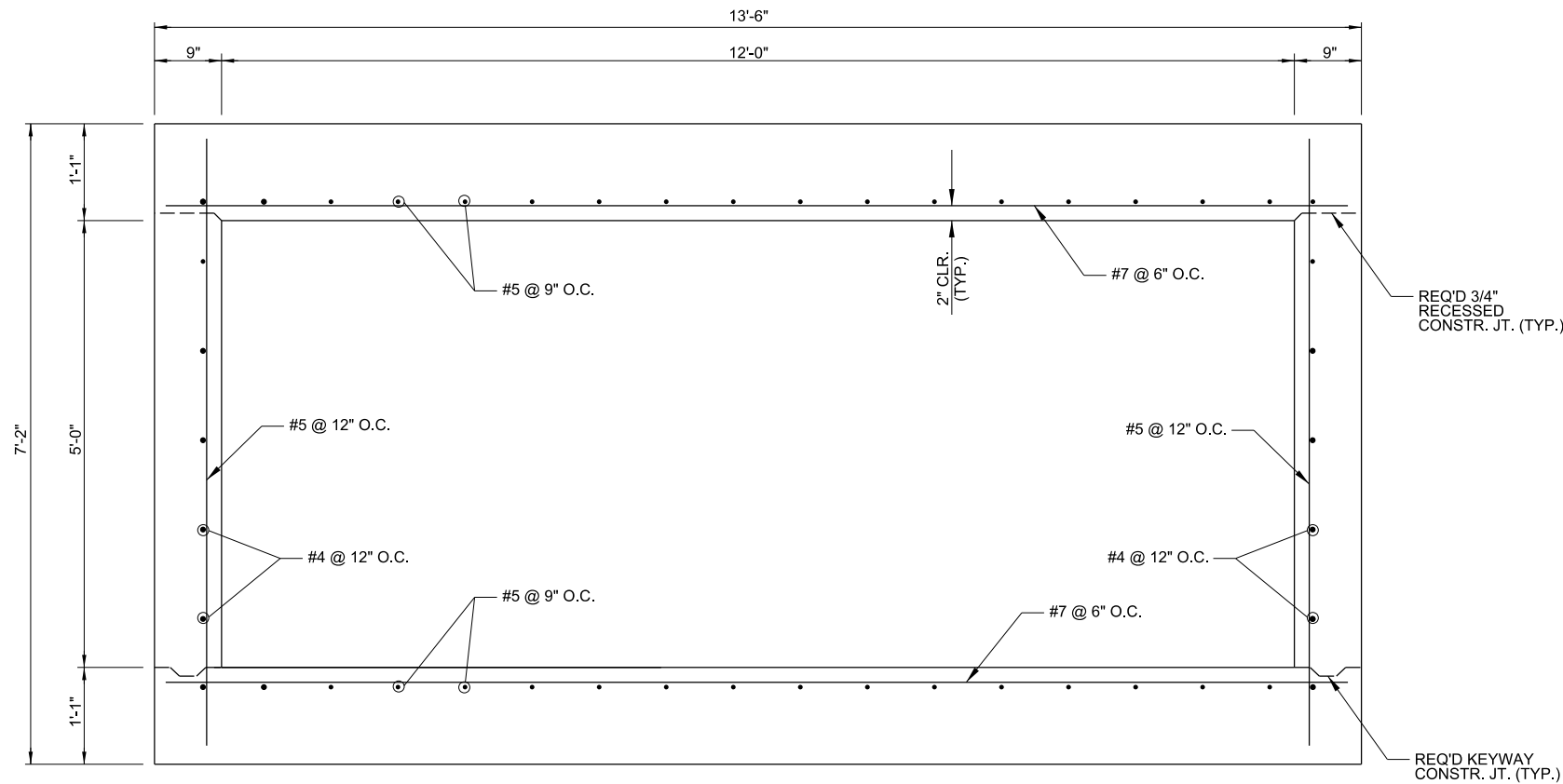
REVERSE THROAT CURB INLET
 N.T.S.



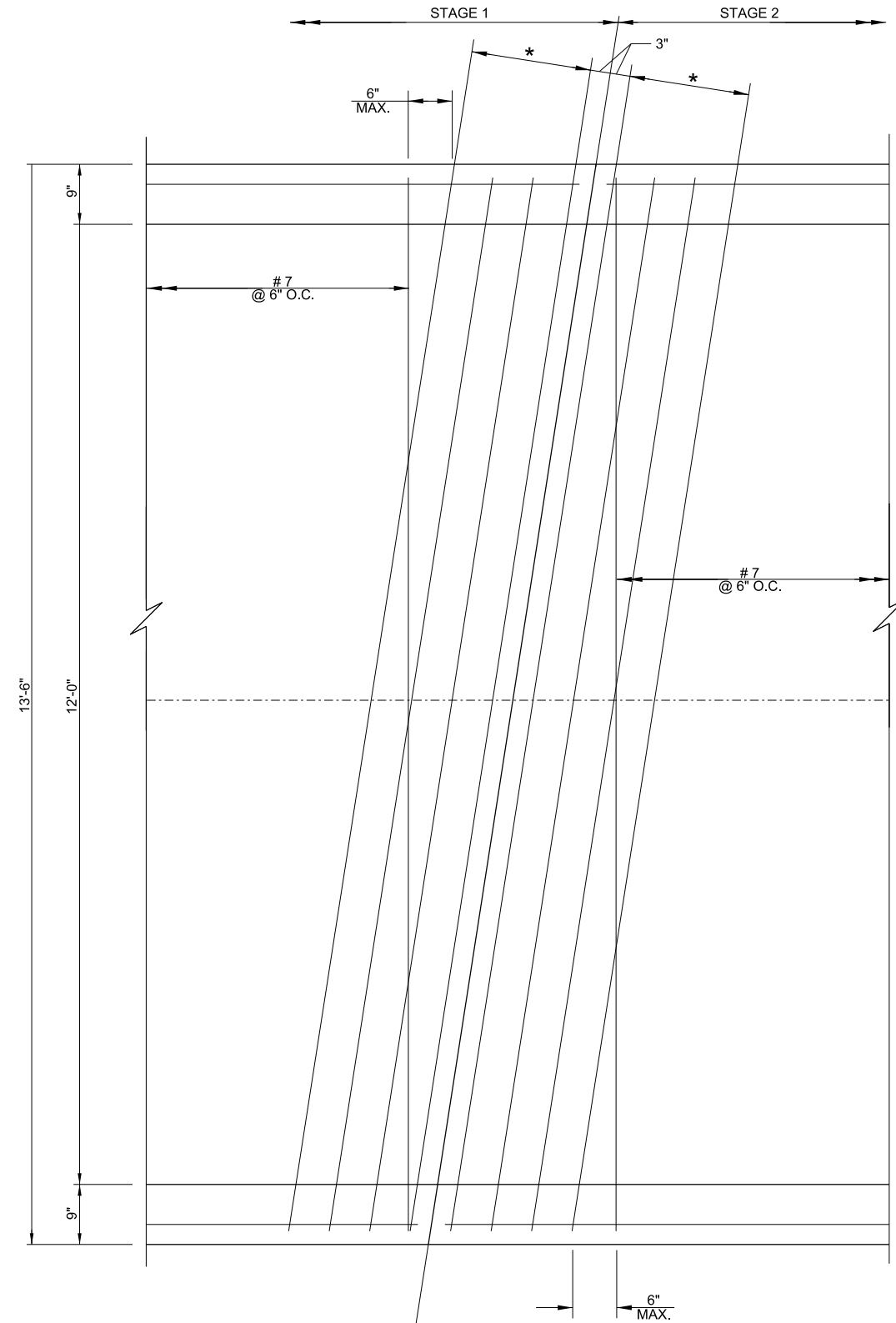
SECTION A-A
 (SHOWING BOTTOM MAT OF TOP SLAB)

GENERAL NOTES:

1. ALL CONSTRUCTION AND MATERIAL REQUIREMENTS SHALL BE IN ACCORDANCE WITH THE CITY OF CONWAY CONTRACT CONDITIONS AND SPECIFICATIONS.
2. CONCRETE: CONCRETE SHALL BE POURED IN THE DRY AND ALL EXPOSED CORNERS TO BE CHAMFERED 3/4". ALL CONCRETE SHALL BE CLASS S WITH A MINIMUM 28 DAY COMPRESSION STRENGTH F'C = 3,500 PSI.
3. REINFORCING STEEL: ALL REINFORCING STEEL SHALL CONFORM TO AASHTO M31 OR M53, GRADE 60 AND HAVE A 2" MINIMUM CLEARANCE UNLESS OTHERWISE NOTED ON PLANS.
4. REINFORCING STEEL IN BOTTOM SLAB SHALL BE SUPPORTED ON BAR CHAIRS. CHAIRS SHALL BE SUPPORTED ON TIMBER PLANKS OR CLASS C CONCRETE STRIPS SPACED AT 4.0 FEET CENTERS. THE TOP SUPPORTS SHALL BE AT THE ELEVATION OF THE BOTTOM OF THE FOOTING.
5. REINFORCING STEEL IN THE TOP SLAB SHALL BE SUPPORTED ON SLAB SPACERS.
6. REINFORCING STEEL IN THE WALLS SHALL BE HELD IN PLACE BY METAL CHAIRS. MAXIMUM SPACING OF CHAIRS SHALL BE ON 6.0 FOOT CENTERS.
7. COST OF METAL CHAIRS, WOOD PLANKS OR CONCRETE STRIPS SHALL BE INCLUDED ON OTHER ITEMS OF WORK.
8. EXCAVATION AND BACKFILL REQUIRED TO CONSTRUCT THE BOX CULVERT WILL NOT BE MEASURED FOR SEPERATE PAYMENT, BUT WILL BE CONSIDERED SUBSIDIARY WORK PERTAINING TO THE CONSTRUCTION OF THE BOX CULVERT.



TYPICAL SECTION
N.T.S.



DETAIL "A"
N.T.S.



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CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

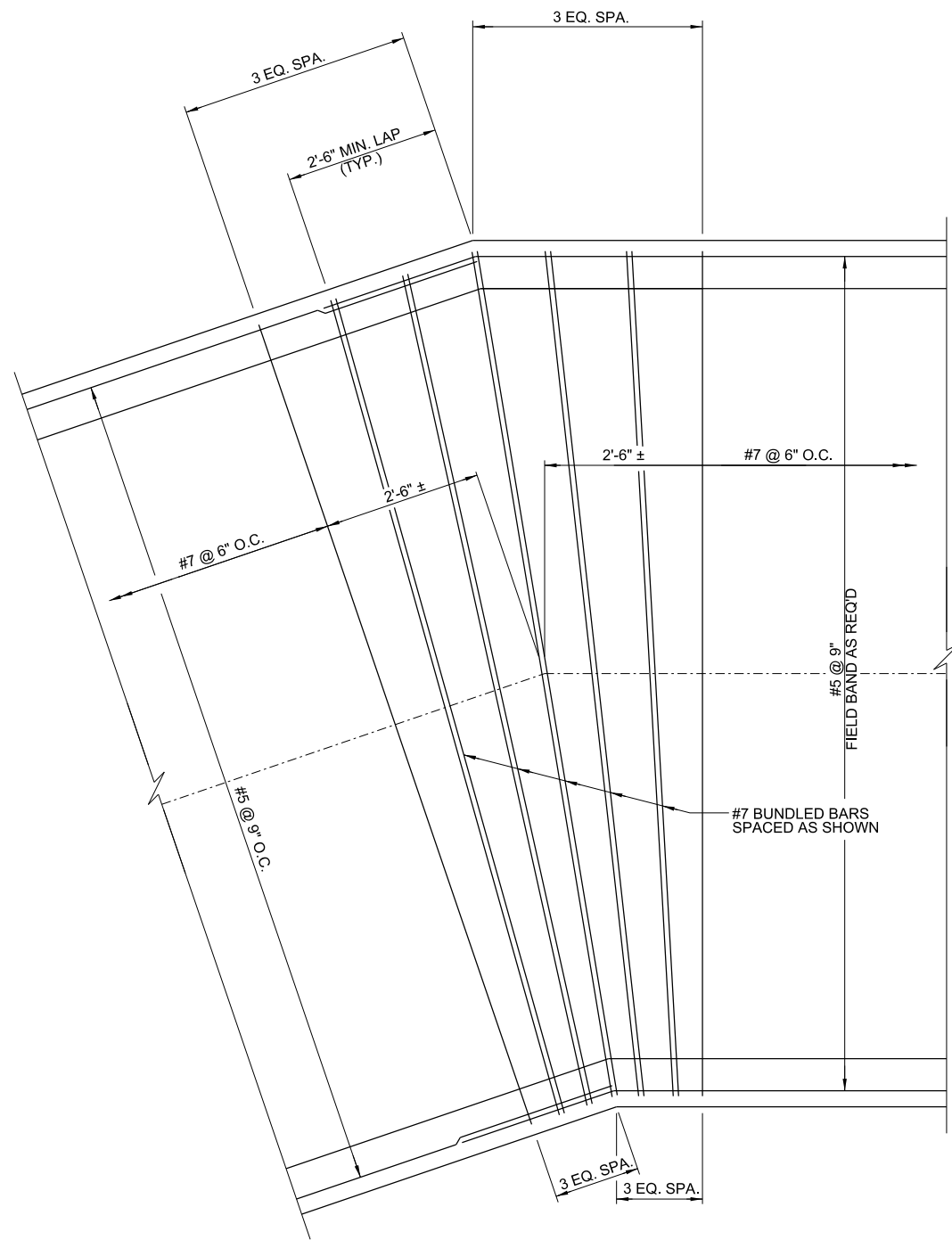
MISCELLANEOUS
DETAILS
(SHEET 6 OF 10)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: WMM
DRAWN BY: HJB

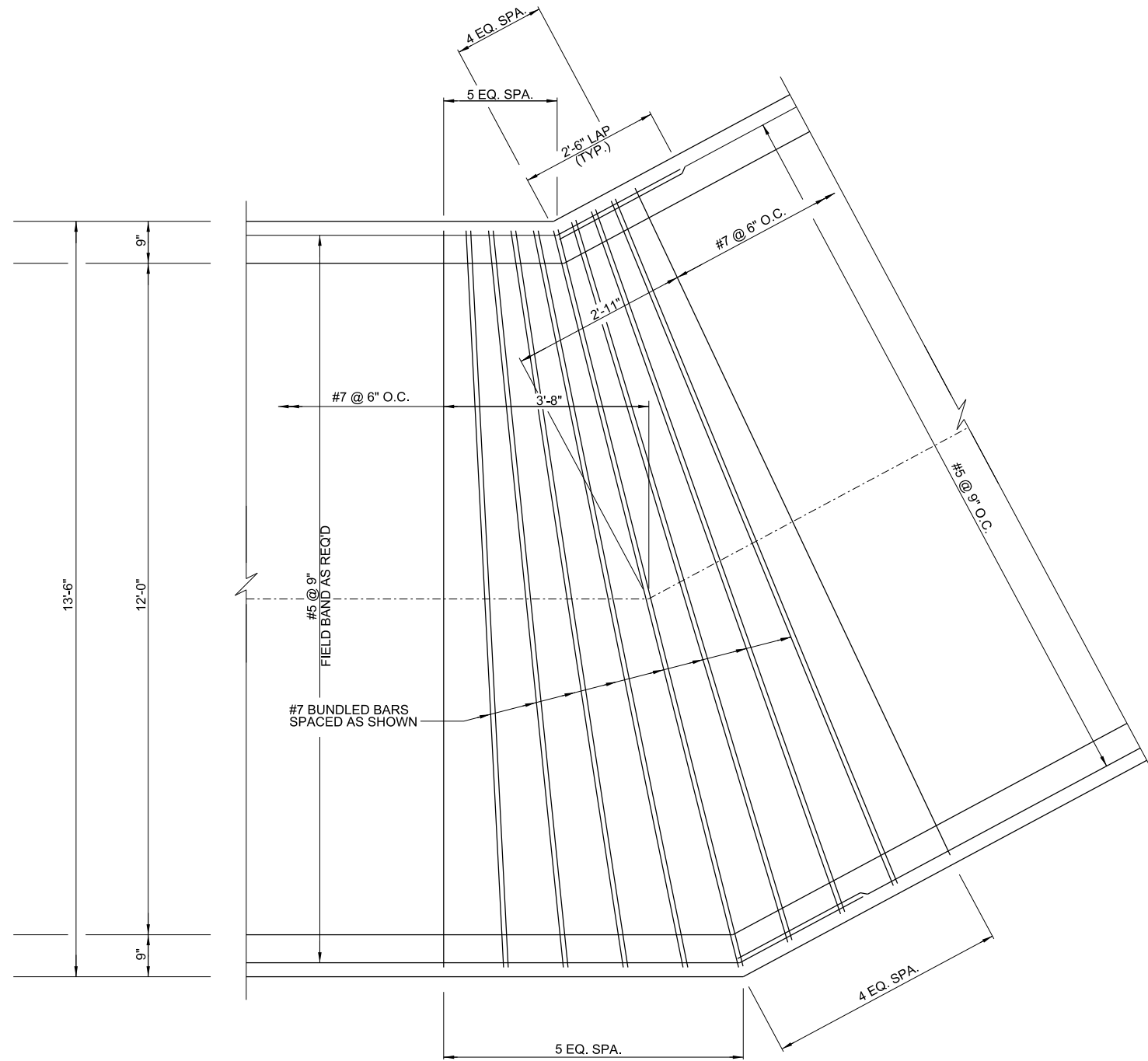
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DRAWING NUMBER
C-206

SHEET NUMBER
16



"DETAIL B"
 N.T.S.



"DETAIL C"
 N.T.S.



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

MISCELLANEOUS
 DETAILS
 (SHEET 7 OF 10)

JOB NO.: 18147010
 DATE: MAY 2020
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DRAWING NUMBER
C-207

SHEET NUMBER
17



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CITY OF CONWAY
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 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
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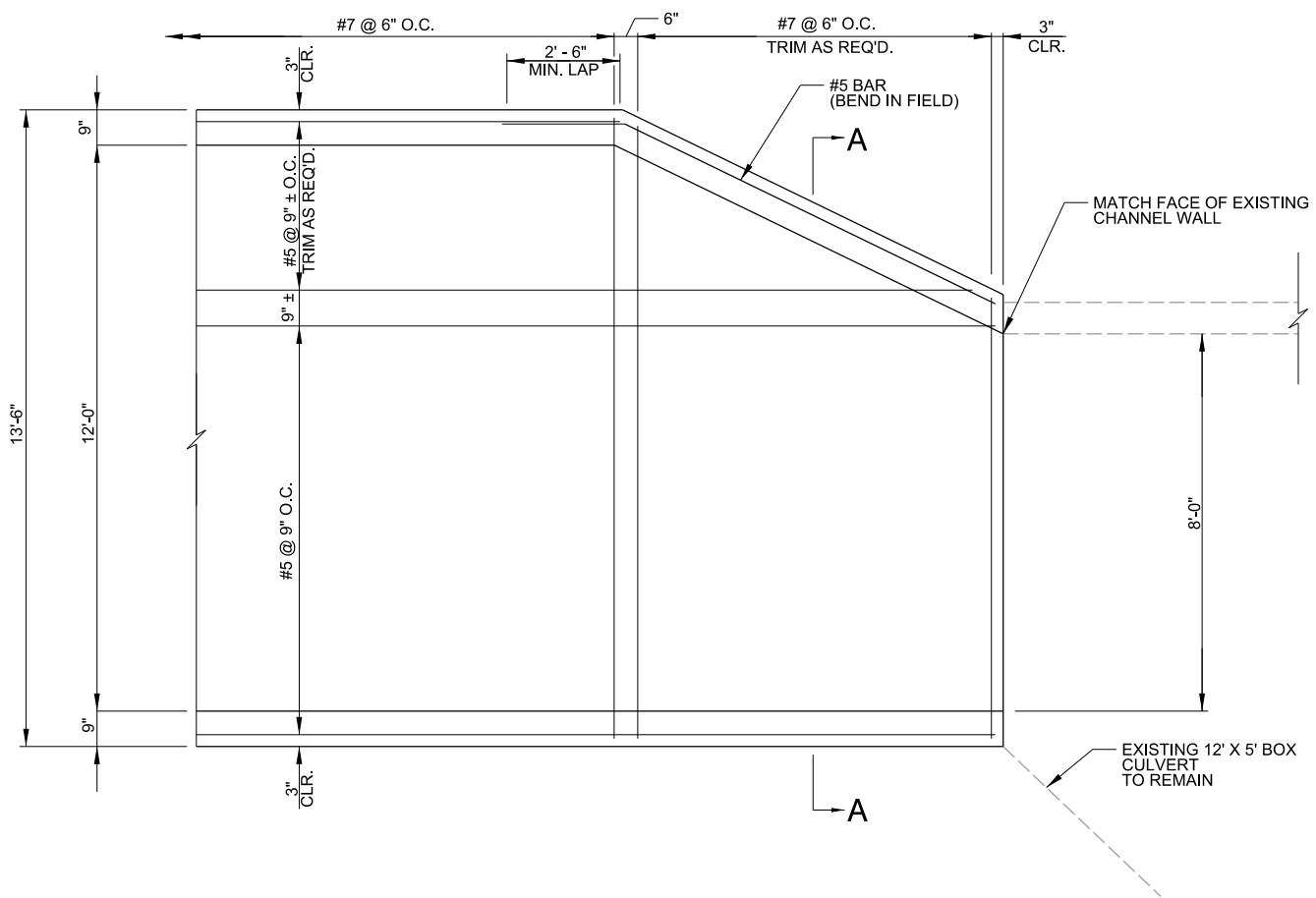
MISCELLANEOUS
 DETAILS
 (SHEET 8 OF 10)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: WMM
 DRAWN BY: HJB

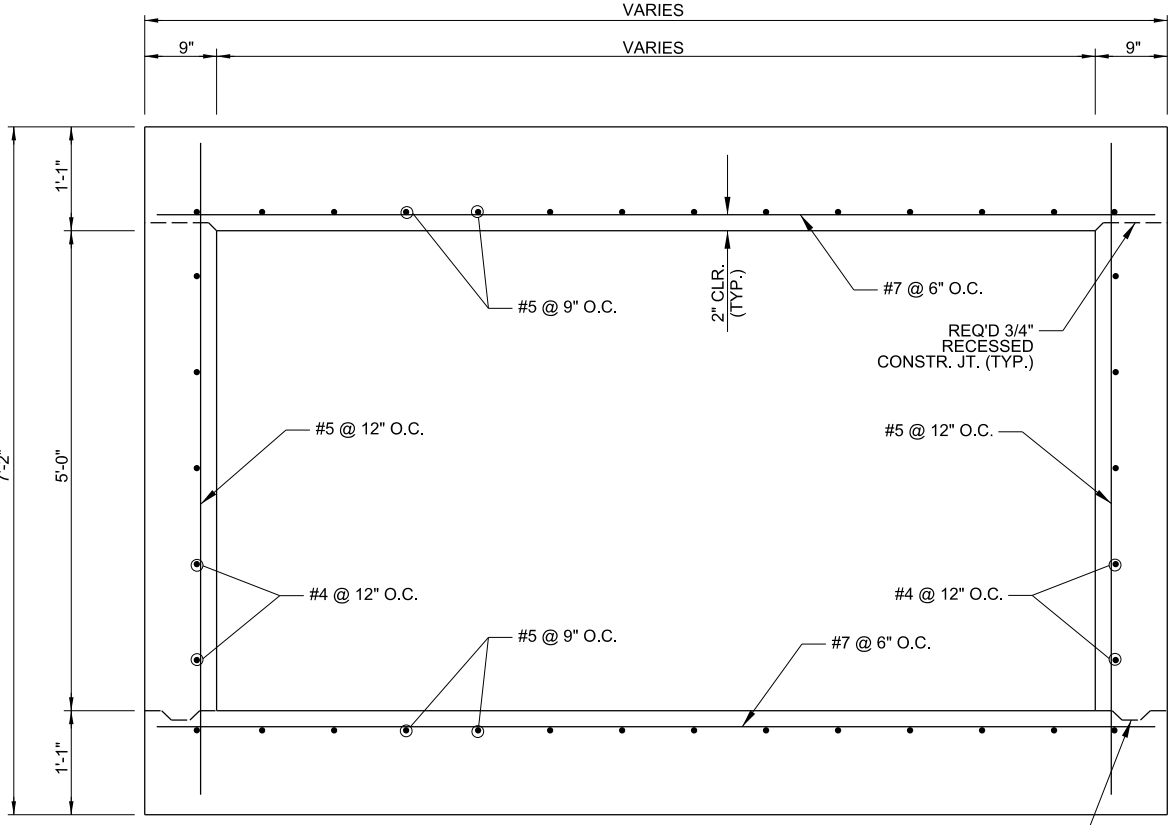
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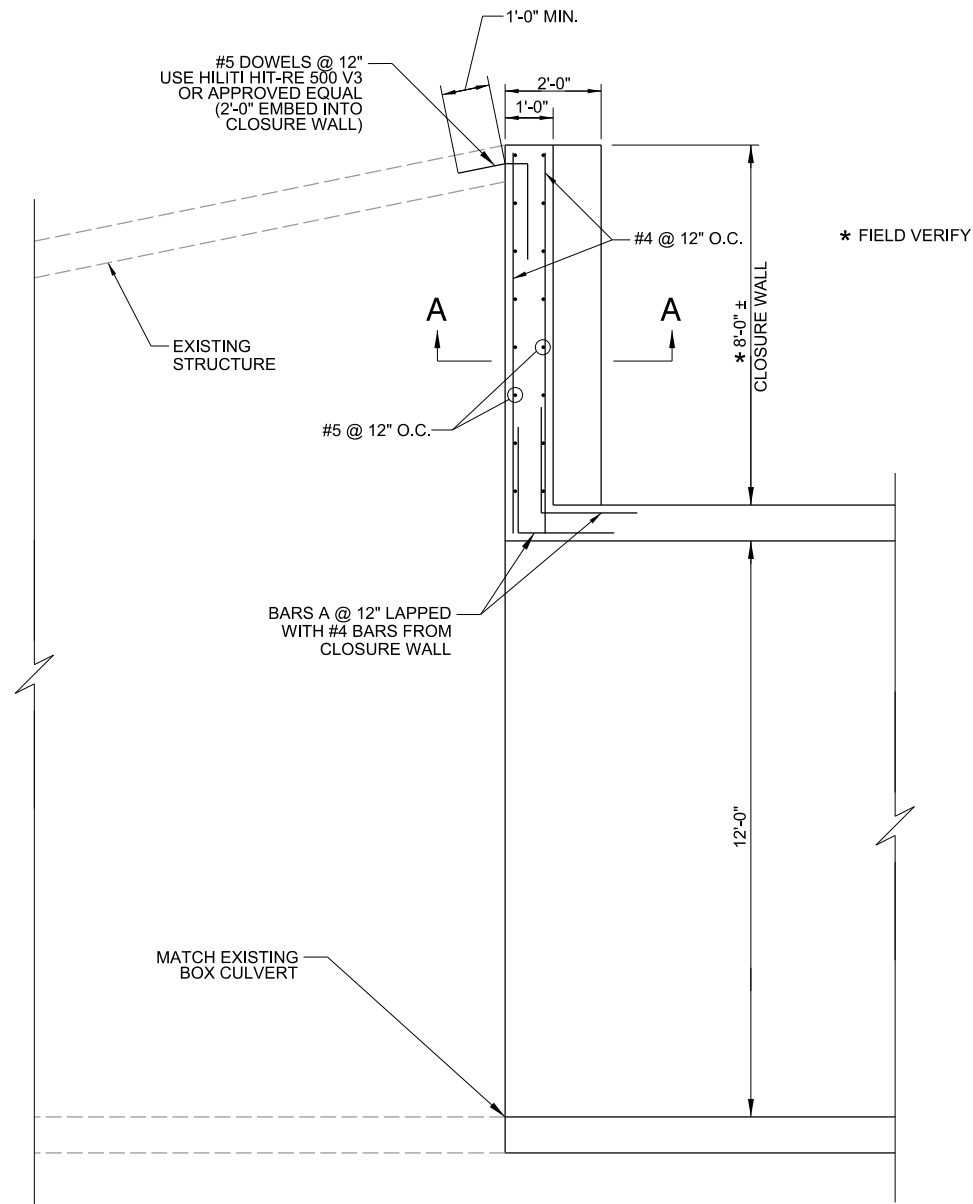


PLAN OF TRANSITION TO EXISTING CHANNEL
 N.T.S.

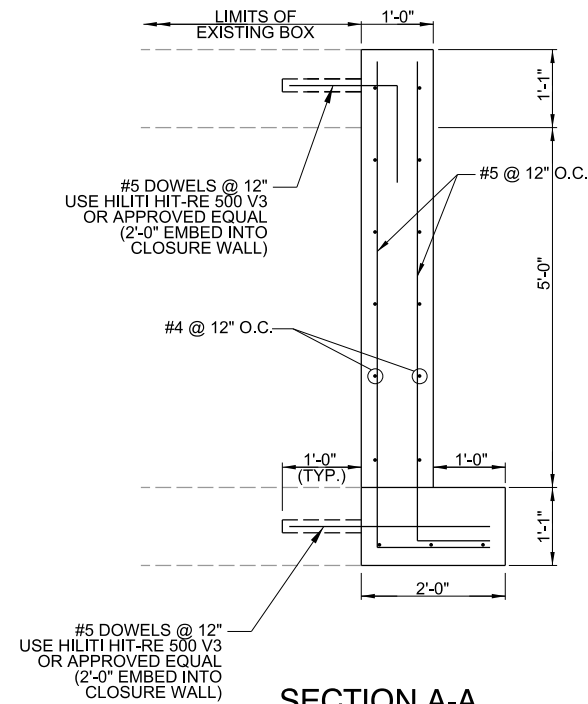


SECTION A-A
 N.T.S.

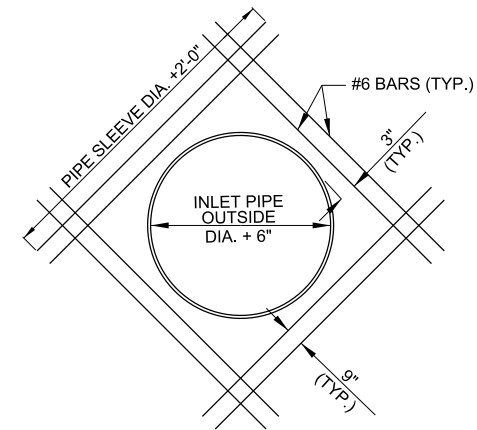
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 WORKSPACE\Garver_2012
 L:\2018\18147010 - Conway - Donaghey Avenue\Drawings\DAV\201-HJD.dgn



PLAN OF CLOSURE WALL
 N.T.S.



SECTION A-A
 N.T.S.



NOTE:
 SIDEWALL LONGITUDINAL AND TRANSVERSE REINFORCEMENT
 NOT SHOWN FOR CLARITY. ADJUST LONGITUDINAL AND
 TRANSVERSE REINFORCEMENT TO AVOID PIPE SLEEVE.

SIDEWALL REINFORCING DETAIL AT PIPE SLEEVE
 N.T.S.



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

MISCELLANEOUS
 DETAILS
 (SHEET 9 OF 10)

JOB NO.: 18147010
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SHEET
 NUMBER **19**



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CITY OF CONWAY
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 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
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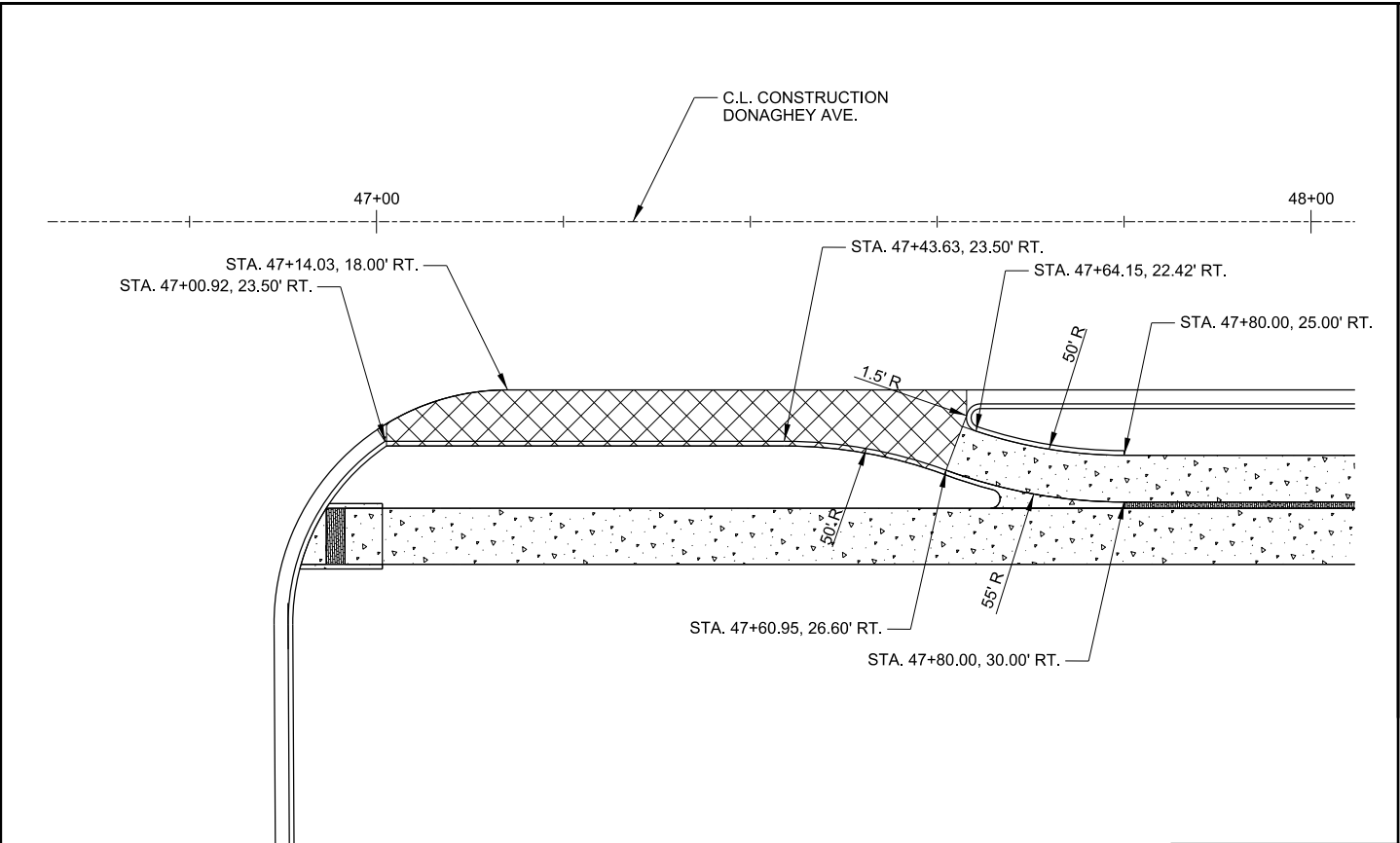
MISCELLANEOUS
 DETAILS
 (SHEET 10 OF 10)

JOB NO.: 18147010
 DATE: MAY 2020
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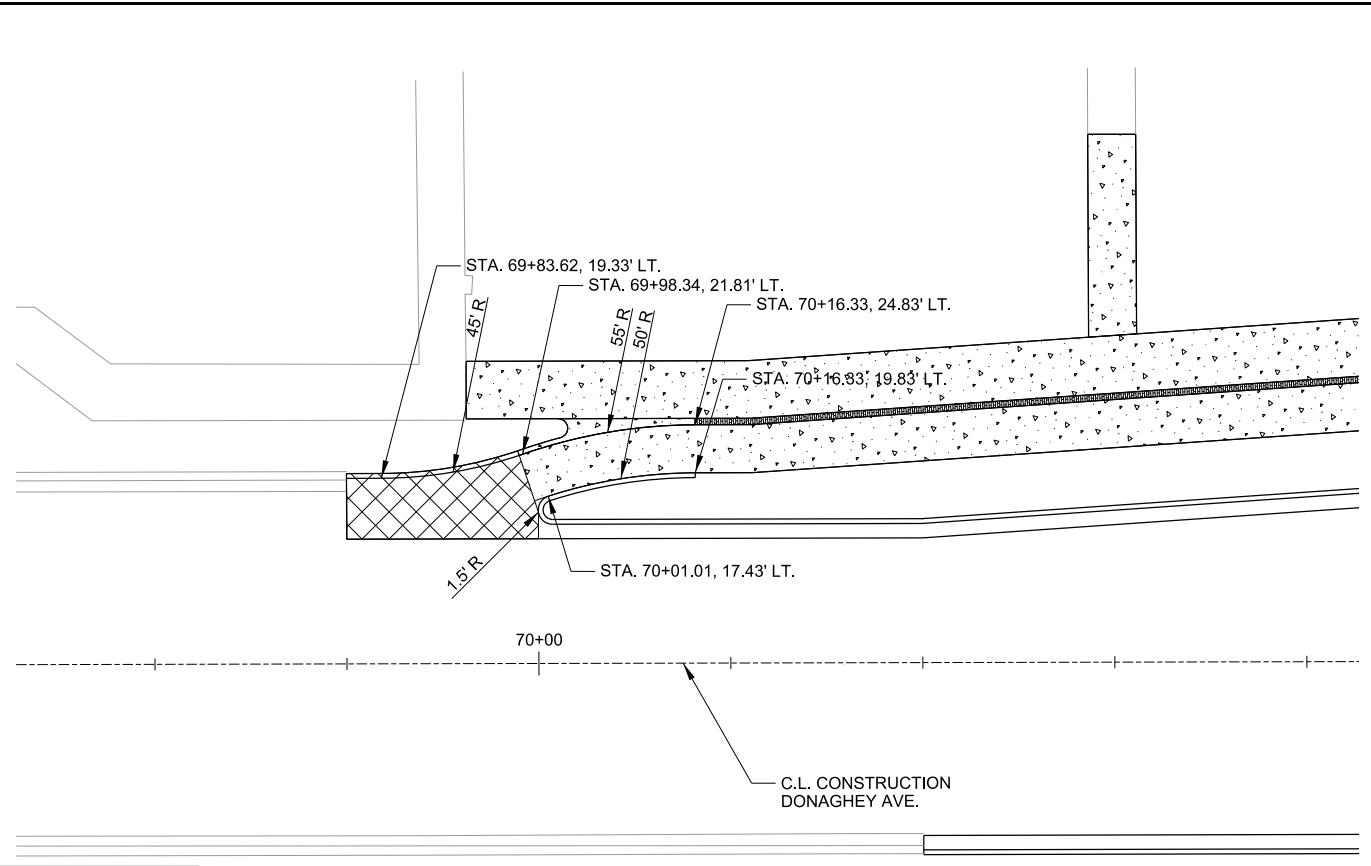
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C-210

SHEET NUMBER
20



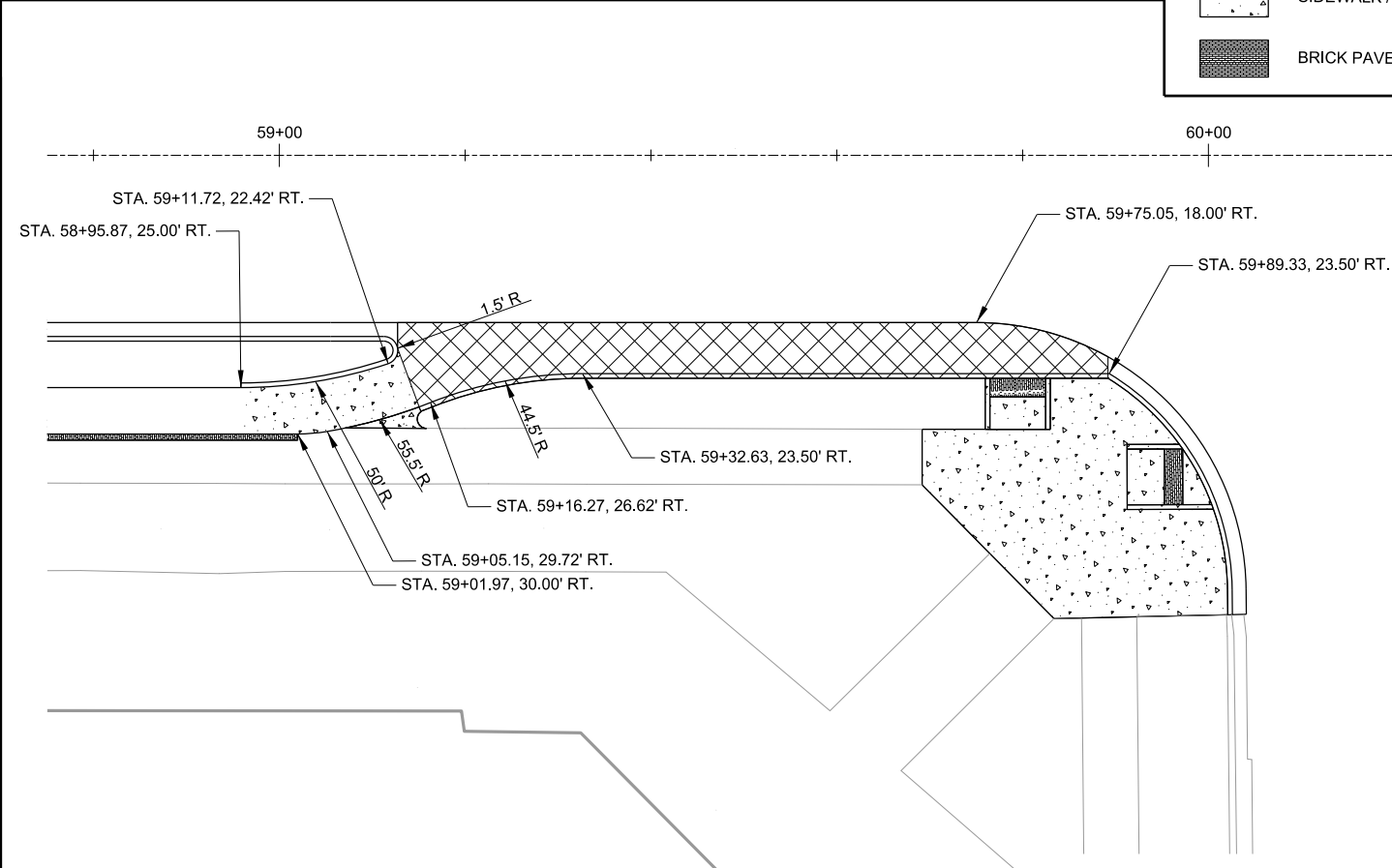
ROBINS ST. CYCLE TRACK TRANSITION DETAIL
 N.T.S.



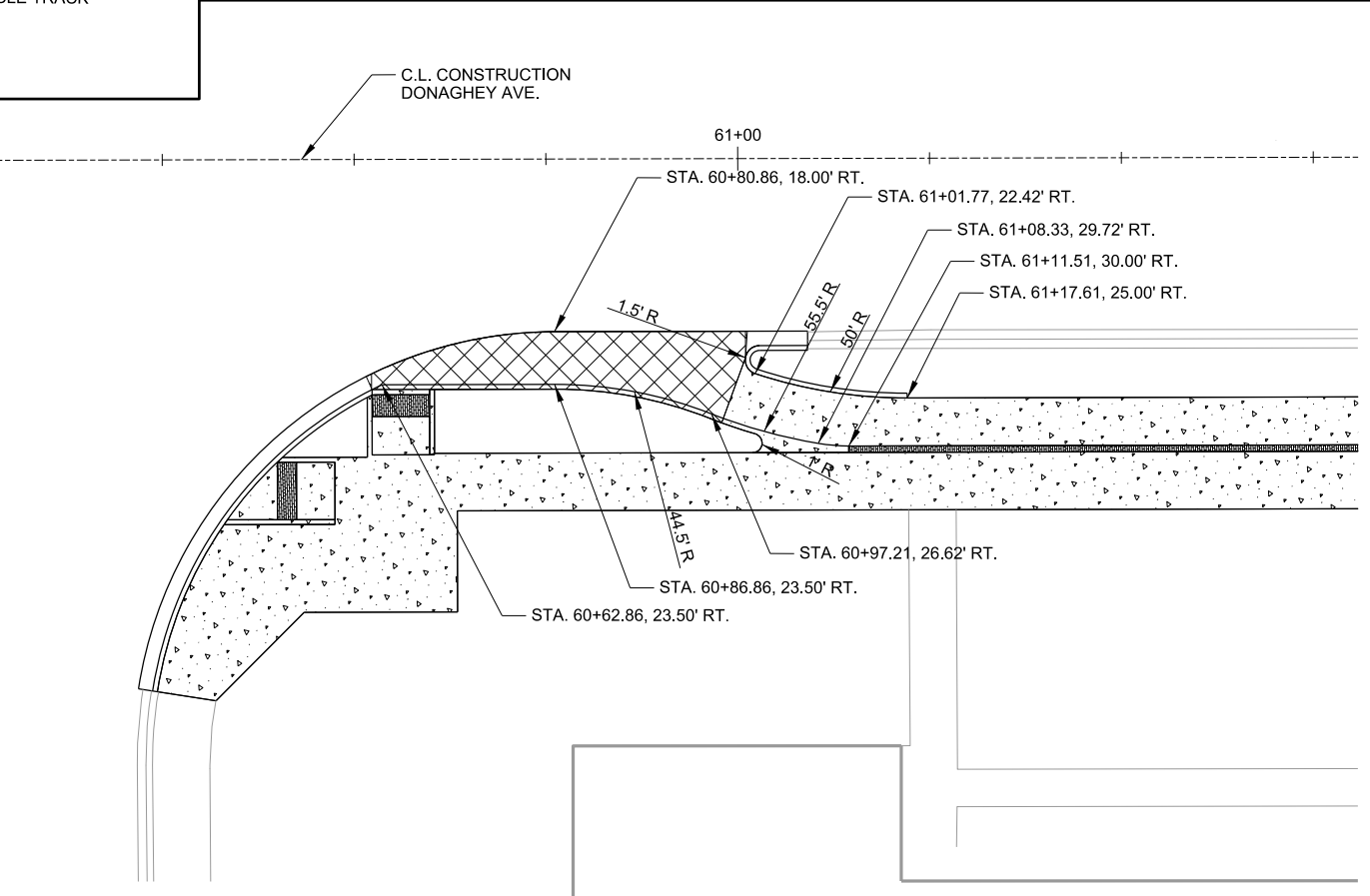
COLLEGE AVE. CYCLE TRACK TRANSITION DETAIL
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LEGEND

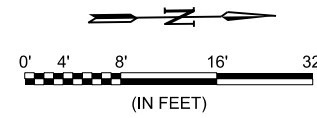
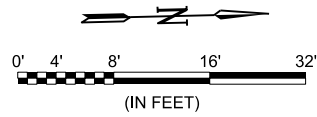
- CONCRETE COMBINATION CURB AND GUTTER TYPE A (6'-0")
- SIDEWALK / CYCLE TRACK
- BRICK PAVERS



BRUCE ST. CYCLE TRACK TRANSITION DETAIL
 N.T.S.



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CITY OF CONWAY
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 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

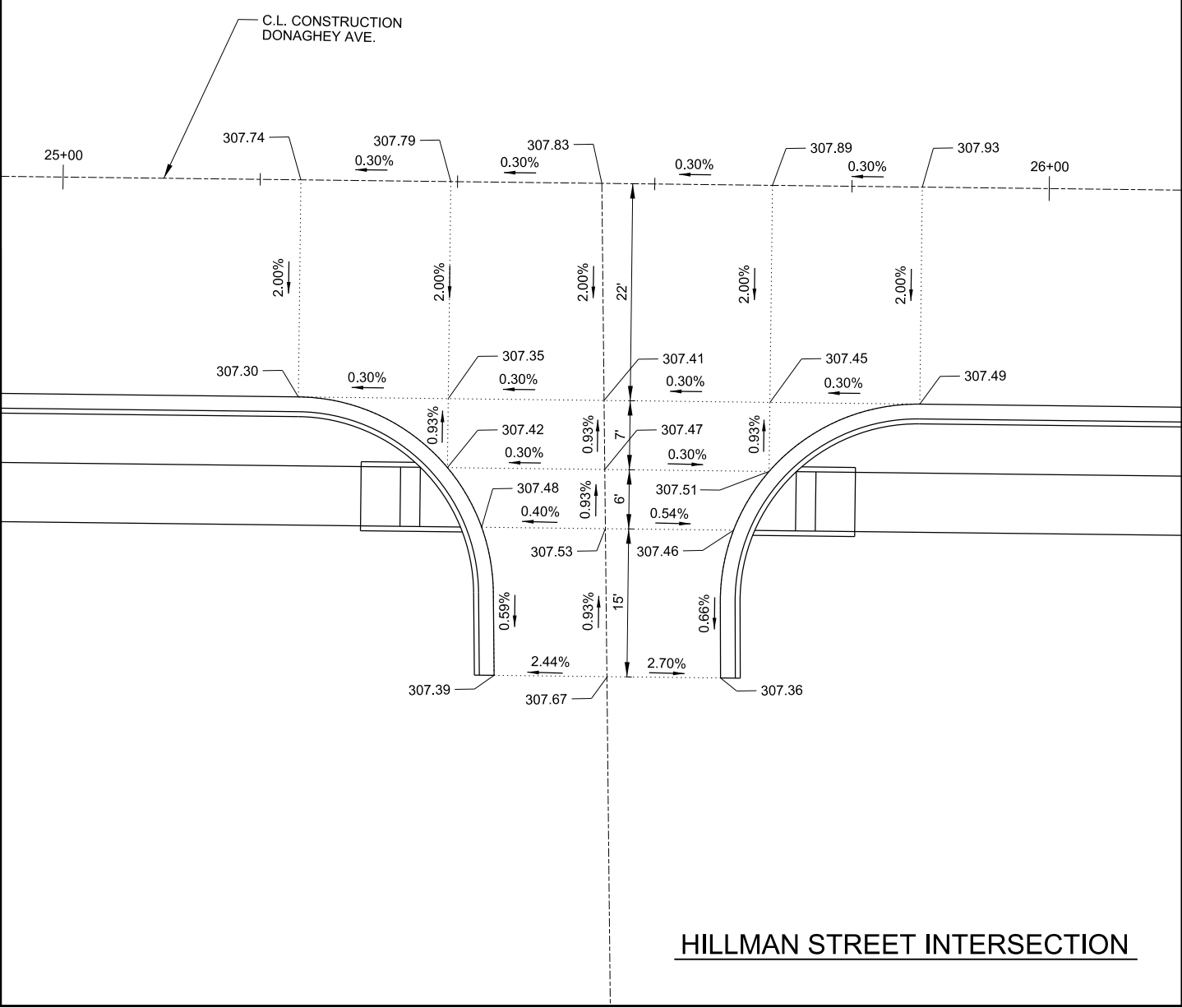
INTERSECTION
 DETAILS
 (SHEET 1 OF 5)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

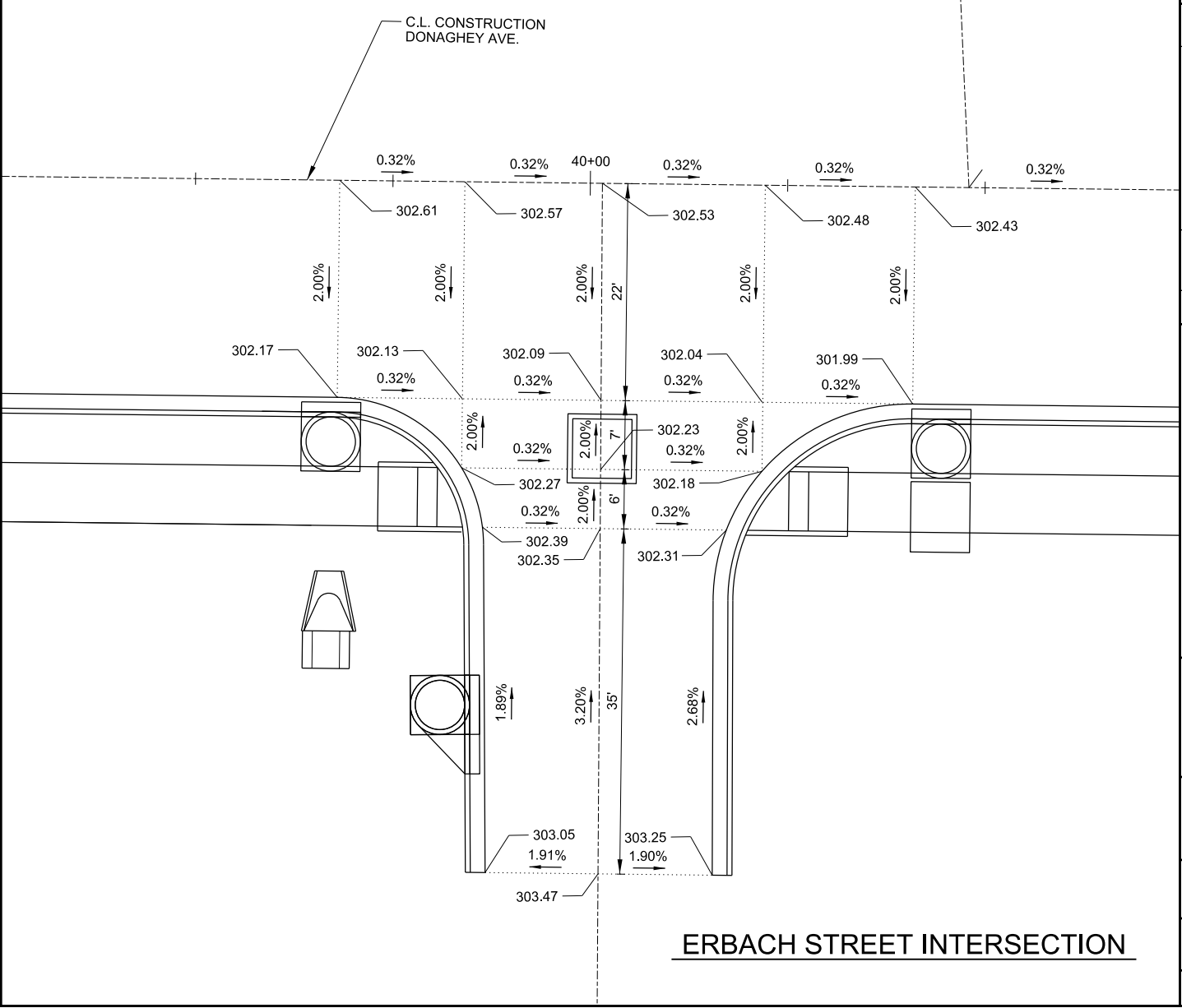
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C-211
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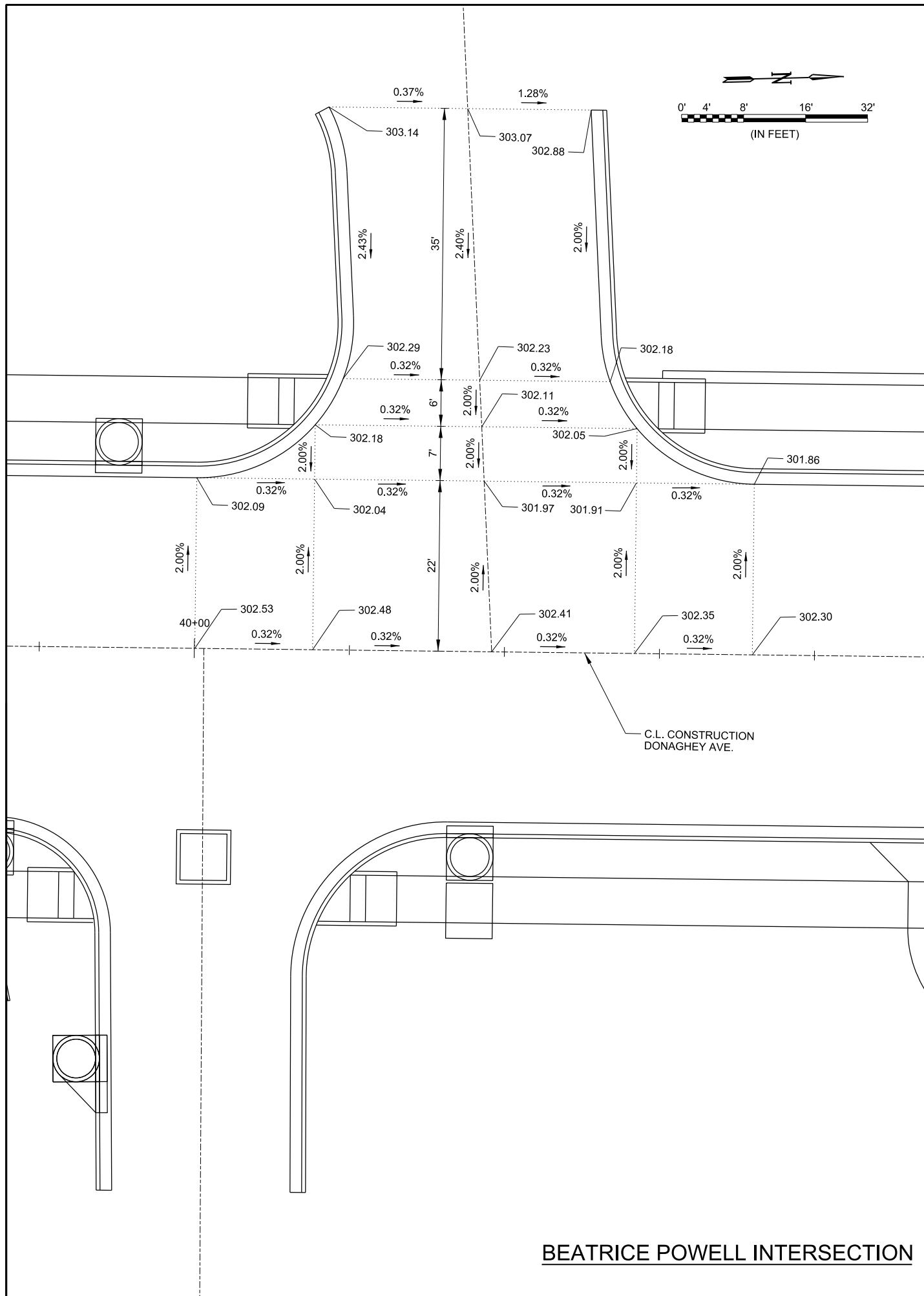


HILLMAN STREET INTERSECTION

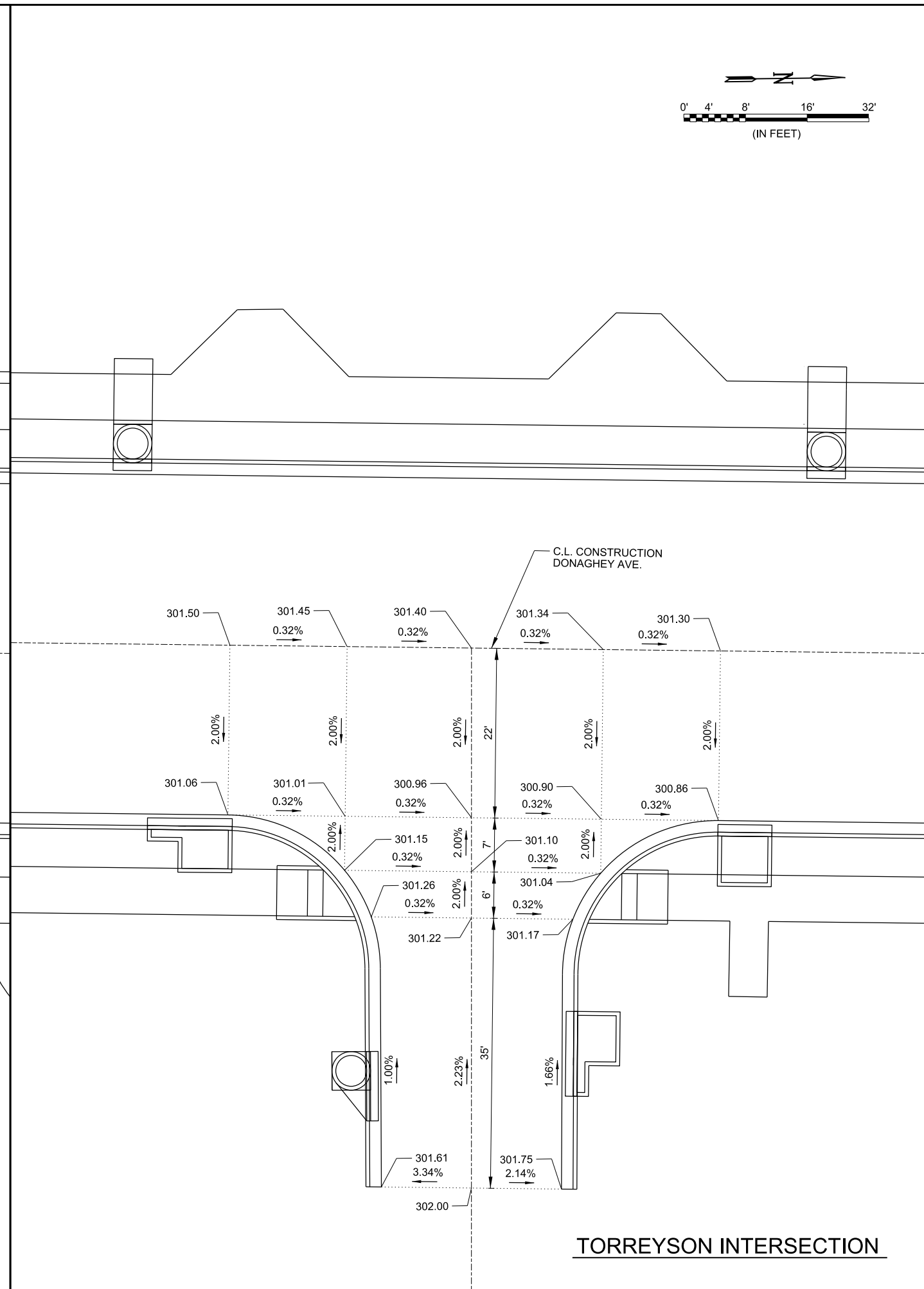


ERBACH STREET INTERSECTION

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BEATRICE POWELL INTERSECTION



TORREYSON INTERSECTION



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 DONAGHEY AVE. IMPROVEMENTS
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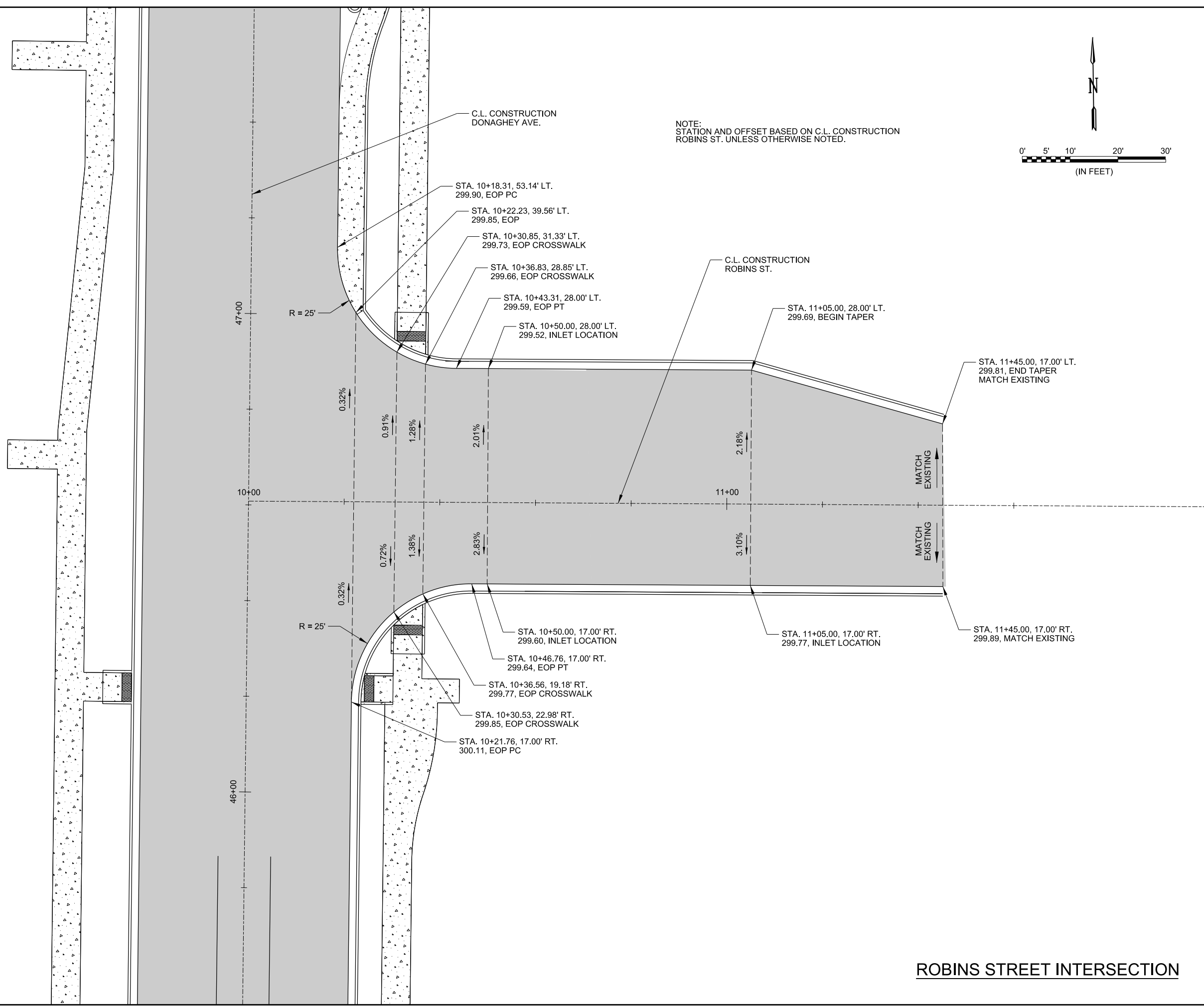
INTERSECTION
 DETAILS
 (SHEET 2 OF 5)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
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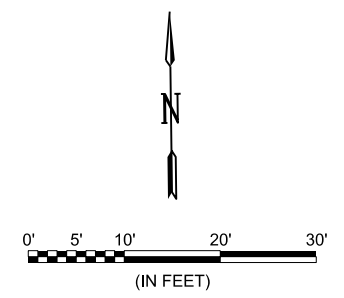
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DRAWING NUMBER
C-212
 SHEET NUMBER
22

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NOTE:
 STATION AND OFFSET BASED ON C.L. CONSTRUCTION
 ROBINS ST. UNLESS OTHERWISE NOTED.



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INTERSECTION
 DETAILS
 (SHEET 3 OF 5)

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DRAWING NUMBER
C-213
 SHEET NUMBER
23

ROBINS STREET INTERSECTION

ROUNDBOUT STATION	B.L. ROUNDBABOUT	INSIDE EDGE OF CIRCULATORY ROADWAY	OUTSIDE EDGE OF TRUCK APRON	INSIDE EDGE OF TRUCK APRON	ELEVATION				
					10+00.00	10+25.00	10+50.00	10+75.00	11+00.00
10+00.00	311.66	312.08	312.43	312.60					
10+25.00	311.93	312.35	312.70	312.88					
10+50.00	312.21	315.63	312.98	313.15					
10+75.00	312.40	312.82	313.17	313.34					
11+00.00	312.46	312.88	313.23	313.41					
11+25.00	312.41	312.83	313.18	313.35					
11+50.00	312.23	312.65	313.00	313.18					
11+75.00	311.93	312.35	312.70	312.88					
12+00.00	311.51	311.93	312.28	312.46					
12+25.00	311.06	311.48	311.82	312.00					
12+50.00	310.67	311.09	311.44	311.62					
12+75.00	310.45	310.87	311.21	311.39					
13+00.00	310.37	310.79	311.14	311.31					
13+25.00	310.45	310.87	311.21	311.39					
13+50.00	310.67	311.09	311.44	311.62					
13+75.00	310.95	311.37	311.72	311.89					
14+00.00	311.22	311.64	311.99	312.17					
14+25.00	311.50	311.92	312.27	312.44					

RADIUS	LENGTH	RADIUS POINT			
		STATION	OFFSET	ALIGNMENT	
EDGE OF PAVEMENT CURVE DATA					
1	62.00	44.92	72+29.91	88.00	RT DONAGHEY AVE.
2	102.00	78.65	11+17.15	130.00	RT COLLEGE AVE. (EAST)
3	62.00	43.44	11+05.17	85.50	LT COLLEGE AVE. (EAST)
4	102.00	82.81	74+49.83	130.00	RT DONAGHEY AVE.
5	62.00	43.15	74+38.32	85.00	LT DONAGHEY AVE.
6	102.00	81.47	8+85.49	125.50	LT COLLEGE AVE. (WEST)
7	62.00	29.07	8+90.45	91.00	RT COLLEGE AVE. (WEST)
8	102.00	106.36	72+11.94	126.69	LT DONAGHEY AVE.

NOTE:
STATION AND OFFSET BASED ON C.L. CONSTRUCTION
DONAGHEY AVE. UNLESS OTHERWISE NOTED.



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CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

INTERSECTION
DETAILS
(SHEET 4 OF 5)

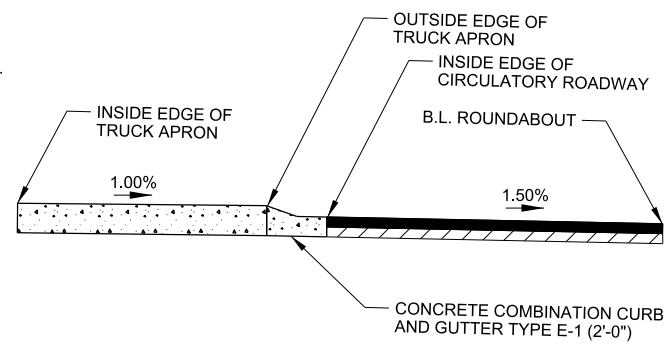
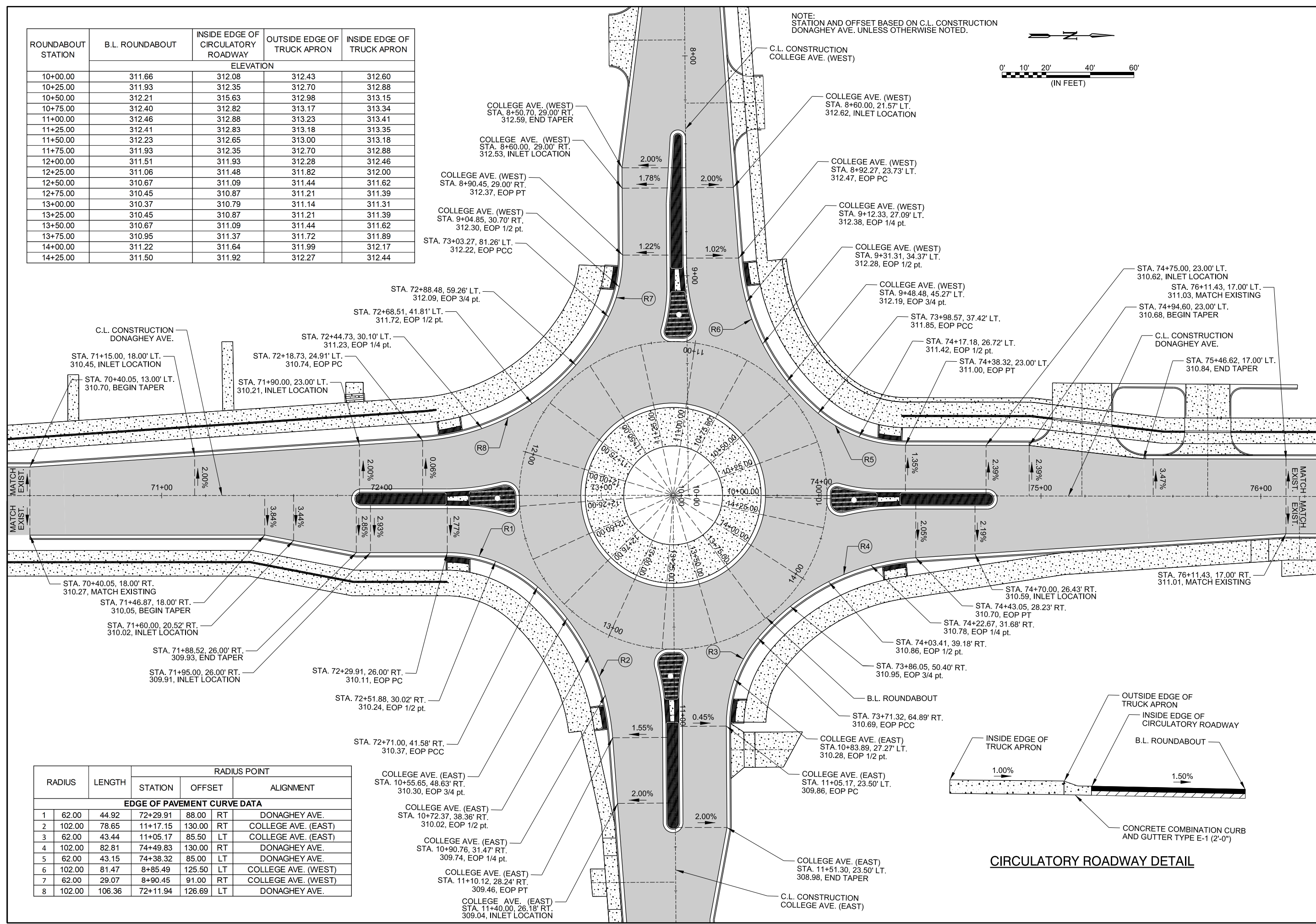
JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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SHEET NUMBER
24

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CIRCULATORY ROADWAY DETAIL



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

INTERSECTION
 DETAILS
 (SHEET 5 OF 5)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
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C-215

SHEET
 NUMBER **25**



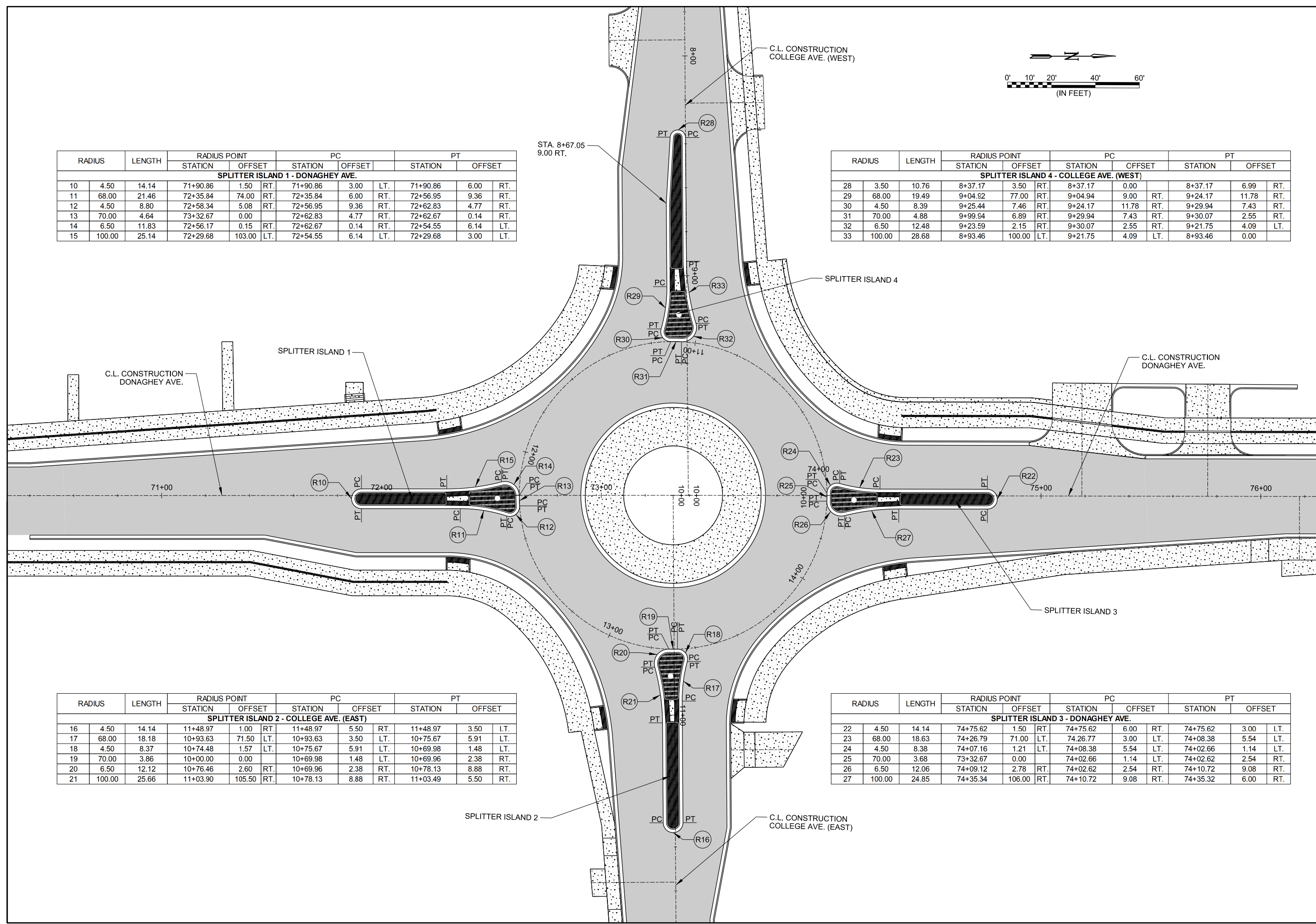
RADIUS	LENGTH	RADIUS POINT		PC			PT	
		STATION	OFFSET	STATION	OFFSET	STATION	OFFSET	
SPLITTER ISLAND 1 - DONAGHEY AVE.								
10	4.50	14.14	71+90.86	1.50	RT.	71+90.86	3.00	LT.
11	68.00	21.46	72+35.84	74.00	RT.	72+35.84	6.00	RT.
12	4.50	8.80	72+58.34	5.08	RT.	72+56.95	9.36	RT.
13	70.00	4.64	73+32.67	0.00		72+62.83	4.77	RT.
14	6.50	11.83	72+56.17	0.15	RT.	72+62.67	0.14	RT.
15	100.00	25.14	72+29.68	103.00	LT.	72+54.55	6.14	LT.

RADIUS	LENGTH	RADIUS POINT		PC			PT	
		STATION	OFFSET	STATION	OFFSET	STATION	OFFSET	
SPLITTER ISLAND 4 - COLLEGE AVE. (WEST)								
28	3.50	10.76	8+37.17	3.50	RT.	8+37.17	0.00	
29	68.00	19.49	9+04.92	77.00	RT.	9+04.94	9.00	RT.
30	4.50	8.39	9+25.44	7.46	RT.	9+24.17	11.78	RT.
31	70.00	4.88	9+99.94	6.89	RT.	9+29.94	7.43	RT.
32	6.50	12.48	9+23.59	2.15	RT.	9+30.07	2.55	RT.
33	100.00	28.68	8+93.46	100.00	LT.	9+21.75	4.09	LT.

RADIUS	LENGTH	RADIUS POINT		PC			PT	
		STATION	OFFSET	STATION	OFFSET	STATION	OFFSET	
SPLITTER ISLAND 2 - COLLEGE AVE. (EAST)								
16	4.50	14.14	11+48.97	1.00	RT.	11+48.97	5.50	RT.
17	68.00	18.18	10+93.63	71.50	LT.	10+93.63	3.50	LT.
18	4.50	8.37	10+74.48	1.57	LT.	10+75.67	5.91	LT.
19	70.00	3.86	10+00.00	0.00		10+69.98	1.48	LT.
20	6.50	12.12	10+76.46	2.60	RT.	10+69.96	2.38	RT.
21	100.00	25.66	11+03.90	105.50	RT.	10+78.13	8.88	RT.

RADIUS	LENGTH	RADIUS POINT		PC			PT	
		STATION	OFFSET	STATION	OFFSET	STATION	OFFSET	
SPLITTER ISLAND 3 - DONAGHEY AVE.								
22	4.50	14.14	74+75.62	1.50	RT.	74+75.62	6.00	RT.
23	68.00	18.63	74+26.79	71.00	LT.	74+26.77	3.00	LT.
24	4.50	8.38	74+07.16	1.21	LT.	74+08.38	5.54	LT.
25	70.00	3.68	73+32.67	0.00		74+02.66	1.14	LT.
26	6.50	12.06	74+09.12	2.78	RT.	74+02.62	2.54	RT.
27	100.00	24.85	74+35.34	106.00	RT.	74+10.72	9.08	RT.

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EROSION CONTROL NOTES

1. THE SYMBOLS SHOWN IN THE SHEET REPRESENT EROSION CONTROL DEVICES AS DETAILED IN THE CITY OF CONWAY STANDARD DETAILS FOR ROADWAY AND DRAINAGE CONSTRUCTION. THE SYMBOLS ARE NOT TO SCALE AND REPRESENT THE GENERAL LOCATION TO WHICH THE DEVICES SHALL BE PLACED. NO WORK OR EROSION CONTROL DEVICES SHALL BE PLACED OUTSIDE THE EXISTING RIGHT OF WAY, PROPOSED RIGHTOF WAY,OR TEMPORARY CONSTRUCTION EASEMENTS.
2. ALL DISTURBED AREAS CONTAINING EXPOSED SOIL SHALL RECEIVE TEMPORARY EROSION AND SEDIMENT CONTROL APPLICATIONS. CONTRACTOR MAY CHOOSE TO UTILIZE ALTERNATIVE EROSION CONTROL PRODUCTS SUCH AS WATTLES AS APPROVED BY THE ENGINEER.
3. WIRE REINFORCED SILT FENCE SHALL BE PLACED PRIOR TO THE CLEARING AND GRUBBING OPERATIONS.
4. SEE CITY OF CONWAY DETAILS FOR ROADWAY AND DRAINAGE CONSTRUCTION EC-1 THROUGH EC-8 FOR TEMPORARY EROSION CONTROL DEVICES.
5. POST-GRADING SLOPES WILL NOT BE SIGNIFICANTLY STEEPER THAN EXISTING GRADES.
6. LOCATION OF OFFSITE STORAGE OF MATERIALS IS TO BE DETERMINED BY THE CONTRACTOR. THE SWPPP WILL BE UPDATED ACCORDINGLY.
7. PAVED CONSTRUCTION ENTRANCES/EXITS EXIST ALONG THE PROPOSED ROUTE.
8. ALL DISTURBED AREAS WILL BE SEEDED OR SODDED UPON COMPLETION.
9. DROP INLET SILT FENCE AND CURB INLET PROTECTION ARE SHOWN FOR ALL PROPOSED CURB INLETS. DROP INLET SILT FENCE SHALL BE MAINTAINED THROUGHOUT CONSTRUCTION OF THE INLET PRIOR TO CONSTRUCTING THE INLET TOP. CURB INLET PROTECTION SHALL BE INSTALLED FOLLOWING CONSTRUCTION OF THE INLET TOP.

SEQUENCE OF CONSTRUCTION OF E & SC FEATURES

1. INSTALL WIRE REINFORCED SILT FENCE
2. CLEAR / GRUB ACTIVITIES.

EROSION CONTROL MEASURES TO BE PLACED DURING APPROPRIATE STAGES. THESE DEVICES SHALL BE LEFT IN PLACE AS LONG AS REQUIRED TO CONTROL EROSION.

LEGEND

— 285 —	EXISTING CONTOUR
—— 285 ——	PROPOSED CONTOUR
←	DRAINAGE FLOW DIRECTION
— SILT —	WIRE REINFORCED SILT FENCE
- - - - -	DISTURBED AREA BOUNDARY
— TOB —	TOP OF BANK
— TOS —	TOE OF SLOPE
EC-4	CONCRETE WASHOUT
EC-5	CURB INLET PROTECTION
EC-6	DROP INLET SILT FENCE
	STABILIZED CONSTRUCTION ENTRANCE/EXIT

TOTAL DISTURBED AREA = 7.88 ACRES

REVISION BOX

DATE OF REVISION	REVISION

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STATE OF ARKANSAS
 CERTIFICATE OF AUTHORITY
 GARVER
 LLC
 No. 766
 ARKANSAS-ENGINEER

STATE OF ARKANSAS
Justin L. Tackett
 LICENSED PROFESSIONAL ENGINEER
 No. 14994
 JUSTIN L. TACKETT

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BY	DATE	DESCRIPTION	REV.

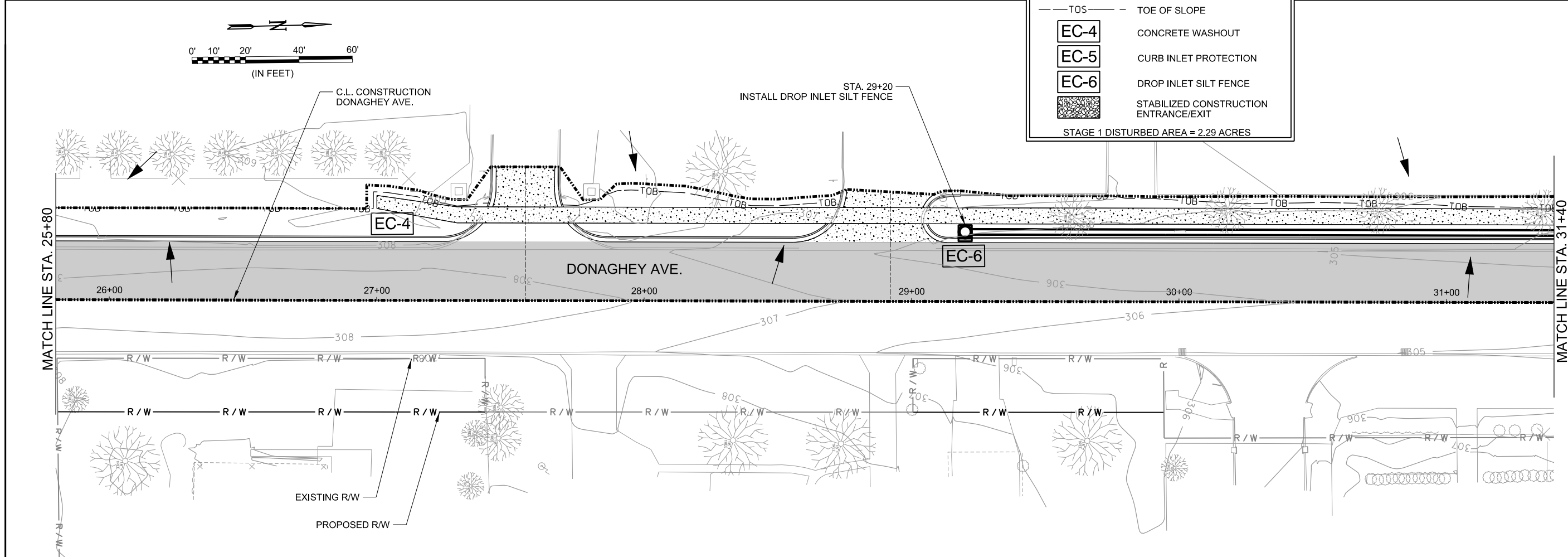
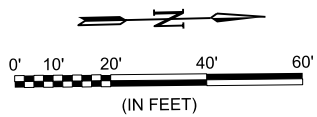
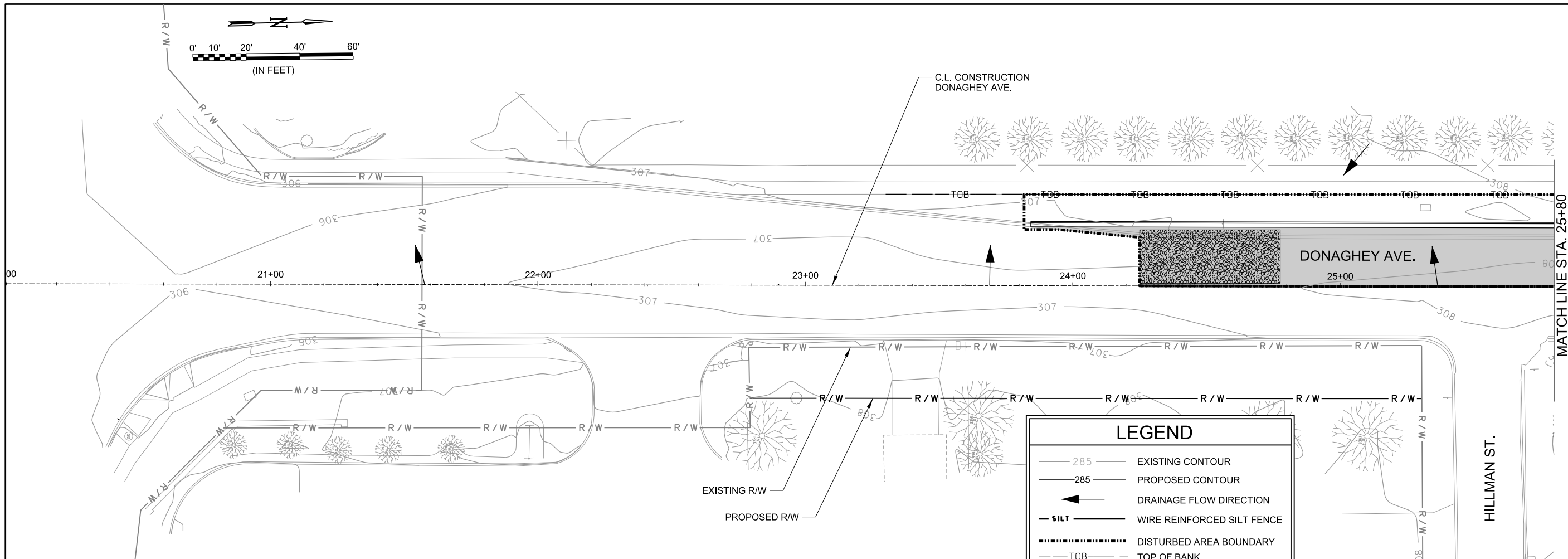
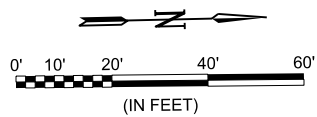
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TEMPORARY EROSION CONTROL NOTES AND REVISION BOX

JOB NO.: 18147010
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C-301
 SHEET NUMBER **26**



LEGEND

- 285 EXISTING CONTOUR
- 285 PROPOSED CONTOUR
- DRAINAGE FLOW DIRECTION
- WIRE REINFORCED SILT FENCE
- DISTURBED AREA BOUNDARY
- TOP OF BANK
- TOE OF SLOPE
- CONCRETE WASHOUT
- CURB INLET PROTECTION
- DROP INLET SILT FENCE
- STABILIZED CONSTRUCTION ENTRANCE/EXIT

STAGE 1 DISTURBED AREA = 2.29 ACRES



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

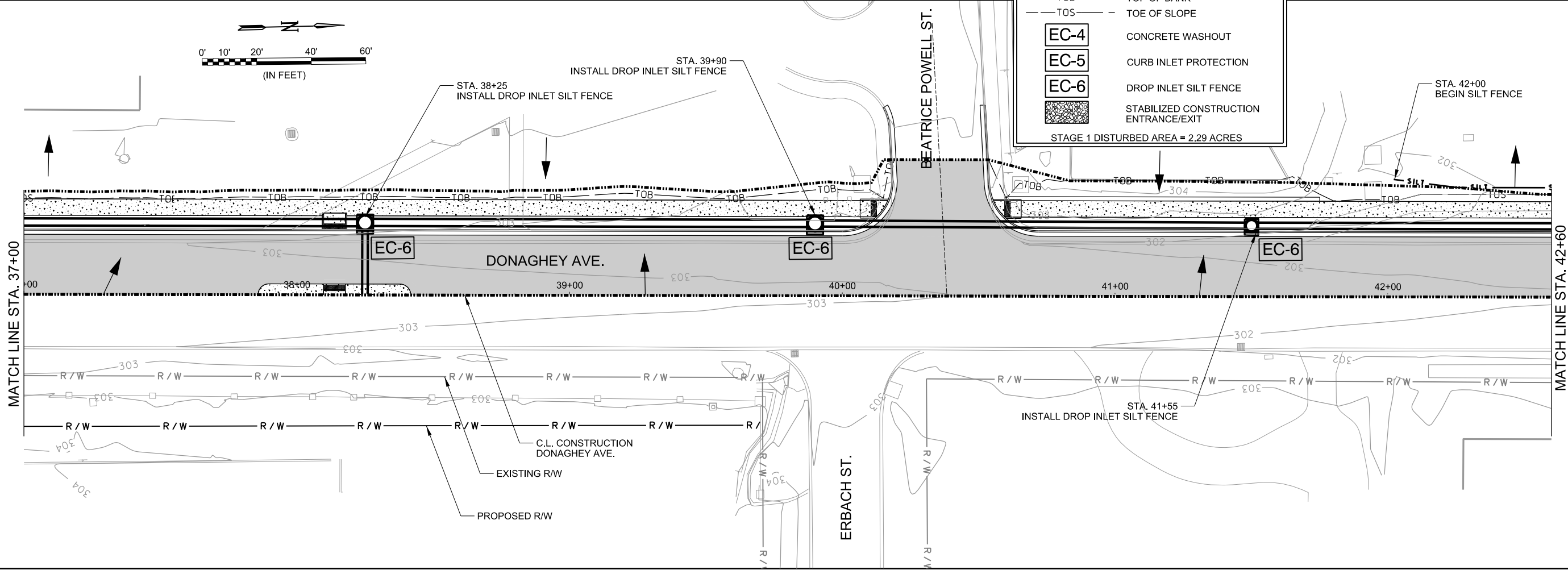
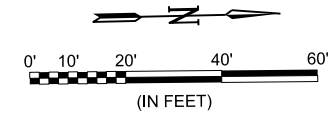
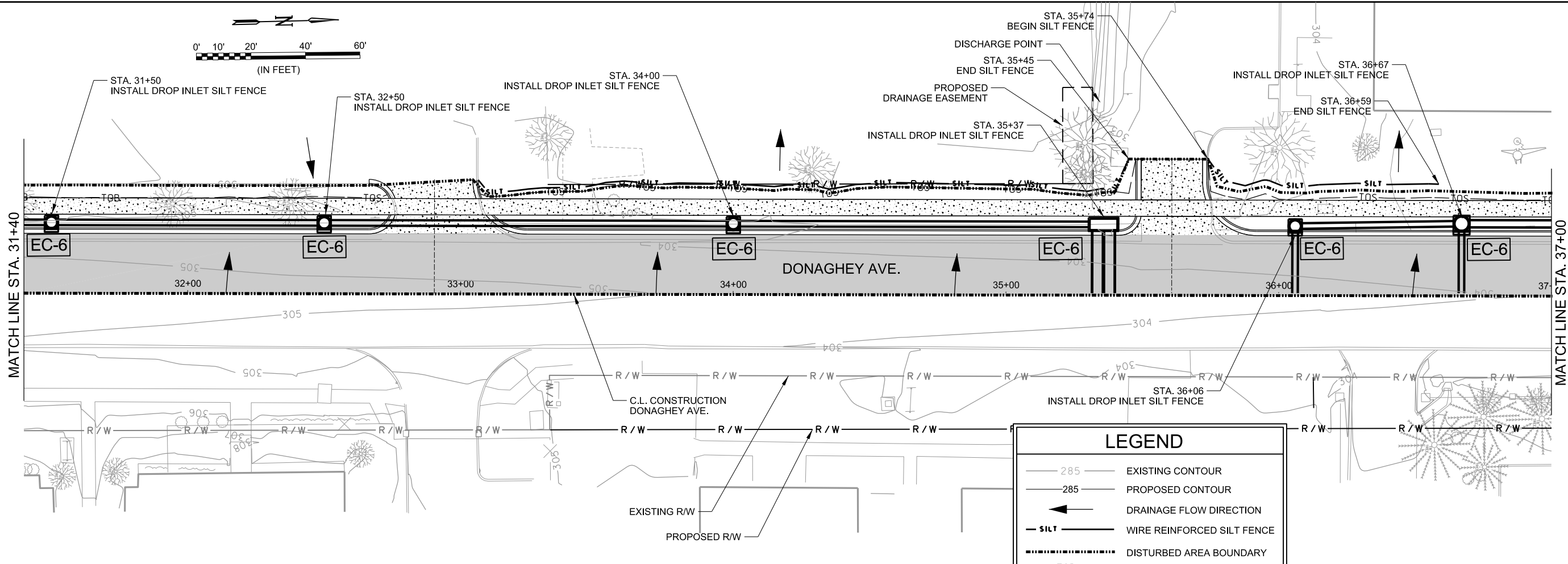
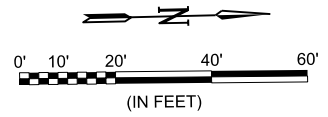
TEMPORARY EROSION CONTROL PLAN - STAGE 1 (SHEET 1 OF 3)

JOB NO.: 18147010
 DATE: MAY 2020
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C-302
 SHEET NUMBER
27

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CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

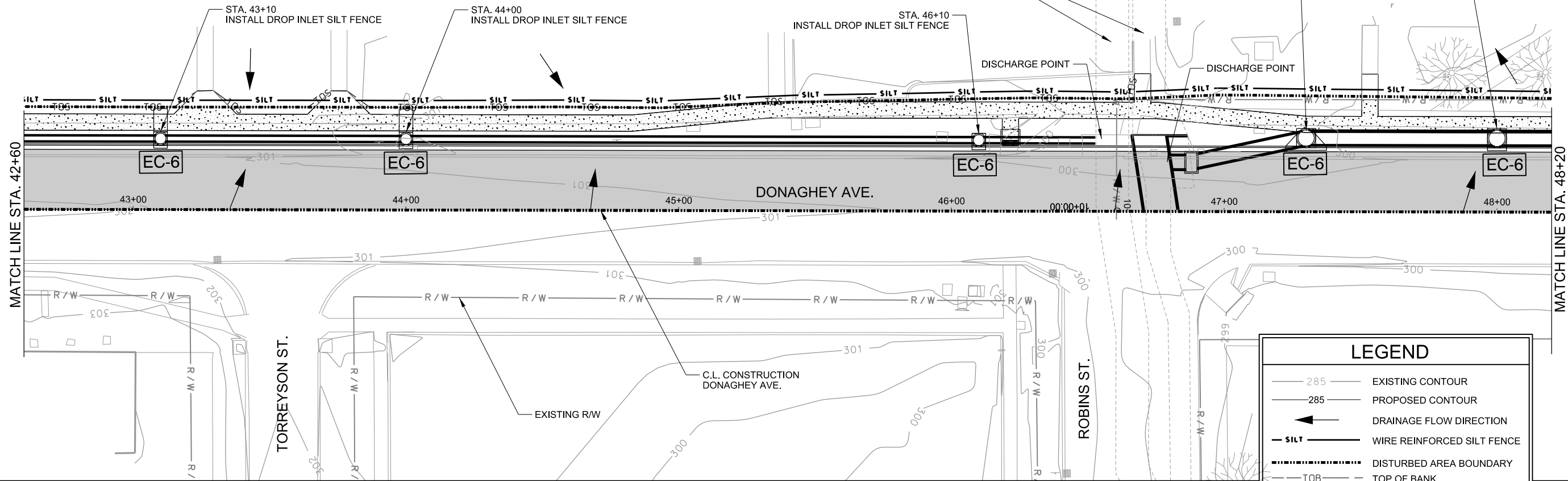
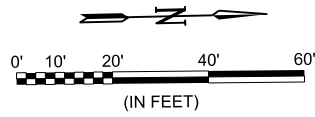
TEMPORARY EROSION CONTROL PLAN - STAGE 1 (SHEET 2 OF 3)

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 SHEET NUMBER
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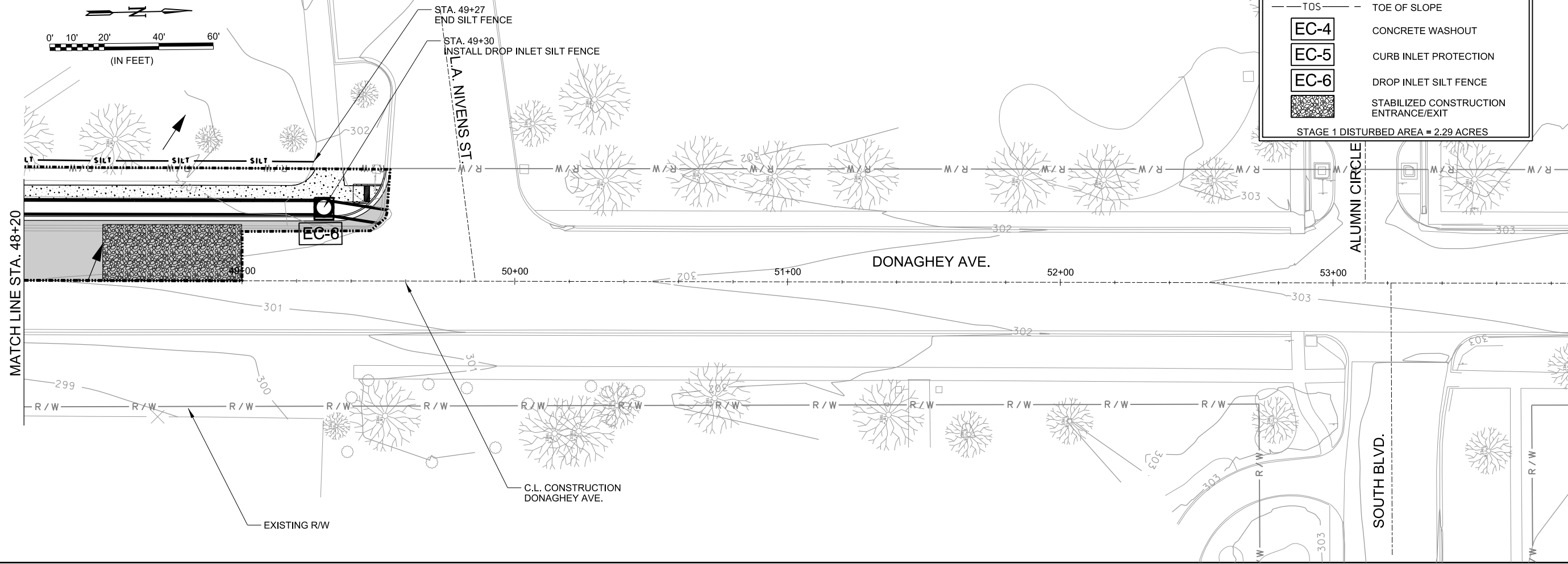
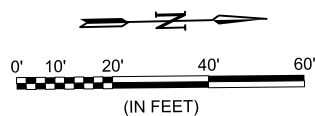
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LEGEND

- 285 — EXISTING CONTOUR
- 285 — PROPOSED CONTOUR
- ← DRAINAGE FLOW DIRECTION
- SILT — WIRE REINFORCED SILT FENCE
- DISTURBED AREA BOUNDARY
- - - TOB - - - TOP OF BANK
- - - TOS - - - TOE OF SLOPE
- EC-4 CONCRETE WASHOUT
- EC-5 CURB INLET PROTECTION
- EC-6 DROP INLET SILT FENCE
- STABILIZED CONSTRUCTION ENTRANCE/EXIT

STAGE 1 DISTURBED AREA = 2.29 ACRES



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TEMPORARY EROSION CONTROL PLAN - STAGE 1 (SHEET 3 OF 3)

JOB NO.: 18147010
 DATE: MAY 2020
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 SHEET NUMBER
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STATE OF ARKANSAS
Justin L. Tackett
 LICENSED PROFESSIONAL ENGINEER
 No. 14994
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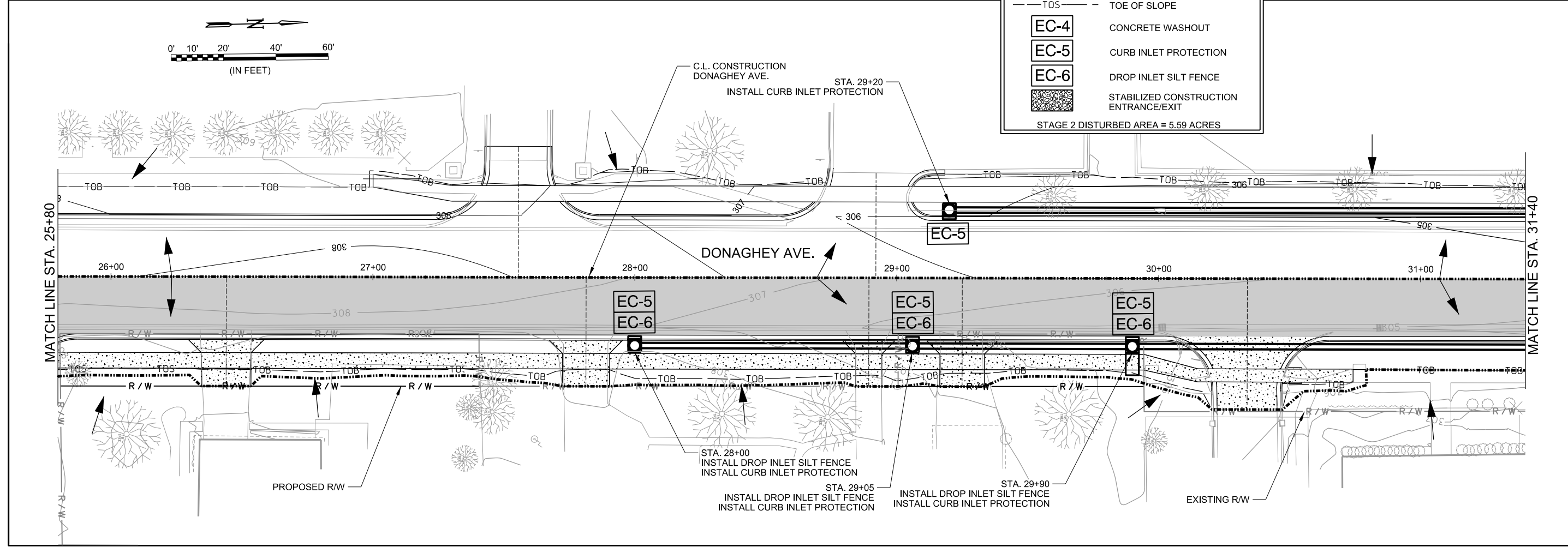
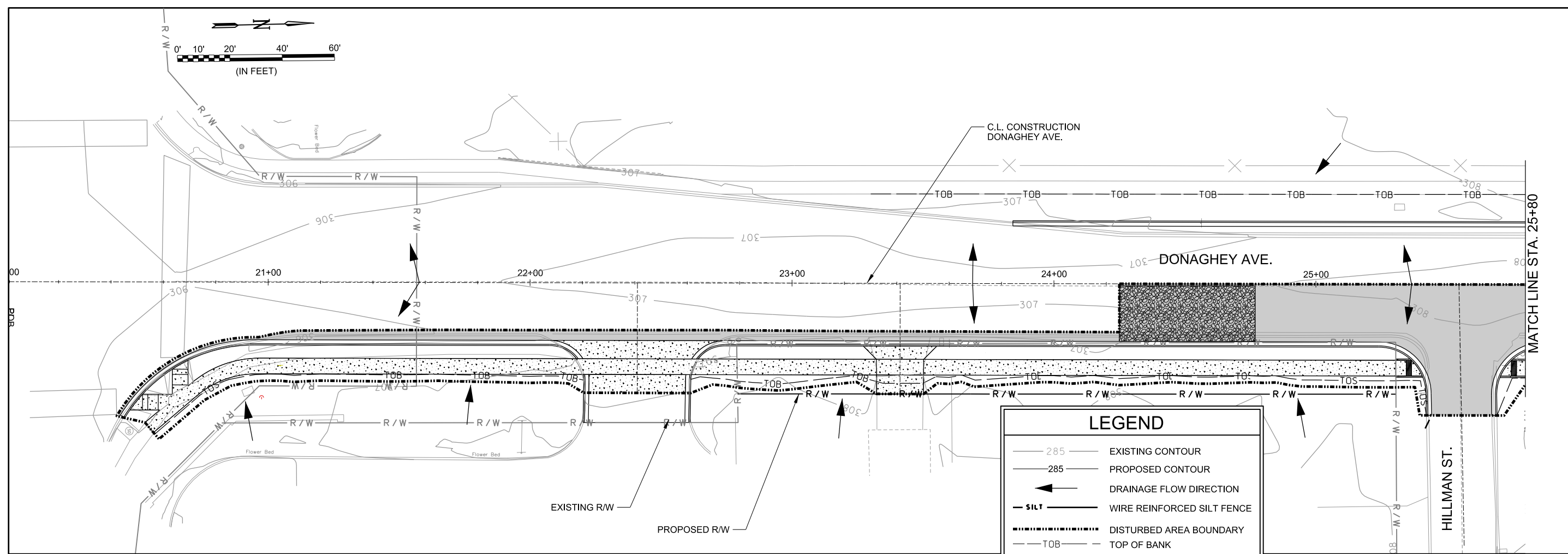


CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TEMPORARY EROSION CONTROL PLAN - STAGE 2 (SHEET 1 OF 7)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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C-305
 SHEET NUMBER
30



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STATE OF ARKANSAS
Justin L. Tackett
 LICENSED PROFESSIONAL ENGINEER
 No. 14994
 JUSTIN L. TACKETT
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BY	DATE	DESCRIPTION



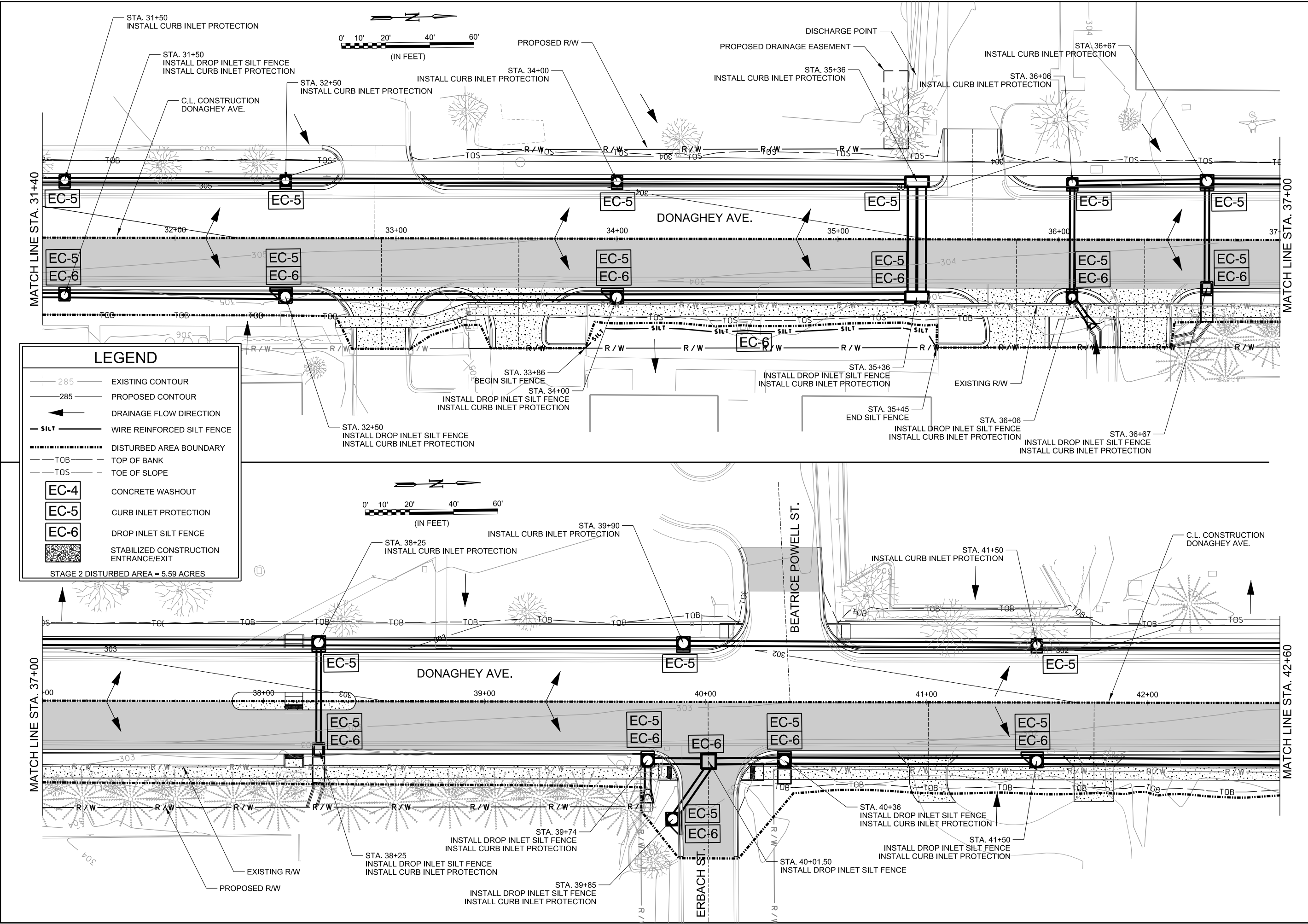
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TEMPORARY EROSION CONTROL PLAN - STAGE 2 (SHEET 2 OF 7)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-306
 SHEET NUMBER
31



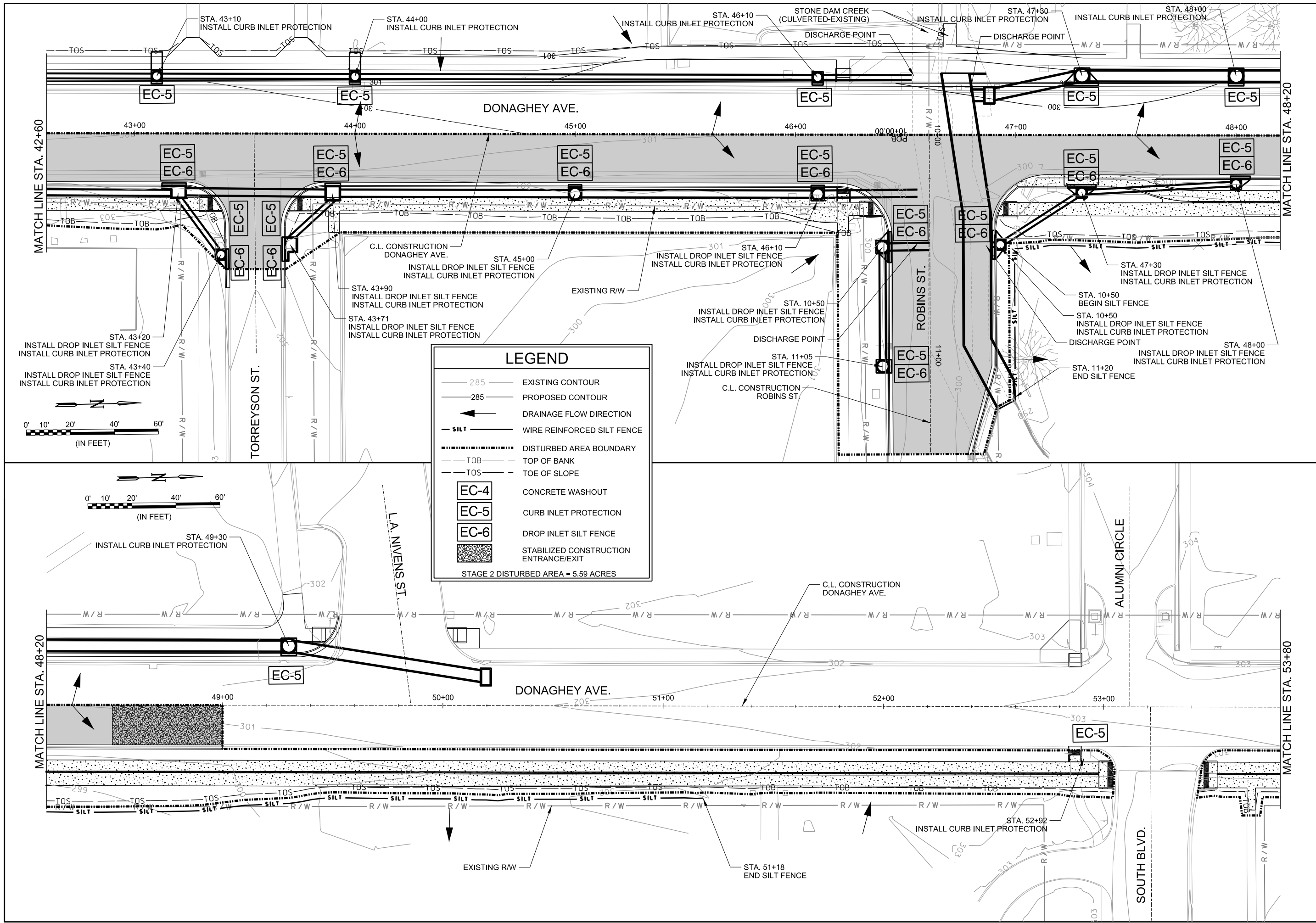
LEGEND

- 285 — EXISTING CONTOUR
- 285 — PROPOSED CONTOUR
- ← DRAINAGE FLOW DIRECTION
- SILT — WIRE REINFORCED SILT FENCE
- — — — — DISTURBED AREA BOUNDARY
- TOB — TOP OF BANK
- TOS — TOE OF SLOPE
- EC-4 CONCRETE WASHOUT
- EC-5 CURB INLET PROTECTION
- EC-6 DROP INLET SILT FENCE
- STABILIZED CONSTRUCTION ENTRANCE/EXIT

STAGE 2 DISTURBED AREA = 5.59 ACRES

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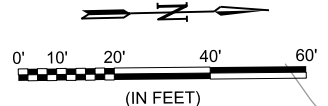
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LEGEND

- 285 — EXISTING CONTOUR
- 285 — PROPOSED CONTOUR
- ← DRAINAGE FLOW DIRECTION
- SILT — WIRE REINFORCED SILT FENCE
- DISTURBED AREA BOUNDARY
- TOB --- TOP OF BANK
- TOS --- TOE OF SLOPE
- EC-4 CONCRETE WASHOUT
- EC-5 CURB INLET PROTECTION
- EC-6 DROP INLET SILT FENCE
- STABILIZED CONSTRUCTION ENTRANCE/EXIT

STAGE 2 DISTURBED AREA = 5.59 ACRES



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TEMPORARY EROSION CONTROL PLAN - STAGE 2 (SHEET 3 OF 7)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-307

SHEET NUMBER
32



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REV.	DATE	DESCRIPTION	BY



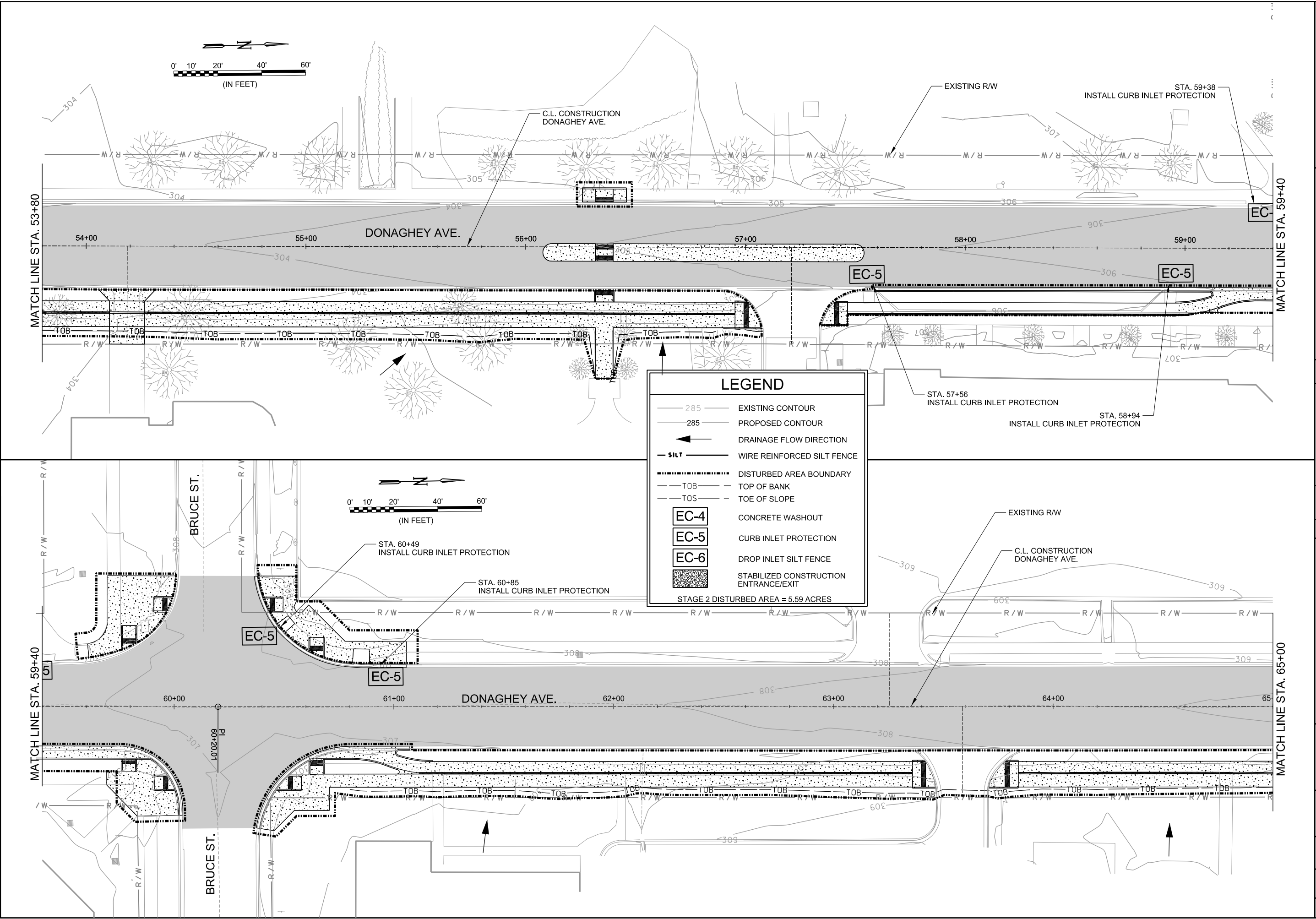
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TEMPORARY EROSION CONTROL PLAN - STAGE 2 (SHEET 4 OF 7)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-308
 SHEET NUMBER
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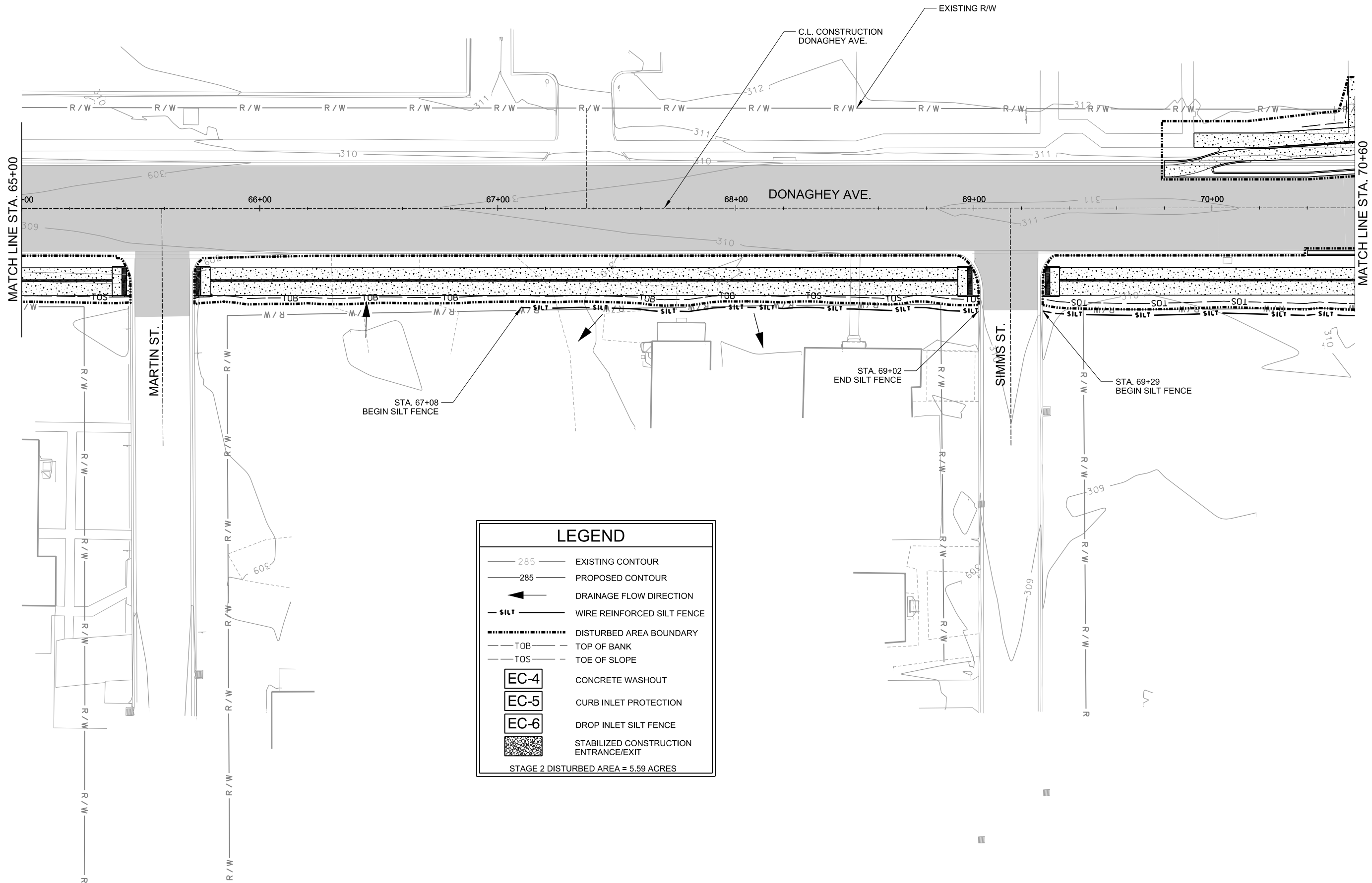


LEGEND

- 285 — EXISTING CONTOUR
- 285 — PROPOSED CONTOUR
- ← DRAINAGE FLOW DIRECTION
- SILT — WIRE REINFORCED SILT FENCE
- DISTURBED AREA BOUNDARY
- TOB --- TOP OF BANK
- TOS --- TOE OF SLOPE
- EC-4 CONCRETE WASHOUT
- EC-5 CURB INLET PROTECTION
- EC-6 DROP INLET SILT FENCE
- STABILIZED CONSTRUCTION ENTRANCE/EXIT

STAGE 2 DISTURBED AREA = 5.59 ACRES

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LEGEND

	285	EXISTING CONTOUR
	285	PROPOSED CONTOUR
		DRAINAGE FLOW DIRECTION
		WIRE REINFORCED SILT FENCE
		DISTURBED AREA BOUNDARY
	TOB	TOP OF BANK
	TOS	TOE OF SLOPE
	EC-4	CONCRETE WASHOUT
	EC-5	CURB INLET PROTECTION
	EC-6	DROP INLET SILT FENCE
		STABILIZED CONSTRUCTION ENTRANCE/EXIT

STAGE 2 DISTURBED AREA = 5.59 ACRES



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CITY OF CONWAY
 CONWAY, ARKANSAS

**DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)**

TEMPORARY EROSION CONTROL PLAN - STAGE 2 (SHEET 5 OF 7)

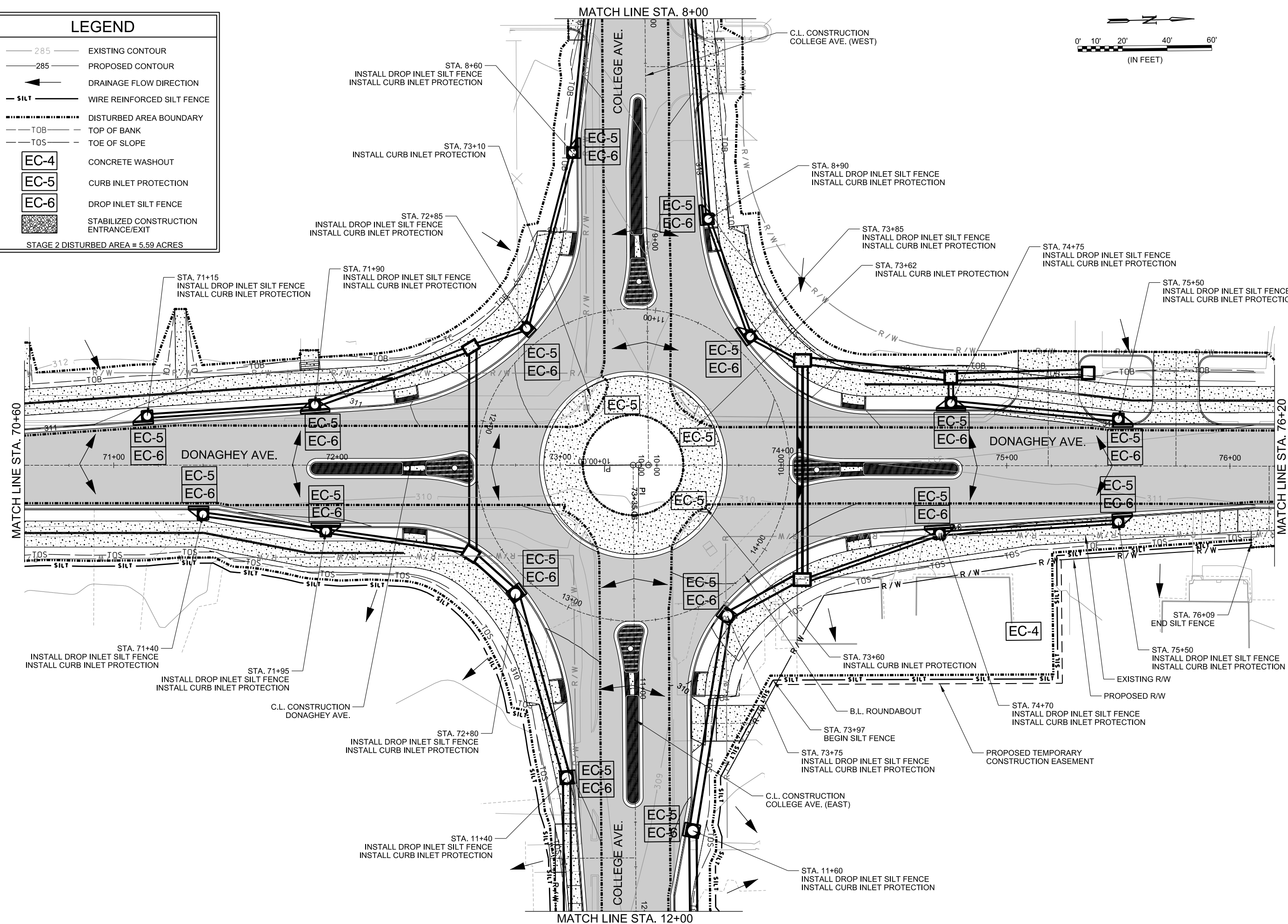
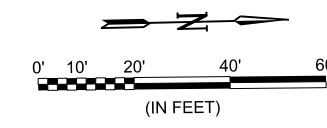
JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-309
 SHEET NUMBER **34**

LEGEND

- 285 — EXISTING CONTOUR
 - 285 — PROPOSED CONTOUR
 - ← DRAINAGE FLOW DIRECTION
 - SILT — WIRE REINFORCED SILT FENCE
 - DISTURBED AREA BOUNDARY
 - TOB --- TOP OF BANK
 - TOS --- TOE OF SLOPE
 - EC-4 CONCRETE WASHOUT
 - EC-5 CURB INLET PROTECTION
 - EC-6 DROP INLET SILT FENCE
 - STABILIZED CONSTRUCTION ENTRANCE/EXIT
- STAGE 2 DISTURBED AREA = 5.59 ACRES



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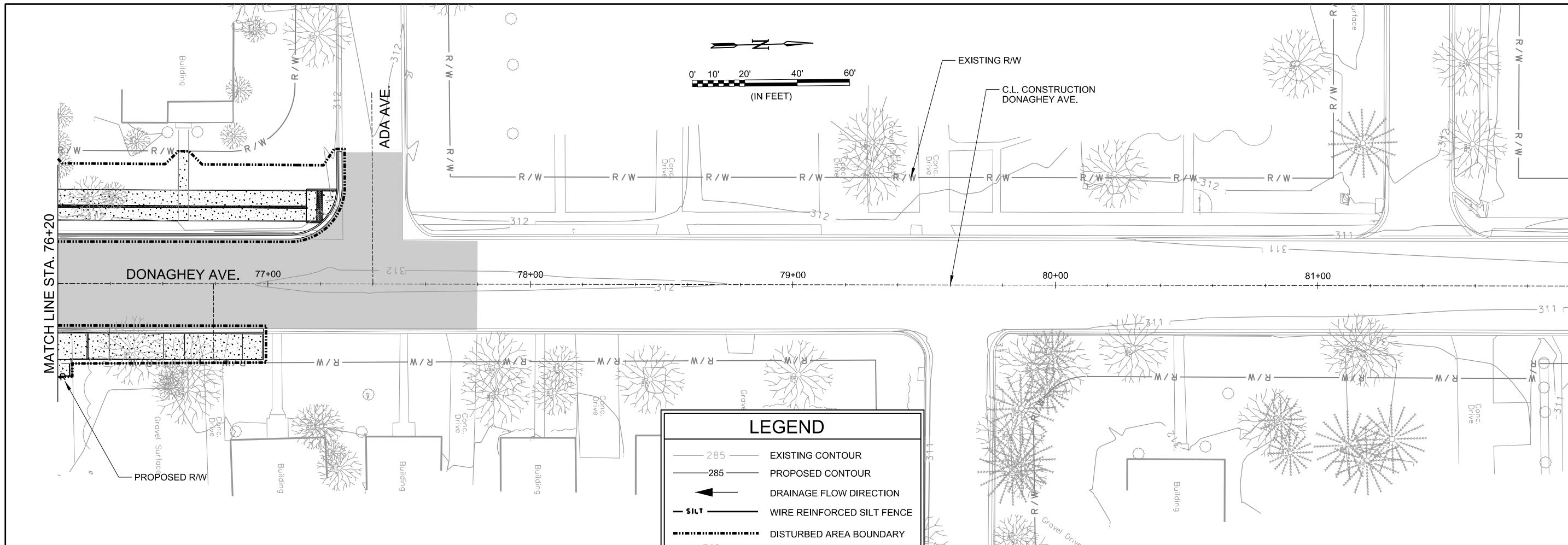


CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TEMPORARY EROSION CONTROL PLAN - STAGE 2 (SHEET 6 OF 7)

JOB NO.: 18147010
 DATE: MAY 2020
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 DRAWING NUMBER
C-310
 SHEET NUMBER
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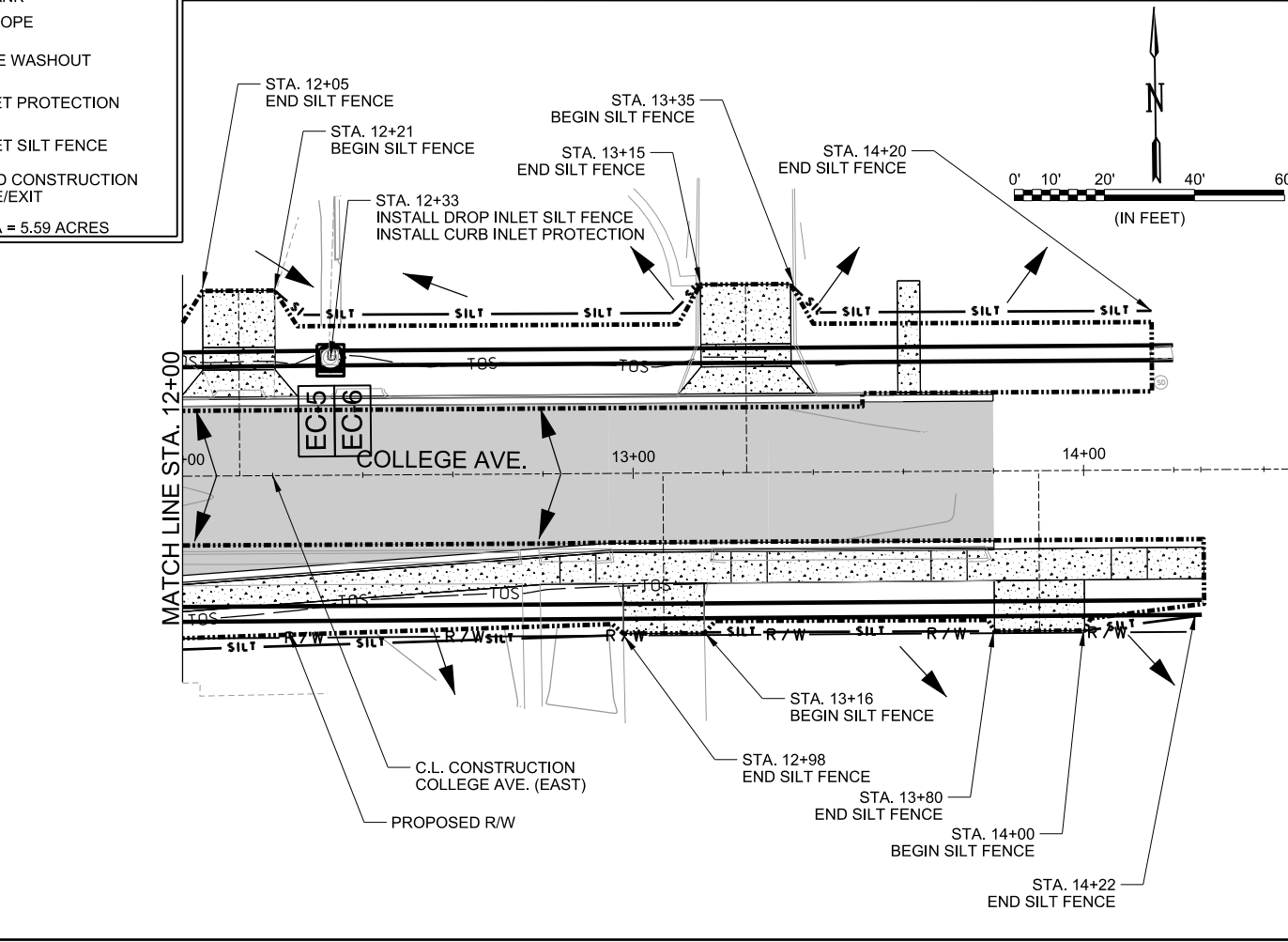
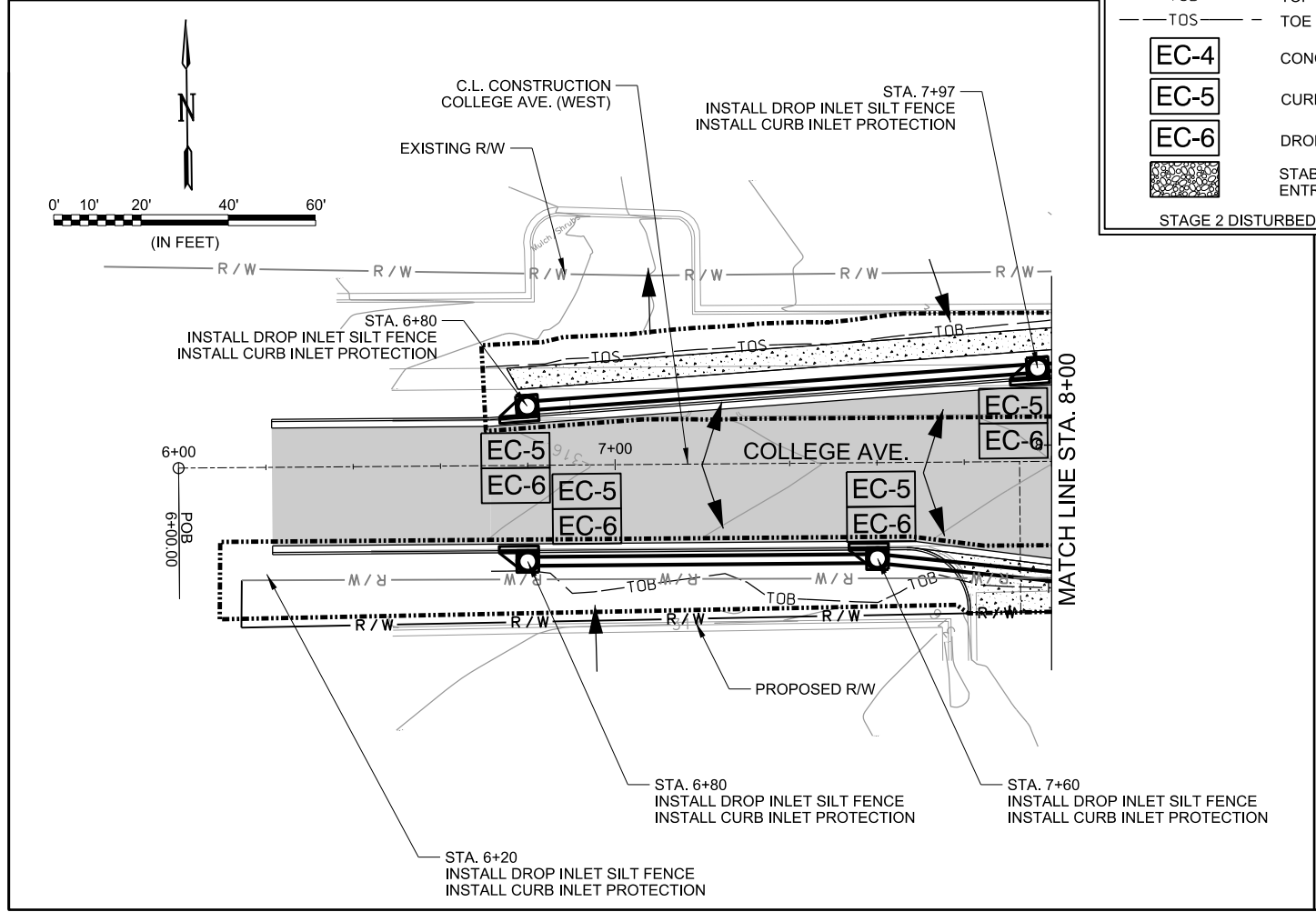
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LEGEND

- 285 EXISTING CONTOUR
- 285 PROPOSED CONTOUR
- DRAINAGE FLOW DIRECTION
- WIRE REINFORCED SILT FENCE
- DISTURBED AREA BOUNDARY
- TOP OF BANK
- TOE OF SLOPE
- CONCRETE WASHOUT
- CURB INLET PROTECTION
- DROP INLET SILT FENCE
- STABILIZED CONSTRUCTION ENTRANCE/EXIT

STAGE 2 DISTURBED AREA = 5.59 ACRES



REV.	DATE	DESCRIPTION	BY

CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

TEMPORARY EROSION CONTROL PLAN - STAGE 2 (SHEET 7 OF 7)

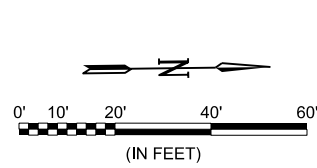
JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
C-311

SHEET NUMBER
36

HIGHWAY 60 (DAVE WARD DRIVE)

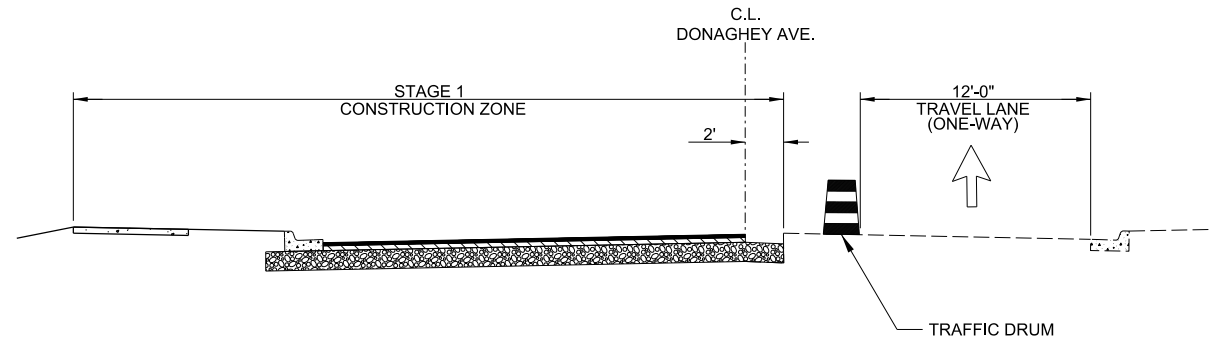


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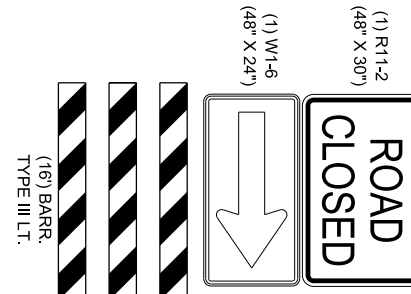
- STAGE 1 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM

STAGE 1 CONSTRUCTION SEQUENCE

1. PLACE TRAFFIC CONTROL DEVICES AS SHOWN.
2. CONSTRUCT STAGE 1 IMPROVEMENTS INCLUDING UNDERCUT EXCAVATION AND BACKFILL, DRAINAGE, AGGREGATE BASE COURSE, CURB AND GUTTER, ACHM BINDER COURSE, FIRST LIFT OF ACHM SURFACE COURSE, ROBINS ST. TRAFFIC SIGNAL, AND SIDEWALK.

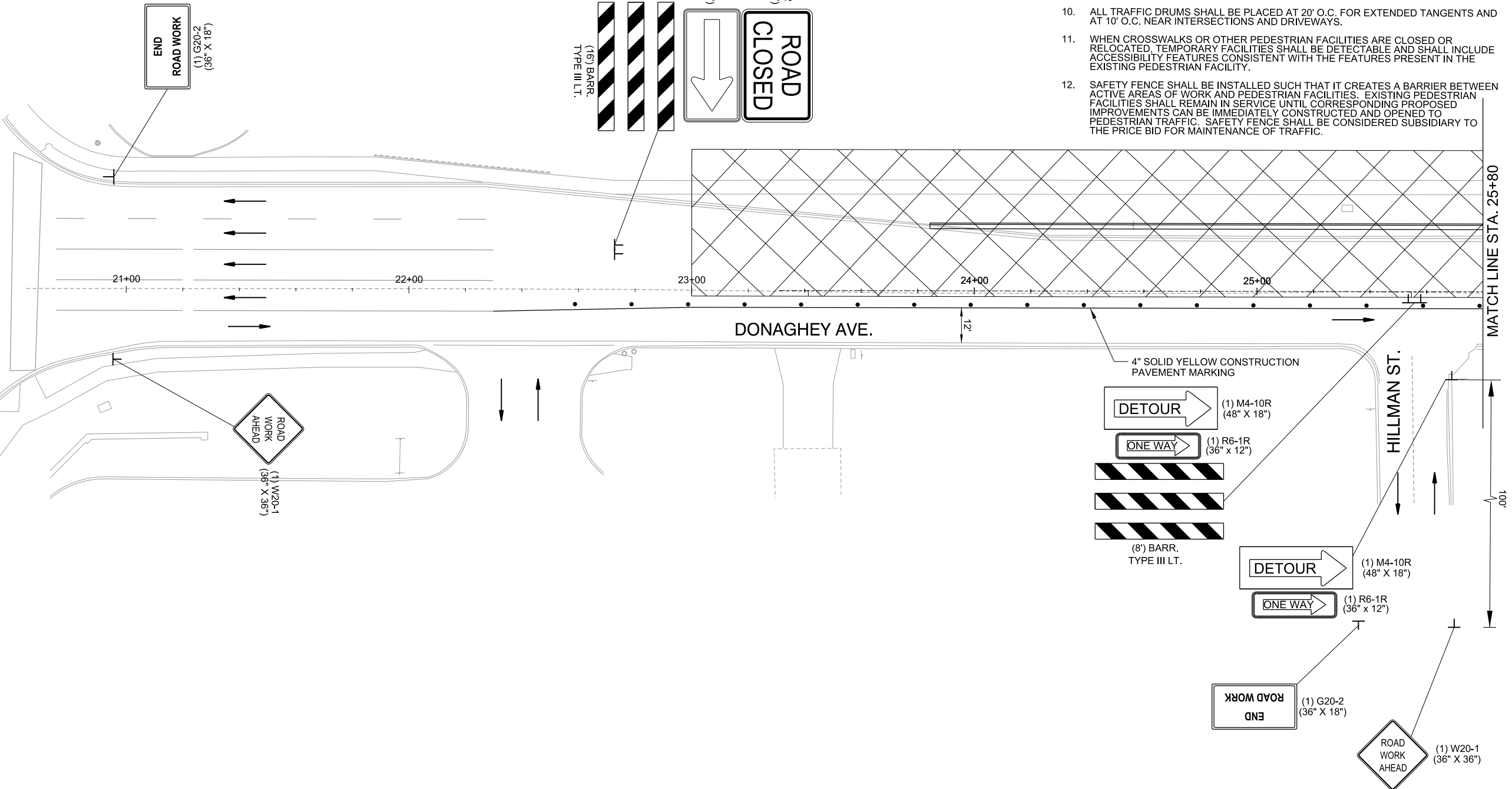


STAGE 1 TYPICAL SECTION



GENERAL NOTES:

1. THE MAINTENANCE OF TRAFFIC AS SHOWN IN THE PLANS IS PROVIDED TO THE CONTRACTOR AS MINIMUM CONTROLS AND AS GUIDANCE. THE CONTRACTOR SHALL PREPARE AND SUBMIT TO THE ENGINEER AND OWNER FOR APPROVAL A DETAILED MAINTENANCE OF TRAFFIC PLAN IN ACCORDANCE WITH THE SPECIFICATIONS PRIOR TO START OF CONSTRUCTION.
2. ALL EXISTING SIGNS NEEDED TO MAINTAIN TRAFFIC SHALL REMAIN IN PLACE AND IN CLEAR SIGHT UNLESS OTHERWISE NOTED.
3. TWO WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT AS SHOWN.
4. SHORT DURATION STREET CLOSURES MAY BE REQUIRED FOR PIPE INSTALLATIONS AND OTHER ITEMS OF WORK. CONTRACTOR SHALL PREPARE AND SUBMIT TO THE ENGINEER AND OWNER FOR APPROVAL A DETAILED MAINTENANCE OF TRAFFIC PLAN. TRAFFIC CONTROL FOR SHORT DURATION CLOSURES SHALL BE CONSIDERED SUBSIDIARY TO THE PRICE BID FOR MAINTENANCE OF TRAFFIC.
5. ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO AND BE PLACED IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS.
6. THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY DRAINAGE AS REQUIRED.
7. CHANGEABLE MESSAGE SIGNS SHALL BE LOCATED AS DIRECTED BY THE ENGINEER. MESSAGE SHALL BE COORDINATED WITH AND APPROVED BY THE ENGINEER.
8. CONTRACTOR SHALL NOTIFY THE ENGINEER (TWO WEEKS IN ADVANCE, MINIMUM) PRIOR TO ADVANCING TO SUBSEQUENT STAGES OF CONSTRUCTION.
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10. ALL TRAFFIC DRUMS SHALL BE PLACED AT 20' O.C. FOR EXTENDED TANGENTS AND AT 10' O.C. NEAR INTERSECTIONS AND DRIVEWAYS.
11. WHEN CROSSWALKS OR OTHER PEDESTRIAN FACILITIES ARE CLOSED OR RELOCATED, TEMPORARY FACILITIES SHALL BE DETECTABLE AND SHALL INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH THE FEATURES PRESENT IN THE EXISTING PEDESTRIAN FACILITY.
12. SAFETY FENCE SHALL BE INSTALLED SUCH THAT IT CREATES A BARRIER BETWEEN ACTIVE AREAS OF WORK AND PEDESTRIAN FACILITIES. EXISTING PEDESTRIAN FACILITIES SHALL REMAIN IN SERVICE UNTIL CORRESPONDING PROPOSED IMPROVEMENTS CAN BE IMMEDIATELY CONSTRUCTED AND OPENED TO PEDESTRIAN TRAFFIC. SAFETY FENCE SHALL BE CONSIDERED SUBSIDIARY TO THE PRICE BID FOR MAINTENANCE OF TRAFFIC.



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REV.	DATE	DESCRIPTION	BY



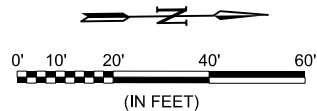
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF TRAFFIC PLAN - STAGE 1 (SHEET 1 OF 12)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-401
 SHEET NUMBER
37

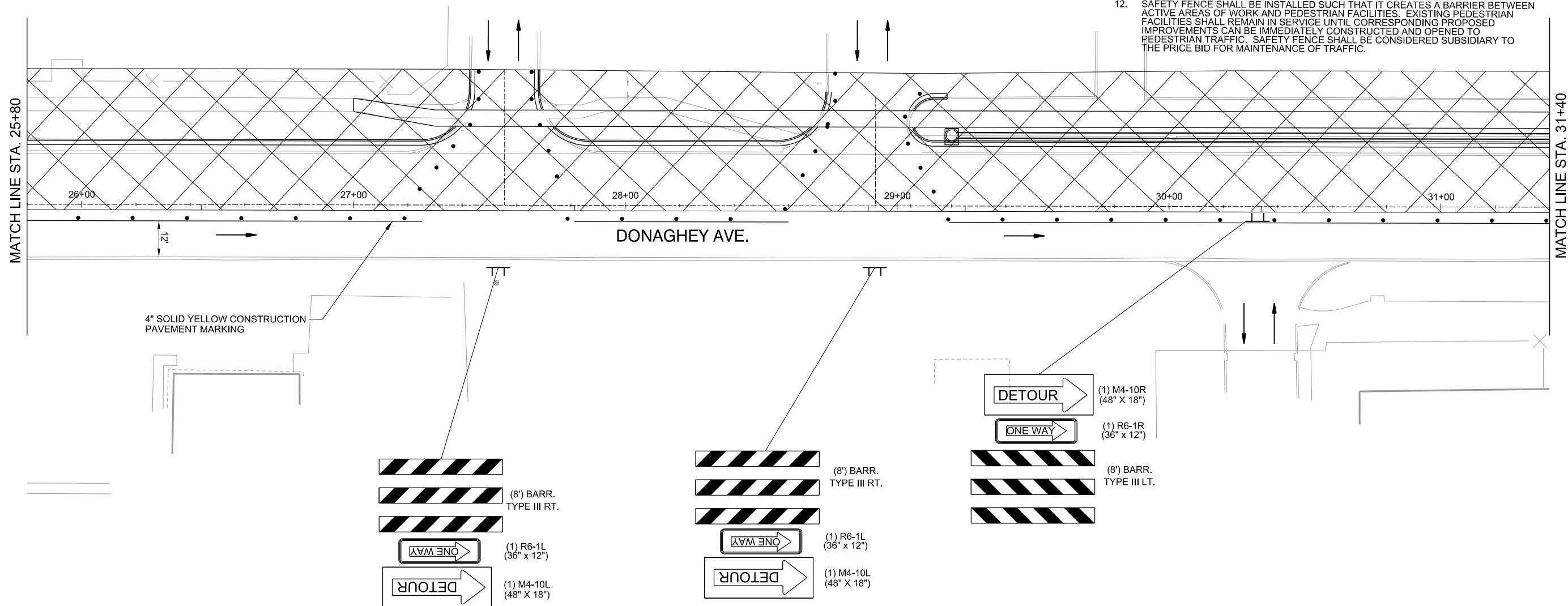


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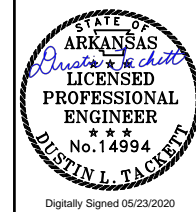
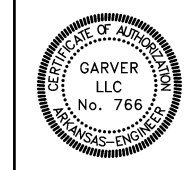
- STAGE 1 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM

GENERAL NOTES:

1. THE MAINTENANCE OF TRAFFIC AS SHOWN IN THE PLANS IS PROVIDED TO THE CONTRACTOR AS MINIMUM CONTROLS AND AS GUIDANCE. THE CONTRACTOR SHALL PREPARE AND SUBMIT TO THE ENGINEER AND OWNER FOR APPROVAL A DETAILED MAINTENANCE OF TRAFFIC PLAN IN ACCORDANCE WITH THE SPECIFICATIONS PRIOR TO START OF CONSTRUCTION.
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12. SAFETY FENCE SHALL BE INSTALLED SUCH THAT IT CREATES A BARRIER BETWEEN ACTIVE AREAS OF WORK AND PEDESTRIAN FACILITIES. EXISTING PEDESTRIAN FACILITIES SHALL REMAIN IN SERVICE UNTIL CORRESPONDING PROPOSED IMPROVEMENTS CAN BE IMMEDIATELY CONSTRUCTED AND OPENED TO PEDESTRIAN TRAFFIC. SAFETY FENCE SHALL BE CONSIDERED SUBSIDIARY TO THE PRICE BID FOR MAINTENANCE OF TRAFFIC.



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

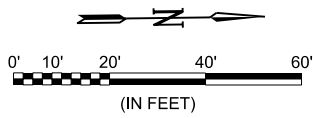
MAINTENANCE OF TRAFFIC PLAN - STAGE 1 (SHEET 2 OF 12)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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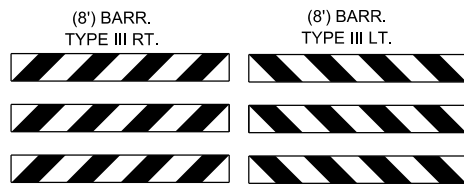
DRAWING NUMBER
C-402
SHEET NUMBER **38**

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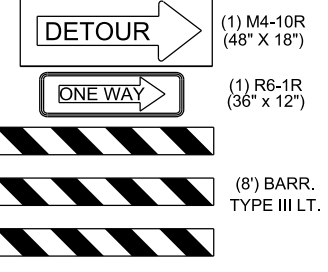
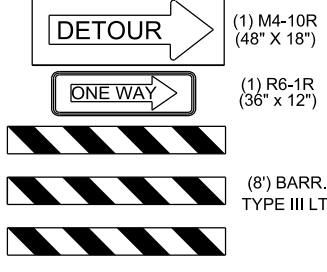
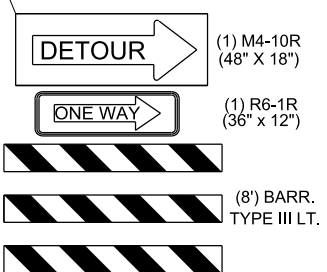
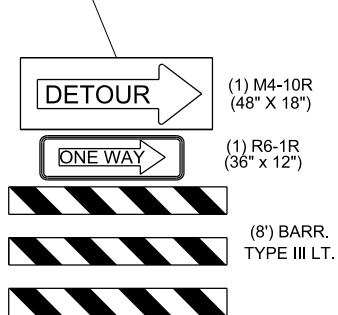
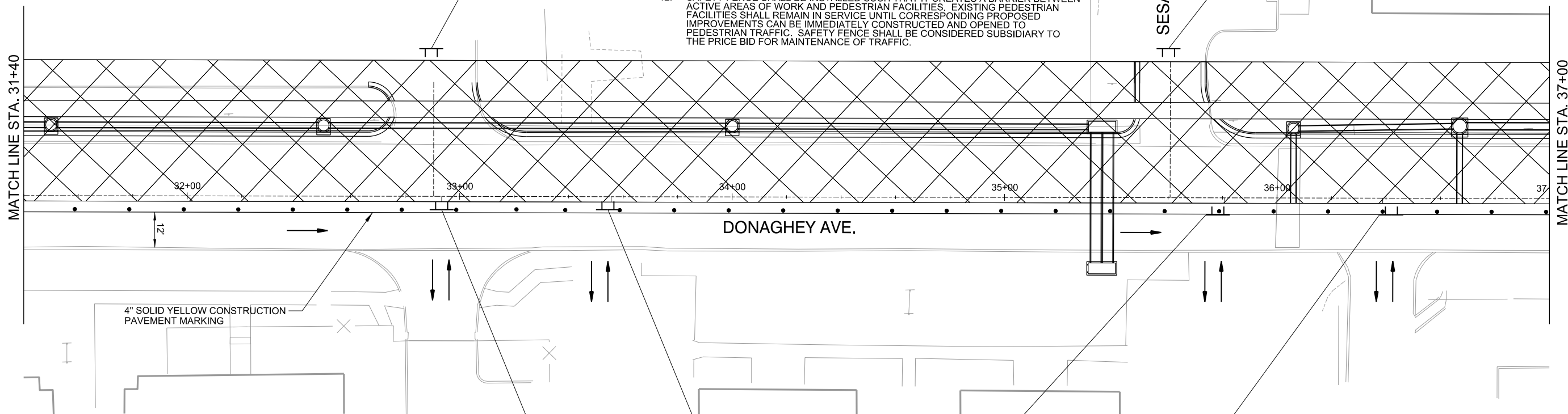
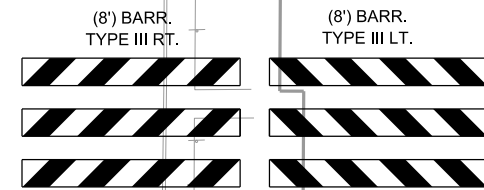
LEGEND

- STAGE 1 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF TRAFFIC PLAN - STAGE 1 (SHEET 3 OF 12)

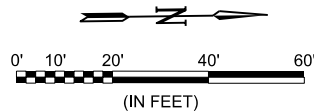
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DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
C-403

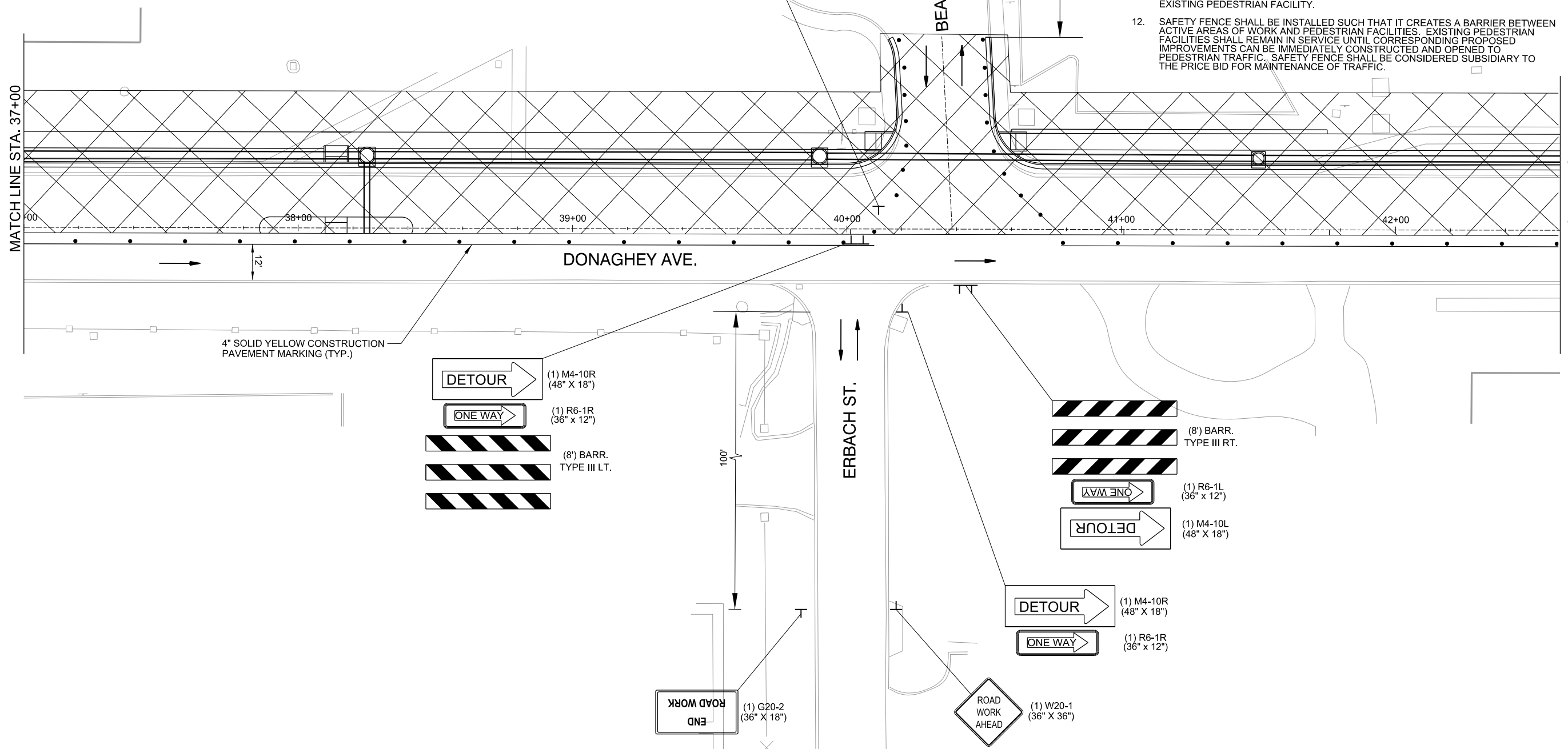
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LEGEND

- STAGE 1 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM



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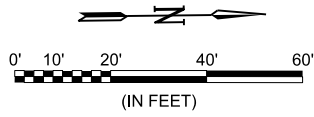
CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF TRAFFIC PLAN - STAGE 1 (SHEET 4 OF 12)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
C-404
SHEET NUMBER
40



LEGEND

- STAGE 1 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM

- (1) M4-10R (48" X 18")
- (1) R6-1R (36" X 12")
- (8') BARR. TYPE III LT.

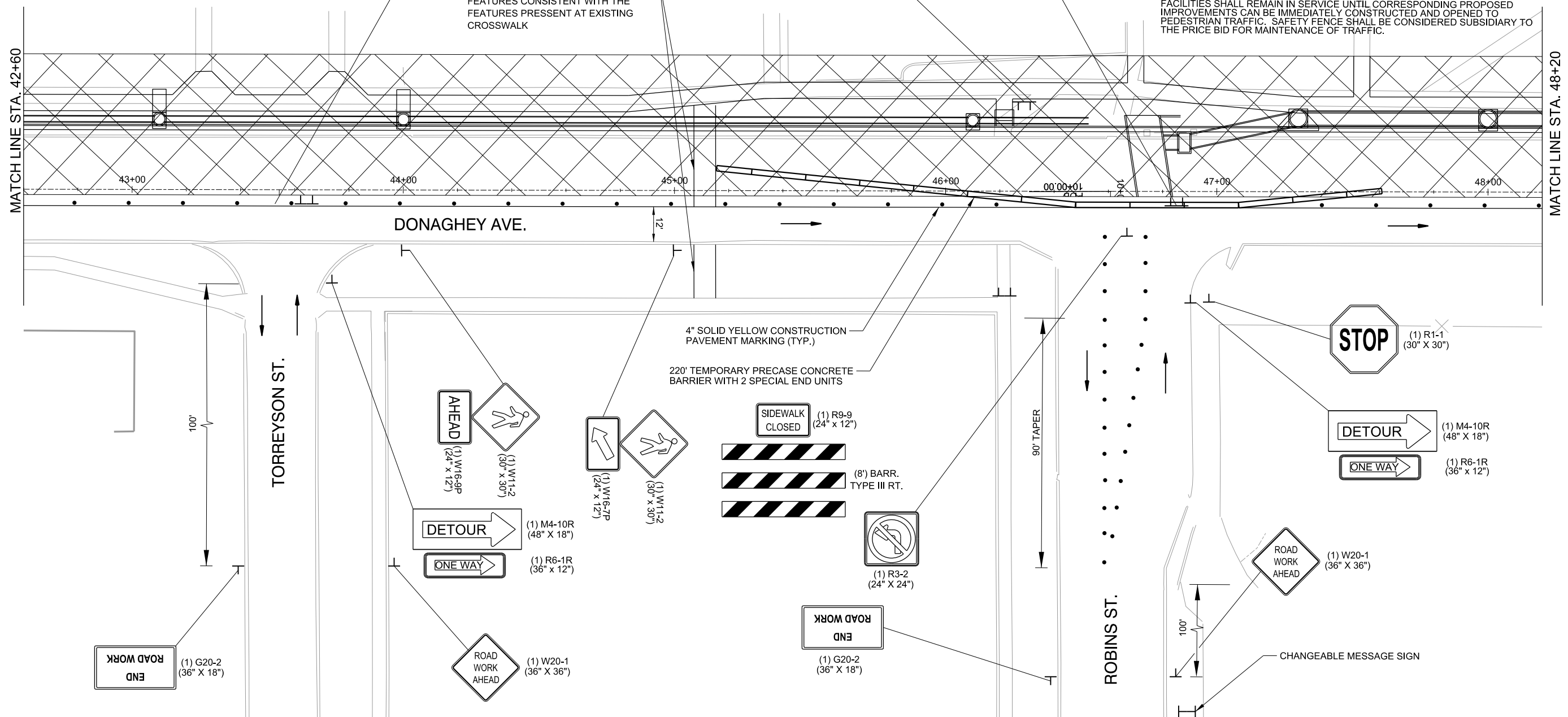
- (8') BARR. TYPE III LT.
- (1) R9-9 (24" X 12")

- (1) M4-10R (48" X 18")
- (1) R6-1R (36" X 12")
- (8') BARR. TYPE III LT.

PROVIDE TEMPORARY DETECTABLE FACILITIES INCLUDING ACCESSIBILITY FEATURES CONSISTENT WITH THE FEATURES PRESENT AT EXISTING CROSSWALK

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CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

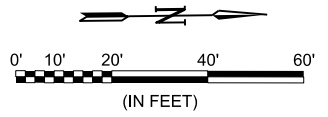
MAINTENANCE OF TRAFFIC PLAN - STAGE 1 (SHEET 5 OF 12)






JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

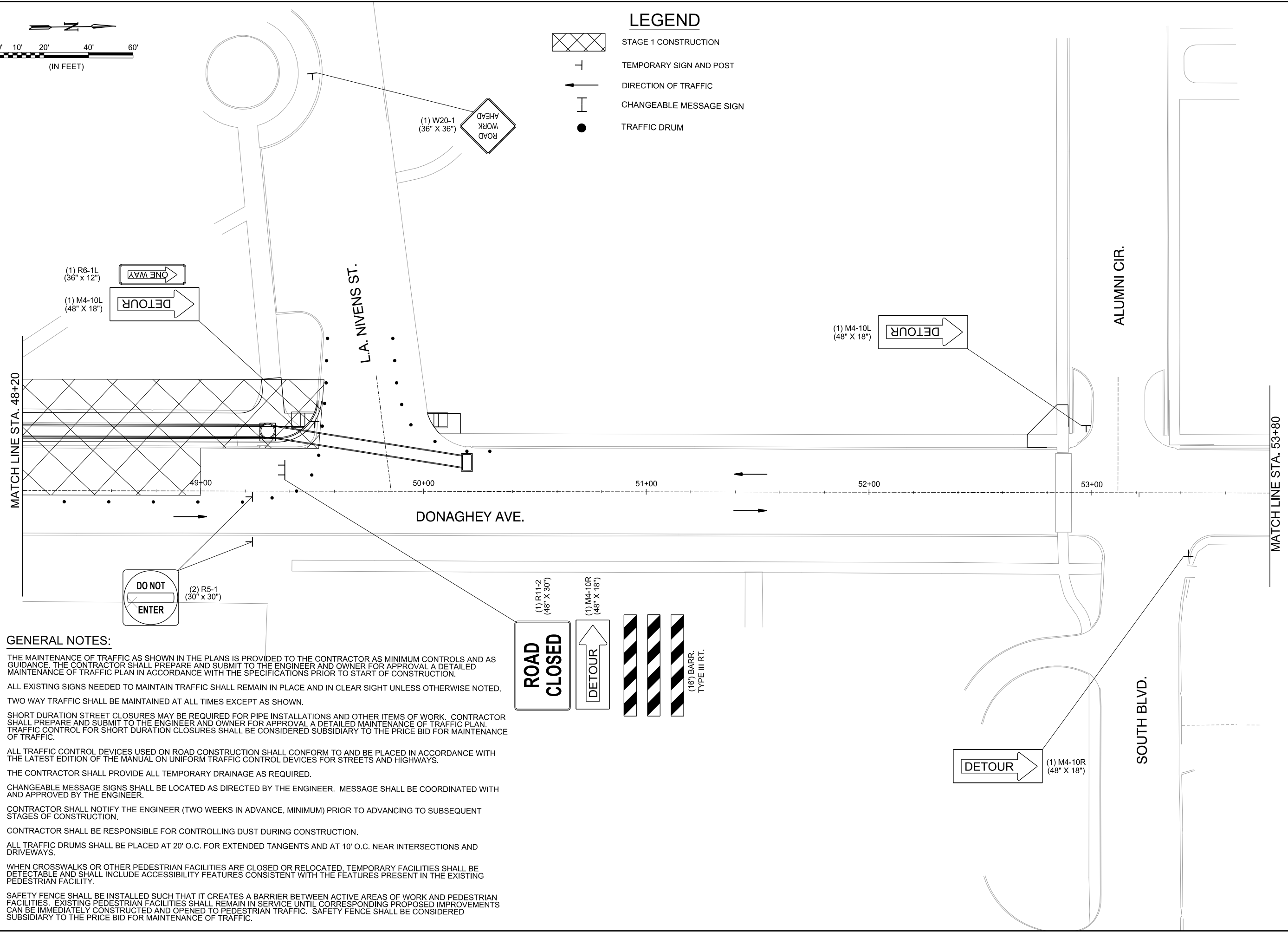
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DRAWING NUMBER
C-405
SHEET NUMBER
41

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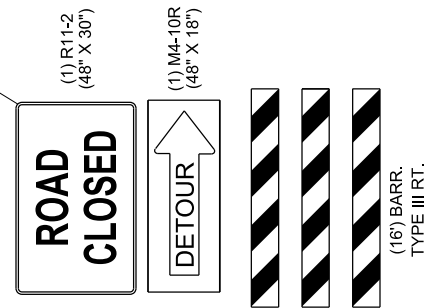


- ### LEGEND
-  STAGE 1 CONSTRUCTION
 -  TEMPORARY SIGN AND POST
 -  DIRECTION OF TRAFFIC
 -  CHANGEABLE MESSAGE SIGN
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 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

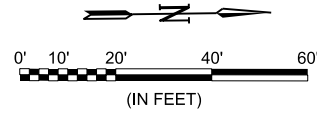
MAINTENANCE OF TRAFFIC PLAN - STAGE 1 (SHEET 6 OF 12)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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 SHEET NUMBER
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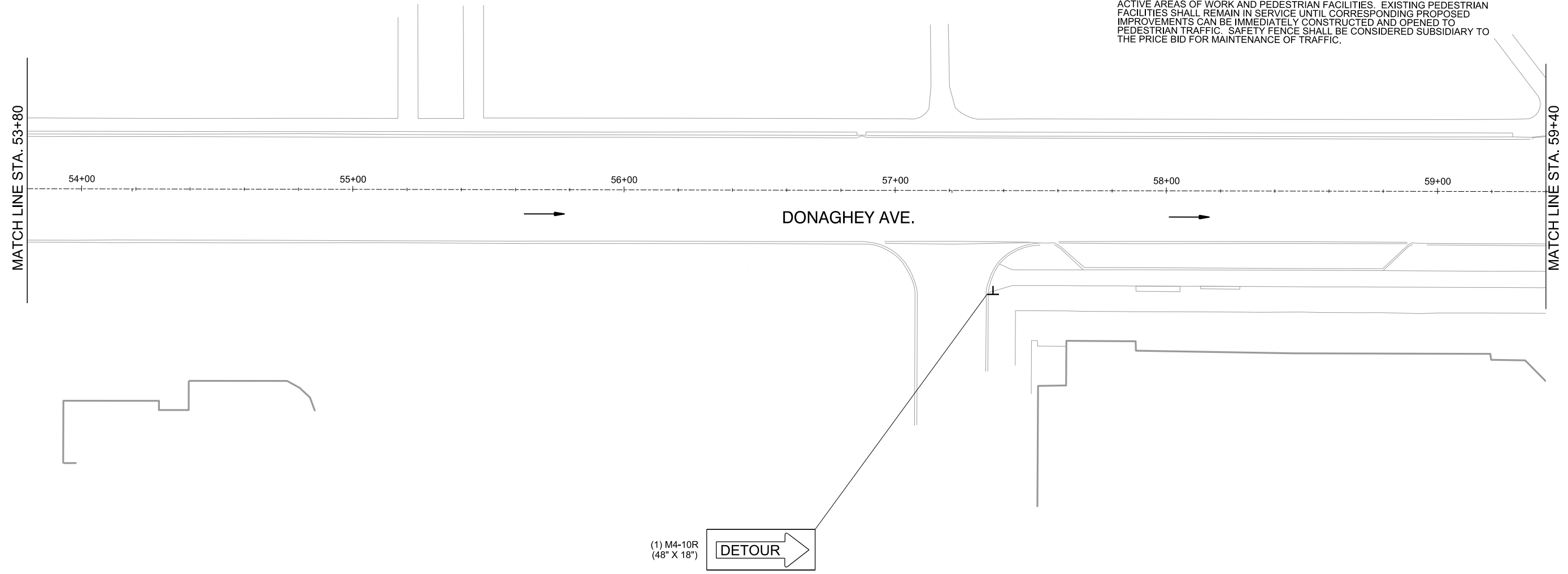


LEGEND

- STAGE 1 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
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CITY OF CONWAY
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PHASE 1
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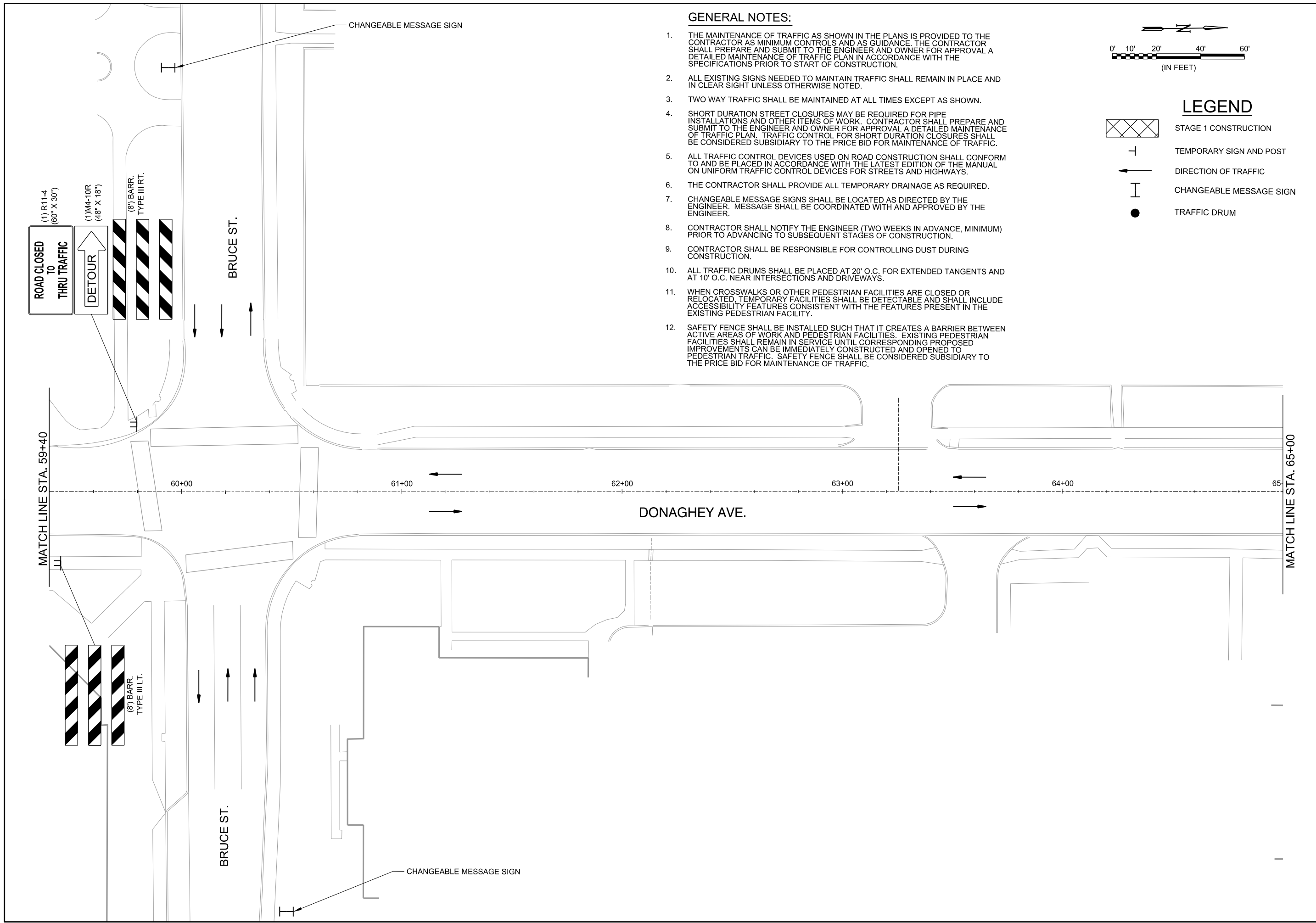
MAINTENANCE OF TRAFFIC PLAN - STAGE 1 (SHEET 7 OF 12)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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SHEET NUMBER
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





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LEGEND

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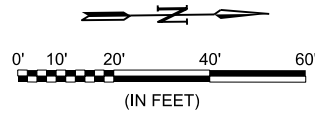
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MAINTENANCE OF TRAFFIC PLAN - STAGE 1 (SHEET 8 OF 12)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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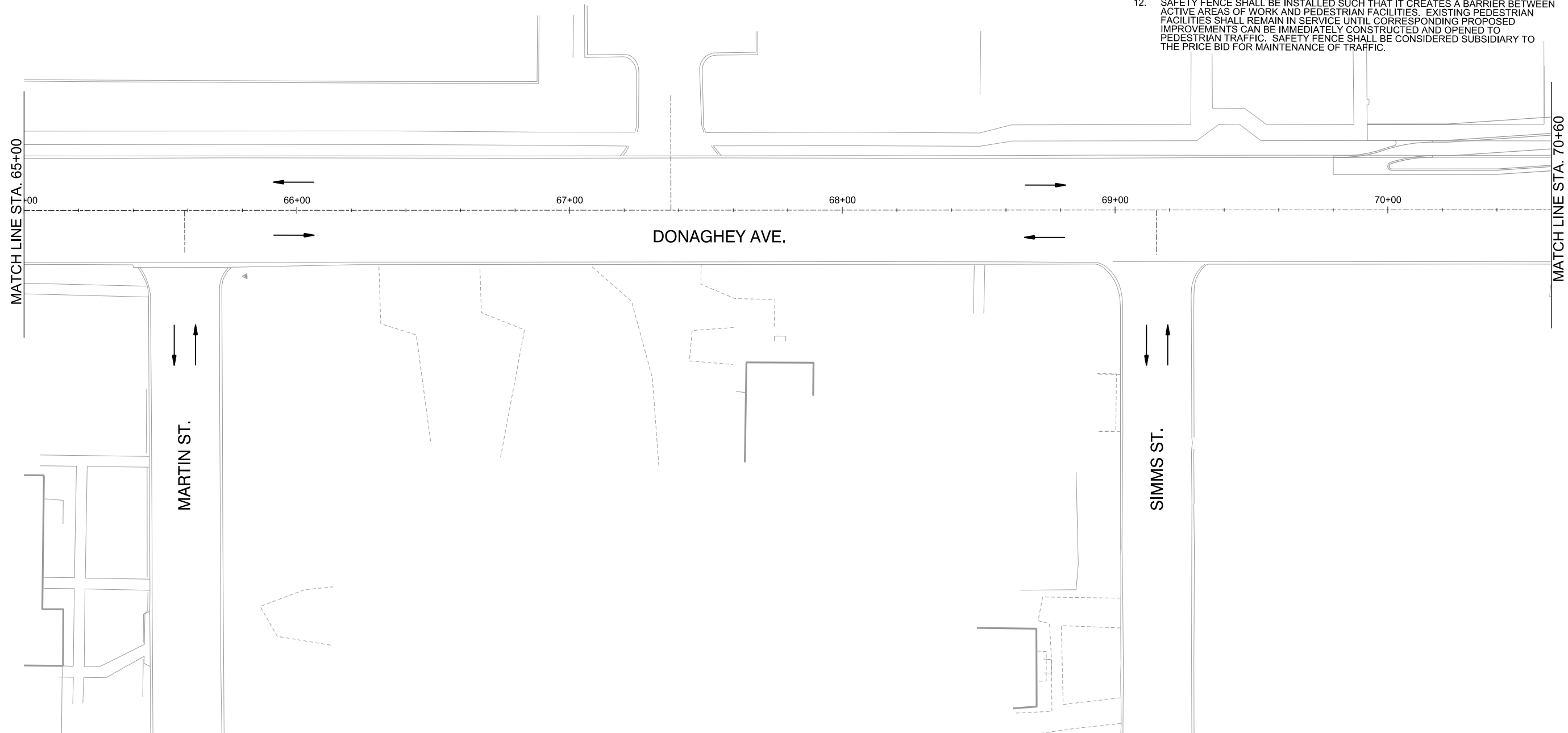


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Digitally Signed 05/23/2020

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF TRAFFIC PLAN - STAGE 1 (SHEET 9 OF 12)

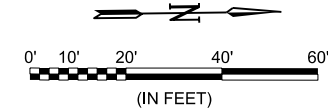
JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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



DRAWING NUMBER
C-409
 SHEET NUMBER **45**

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LEGEND

-  STAGE 1 CONSTRUCTION
-  TEMPORARY SIGN AND POST
-  DIRECTION OF TRAFFIC
-  CHANGEABLE MESSAGE SIGN
-  TRAFFIC DRUM



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Digitally Signed 05/23/2020

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF TRAFFIC PLAN - PHASE STAGE 1 (SHEET 10 OF 12)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
C-410

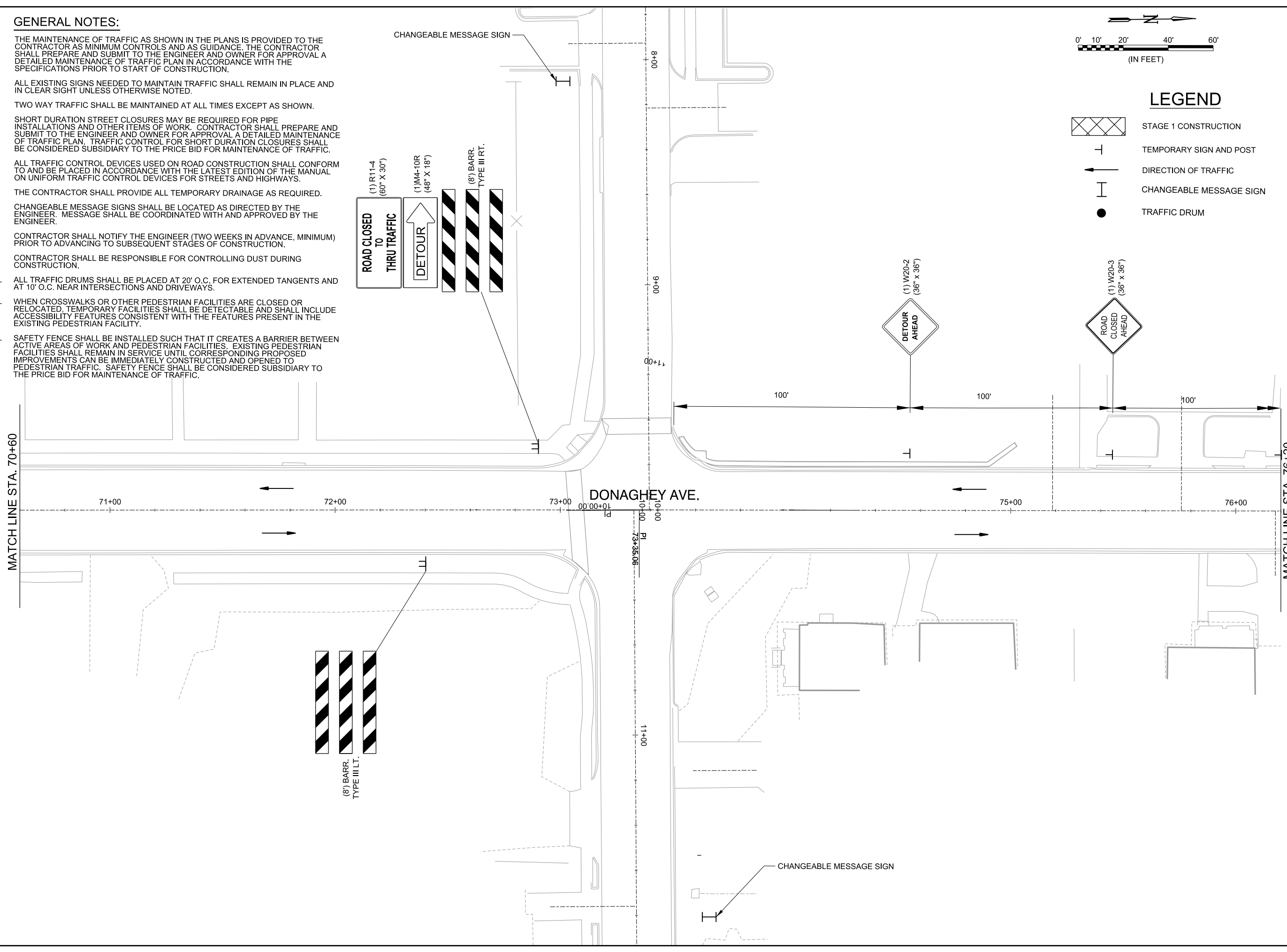
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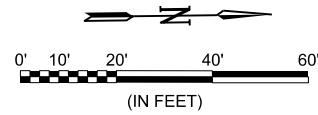
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MATCH LINE STA. 70+60

MATCH LINE STA. 76+20

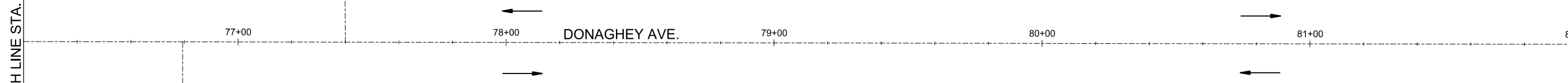
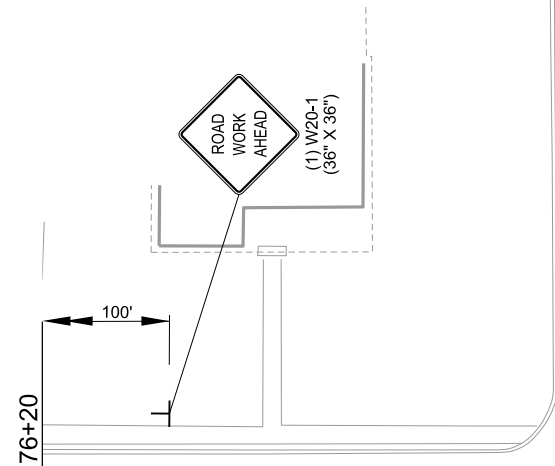
DONAGHEY AVE.





LEGEND

- STAGE 1 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF TRAFFIC PLAN - STAGE 1 (SHEET 11 OF 12)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

BAR IS ONE INCH ON ORIGINAL DRAWING
IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER
C-411
SHEET NUMBER
47



1 **DETOUR**
 (1) M4-9L
 (30" x 24")

2 **DETOUR**
 (1) M4-9R
 (30" x 24")

3 **END
 DETOUR**
 (1) M4-8a
 (24" x 18")

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GARVER
 LLC
 No. 766
 ARKANSAS-ENGINEER

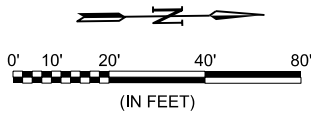
STATE OF
ARKANSAS
Justin L. Tackett
 LICENSED
 PROFESSIONAL
 ENGINEER
 No. 14994
JUSTIN L. TACKETT
 Digitally Signed 05/23/2020

REV.	DATE	DESCRIPTION	BY

CITY OF CONWAY
 CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

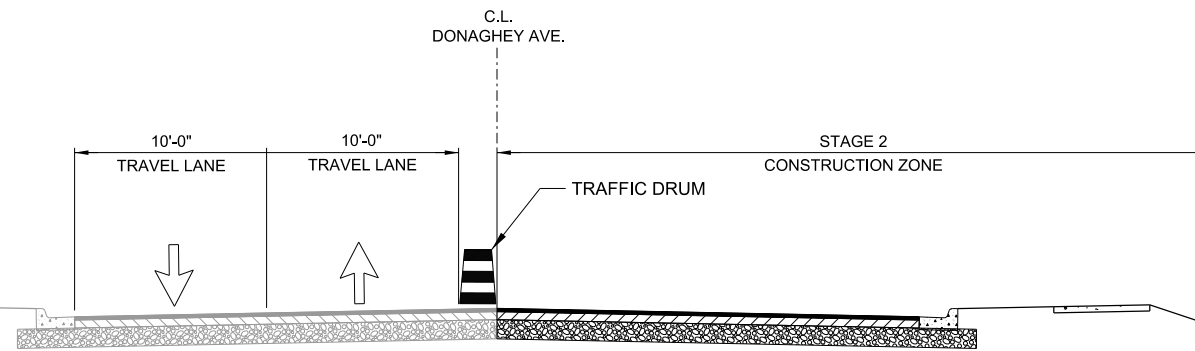
MAINTENANCE OF
 TRAFFIC PLAN -
 STAGE 1
 (SHEET 12 OF 12)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB
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 DRAWING NUMBER
C-412
 SHEET
 NUMBER **48**



LEGEND

- STAGE 2 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM
- VERTICAL PANEL



STAGE 2 TYPICAL SECTION

STAGE 2 CONSTRUCTION SEQUENCE

1. PLACE TRAFFIC CONTROL DEVICES AS SHOWN.
2. OPEN TWO-WAY TRAFFIC UTILIZING COMPLETED STAGE 1 IMPROVEMENTS.
3. CONSTRUCT STAGE 2 IMPROVEMENTS INCLUDING TEMPORARY TRAFFIC SIGNAL, BRUCE ST. TRAFFIC SIGNAL MODIFICATIONS, UNDERCUT EXCAVATION AND BACKFILL, DRAINAGE, AGGREGATE BASE COURSE, ACHM BINDER COURSE, FIRST LIFT OF ACHM SURFACE COURSE, SIDEWALK, AND CYCLE TRACK.

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END ROAD WORK (1) G20-2 (36" X 18")

END ROAD WORK (1) G20-2 (36" X 18")

150' TAPER

4" DOUBLE YELLOW CONSTRUCTION PAVEMENT MARKING
4" SOLID WHITE CONSTRUCTION PAVEMENT MARKING

21+00 22+00 23+00 24+00 25+00

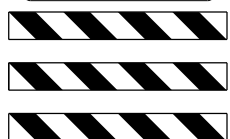
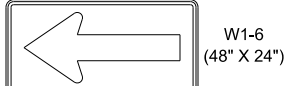
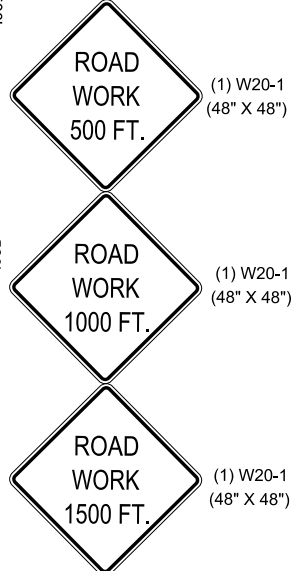
DONAGHEY AVE.

MATCH LINE STA. 25+80

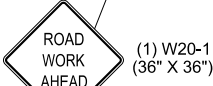
HILLMAN ST.

HIGHWAY 60 (DAVE WARD DRIVE)

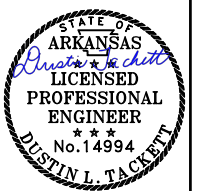
HIGHWAY 60 (DAVE WARD DRIVE)



END ROAD WORK (1) G20-2 (36" X 18")



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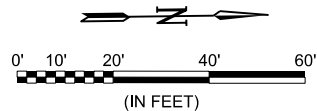
MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 1 OF 13)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
C-413

SHEET NUMBER
49

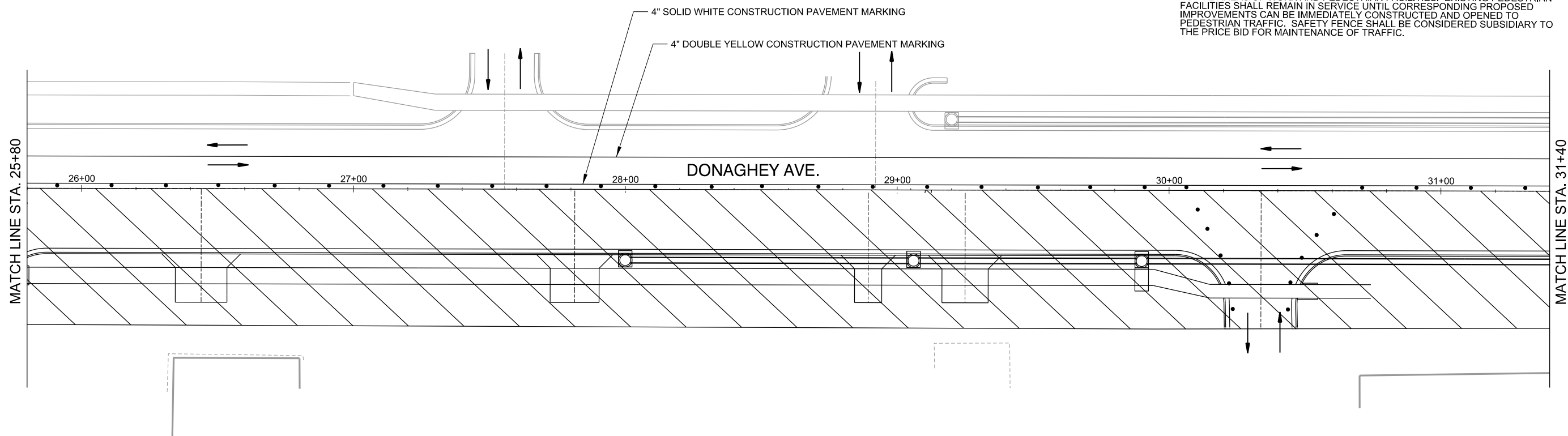


LEGEND

- STAGE 2 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM

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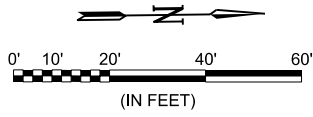
CITY OF CONWAY
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MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 2 OF 13)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

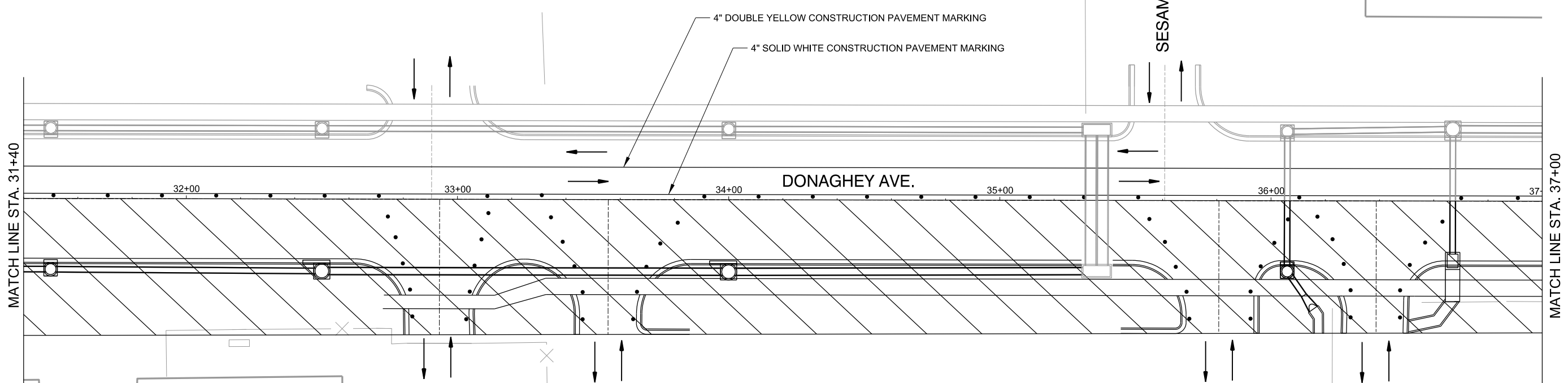
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DRAWING NUMBER
C-414
SHEET NUMBER **50**



LEGEND

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- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM

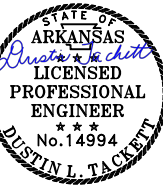


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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

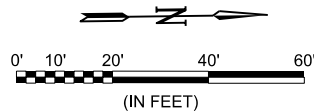
MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 3 OF 13)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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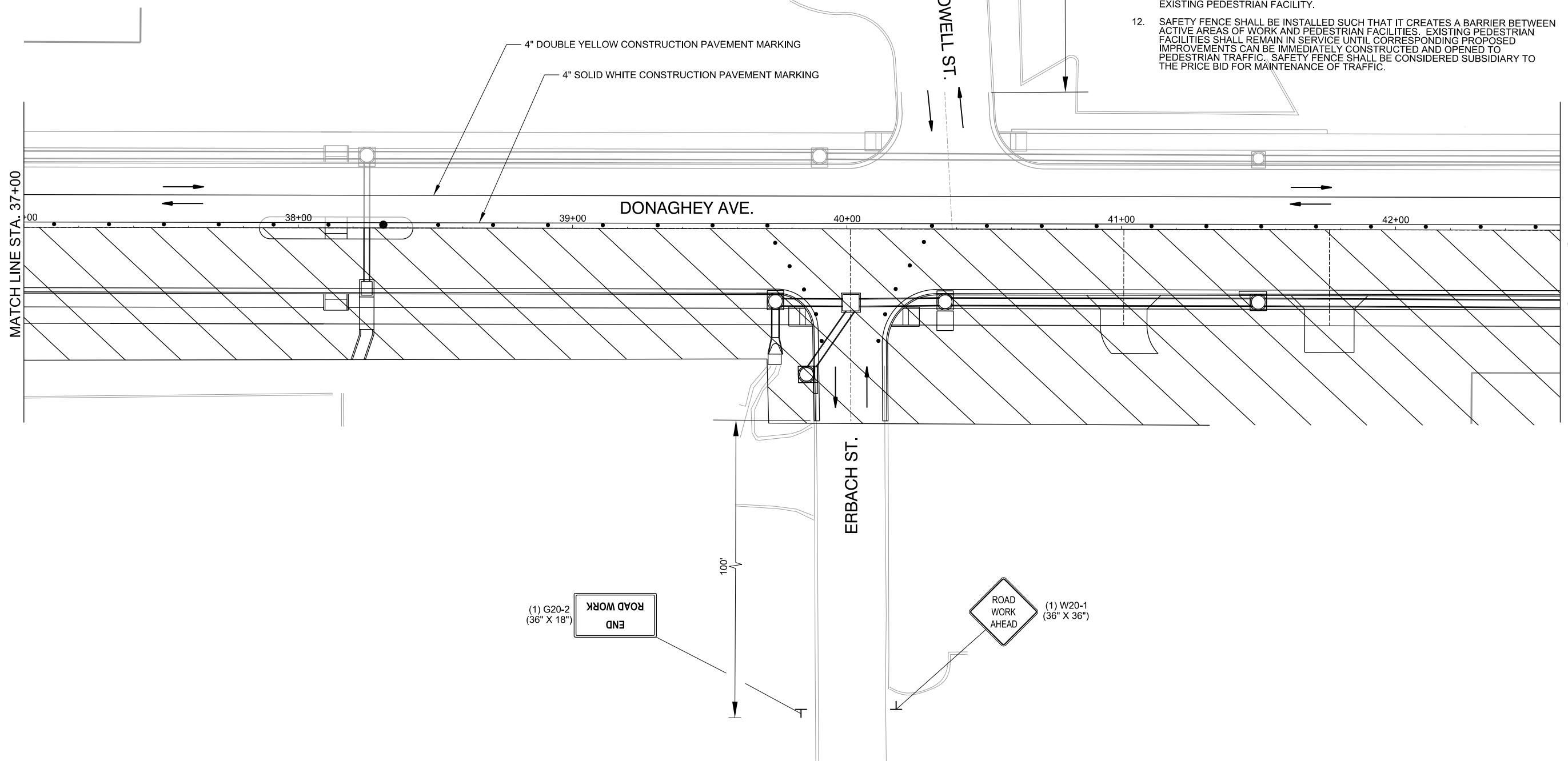
DRAWING NUMBER
C-415

SHEET NUMBER
51



LEGEND

- STAGE 2 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM



GENERAL NOTES:

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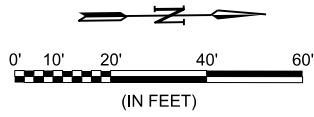
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 4 OF 13)






JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-416
 SHEET NUMBER
52

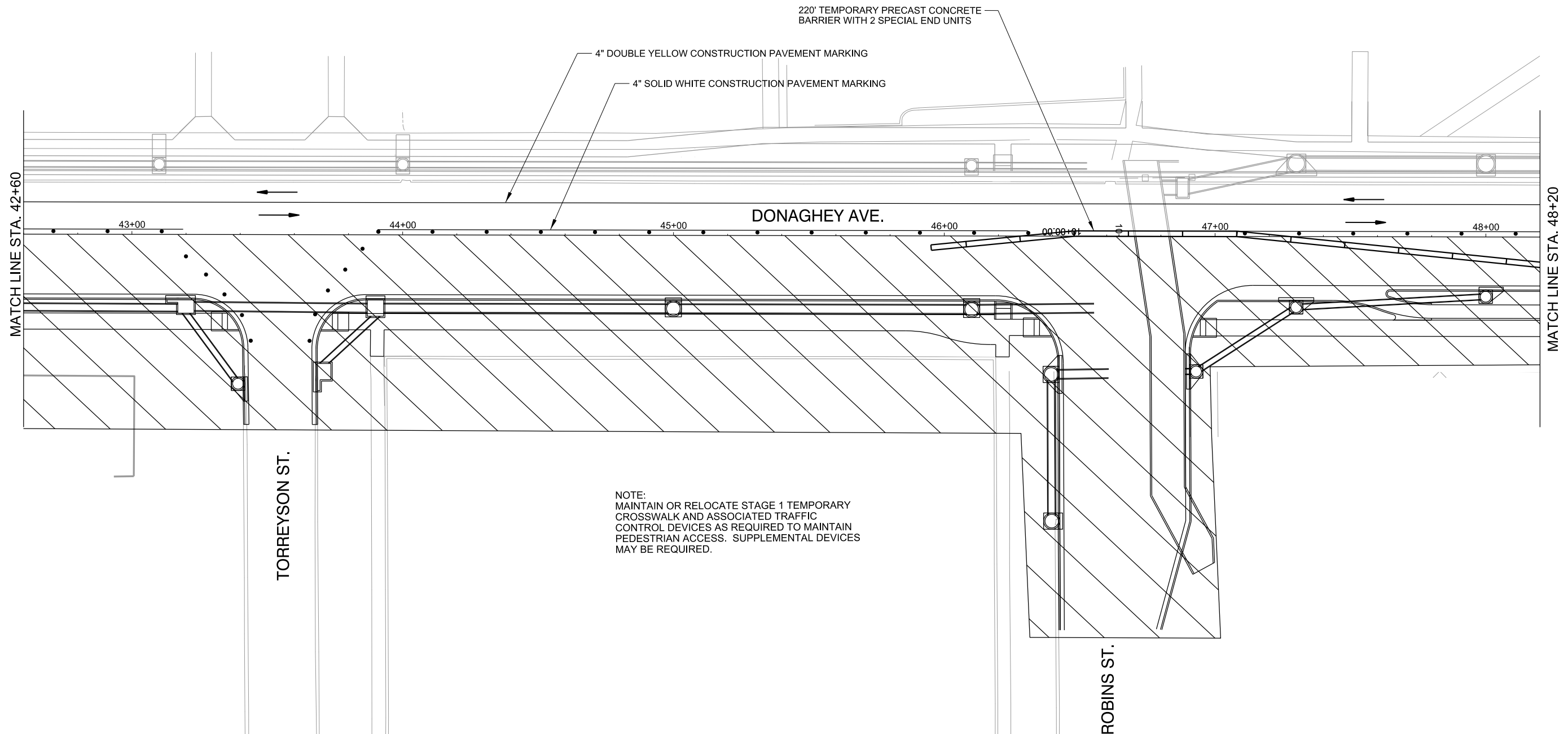


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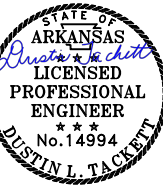
-  STAGE 2 CONSTRUCTION
-  TEMPORARY SIGN AND POST
-  DIRECTION OF TRAFFIC
-  CHANGEABLE MESSAGE SIGN
-  TRAFFIC DRUM

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CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
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(DAVE WARD DR. TO ADA AVE.)

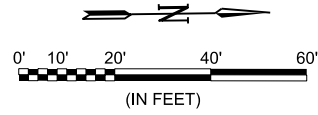
MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 5 OF 13)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
C-417

SHEET NUMBER
53

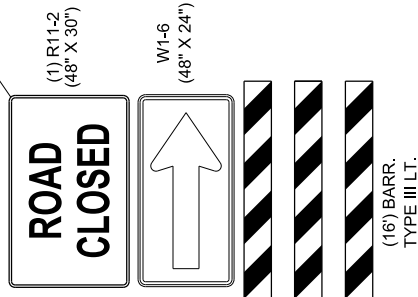
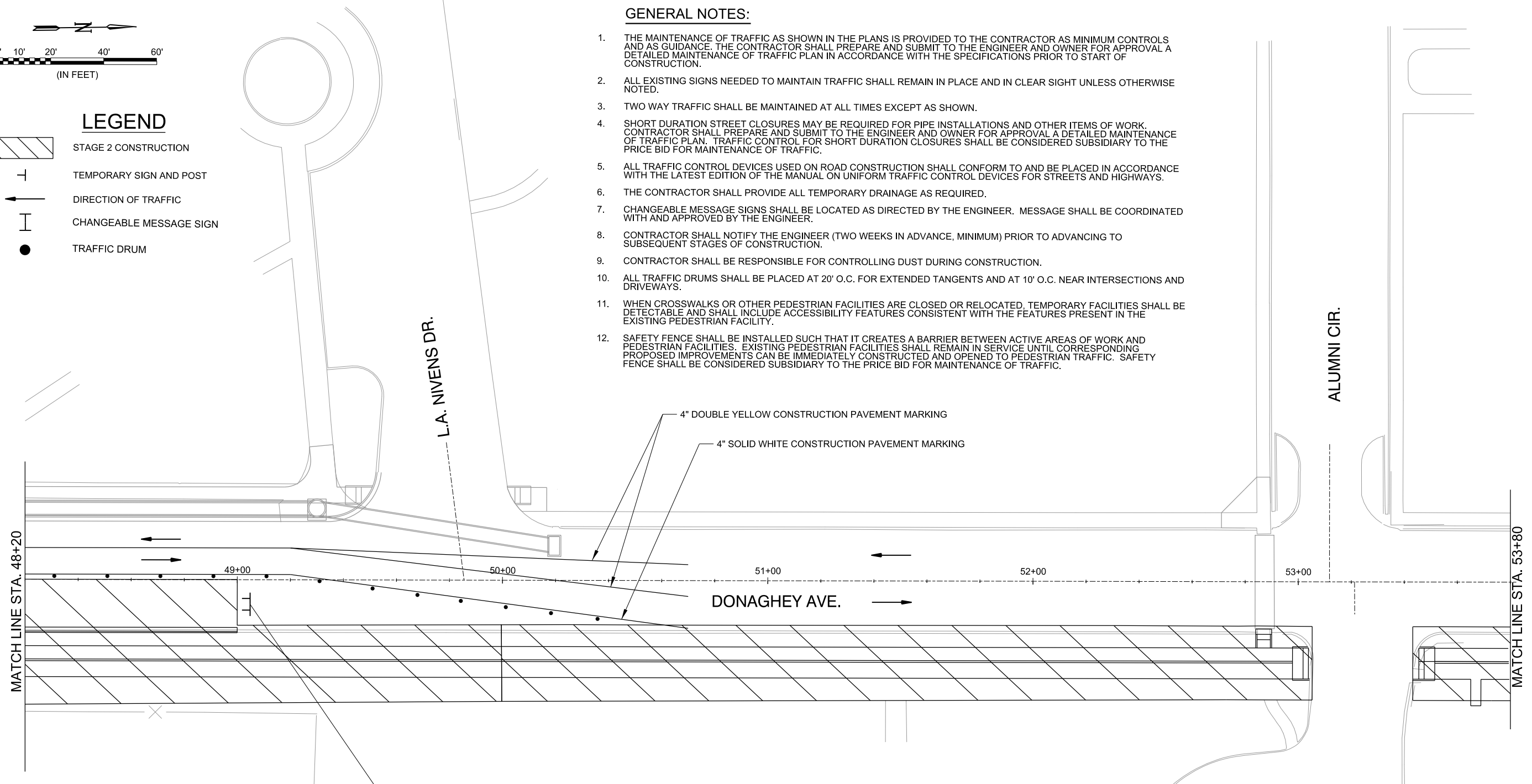


LEGEND

- STAGE 2 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM

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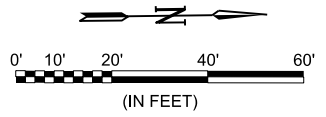
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 6 OF 13)






JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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C-418
 SHEET NUMBER
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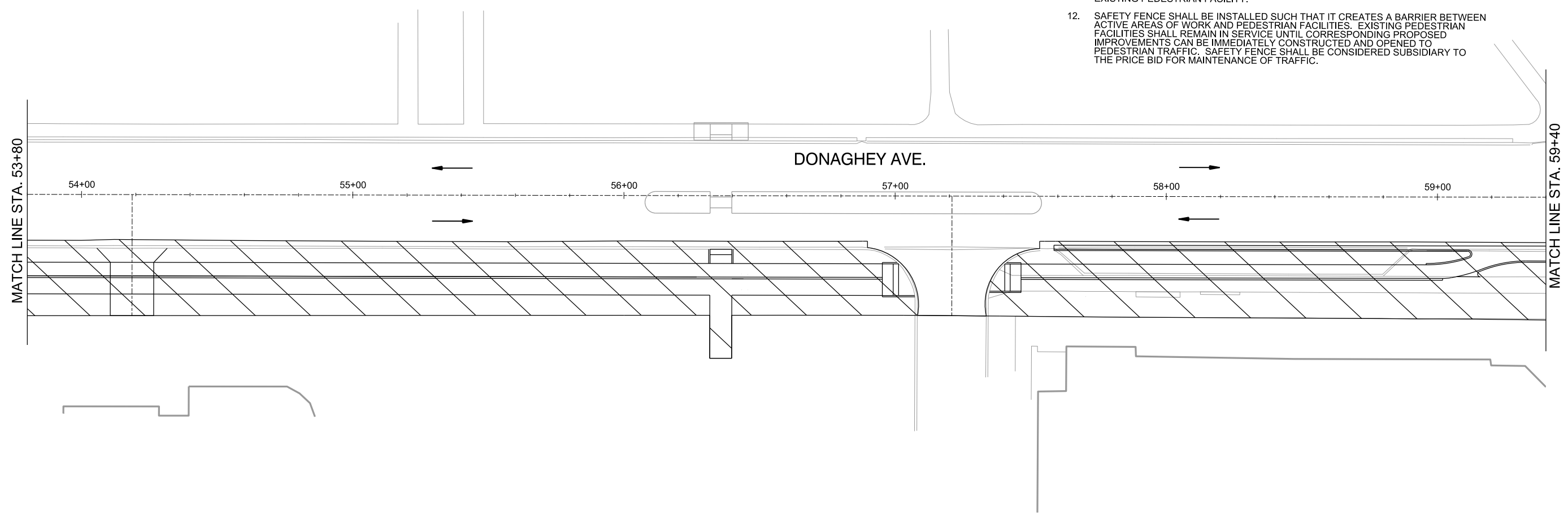


LEGEND

-  STAGE 2 CONSTRUCTION
-  TEMPORARY SIGN AND POST
-  DIRECTION OF TRAFFIC
-  CHANGEABLE MESSAGE SIGN
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CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 7 OF 13)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

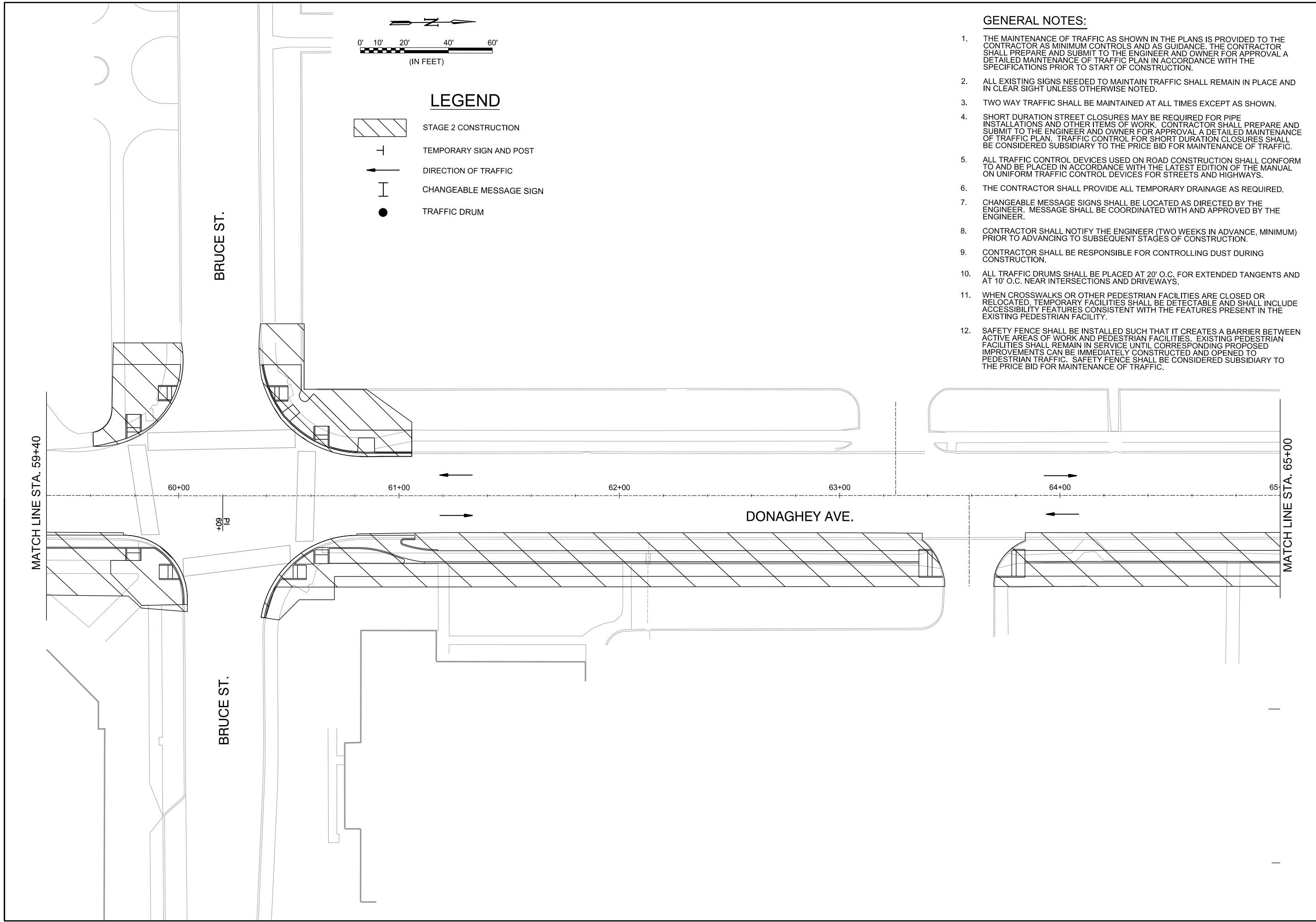
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C-419

SHEET NUMBER
55

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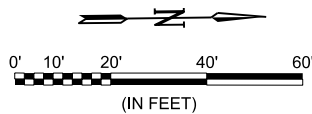
CITY OF CONWAY
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MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 8 OF 13)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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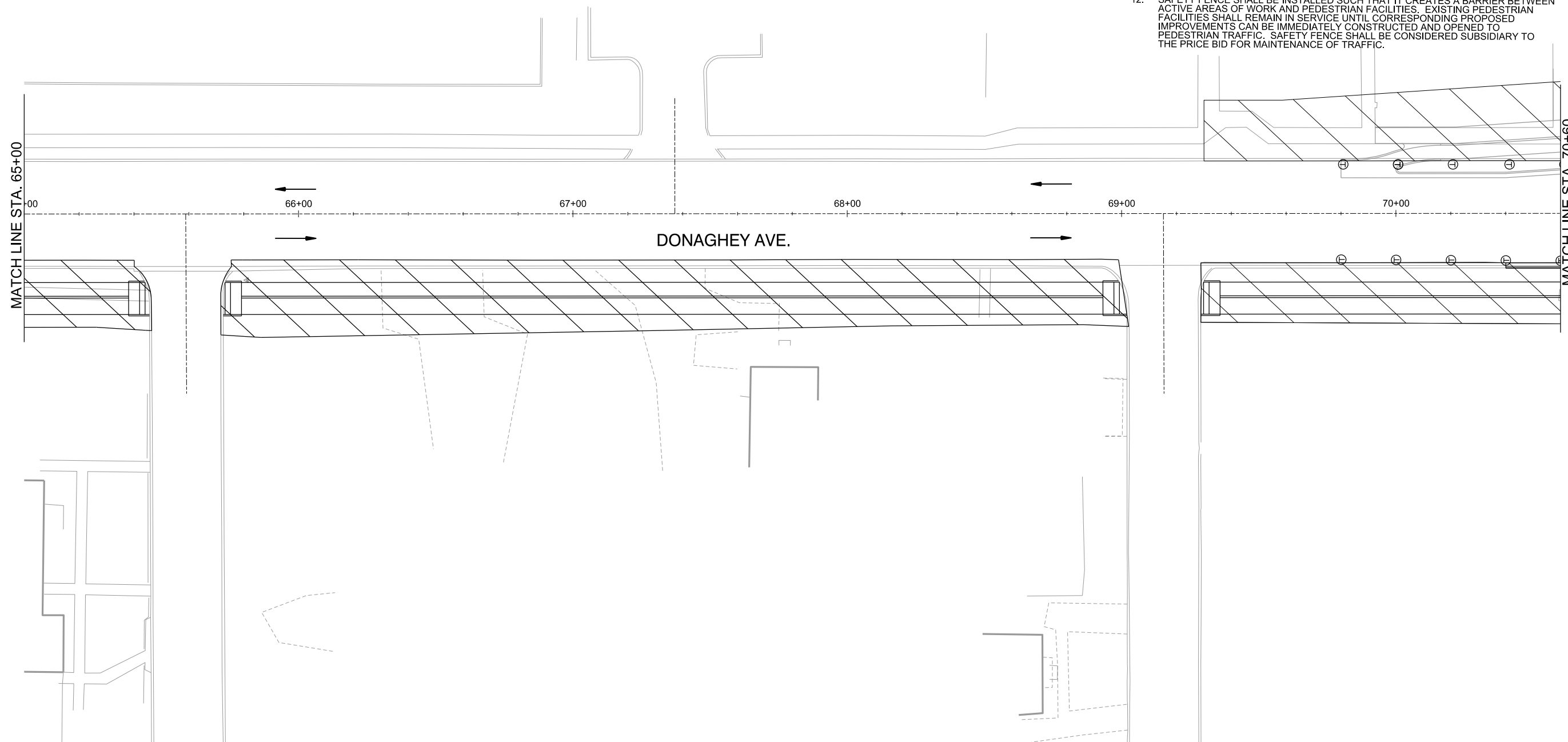


LEGEND

- STAGE 2 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
- TRAFFIC DRUM
- VERTICAL PANEL

GENERAL NOTES:

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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

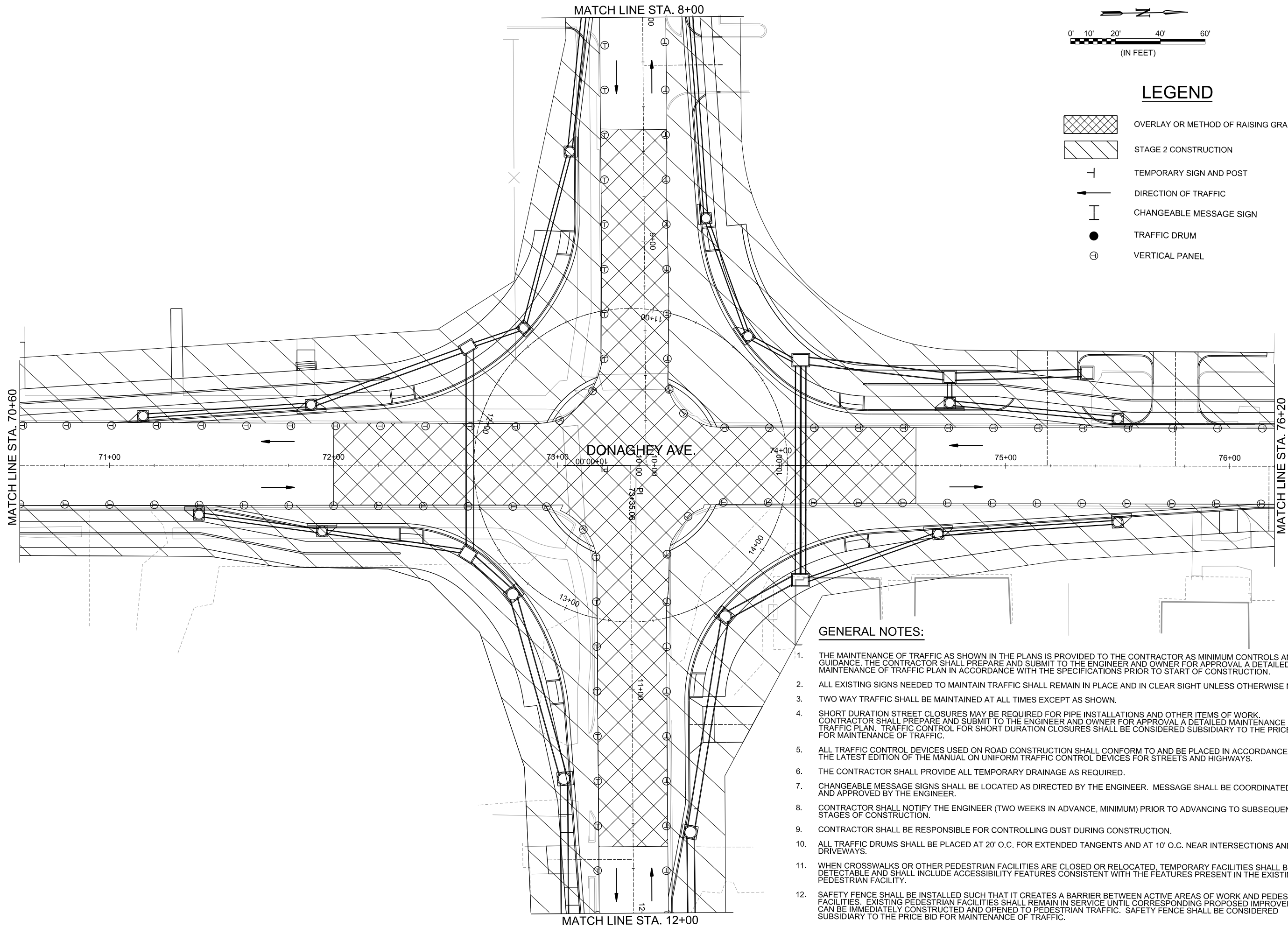
MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 9 OF 13)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB








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C-421
 SHEET NUMBER **57**

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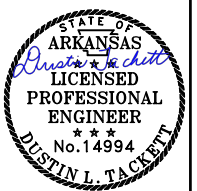
-  OVERLAY OR METHOD OF RAISING GRADE
-  STAGE 2 CONSTRUCTION
-  TEMPORARY SIGN AND POST
-  DIRECTION OF TRAFFIC
-  CHANGEABLE MESSAGE SIGN
-  TRAFFIC DRUM
-  VERTICAL PANEL

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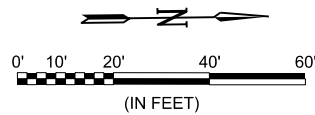
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 10 OF 13)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-422
 SHEET NUMBER
58

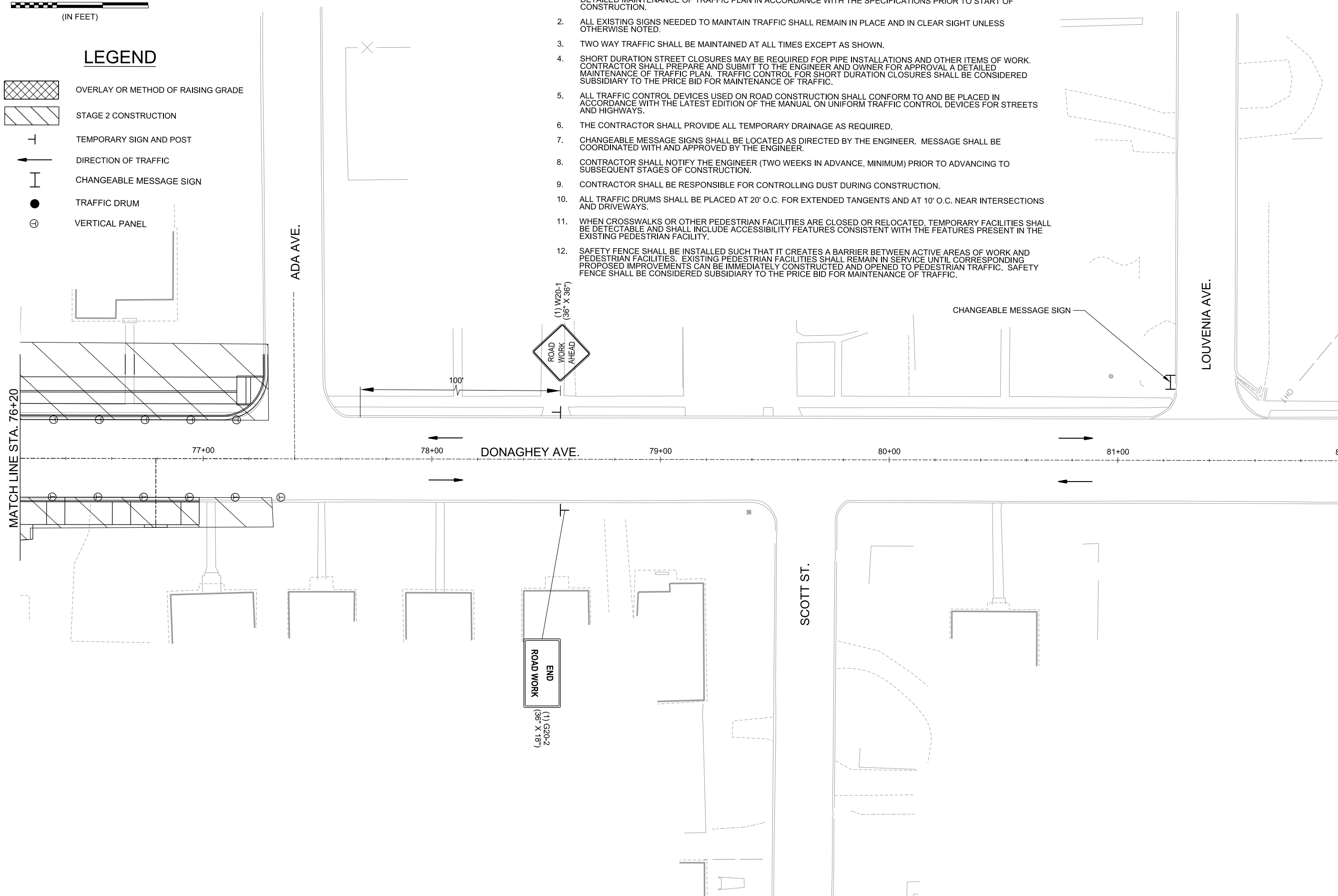


LEGEND

- OVERLAY OR METHOD OF RAISING GRADE
- STAGE 2 CONSTRUCTION
- TEMPORARY SIGN AND POST
- DIRECTION OF TRAFFIC
- CHANGEABLE MESSAGE SIGN
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Justin L. TACKETT
No. 14994
* * *

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CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1
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MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 11 OF 13)

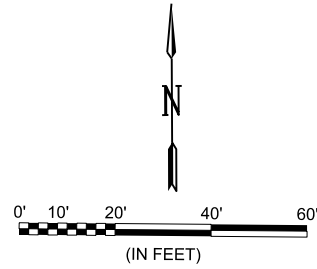
JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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


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SHEET NUMBER
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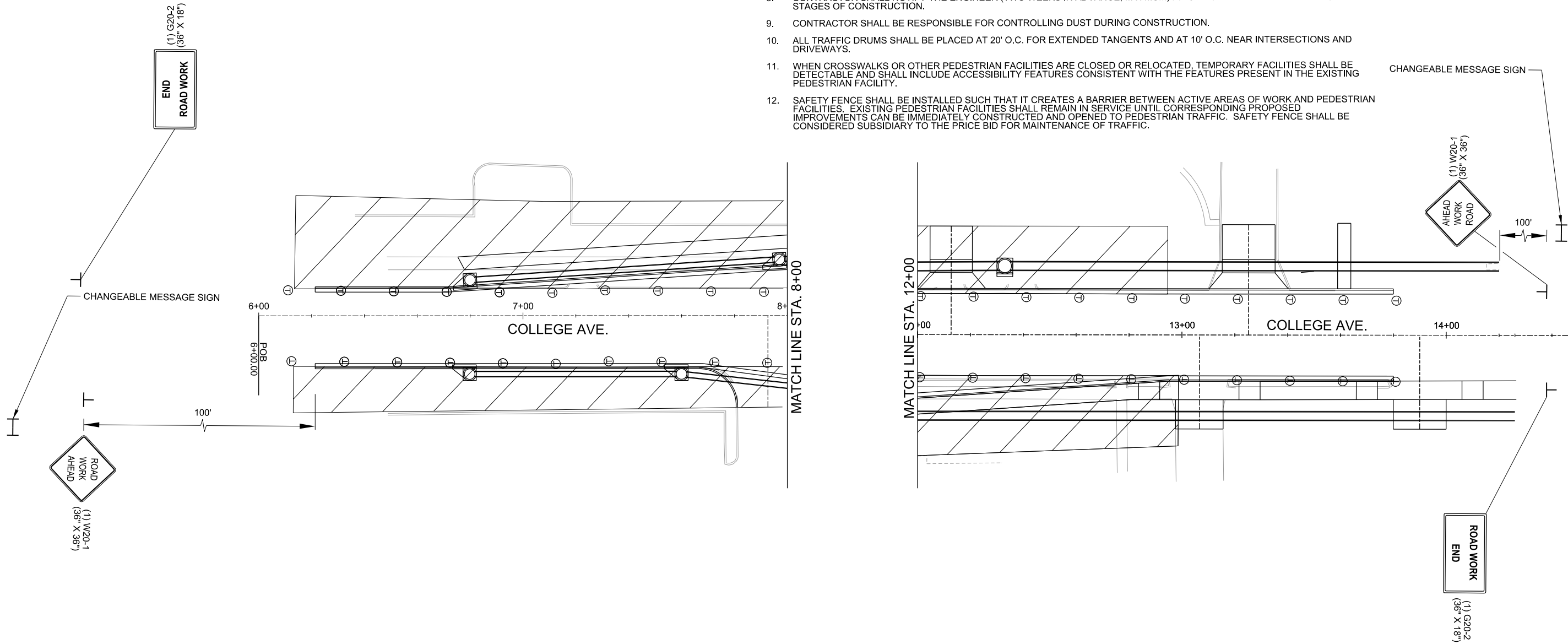


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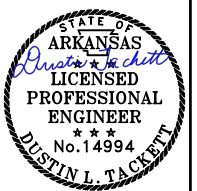
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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

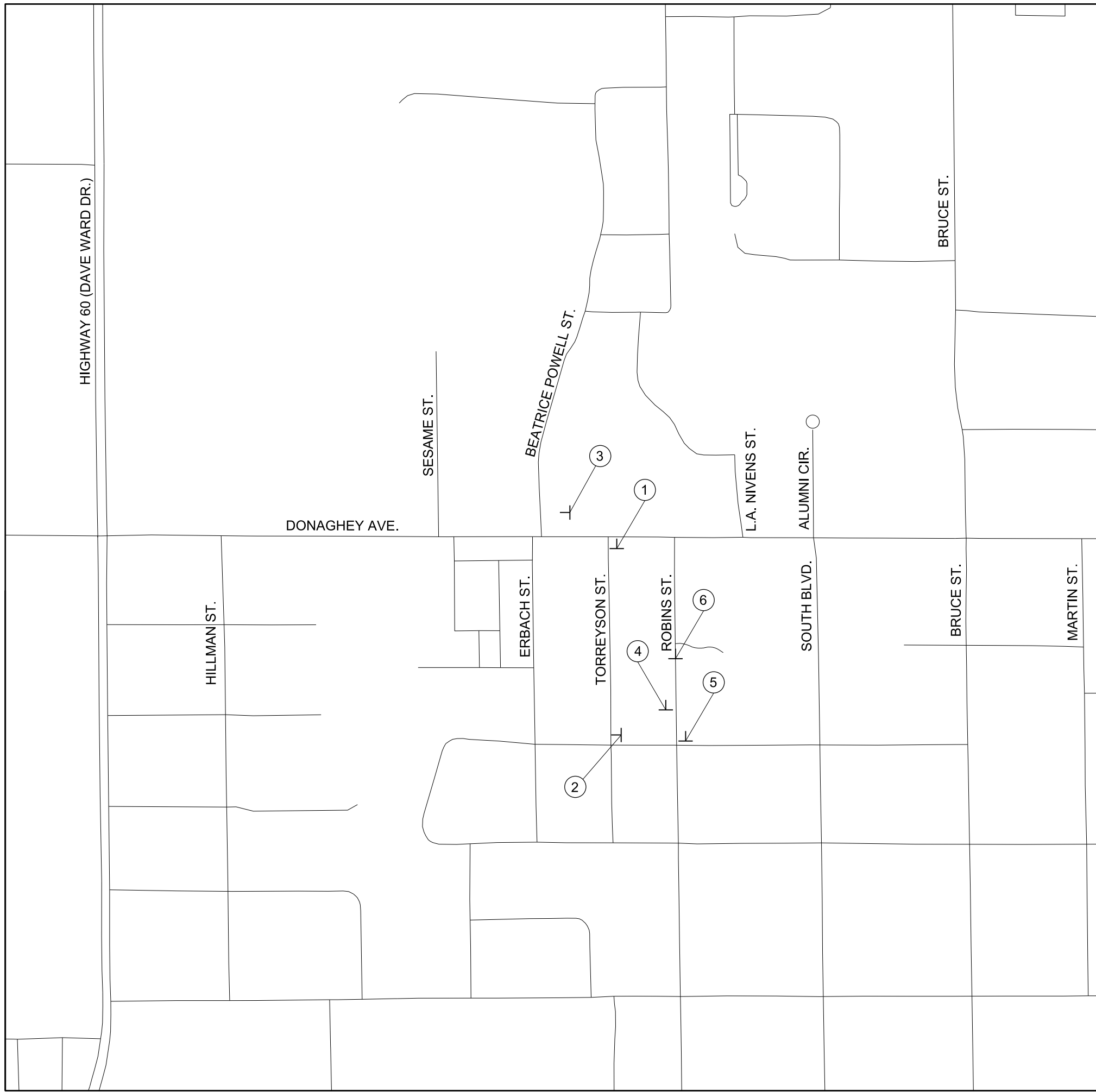
MAINTENANCE OF TRAFFIC PLAN - STAGE 2 (SHEET 12 OF 13)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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C-424

SHEET NUMBER
60



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2 — (1) M4-9R (30" x 24")

3 — (1) M4-8a (24" x 18")

4 — (8') BARR. TYP. III LT.

5 — (1) R11-4 (60" x 30")

 (1) M4-10L (48" x 18")

 (8') BARR. TYP. III RT.

6 — (1) R11-2 (48" x 30")

 (8') BARR. TYP. III LT.

 (8') BARR. TYP. III RT.



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CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF
 TRAFFIC PLAN -
 STAGE 2
 (SHEET 13 OF 13)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

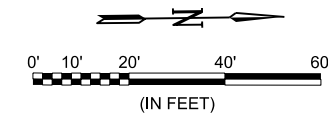
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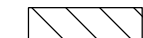




SHEET NUMBER
61

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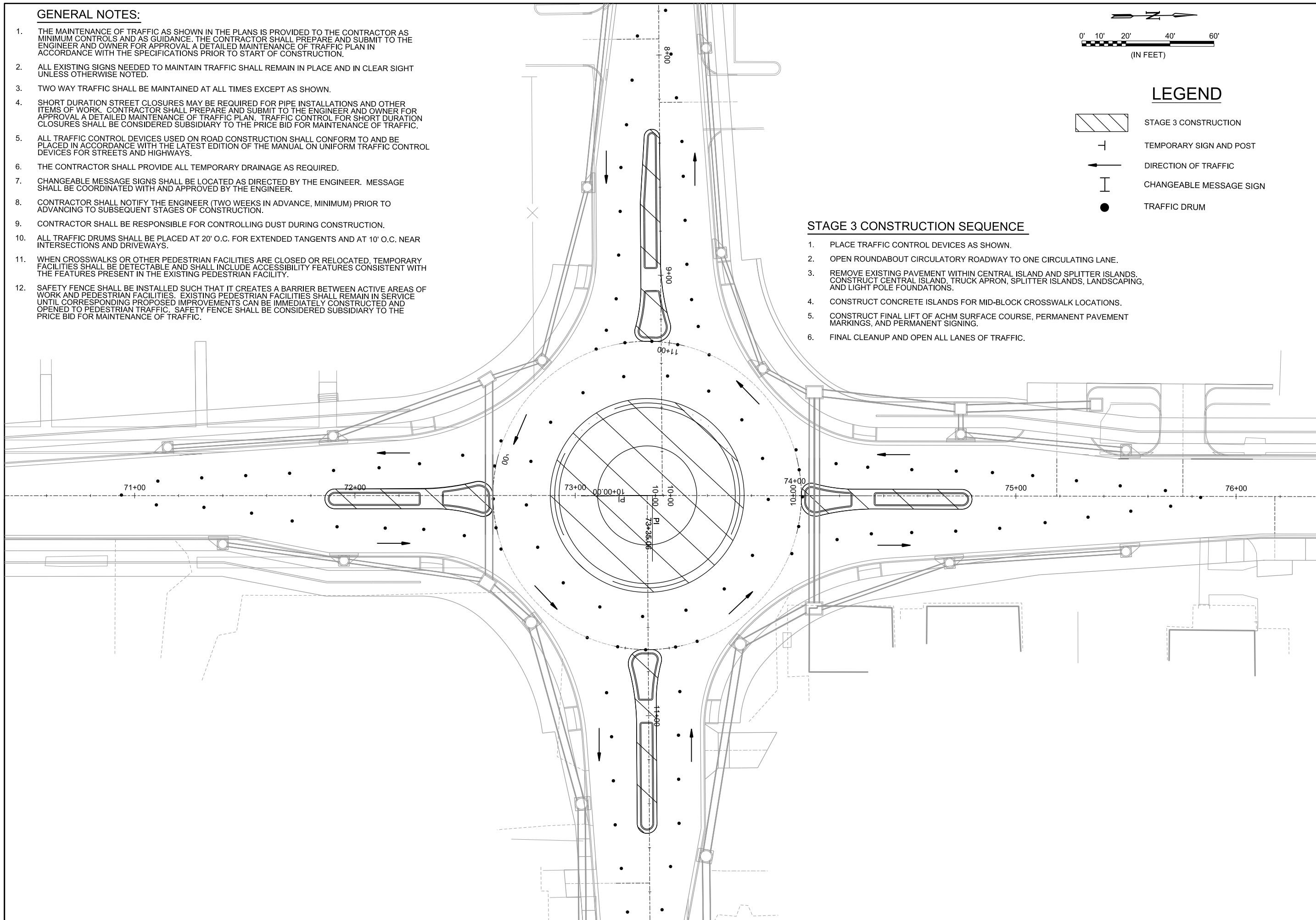



LEGEND

-  STAGE 3 CONSTRUCTION
-  TEMPORARY SIGN AND POST
-  DIRECTION OF TRAFFIC
-  CHANGEABLE MESSAGE SIGN
-  TRAFFIC DRUM


STAGE 3 CONSTRUCTION SEQUENCE

1. PLACE TRAFFIC CONTROL DEVICES AS SHOWN.
2. OPEN ROUNDABOUT CIRCULATORY ROADWAY TO ONE CIRCULATING LANE.
3. REMOVE EXISTING PAVEMENT WITHIN CENTRAL ISLAND AND SPLITTER ISLANDS. CONSTRUCT CENTRAL ISLAND, TRUCK APRON, SPLITTER ISLANDS, LANDSCAPING, AND LIGHT POLE FOUNDATIONS.
4. CONSTRUCT CONCRETE ISLANDS FOR MID-BLOCK CROSSWALK LOCATIONS.
5. CONSTRUCT FINAL LIFT OF ACHM SURFACE COURSE, PERMANENT PAVEMENT MARKINGS, AND PERMANENT SIGNING.
6. FINAL CLEANUP AND OPEN ALL LANES OF TRAFFIC.






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STATE OF ARKANSAS
Justin L. Tackett
LICENSED PROFESSIONAL ENGINEER
No. 14994
JUSTIN L. TACKETT
Digitally Signed 05/23/2020

REV.	DATE	DESCRIPTION	BY




CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

MAINTENANCE OF TRAFFIC PLAN - STAGE 3

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
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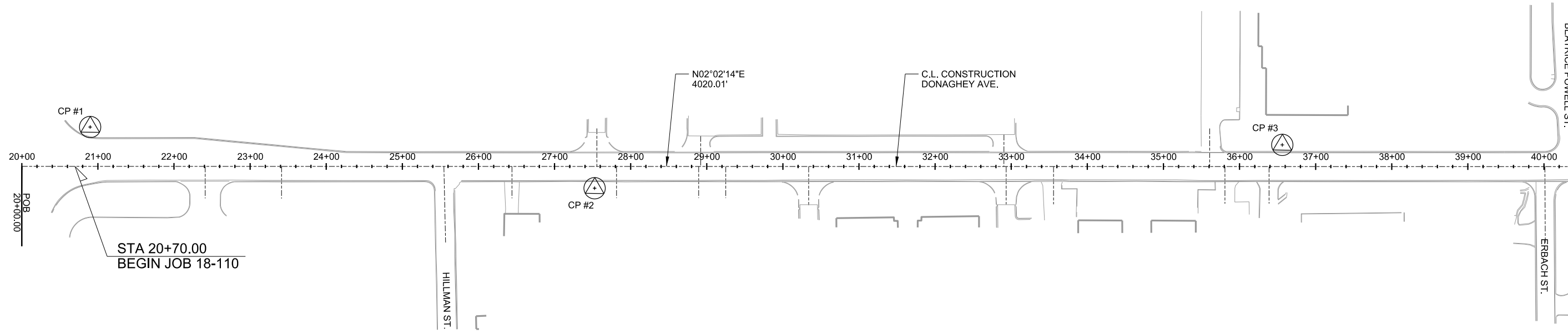
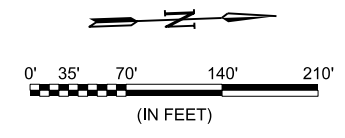
SHEET NUMBER
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 PROFESSIONAL SERVICES
 AGREEMENT FOR THIS WORK.



STATE OF ARKANSAS
Justin L. Tackett
 LICENSED PROFESSIONAL ENGINEER
 No. 14994
 JUSTIN L. TACKETT
 Digitally Signed 05/23/2020



SURVEY CONTROL POINTS						
POINT No.	NORTHING	EASTING	STATION	OFFSET	ELEVATION	DESCRIPTION
1	268579.35727	1176270.22811	20+89.81	51.74' LT.	306.51	1/2" REBAR WITH 3 STAKES SURROUNDING
2	269238.89738	1176374.08496	27+52.63	28.61' RT.	308.74	1/2" REBAR WITH 3 STAKES SURROUNDING
3	270143.89972	1176349.13759	36+56.17	28.50' LT	303.55	1/2" REBAR WITH 3 STAKES SURROUNDING
4	271098.63418	1176435.90068	46+13.38	24.27' RT	300.52	1/2" REBAR WITH 3 STAKES SURROUNDING
5	271779.38715	1176464.38400	52+94.72	28.54' RT	302.89	1/2" REBAR WITH 3 STAKES SURROUNDING
6	272549.94944	1176427.60202	60+63.57	35.51' LT	307.93	1/2" REBAR WITH 3 STAKES SURROUNDING
7	273063.03436	1176512.10435	65+79.18	31.89' RT	309.43	1/2" REBAR WITH 3 STAKES SURROUNDING
8	273432.30308	1176518.43294	69+48.46	25.94' RT	310.30	1/2" REBAR WITH 3 STAKES SURROUNDING
9	273851.34760	1176475.83920	73+65.79	30.63' LT	310.97	1/2" REBAR WITH 3 STAKES SURROUNDING
10	274422.04432	1176549.70055	79+38.74	22.98' RT	311.60	1/2" REBAR WITH 3 STAKES SURROUNDING
11	275075.52547	1176520.69057	85+90.78	29.14' LT	312.10	1/2" REBAR WITH 3 STAKES SURROUNDING
12	275834.21263	1176619.75972	93+52.50	43.01' RT	314.73	1/2" REBAR WITH 3 STAKES SURROUNDING
13	276416.95814	1176524.31548	99+31.50	73.00' LT	315.44	1/2" REBAR WITH 3 STAKES SURROUNDING

ROBINS ST. COORDINATES			
STATION	TYPE	NORTHING	EASTING
10+00.00	POB	271146.8680	1176413.3268
13+00.00	POE	271137.9334	1176713.1938

COLLEGE AVE. (WEST) COORDINATES			
STATION	TYPE	NORTHING	EASTING
6+00.00	POB	273834.4143	1176105.6575
10+00.00	POE	273824.0510	1176505.5233

COLLEGE AVE. (EAST) COORDINATES			
STATION	TYPE	NORTHING	EASTING
10+00.00	POB	273817.1683	1176505.2846
15+00.00	POE	273803.6179	1177005.1010

DONAGHEY AVE. COORDINATES			
STATION	TYPE	NORTHING	EASTING
20+00.00	POB	268487.7641	1176318.7430
60+20.01	PI	272505.2334	1176461.6437
73+35.06	PI	273819.5536	1176505.3640
99+61.50	PI	276444.3547	1176598.3343
103+00.00	POE	276782.5488	1176612.6220

B.L. ROUNDABOUT COORDINATES			
STATION	TYPE	NORTHING	EASTING
10+00.00	PC	273887.1246	1176507.7575
12+19.91	PCC	273747.2120	1176502.8117
14+39.82	PT	273887.1246	1176507.7575

COORDINATES FOR ALL POINTS ARE GRID
 HORIZONTAL DATUM: NAD 83 (1997) HARN
 VERTICAL DATUM: NAVD 88

BASIS OF BEARINGS:
 ARKANSAS STATE PLANE GRID BEARINGS - ZONE 0301 - ARKANSAS NORTH ZONE
 DETERMINED FROM CAGIS CONTROL POINTS 1004 AND 1004-01
 CONVERGENCE ANGLE: 00°15'14.34" LEFT AT CAGIS CONTROL POINT 1004
 GRID AZIMUTH = ASTRONOMICAL AZIMUTH - CONVERGENCE ANGLE

REV.	DATE	DESCRIPTION

CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

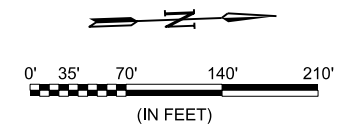
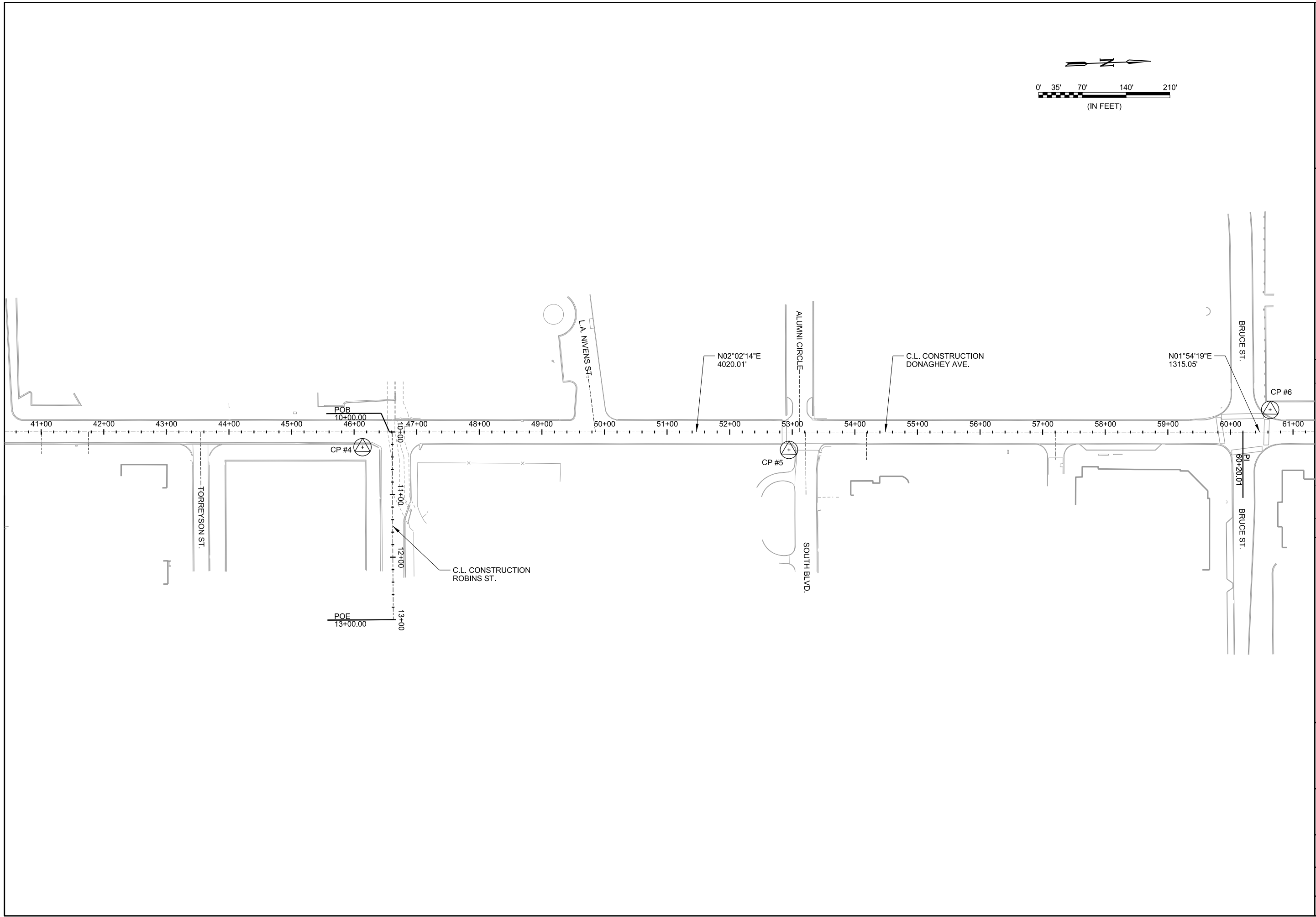
SURVEY CONTROL
 DETAILS
 (SHEET 1 OF 4)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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C-501
 SHEET NUMBER
63

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 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

SURVEY CONTROL
 DETAILS
 (SHEET 2 OF 4)

JOB NO.: 18147010
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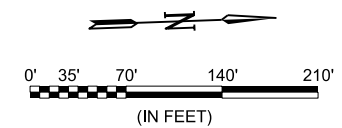
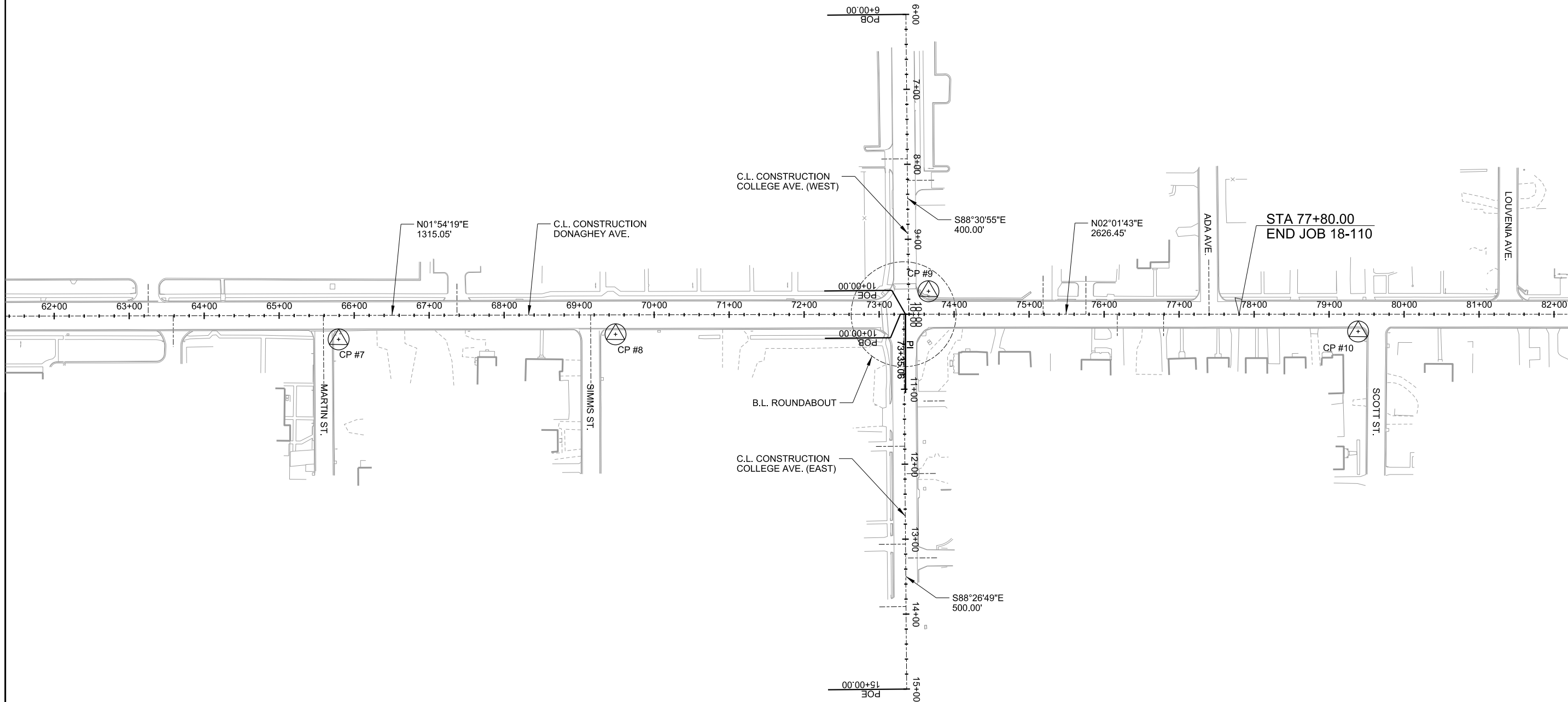
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SHEET NUMBER
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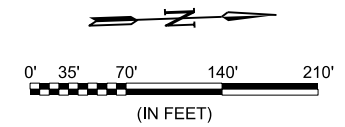
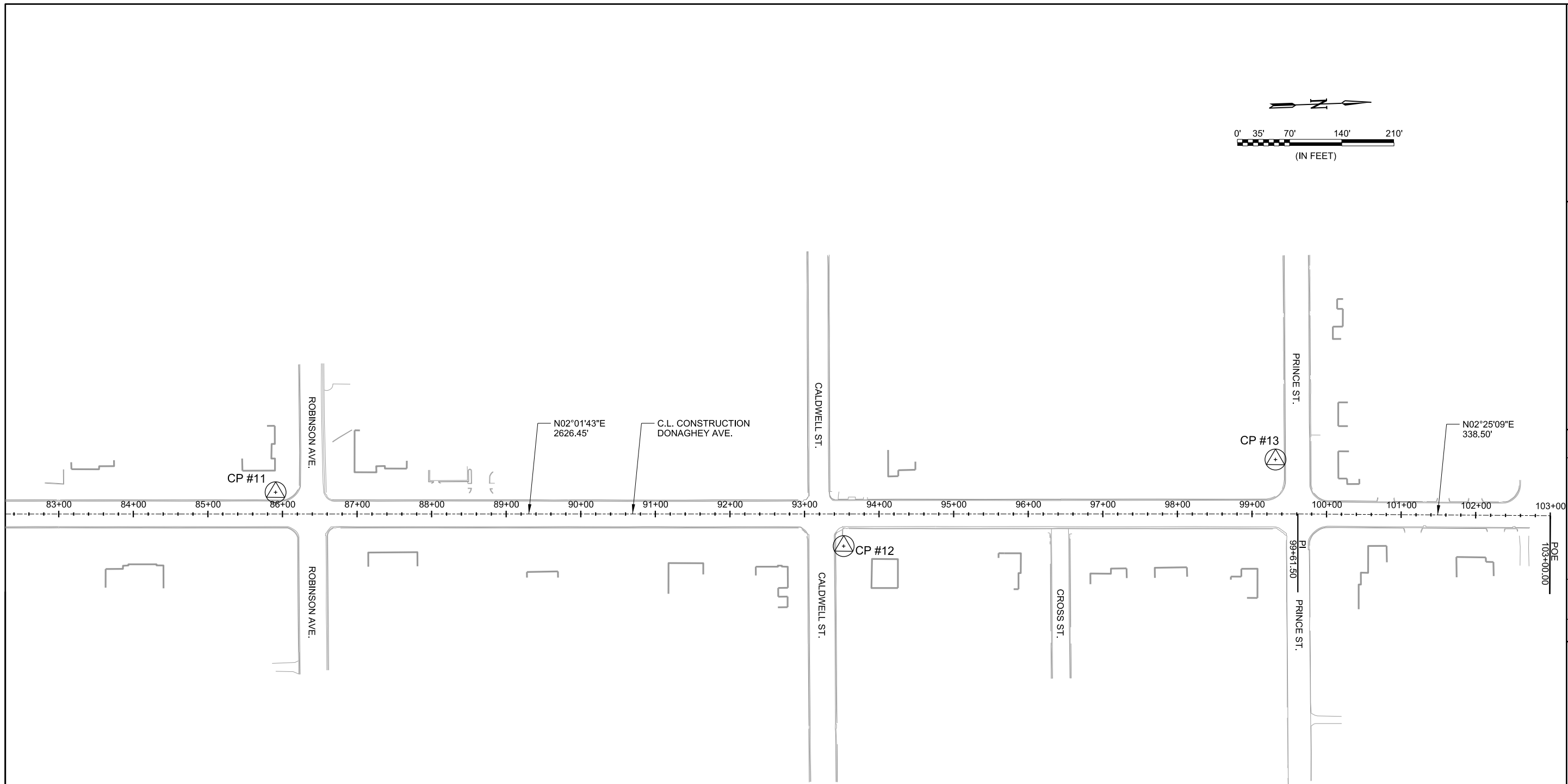
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

SURVEY CONTROL
 DETAILS
 (SHEET 3 OF 4)

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 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
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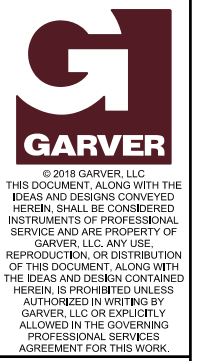
SURVEY CONTROL
 DETAILS
 (SHEET 4 OF 4)

JOB NO.: 18147010
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DRAWING NUMBER
C-504

SHEET NUMBER
66



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 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
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 (DAVE WARD DR. TO ADA AVE.)

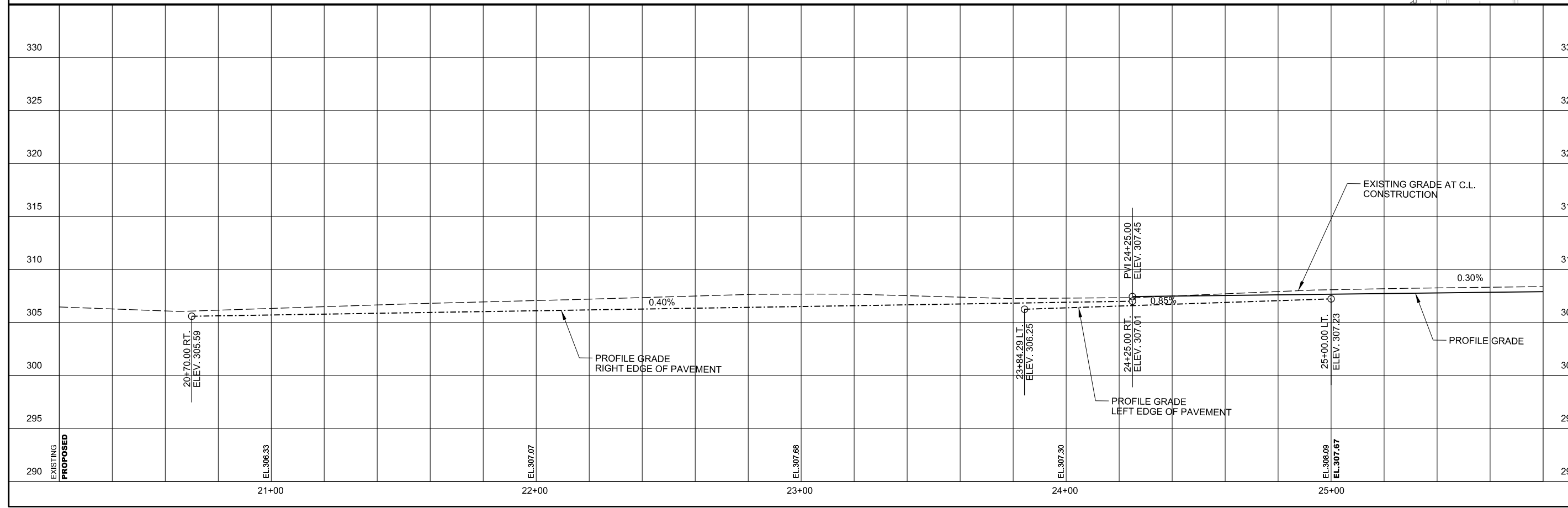
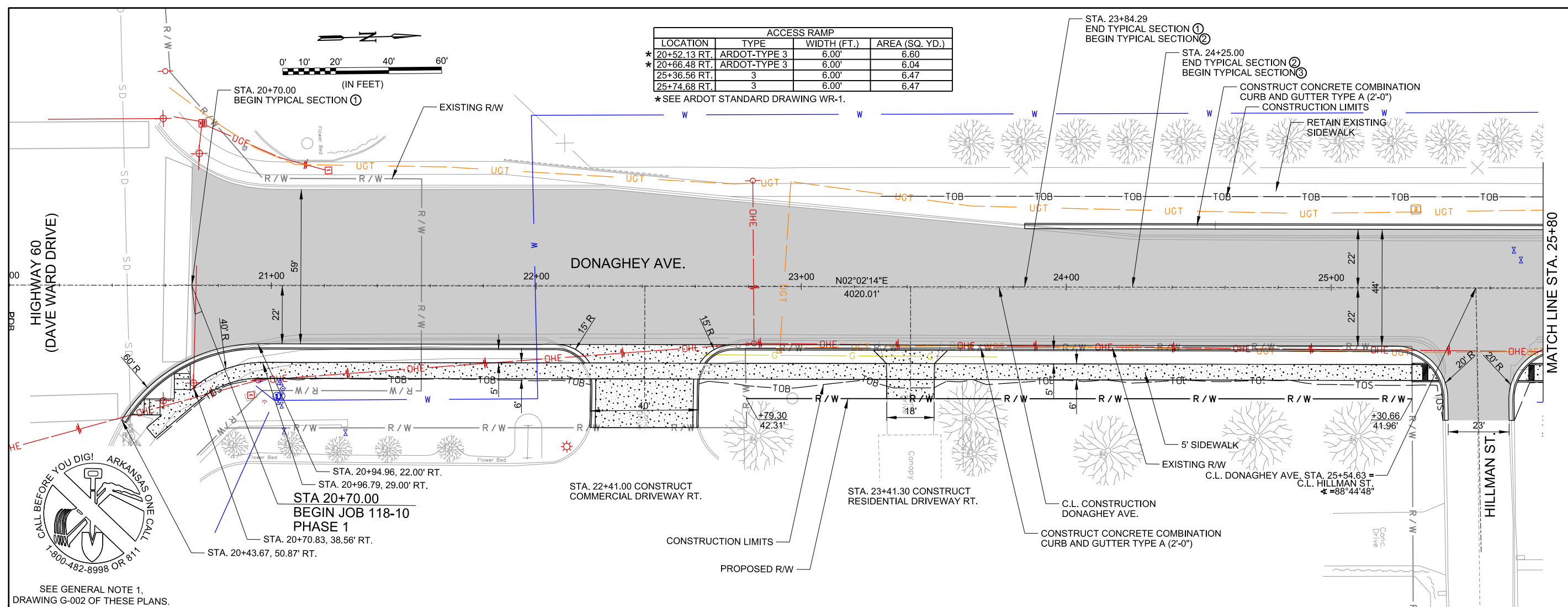
PLAN AND PROFILE - DONAGHEY AVE.
 (SHEET 1 OF 11)

JOB NO.: 18147010
 DATE: MAY 2020
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 DRAWING NUMBER
C-601
 SHEET NUMBER
67

ACCESS RAMP			
LOCATION	TYPE	WIDTH (FT.)	AREA (SQ. YD.)
* 20+52.13 RT.	ARDOT-TYPE 3	6.00'	6.60
* 20+66.48 RT.	ARDOT-TYPE 3	6.00'	6.04
25+36.56 RT.	3	6.00'	6.47
25+74.68 RT.	3	6.00'	6.47

*SEE ARDOT STANDARD DRAWING WR-1.



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SEE GENERAL NOTE 1,
 DRAWING G-002 OF THESE PLANS.





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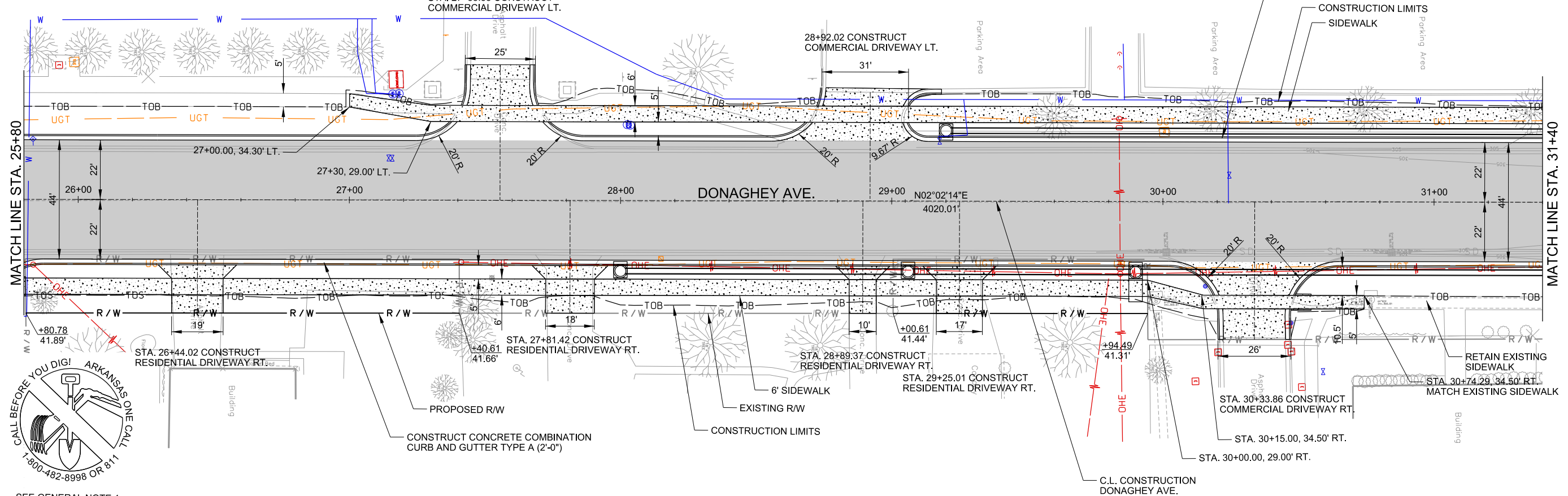
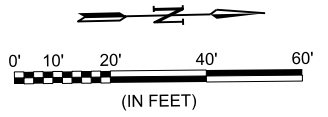
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

PLAN AND PROFILE -
 DONAGHEY AVE.
 (SHEET 2 OF 11)

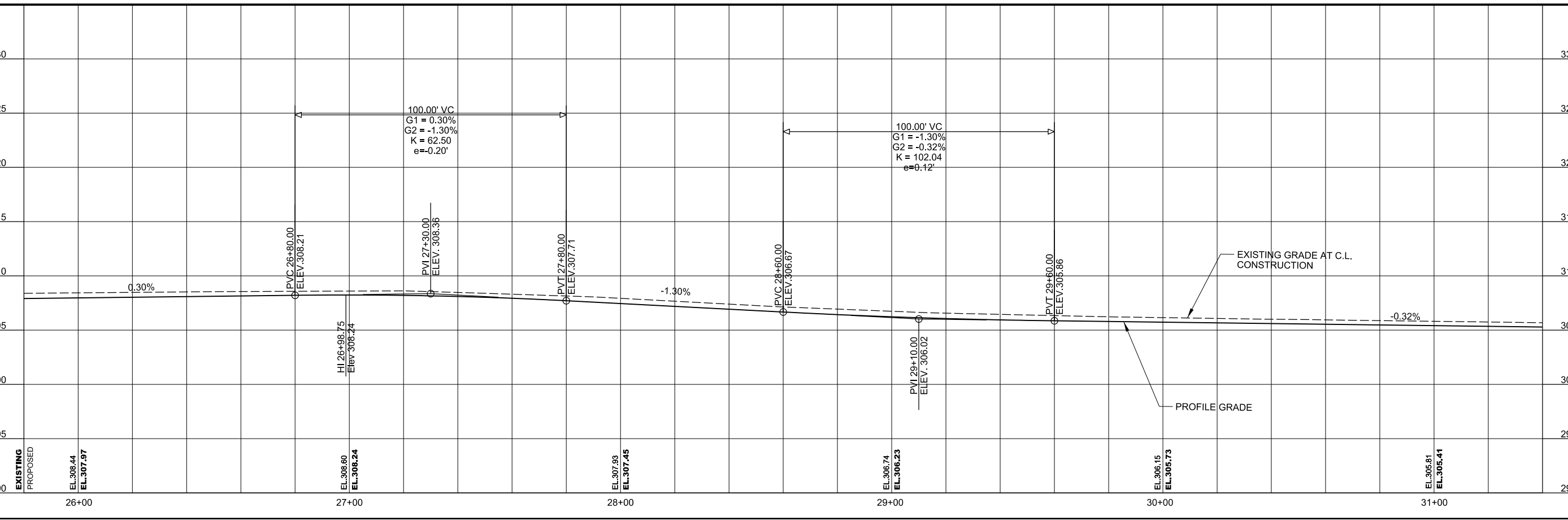
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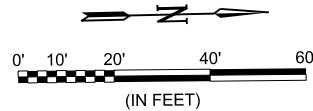
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 SHEET NUMBER
68



SEE GENERAL NOTE 1,
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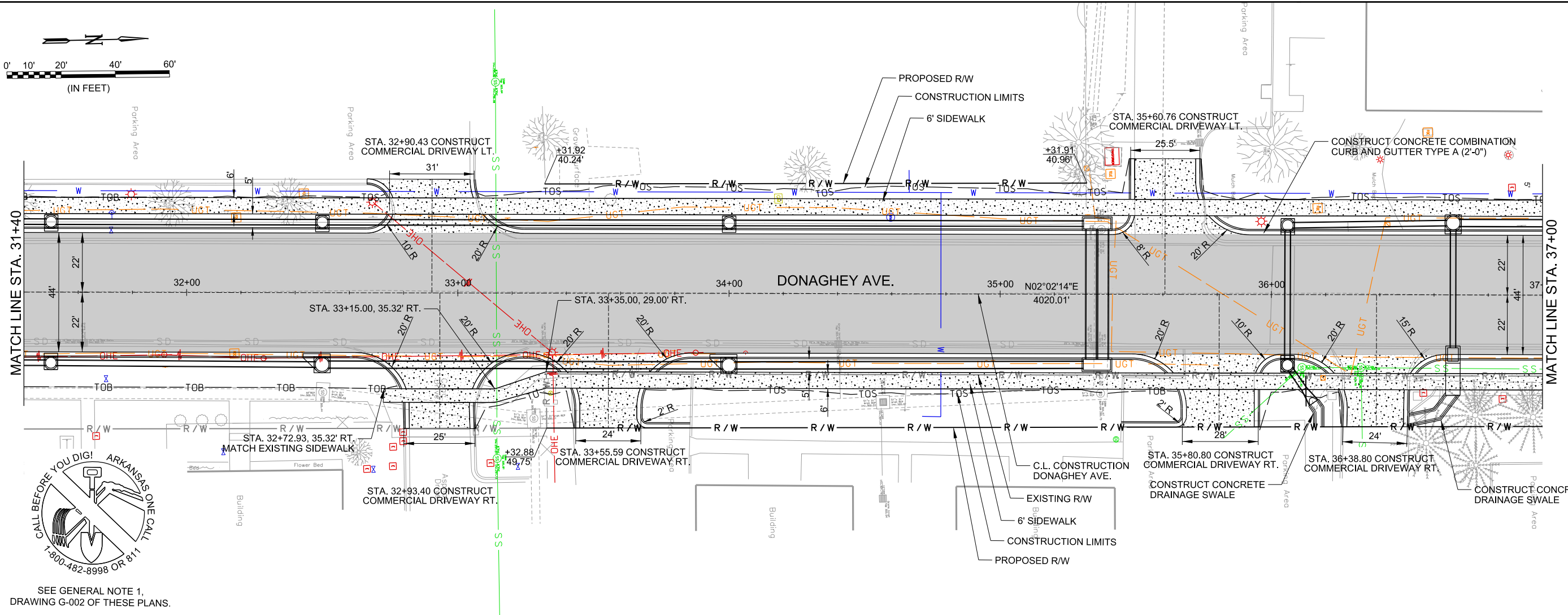


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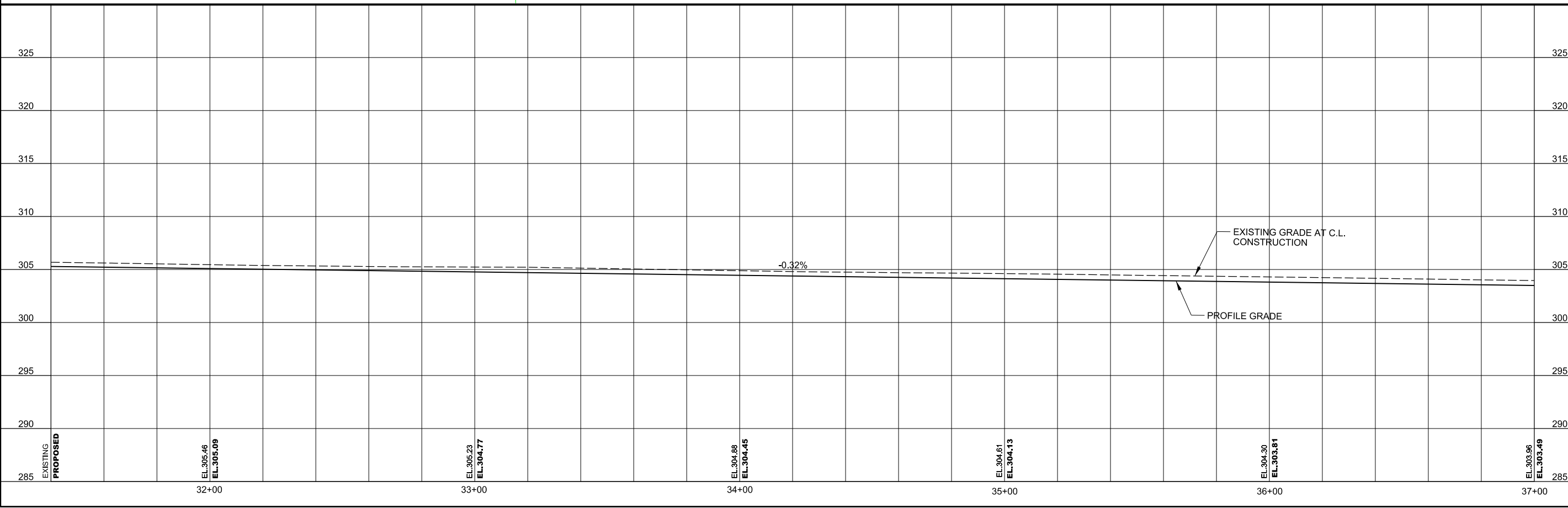


MATCH LINE STA. 31+40

MATCH LINE STA. 37+00



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CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

PLAN AND PROFILE - DONAGHEY AVE. (SHEET 3 OF 11)

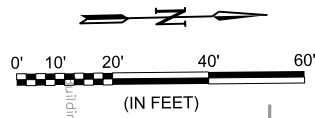
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DRAWING NUMBER
C-603

SHEET NUMBER
69

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ACCESS RAMP			
LOCATION	TYPE	WIDTH (FT.)	AREA (SQ. YD.)
38+13.81 LT.	3	8.00	6.00
38+13.81 RT.	3	8.00	6.00
38+13.81	ISLAND	8.00	7.11
39+84.85 RT.	3	6.00	4.92
40+12.51 LT.	3	6.00	6.46
40+20.48 RT.	3	6.00	6.47
40+59.53 LT.	3	6.00	6.47

MATCH LINE STA. 37+00

MATCH LINE STA. 42+60

CALL BEFORE YOU DIG! ARKANSAS ONE CALL
1-800-482-8998 OR 811

SEE GENERAL NOTE 1,
DRAWING G-002 OF THESE PLANS.

PROPOSED R/W
CONSTRUCT CONCRETE
DRAINAGE SWALE

RETAIN EXISTING
FENCE

ENABLE MIDSTREAM PARTNERS GAS
PIPELINE EASEMENT. COMPANY
PERSONNEL MUST BE PRESENT AND
SUPERVISE ANY EXCAVATION WITHIN 25
FEET OF PIPELINE. CONTACT JEFFERY
CATES AT 501-940-3608. SEE
SPECIFICATIONS FOR ADDITIONAL
INFORMATION AND REQUIREMENTS.

CONSTRUCT CONCRETE
DRAINAGE SWALE

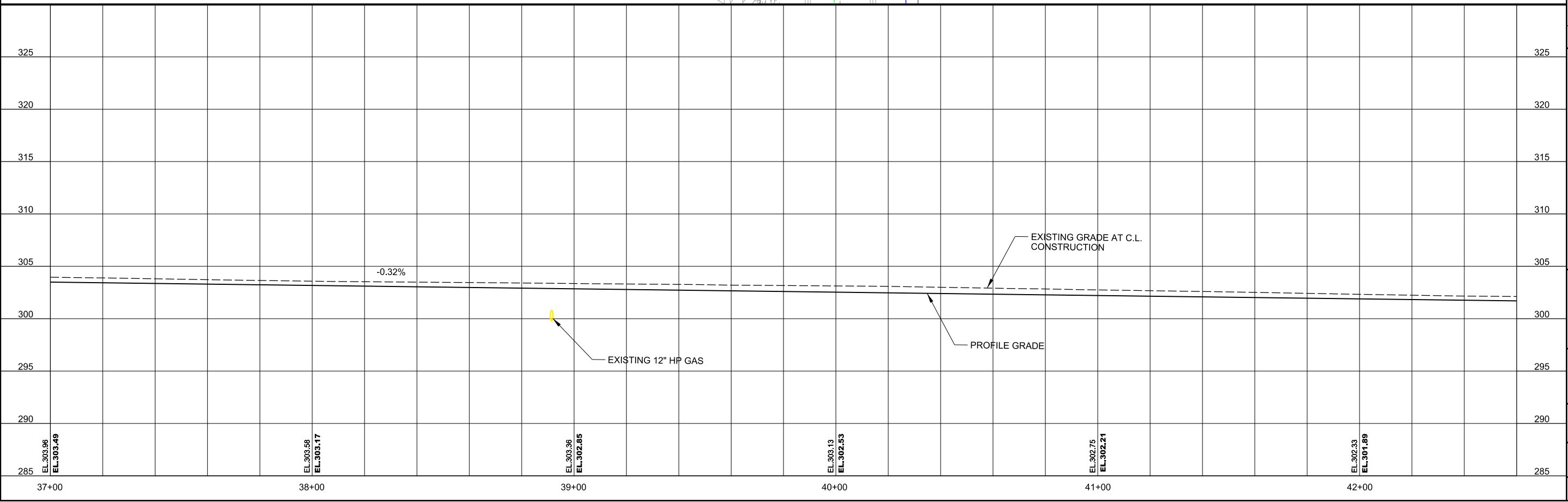
C.L. DONAGHEY AVE. STA. 40+01.23 =
C.L. ERBACH ST.
◀=89°41'57"

STA. 41+00.94 CONSTRUCT
RESIDENTIAL DRIVEWAY RT.

6' SIDEWALK
EXISTING R/W
CONSTRUCTION LIMITS

C.L. CONSTRUCTION
DONAGHEY AVE.

STA. 41+75.95 CONSTRUCT
RESIDENTIAL DRIVEWAY RT.



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CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

PLAN AND PROFILE -
DONAGHEY AVE.
(SHEET 4 OF 11)

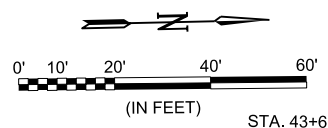
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DATE: MAY 2020
DESIGNED BY: DLT
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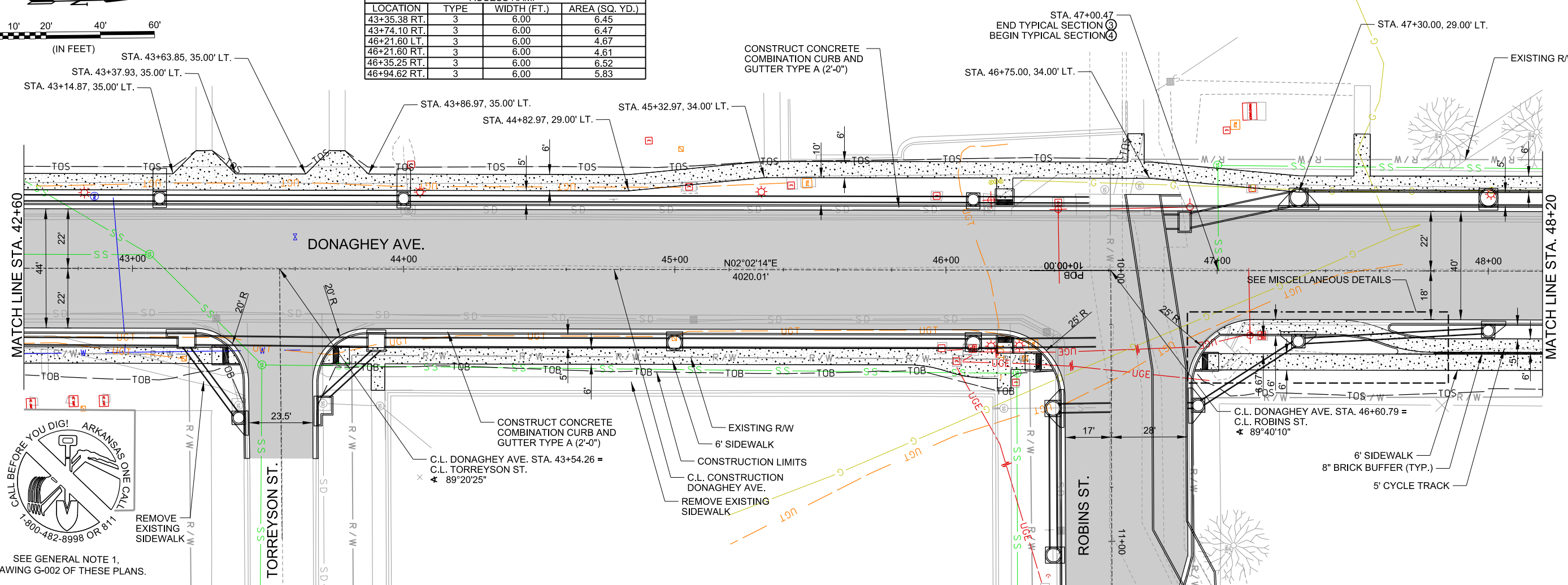
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SHEET
NUMBER **70**

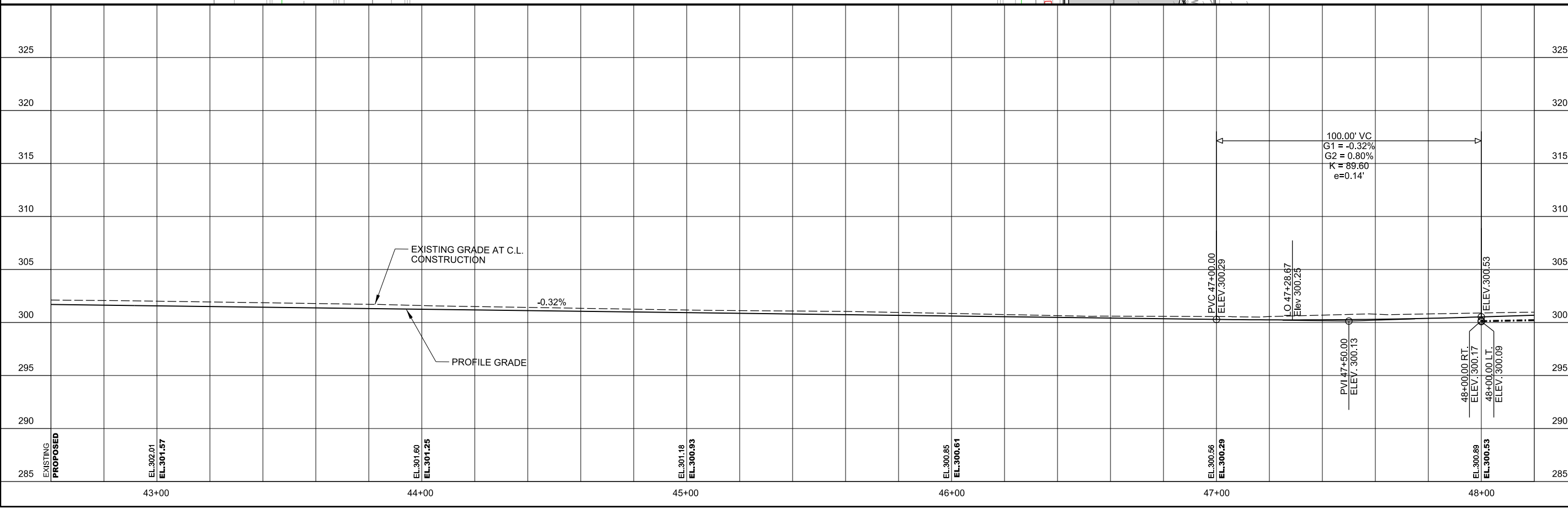
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ACCESS RAMP			
LOCATION	TYPE	WIDTH (FT.)	AREA (SQ. YD.)
43+35.38 RT.	3	6.00	6.45
43+74.10 RT.	3	6.00	6.47
46+21.60 LT.	3	6.00	4.67
46+21.60 RT.	3	6.00	4.61
46+35.25 RT.	3	6.00	6.52
46+94.62 RT.	3	6.00	5.83



SEE GENERAL NOTE 1, DRAWING G-002 OF THESE PLANS.



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PLAN AND PROFILE - DONAGHEY AVE. (SHEET 5 OF 11)

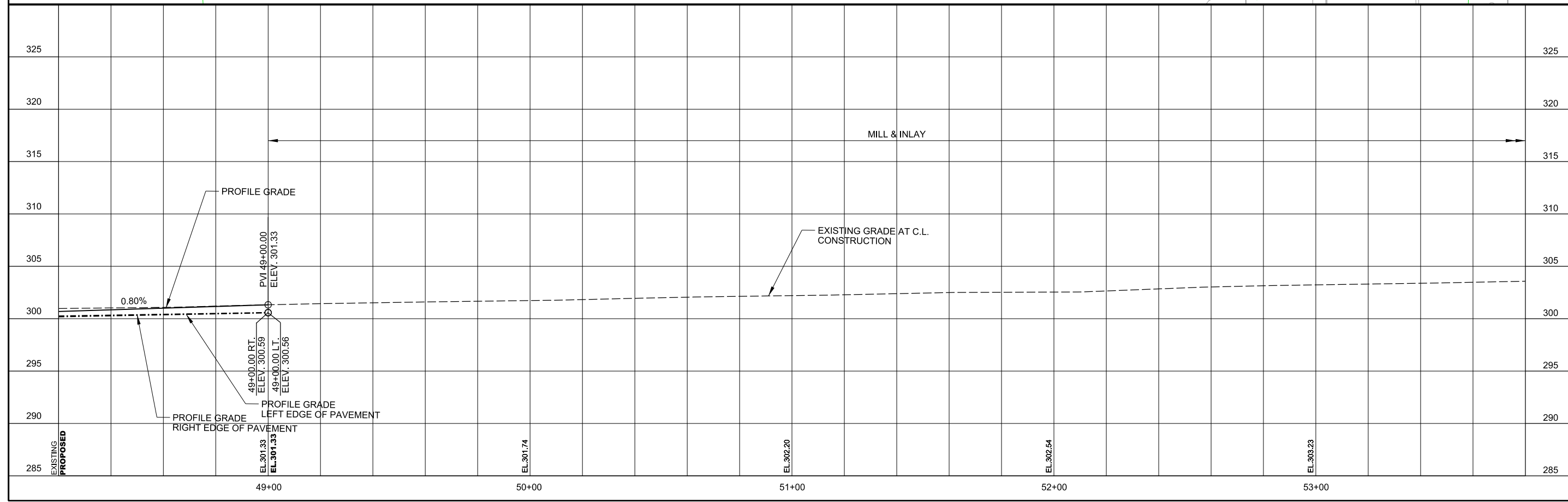
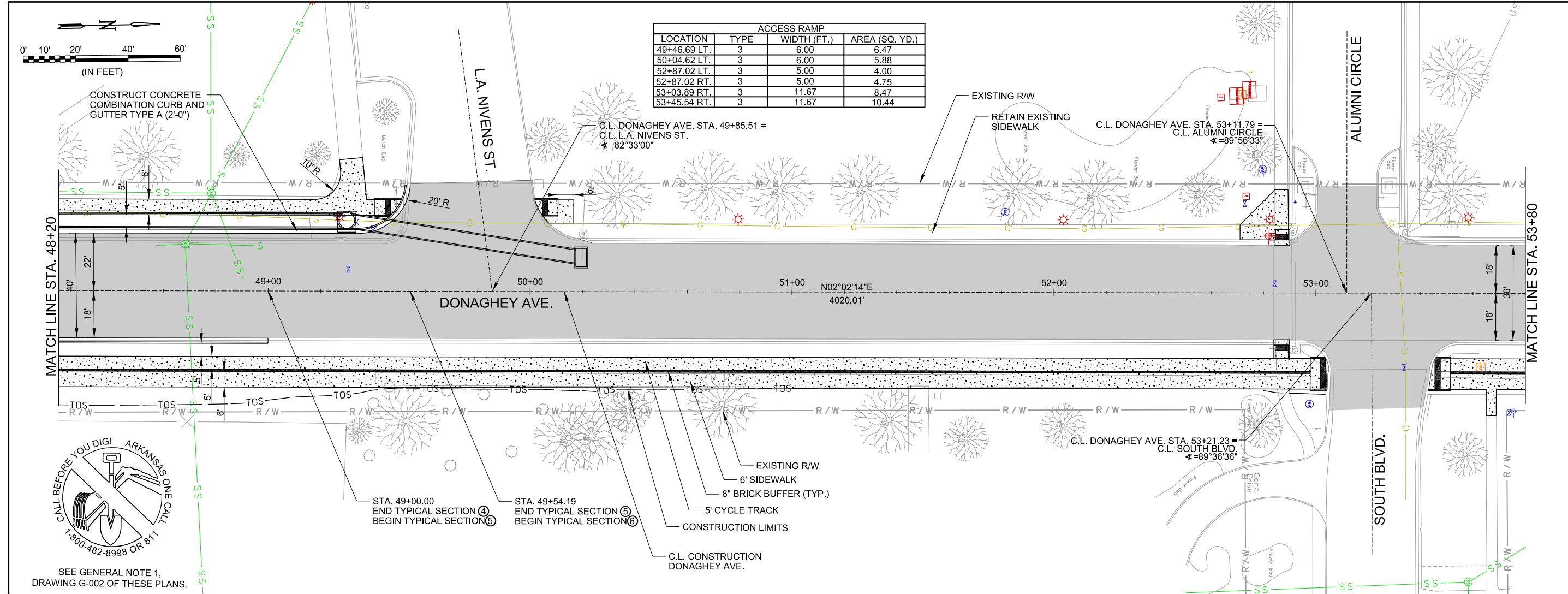
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DRAWING NUMBER
C-605

SHEET NUMBER
71

ACCESS RAMP			
LOCATION	TYPE	WIDTH (FT.)	AREA (SQ. YD.)
49+46.69 LT.	3	6.00	6.47
50+04.62 LT.	3	6.00	5.88
52+87.02 LT.	3	5.00	4.00
52+87.02 RT.	3	5.00	4.75
53+03.89 RT.	3	11.67	8.47
53+45.54 RT.	3	11.67	10.44



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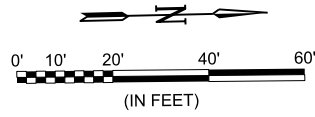
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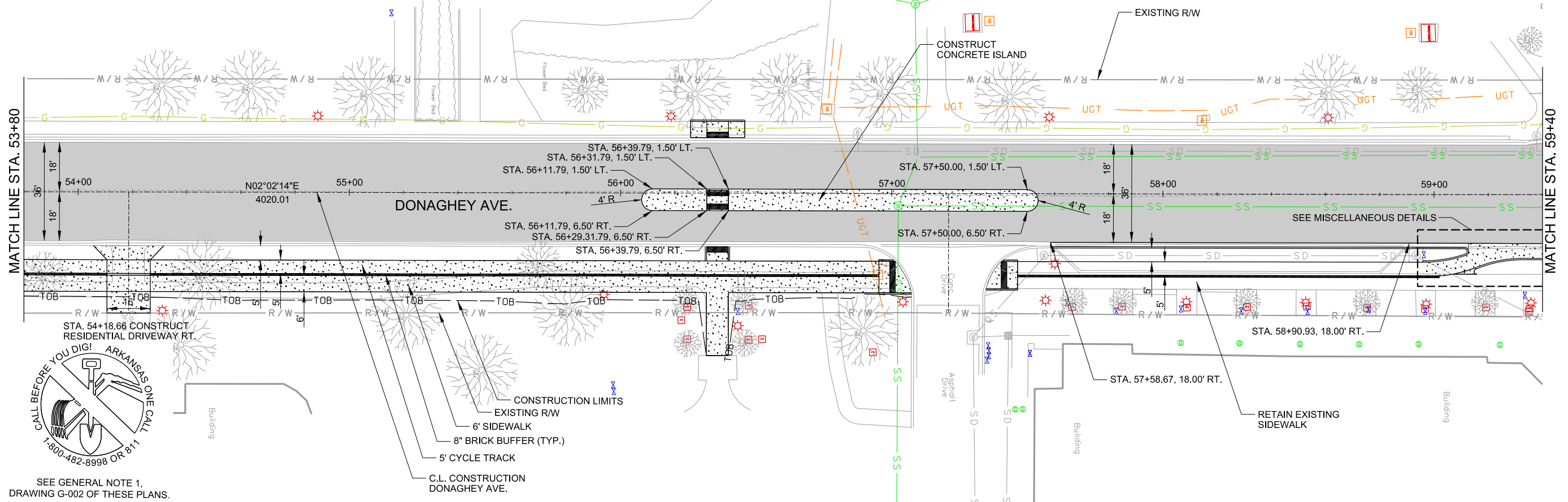
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 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

PLAN AND PROFILE -
 DONAGHEY AVE.
 (SHEET 6 OF 11)

JOB NO.: 18147010
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C-606
 SHEET NUMBER
72



ACCESS RAMP			
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56+35.79 LT.	5	8.00	14.41
56+35.79	ISLAND	8.00	7.11
56+35.79 RT.	3	8.00	5.25
57+01.35 RT.	3	11.67	13.52
57+40.48 RT.	3	10.00	11.47



MATCH LINE STA. 53+80

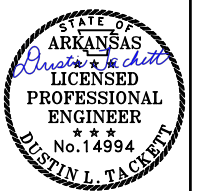
MATCH LINE STA. 59+40

SEE GENERAL NOTE 1, DRAWING G-002 OF THESE PLANS.

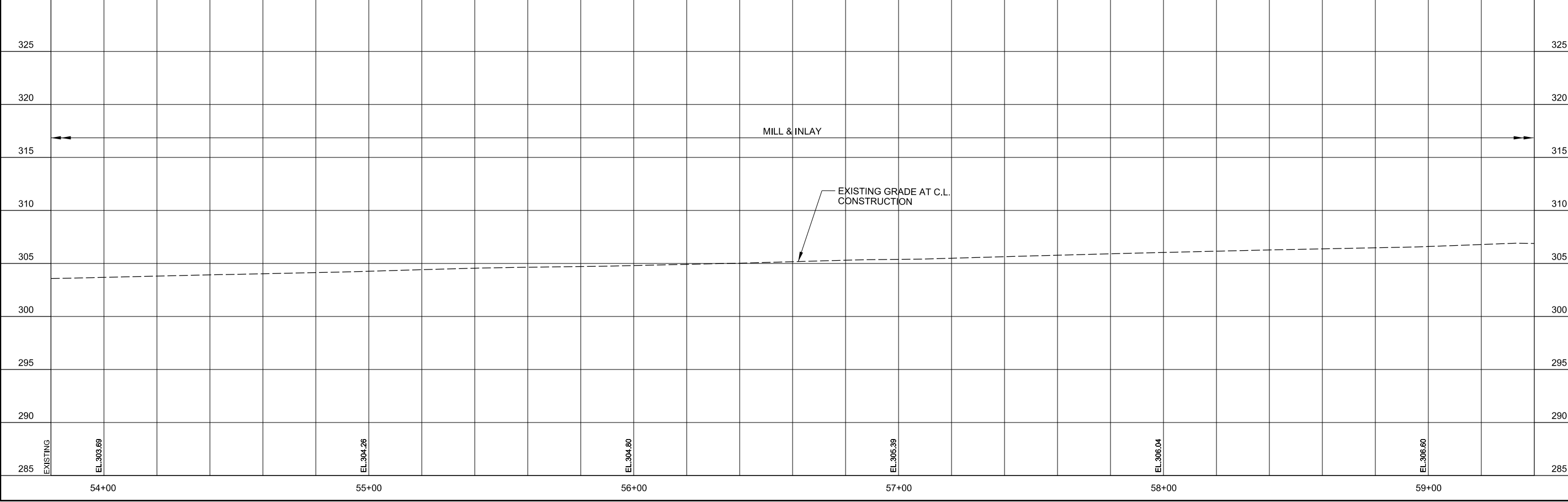
CALL BEFORE YOU DIG! ARKANSAS ONE CALL
1-800-482-8998 OR 811



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

PLAN AND PROFILE - DONAGHEY AVE. (SHEET 7 OF 11)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

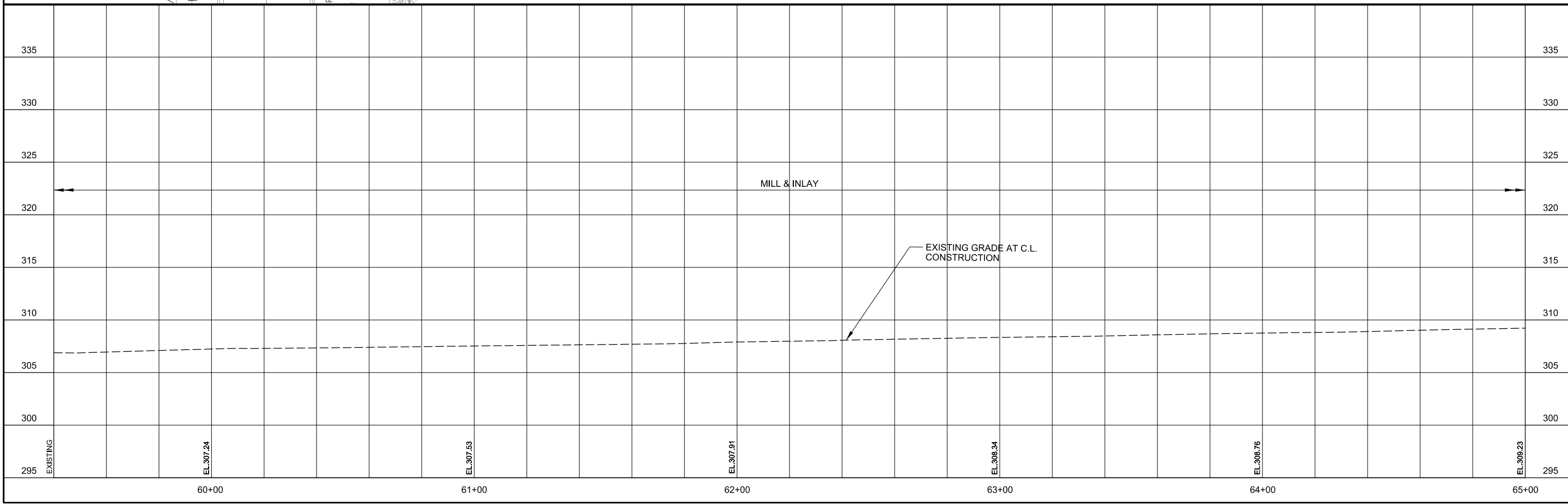
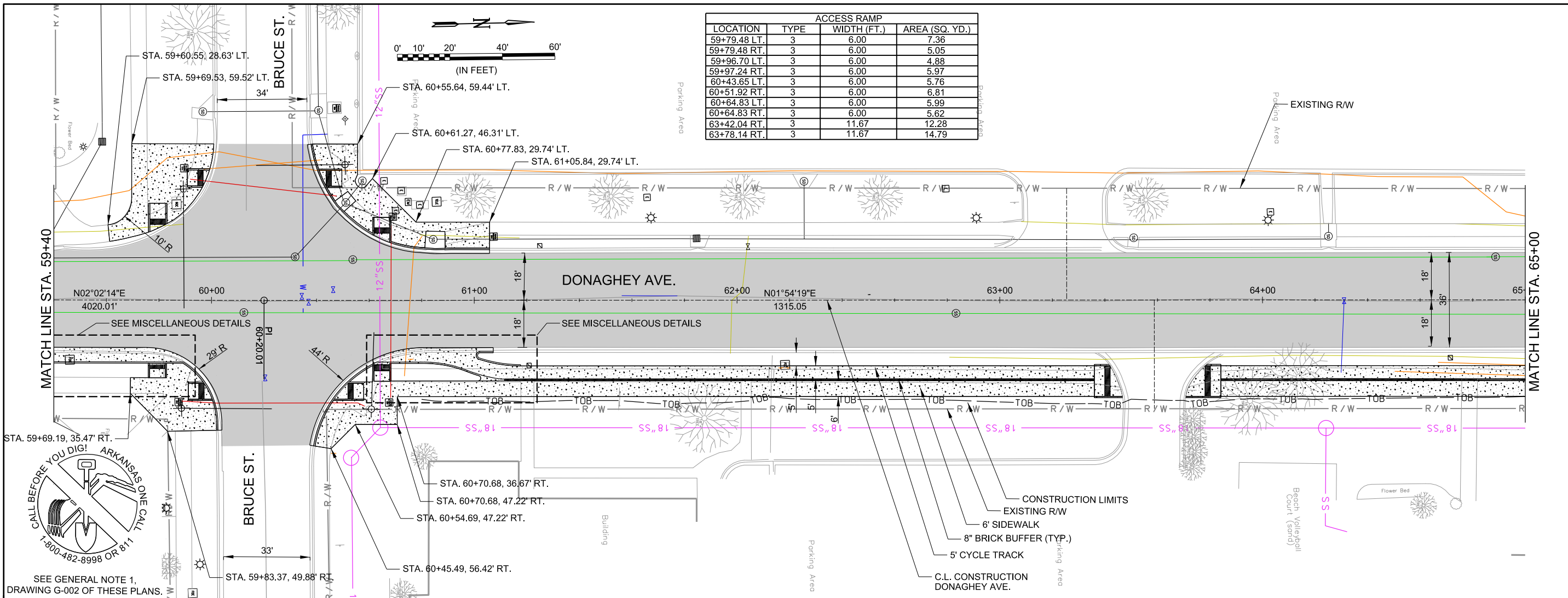
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DRAWING NUMBER
C-607

SHEET NUMBER
73


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5/22/2020 3:47:36 PM
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STATE OF ARKANSAS
 Justin L. Tackett
 LICENSED PROFESSIONAL ENGINEER
 No. 14994
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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

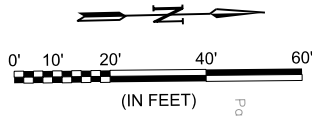
PLAN AND PROFILE -
 DONAGHEY AVE.
 (SHEET 8 OF 11)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

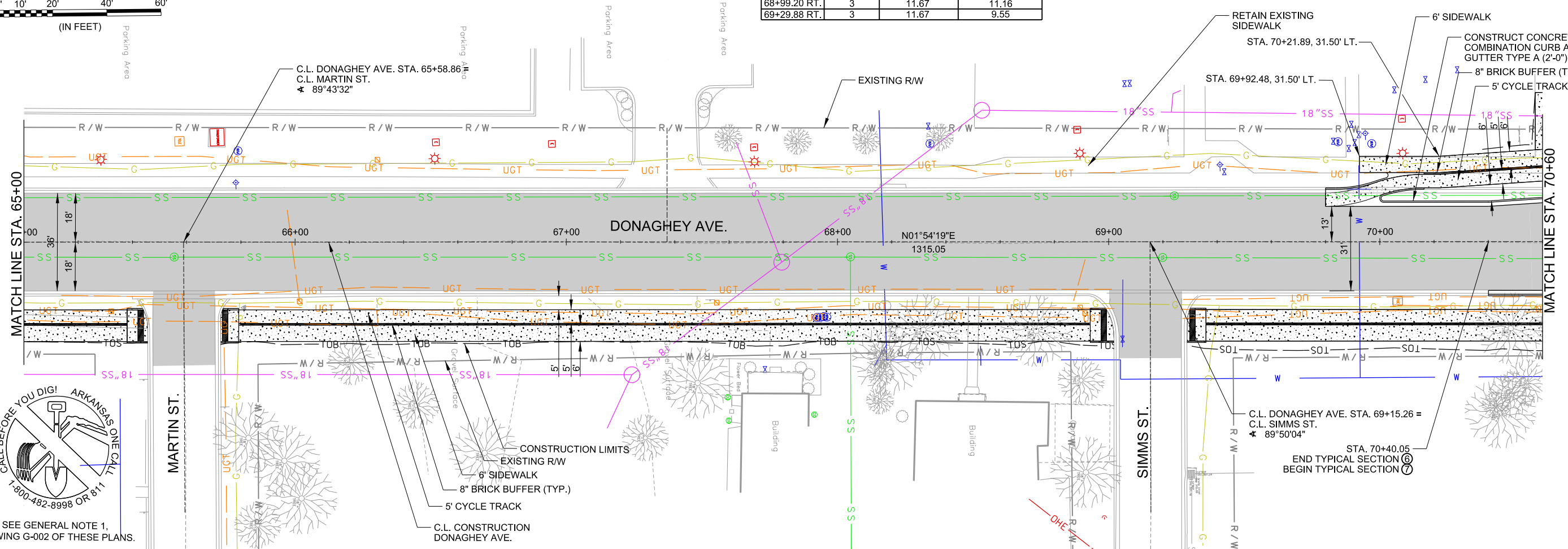
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DRAWING NUMBER
C-608

SHEET NUMBER
74



ACCESS RAMP			
LOCATION	TYPE	WIDTH (FT.)	AREA (SQ. YD.)
65+44.05 RT.	3	11.67	10.21
65+73.20 RT.	3	11.67	9.26
68+99.20 RT.	3	11.67	11.16
69+29.88 RT.	3	11.67	9.55

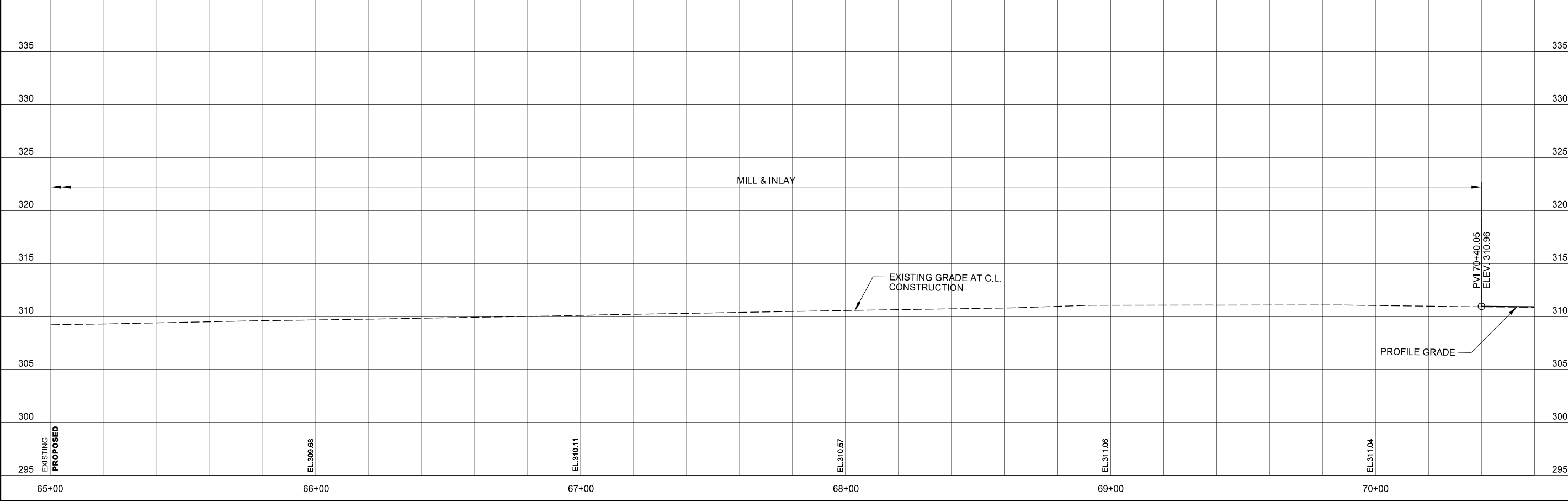


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CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

PLAN AND PROFILE -
DONAGHEY AVE.
(SHEET 9 OF 11)

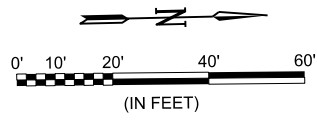
JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
C-609

SHEET NUMBER
75

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CONCRETE STEPS			
STATION	SIDE	WIDTH (FT.)	AREA (SQ. YD.)
71+87.99	LT.	8.00	3.67

CONCRETE ISLAND BEHIND WALK			
STATION	SIDE	WIDTH (FT.)	AREA (SQ. YD.)
73+82.04	LT.	8.00	3.67

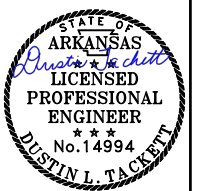
ACCESS RAMPS			
LOCATION	TYPE	WIDTH (FT.)	AREA (SQ. YD.)
74+30.67	ISLAND	10.00	11.20
74+31.27 LT.	3	10.00	7.38
74+33.36 RT.	3	10.00	7.34

ACCESS RAMP			
LOCATION	TYPE	WIDTH (FT.)	AREA (SQ. YD.)
72+31.38 LT.	3	10.00	7.35
72+34.86 RT.	3	10.00	7.37
73+34.67	ISLAND	10.00	10.20

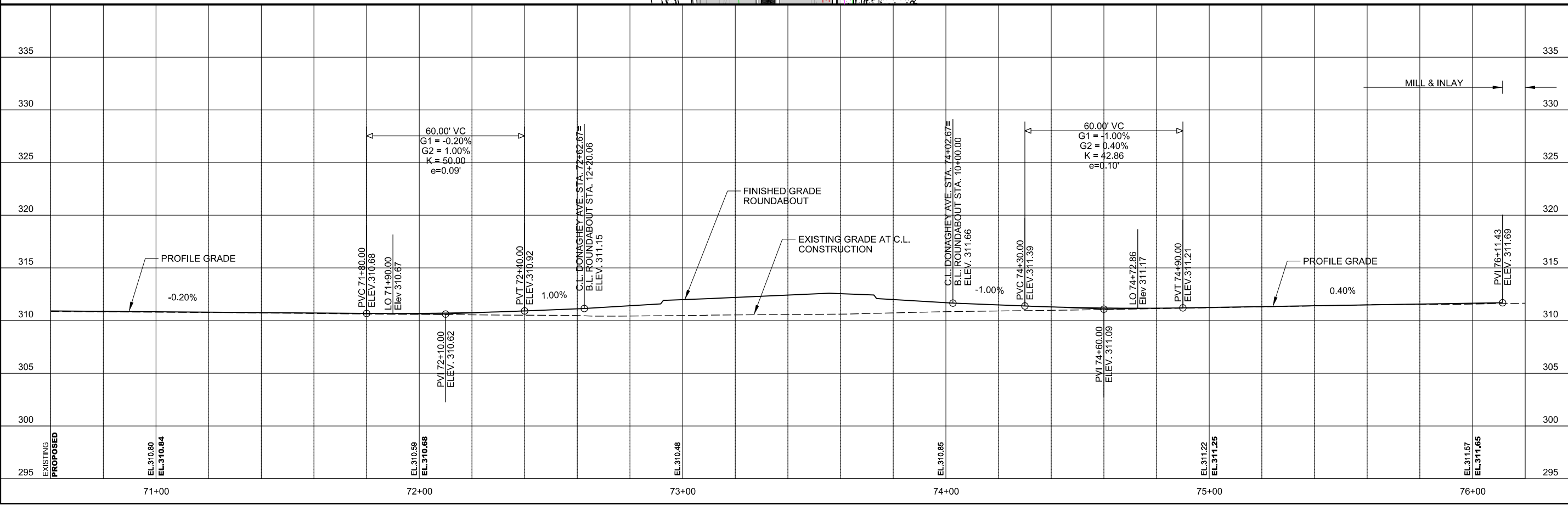
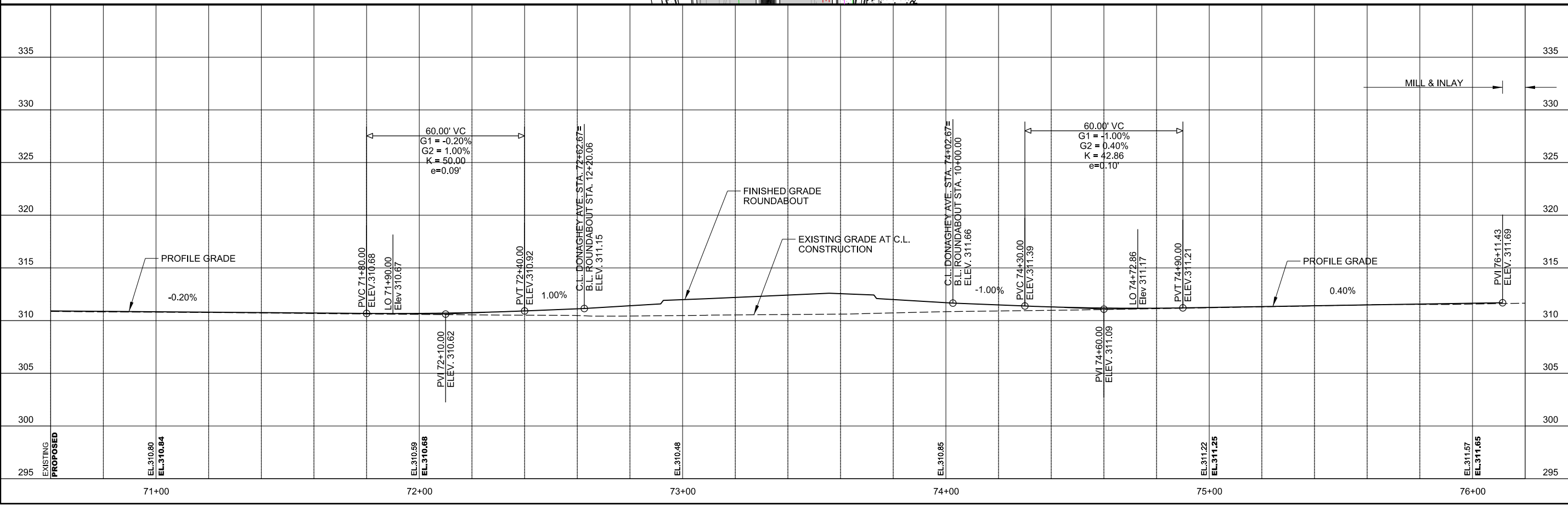
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BY	DESCRIPTION	DATE



CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

PLAN AND PROFILE -
DONAGHEY AVE.
(SHEET 10 OF 11)

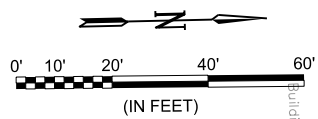
JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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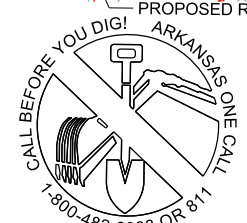
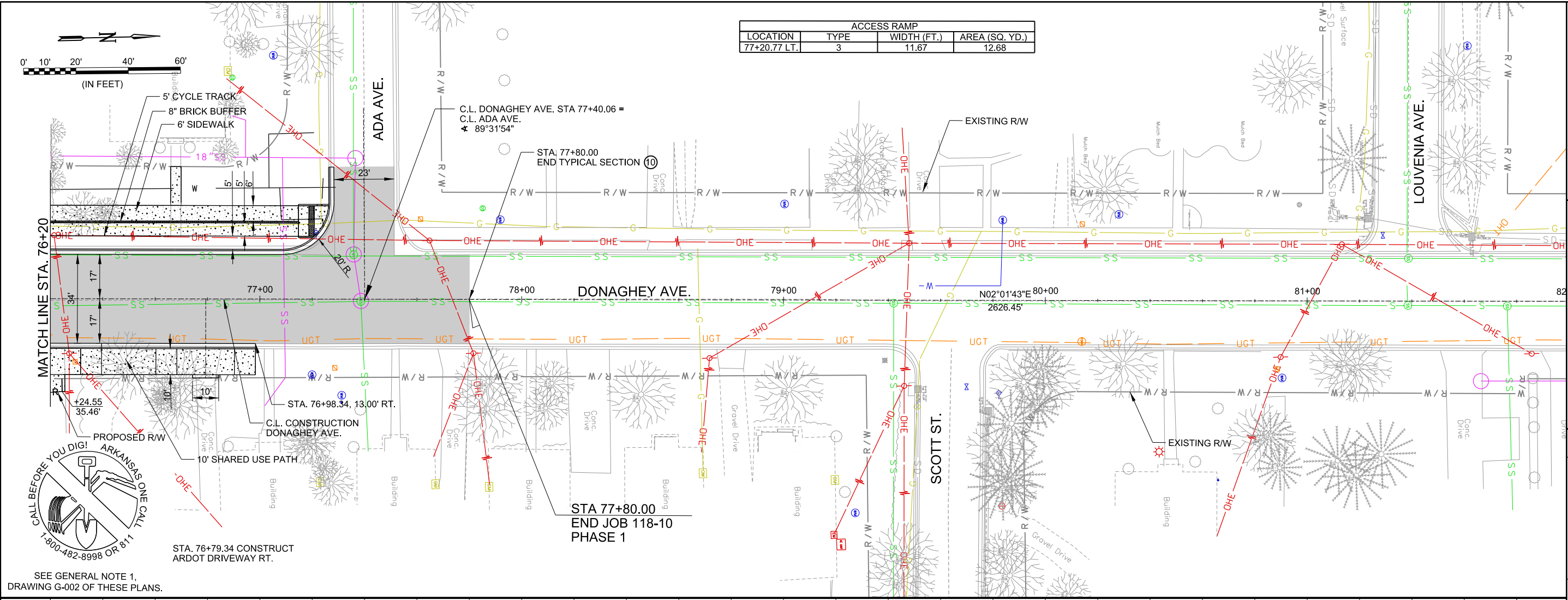
DRAWING NUMBER
C-610

SHEET NUMBER
76

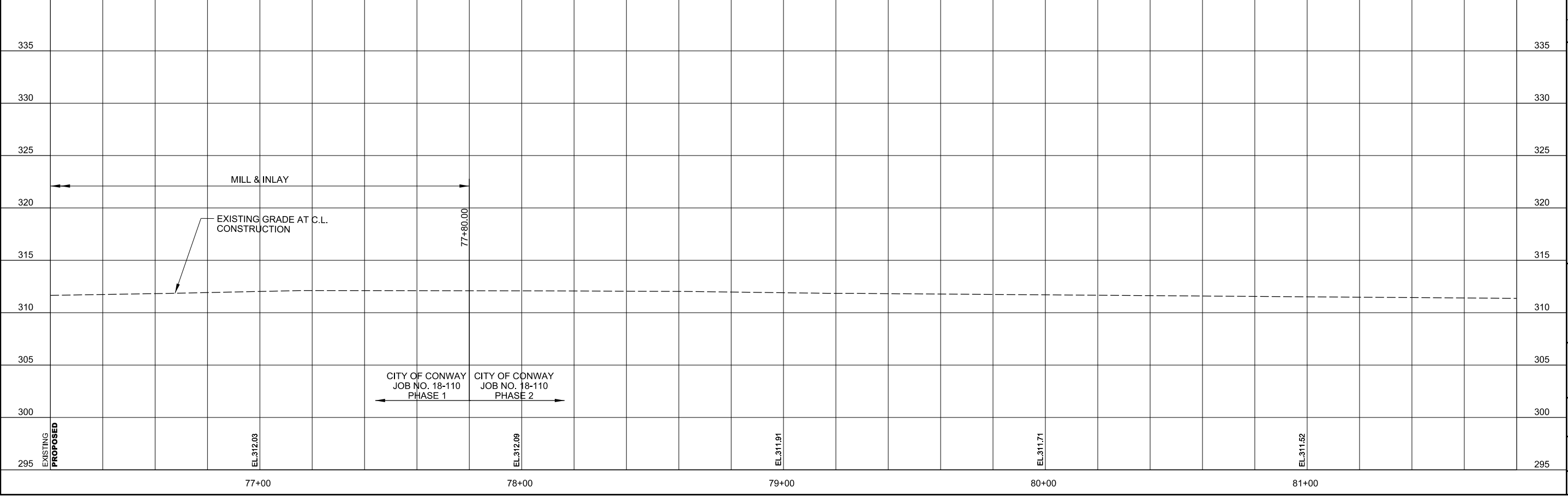
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ACCESS RAMP			
LOCATION	TYPE	WIDTH (FT.)	AREA (SQ. YD.)
77+20.77 LT.	3	11.67	12.68



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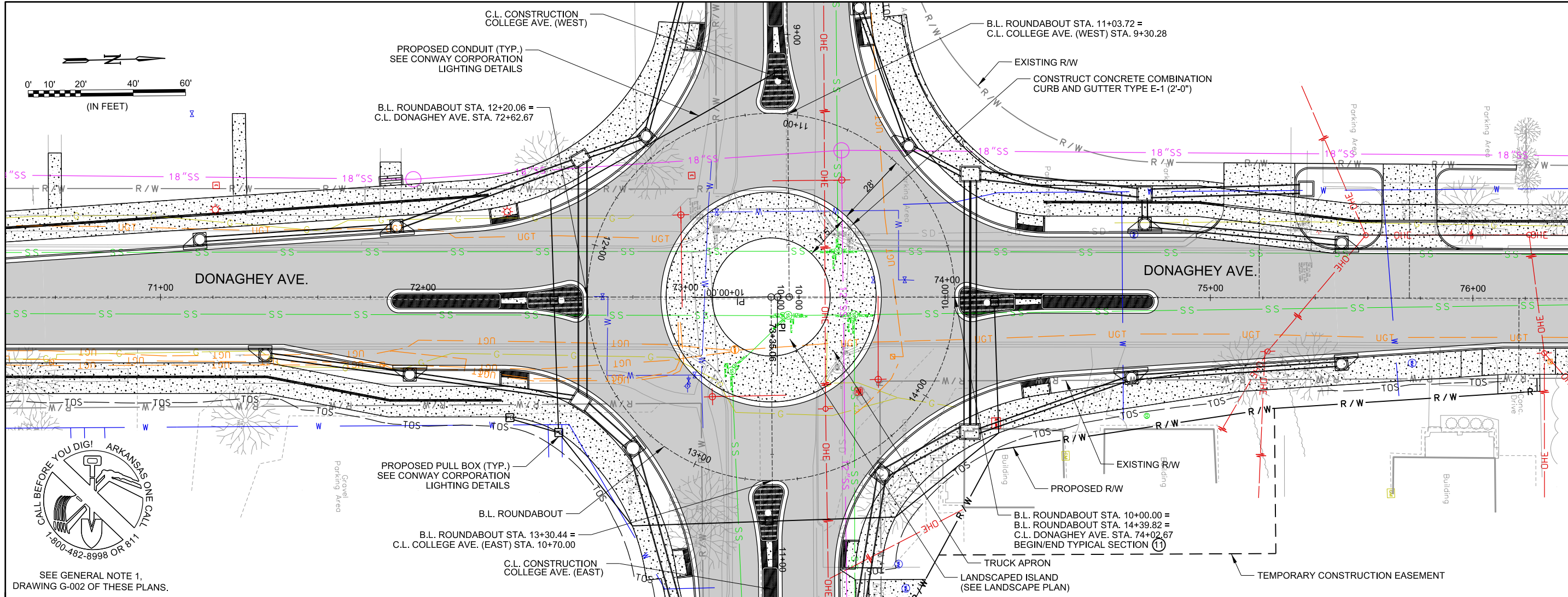
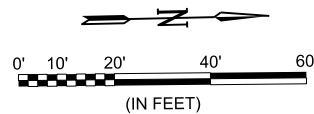
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

PLAN AND PROFILE - DONAGHEY AVE. (SHEET 11 OF 11)

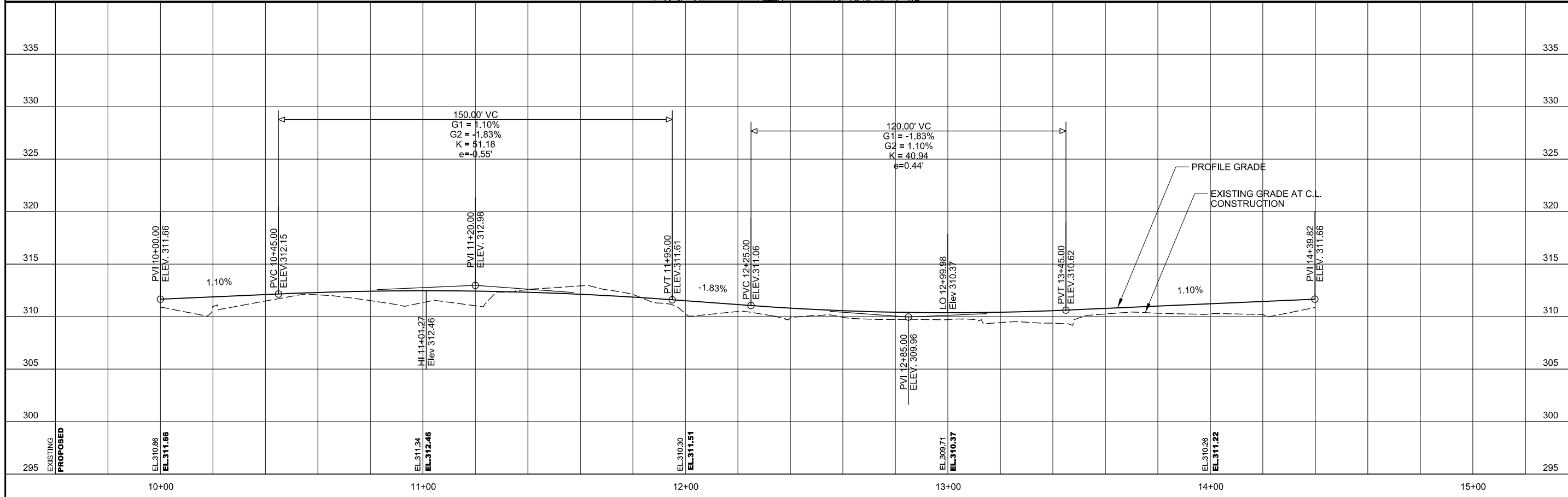
JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-611
 SHEET NUMBER
77



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

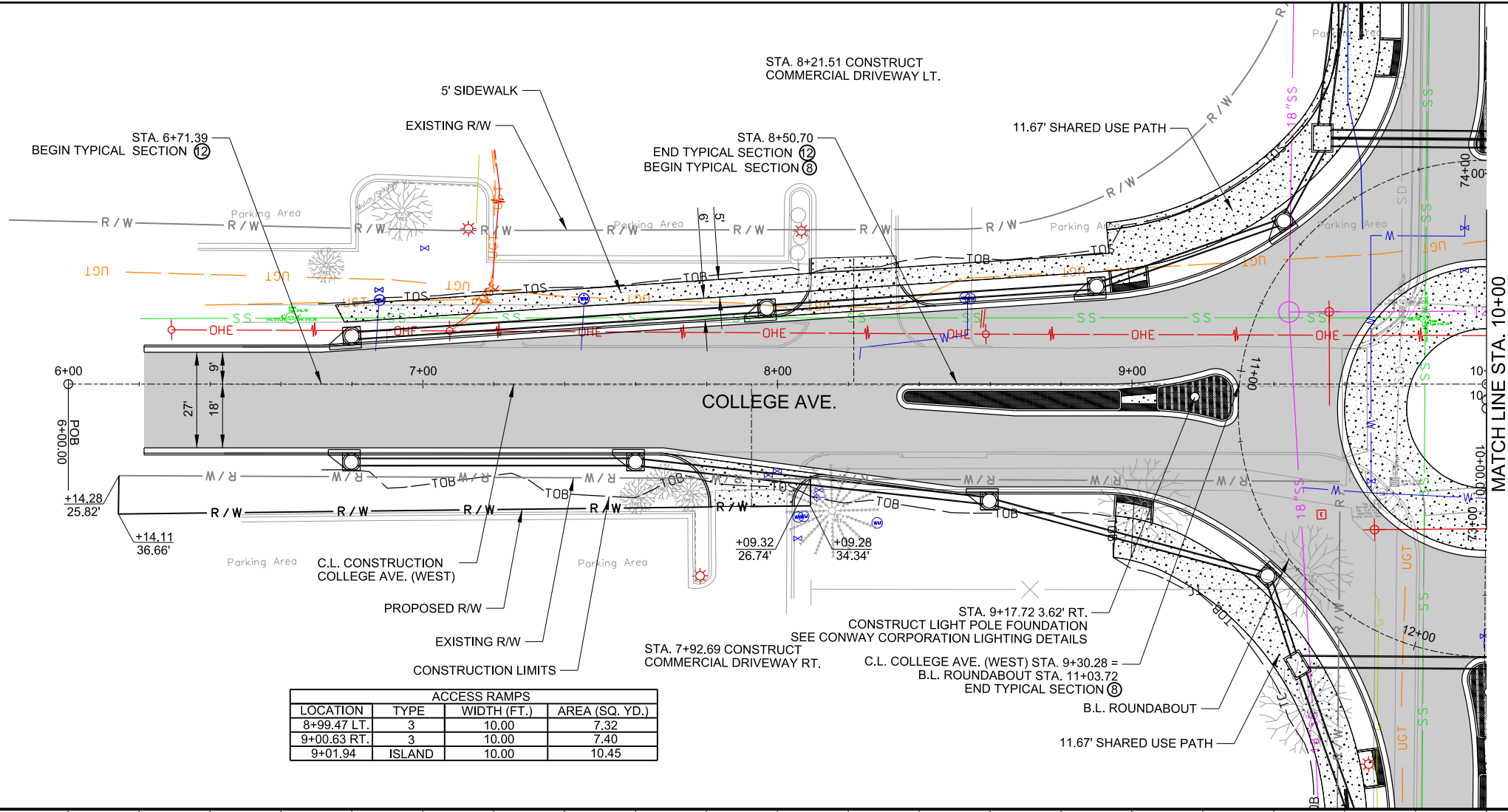
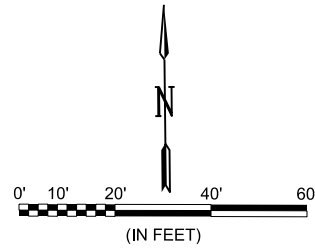
PLAN AND PROFILE -
 ROUNDABOUT

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DRAWING NUMBER
C-612

SHEET NUMBER
78



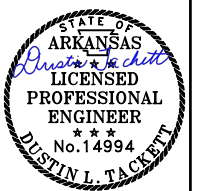
ACCESS RAMPS			
LOCATION	TYPE	WIDTH (FT.)	AREA (SQ. YD.)
8+99.47 LT.	3	10.00	7.32
9+00.63 RT.	3	10.00	7.40
9+01.94	ISLAND	10.00	10.45



SEE GENERAL NOTE 1,
DRAWING G-002 OF THESE PLANS.

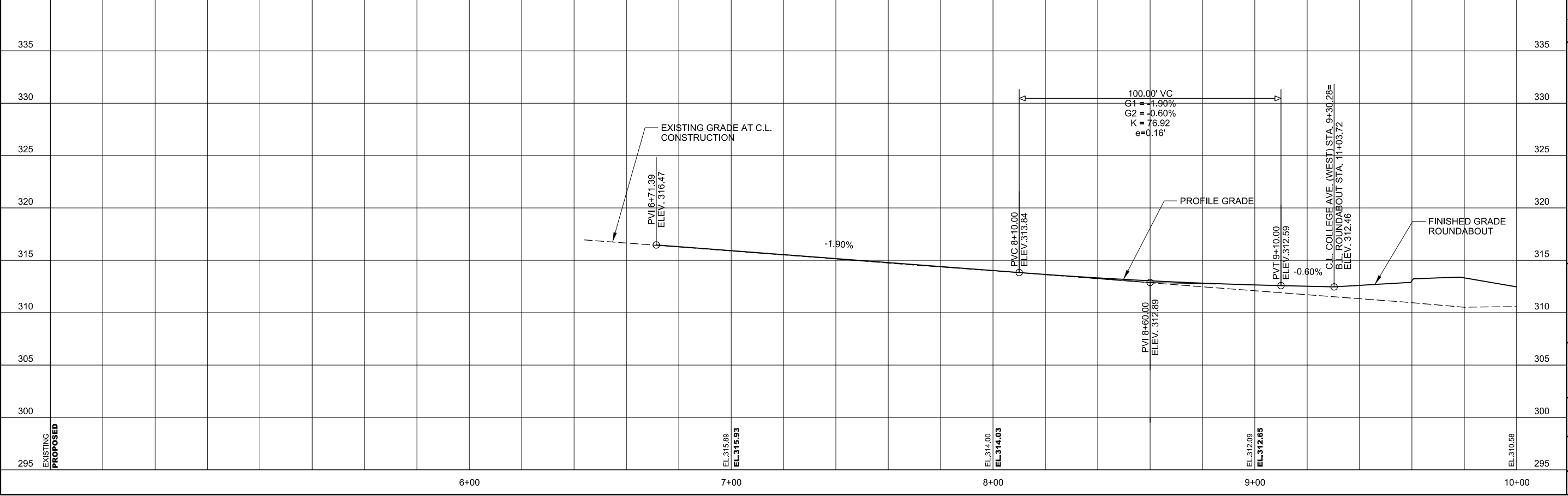


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CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

PLAN AND PROFILE -
COLLEGE AVE.
(SHEET 1 OF 2)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
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DRAWING NUMBER
C-613

SHEET NUMBER
79

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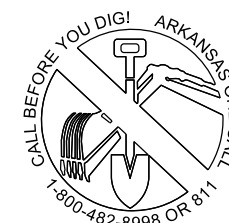


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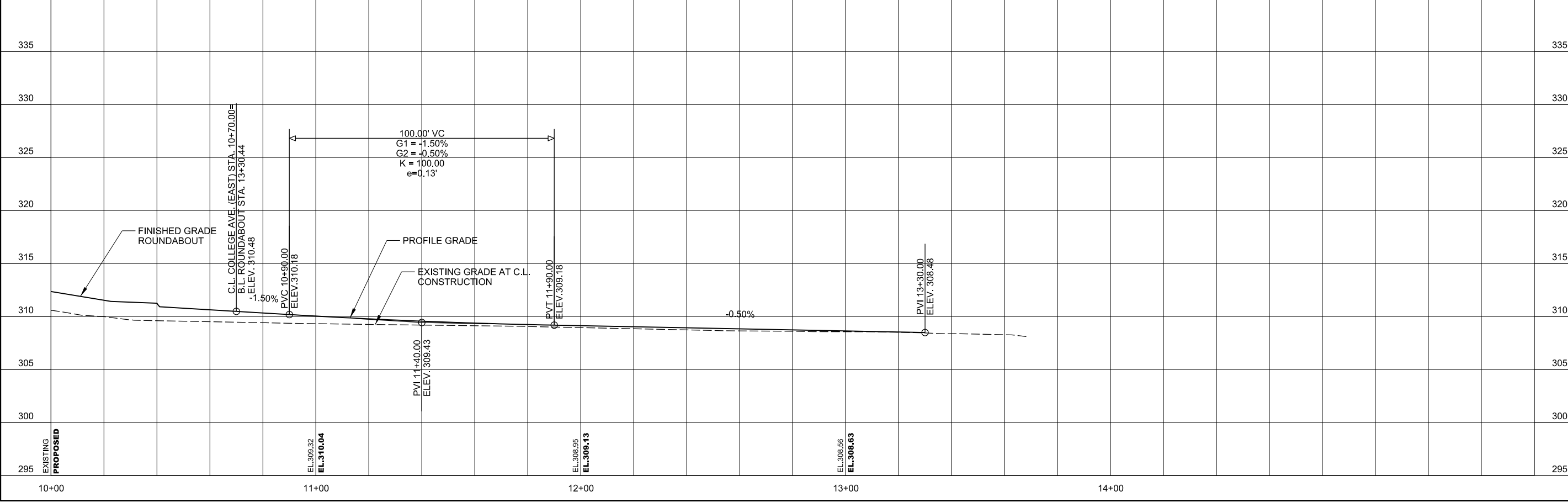
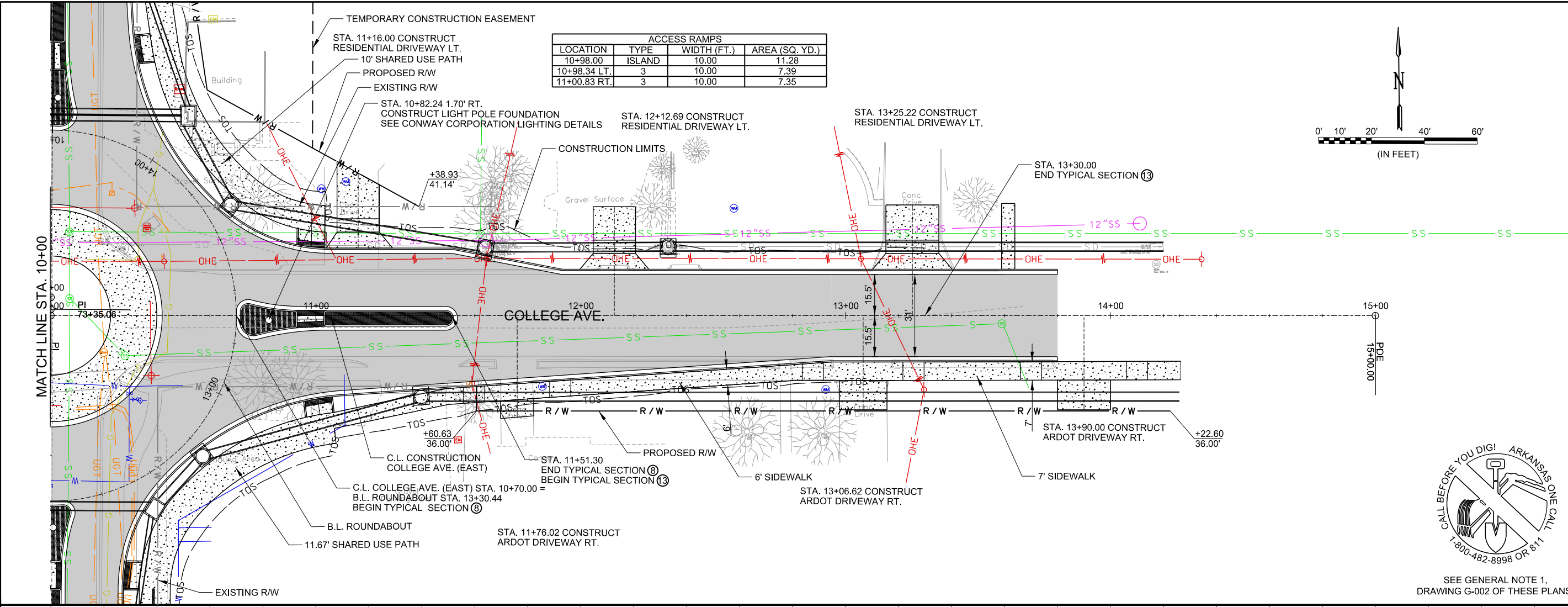
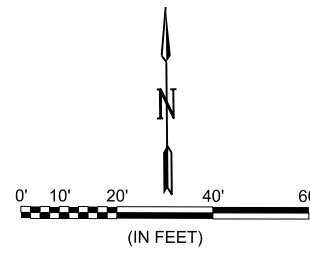
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REV.	DATE	DESCRIPTION



SEE GENERAL NOTE 1, DRAWING G-002 OF THESE PLANS.

ACCESS RAMPS			
LOCATION	TYPE	WIDTH (FT.)	AREA (SQ. YD.)
10+98.00	ISLAND	10.00	11.28
10+98.34 LT.	3	10.00	7.39
11+00.83 RT.	3	10.00	7.35



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

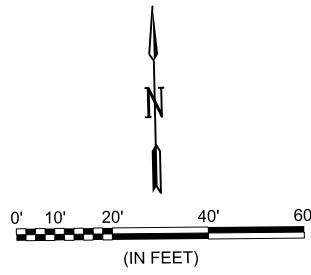
PLAN AND PROFILE -
 COLLEGE AVE.
 (SHEET 2 OF 2)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
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DRAWING NUMBER
C-614

SHEET NUMBER
80



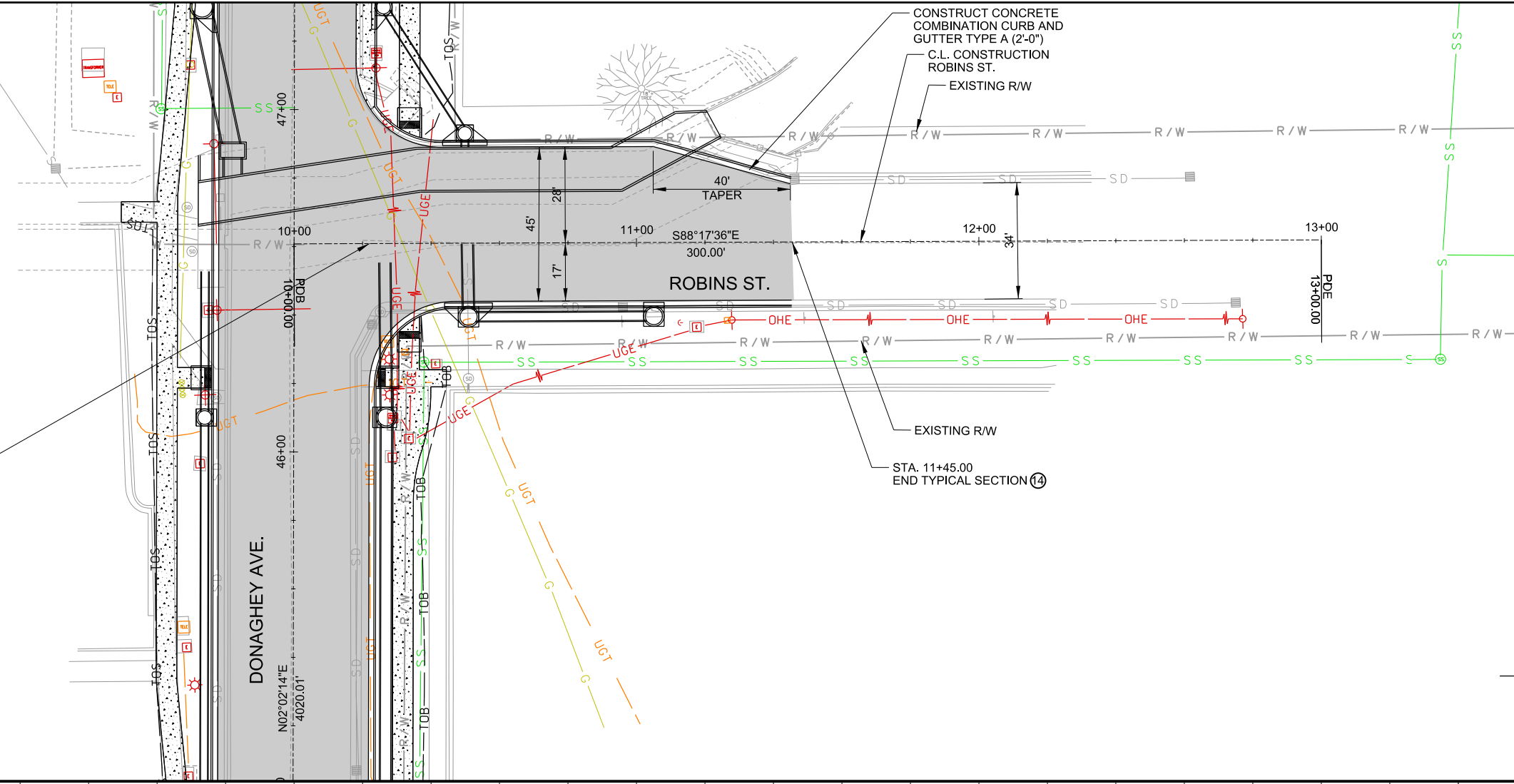
STA. 10+22.00
BEGIN TYPICAL SECTION 14

CONSTRUCT CONCRETE
COMBINATION CURB AND
GUTTER TYPE A (2'-0")
C.L. CONSTRUCTION
ROBINS ST.

EXISTING R/W

EXISTING R/W

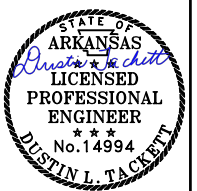
STA. 11+45.00
END TYPICAL SECTION 14



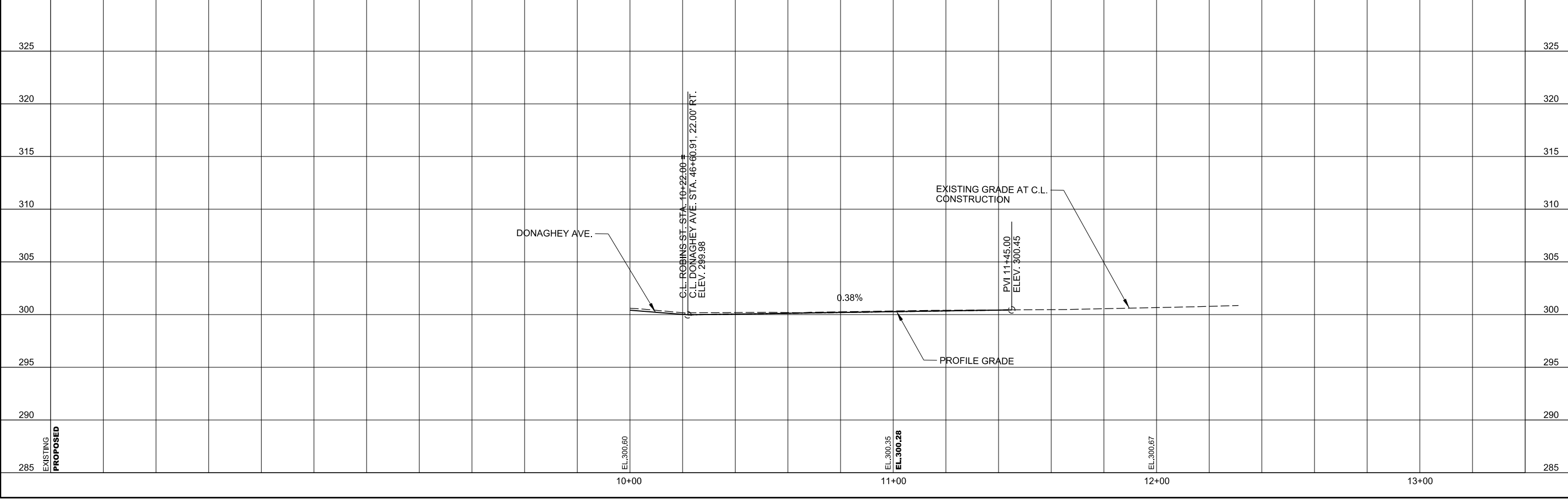
SEE GENERAL NOTE 1,
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CITY OF CONWAY
CONWAY, ARKANSAS
**DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)**

PLAN AND PROFILE -
ROBINS ST.

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

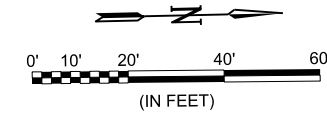
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DRAWING NUMBER
C-615

SHEET NUMBER
81

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NOTE:
STATION AND OFFSET SHOWN TO CENTER OF DRAINAGE STRUCTURE (TYP.)



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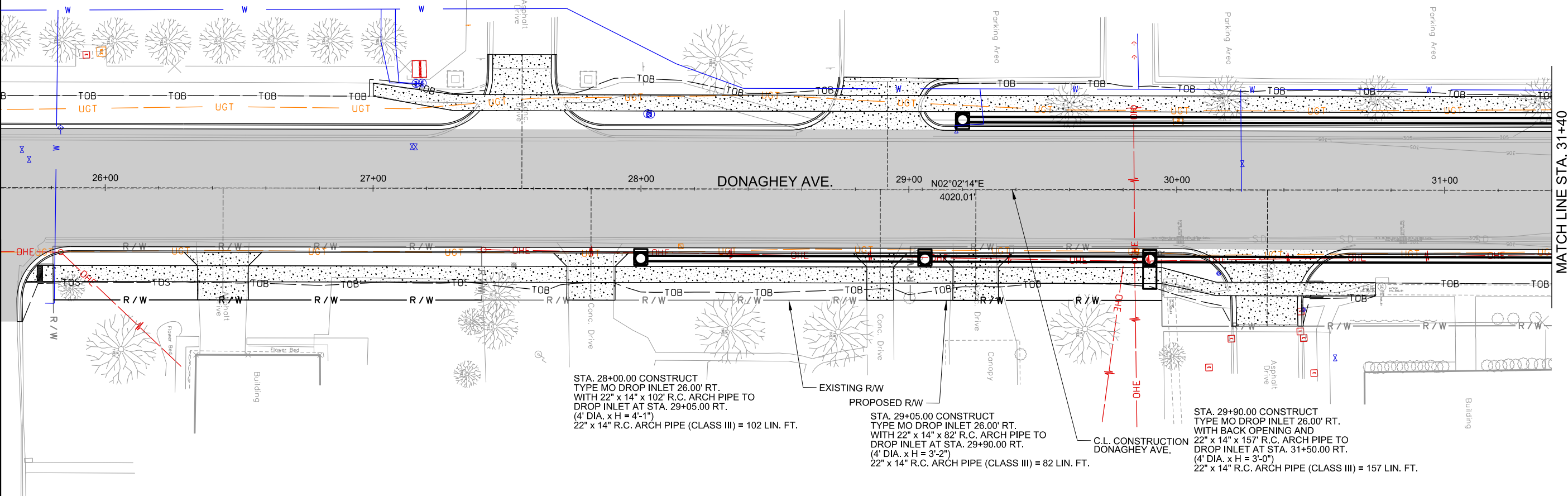
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STA. 29+20.00 CONSTRUCT
TYPE MO DROP INLET 26.00' LT.
WITH 22" x 14" x 227' R.C. ARCH PIPE TO
DROP INLET AT STA. 31+50.00 LT.
(4' DIA. x H = 3'-0")
22" x 14" R.C. ARCH PIPE (CLASS III) = 227 LIN. FT.

STA. 28+00.00 CONSTRUCT
TYPE MO DROP INLET 26.00' RT.
WITH 22" x 14" x 102' R.C. ARCH PIPE TO
DROP INLET AT STA. 29+05.00 RT.
(4' DIA. x H = 4'-1")
22" x 14" R.C. ARCH PIPE (CLASS III) = 102 LIN. FT.

STA. 29+05.00 CONSTRUCT
TYPE MO DROP INLET 26.00' RT.
WITH 22" x 14" x 82' R.C. ARCH PIPE TO
DROP INLET AT STA. 29+90.00 RT.
(4' DIA. x H = 3'-2")
22" x 14" R.C. ARCH PIPE (CLASS III) = 82 LIN. FT.

STA. 29+90.00 CONSTRUCT
TYPE MO DROP INLET 26.00' RT.
WITH BACK OPENING AND
22" x 14" x 157' R.C. ARCH PIPE TO
DROP INLET AT STA. 31+50.00 RT.
(4' DIA. x H = 3'-0")
22" x 14" R.C. ARCH PIPE (CLASS III) = 157 LIN. FT.



MATCH LINE STA. 31+40

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

DRAINAGE PLAN AND PROFILE -
DONAGHEY AVE.
(SHEET 1 OF 11)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
G-701

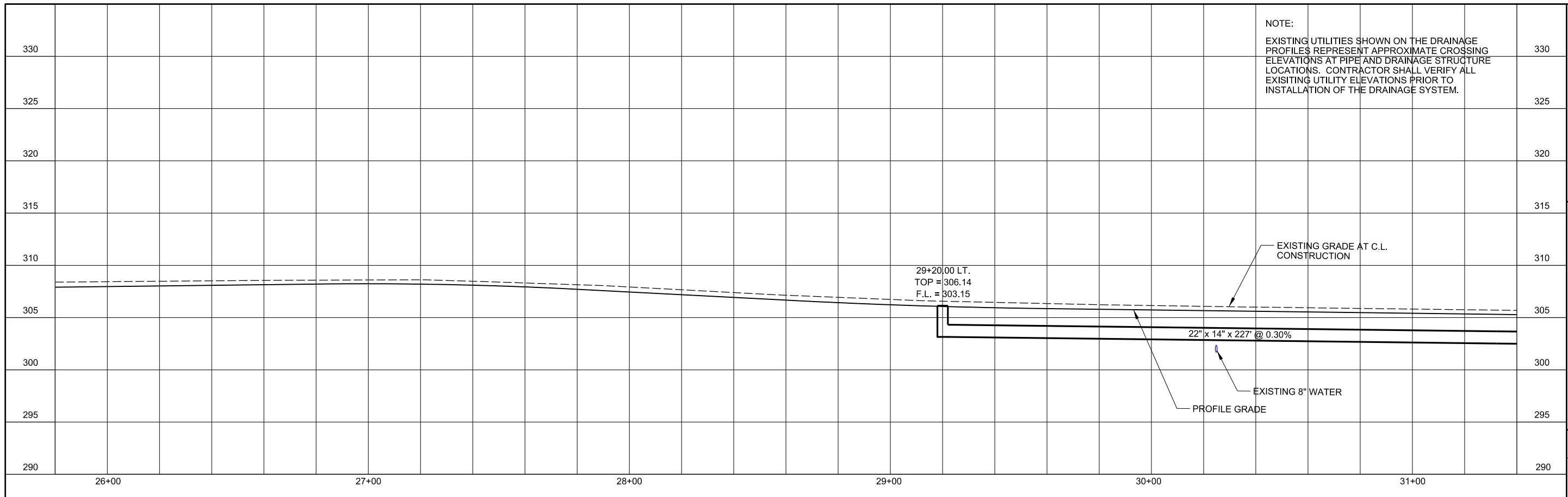
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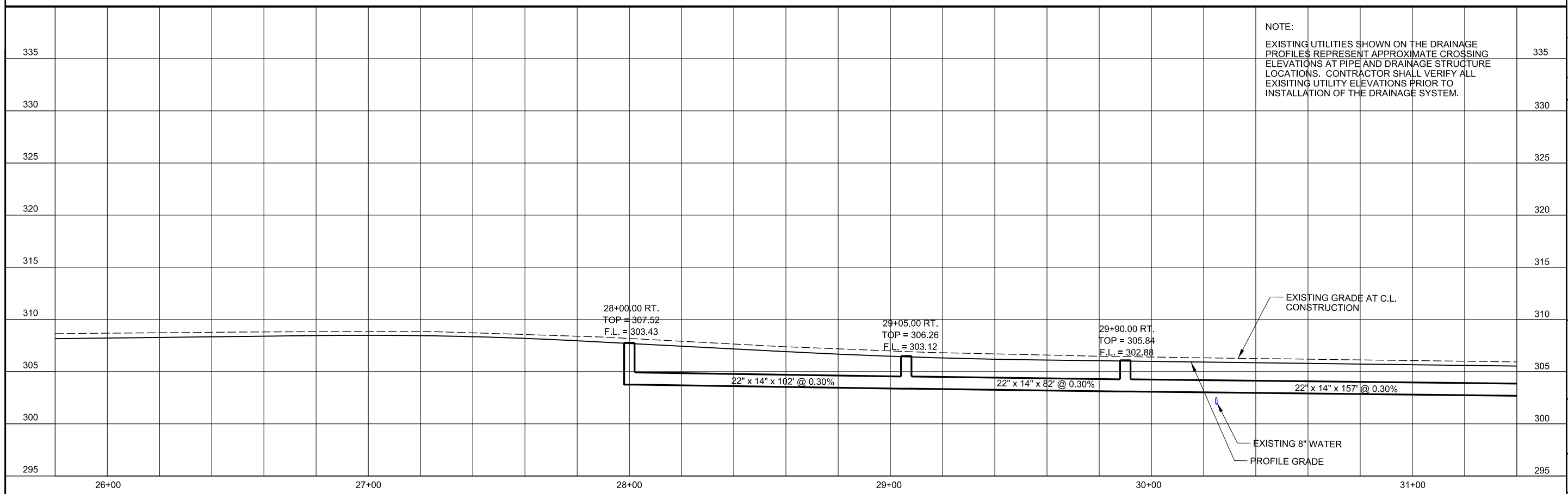


SEE GENERAL NOTE 1,
DRAWING G-002 OF THESE PLANS.


5/22/2020 3:47:53 PM
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LEFT SIDE



RIGHT SIDE



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DATE

REV.



STATE OF ARKANSAS
 LICENSED PROFESSIONAL ENGINEER
 No. 14994
 JUSTIN L. TACKETT
 Digitally Signed 05/23/2020

CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

DRAINAGE PLAN AND PROFILE - DONAGHEY AVE. (SHEET 2 OF 11)

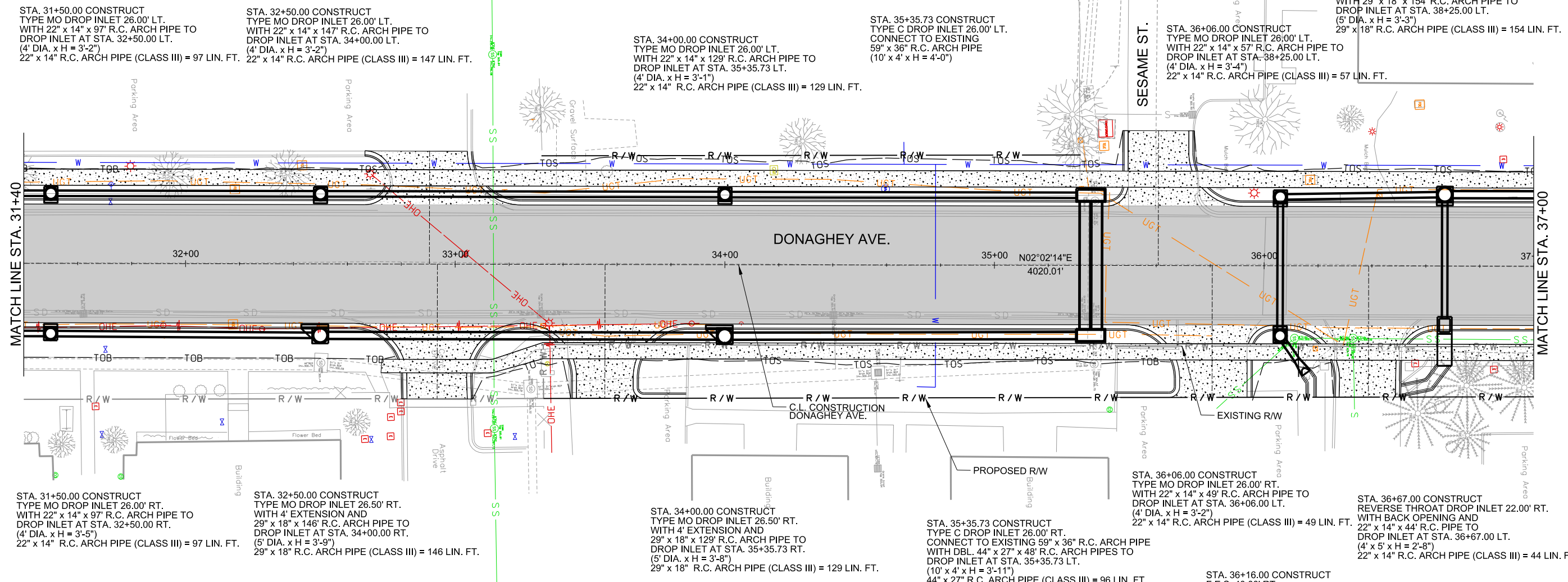
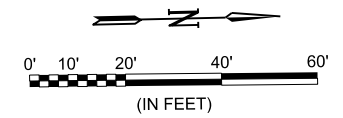
JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-702

SHEET NUMBER
83

NOTE:
STATION AND OFFSET SHOWN TO CENTER OF DRAINAGE STRUCTURE (TYP.)



STA. 31+50.00 CONSTRUCT
TYPE MO DROP INLET 26.00' RT.
WITH 22" x 14" x 97' R.C. ARCH PIPE TO
DROP INLET AT STA. 32+50.00 RT.
(4' DIA. x H = 3'-5")
22" x 14" R.C. ARCH PIPE (CLASS III) = 97 LIN. FT.

STA. 32+50.00 CONSTRUCT
TYPE MO DROP INLET 26.50' RT.
WITH 4' EXTENSION AND
29" x 18" x 146' R.C. ARCH PIPE TO
DROP INLET AT STA. 34+00.00 RT.
(5' DIA. x H = 3'-9")
29" x 18" R.C. ARCH PIPE (CLASS III) = 146 LIN. FT.

STA. 34+00.00 CONSTRUCT
TYPE MO DROP INLET 26.50' RT.
WITH 4' EXTENSION AND
29" x 18" x 129' R.C. ARCH PIPE TO
DROP INLET AT STA. 35+35.73 RT.
(5' DIA. x H = 3'-8")
29" x 18" R.C. ARCH PIPE (CLASS III) = 129 LIN. FT.

STA. 35+35.73 CONSTRUCT
TYPE C DROP INLET 26.00' RT.
CONNECT TO EXISTING 59" x 36" R.C. ARCH PIPE
WITH DBL. 44" x 27" x 48' R.C. ARCH PIPES TO
DROP INLET AT STA. 35+35.73 LT.
(10' x 4' x H = 3'-11")
44" x 27" R.C. ARCH PIPE (CLASS III) = 96 LIN. FT.

STA. 36+06.00 CONSTRUCT
TYPE MO DROP INLET 26.00' RT.
WITH 22" x 14" x 49' R.C. ARCH PIPE TO
DROP INLET AT STA. 36+06.00 LT.
(4' DIA. x H = 3'-2")
22" x 14" R.C. ARCH PIPE (CLASS III) = 49 LIN. FT.

STA. 36+67.00 CONSTRUCT
REVERSE THROAT DROP INLET 22.00' RT.
WITH BACK OPENING AND
22" x 14" x 44' R.C. PIPE TO
DROP INLET AT STA. 36+67.00 LT.
(4' x 5' x H = 2'-8")
22" x 14" R.C. ARCH PIPE (CLASS III) = 44 LIN. FT.



SEE GENERAL NOTE 1,
DRAWING G-002 OF THESE PLANS.



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CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

DRAINAGE PLAN AND
PROFILE -
DONAGHEY AVE.
(SHEET 3 OF 11)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

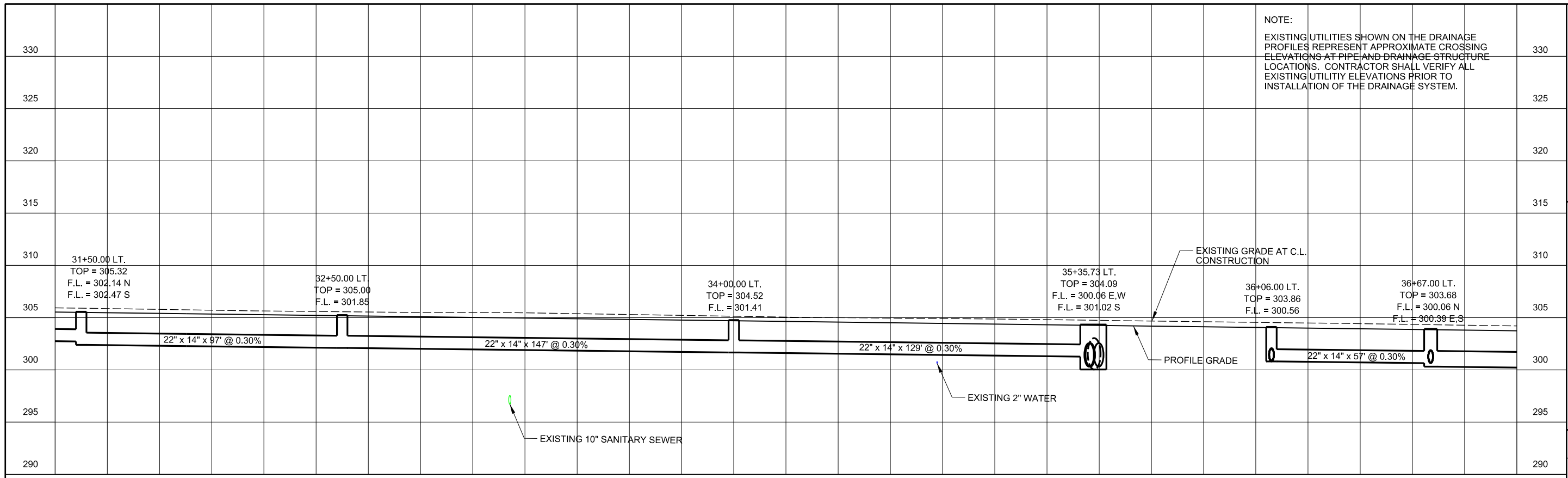
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SHEET NUMBER
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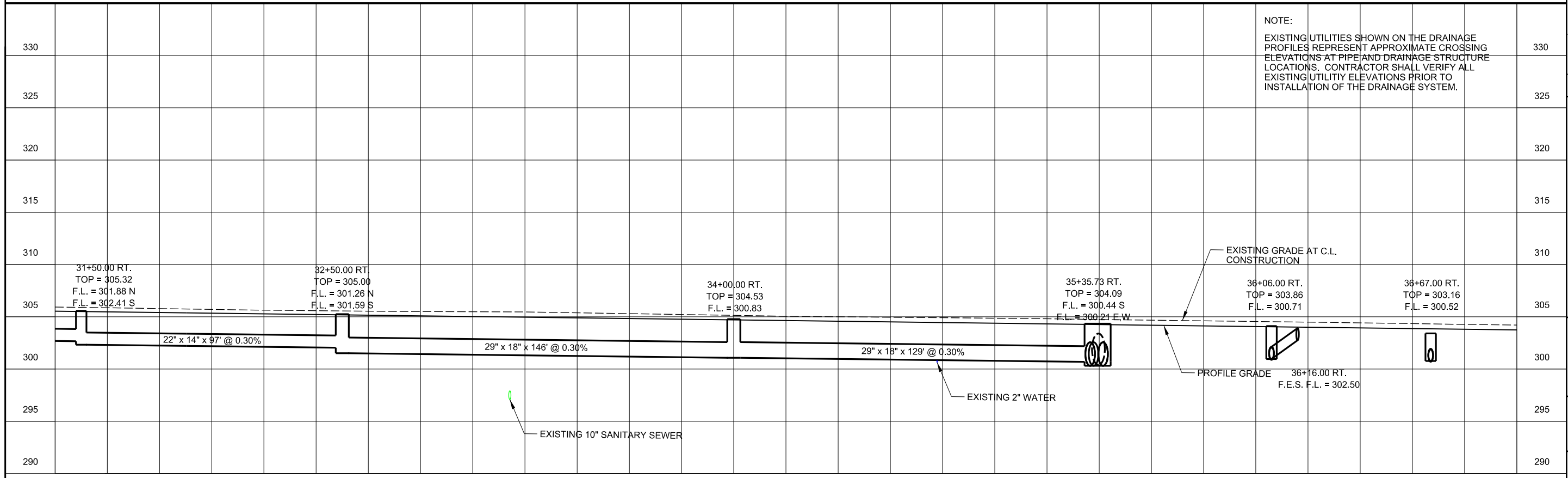
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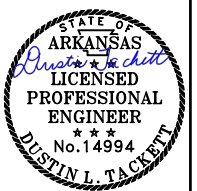
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NOTE:
 EXISTING UTILITIES SHOWN ON THE DRAINAGE PROFILES REPRESENT APPROXIMATE CROSSING ELEVATIONS AT PIPE AND DRAINAGE STRUCTURE LOCATIONS. CONTRACTOR SHALL VERIFY ALL EXISTING UTILITY ELEVATIONS PRIOR TO INSTALLATION OF THE DRAINAGE SYSTEM.

NOTE:
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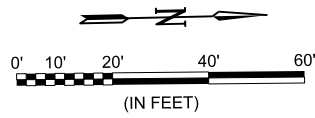
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

DRAINAGE PLAN AND PROFILE - DONAGHEY AVE. (SHEET 4 OF 11)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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C-704
 SHEET NUMBER
85



NOTE:
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CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

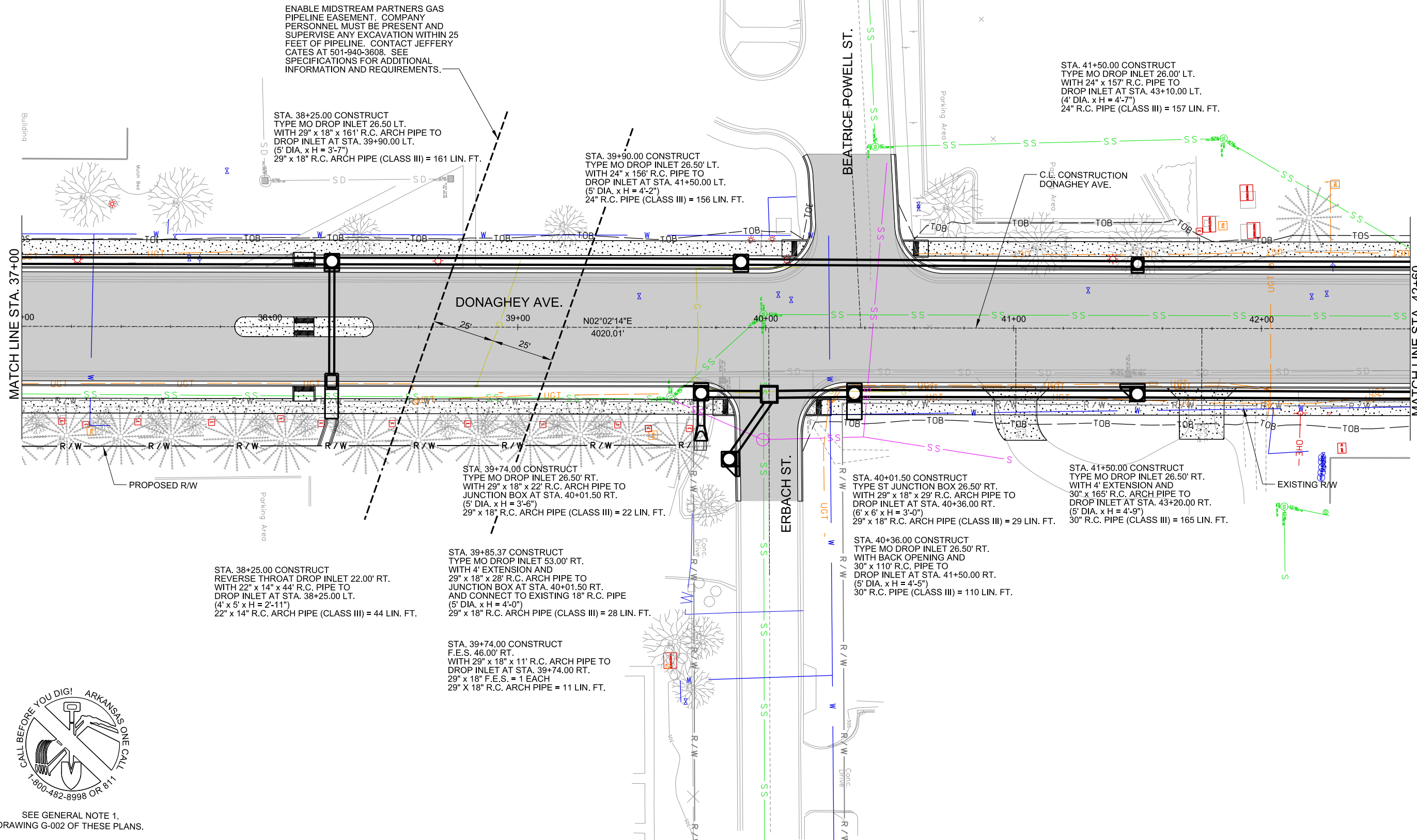
DRAINAGE PLAN AND PROFILE -
DONAGHEY AVE.
(SHEET 5 OF 11)

JOB NO.: 18147010
DATE: MAY 2020
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DRAWING NUMBER
C-705

SHEET NUMBER
86



ENABLE MIDSTREAM PARTNERS GAS PIPELINE EASEMENT. COMPANY PERSONNEL MUST BE PRESENT AND SUPERVISE ANY EXCAVATION WITHIN 25 FEET OF PIPELINE. CONTACT JEFFERY CATES AT 501-940-3608. SEE SPECIFICATIONS FOR ADDITIONAL INFORMATION AND REQUIREMENTS.

STA. 38+25.00 CONSTRUCT TYPE MO DROP INLET 26.50 LT. WITH 29" x 18" x 161' R.C. ARCH PIPE TO DROP INLET AT STA. 39+90.00 LT. (5' DIA. x H = 3'-7") 29" x 18" R.C. ARCH PIPE (CLASS III) = 161 LIN. FT.

STA. 39+90.00 CONSTRUCT TYPE MO DROP INLET 26.50' LT. WITH 24" x 156' R.C. PIPE TO DROP INLET AT STA. 41+50.00 LT. (5' DIA. x H = 4'-2") 24" R.C. PIPE (CLASS III) = 156 LIN. FT.

STA. 41+50.00 CONSTRUCT TYPE MO DROP INLET 26.00' LT. WITH 24" x 157' R.C. PIPE TO DROP INLET AT STA. 43+10.00 LT. (4' DIA. x H = 4'-7") 24" R.C. PIPE (CLASS III) = 157 LIN. FT.

STA. 39+74.00 CONSTRUCT TYPE MO DROP INLET 26.50' RT. WITH 29" x 18" x 22' R.C. ARCH PIPE TO JUNCTION BOX AT STA. 40+01.50 RT. (5' DIA. x H = 3'-6") 29" x 18" R.C. ARCH PIPE (CLASS III) = 22 LIN. FT.

STA. 40+01.50 CONSTRUCT TYPE ST JUNCTION BOX 26.50' RT. WITH 29" x 18" x 29' R.C. ARCH PIPE TO DROP INLET AT STA. 40+36.00 RT. (6' x 6' x H = 3'-0") 29" x 18" R.C. ARCH PIPE (CLASS III) = 29 LIN. FT.

STA. 41+50.00 CONSTRUCT TYPE MO DROP INLET 26.50' RT. WITH 4' EXTENSION AND 30" x 165' R.C. ARCH PIPE TO DROP INLET AT STA. 43+20.00 RT. (5' DIA. x H = 4'-9") 30" R.C. PIPE (CLASS III) = 165 LIN. FT.

STA. 38+25.00 CONSTRUCT REVERSE THROAT DROP INLET 22.00' RT. WITH 22" x 14" x 44' R.C. PIPE TO DROP INLET AT STA. 38+25.00 LT. (4' x 5' x H = 2'-11") 22" x 14" R.C. ARCH PIPE (CLASS III) = 44 LIN. FT.

STA. 39+85.37 CONSTRUCT TYPE MO DROP INLET 53.00' RT. WITH 4' EXTENSION AND 29" x 18" x 28' R.C. ARCH PIPE TO JUNCTION BOX AT STA. 40+01.50 RT. AND CONNECT TO EXISTING 18" R.C. PIPE (5' DIA. x H = 4'-0") 29" x 18" R.C. ARCH PIPE (CLASS III) = 28 LIN. FT.

STA. 39+74.00 CONSTRUCT F.E.S. 46.00' RT. WITH 29" x 18" x 11' R.C. ARCH PIPE TO DROP INLET AT STA. 39+74.00 RT. 29" x 18" F.E.S. = 1 EACH 29" x 18" R.C. ARCH PIPE = 11 LIN. FT.

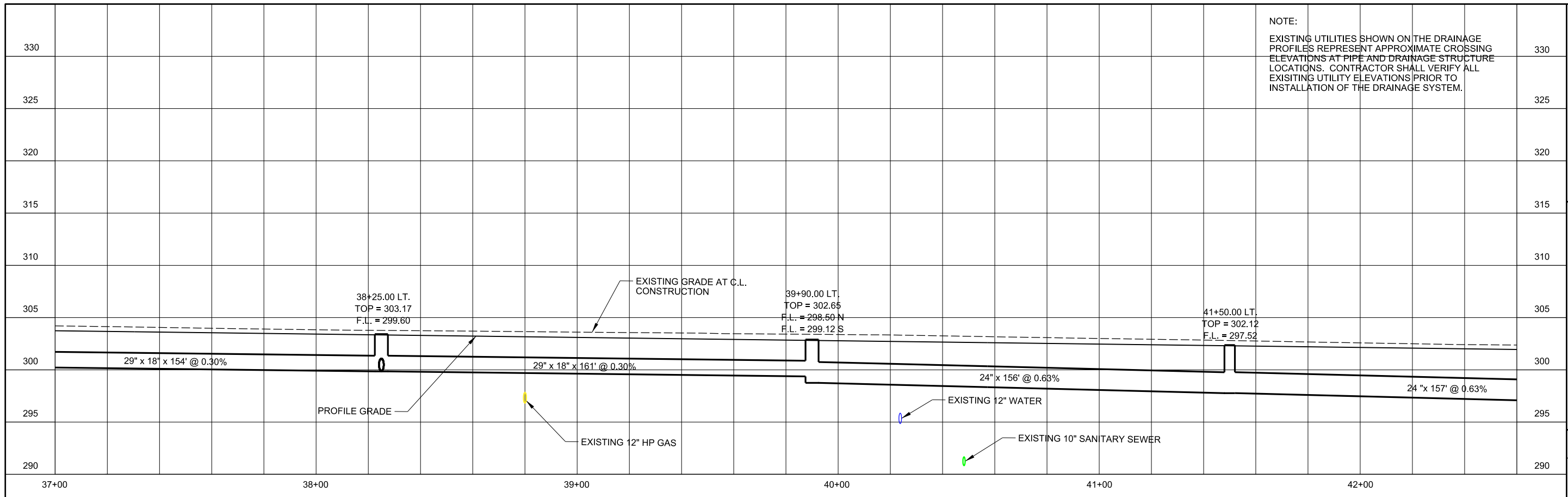
STA. 40+36.00 CONSTRUCT TYPE MO DROP INLET 26.50' RT. WITH BACK OPENING AND 30" x 110' R.C. PIPE TO DROP INLET AT STA. 41+50.00 RT. (5' DIA. x H = 4'-5") 30" R.C. PIPE (CLASS III) = 110 LIN. FT.



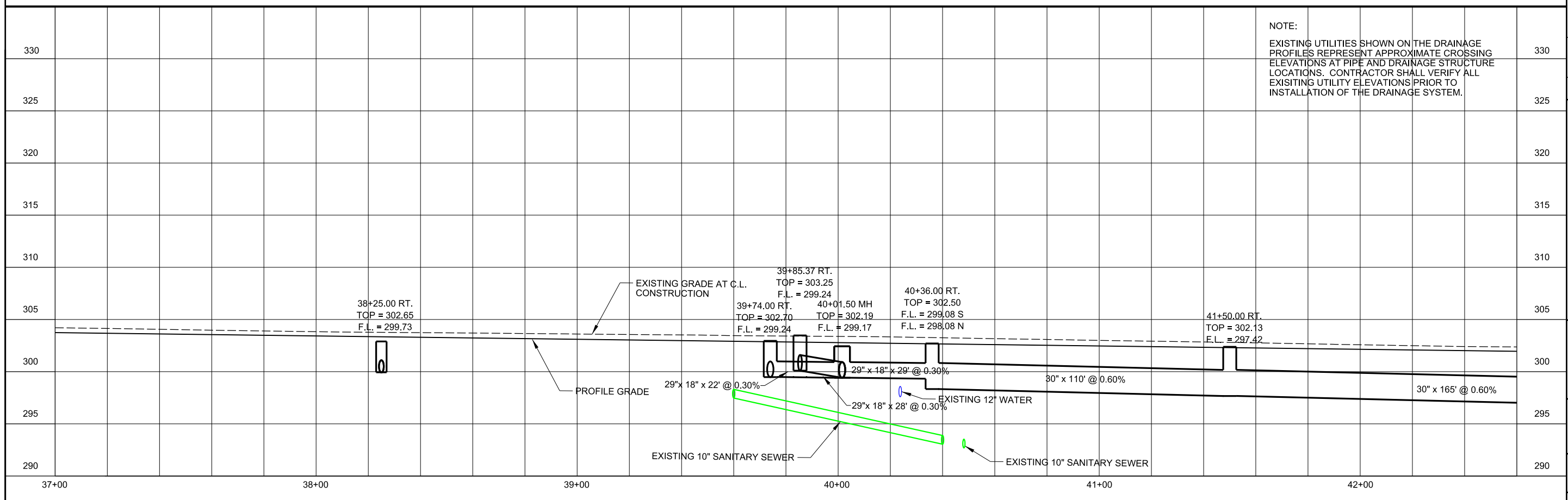
SEE GENERAL NOTE 1,
DRAWING G-002 OF THESE PLANS.

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LEFT SIDE



RIGHT SIDE

NOTE:
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NOTE:
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REV.	DATE



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

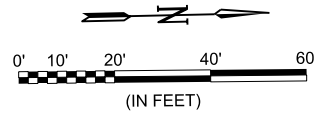
DRAINAGE PLAN AND
 PROFILE -
 DONAGHEY AVE.
 (SHEET 6 OF 11)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
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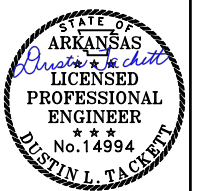
SHEET
 NUMBER **87**



NOTE:
STATION AND OFFSET SHOWN TO CENTER OF DRAINAGE STRUCTURE (TYP.)



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CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

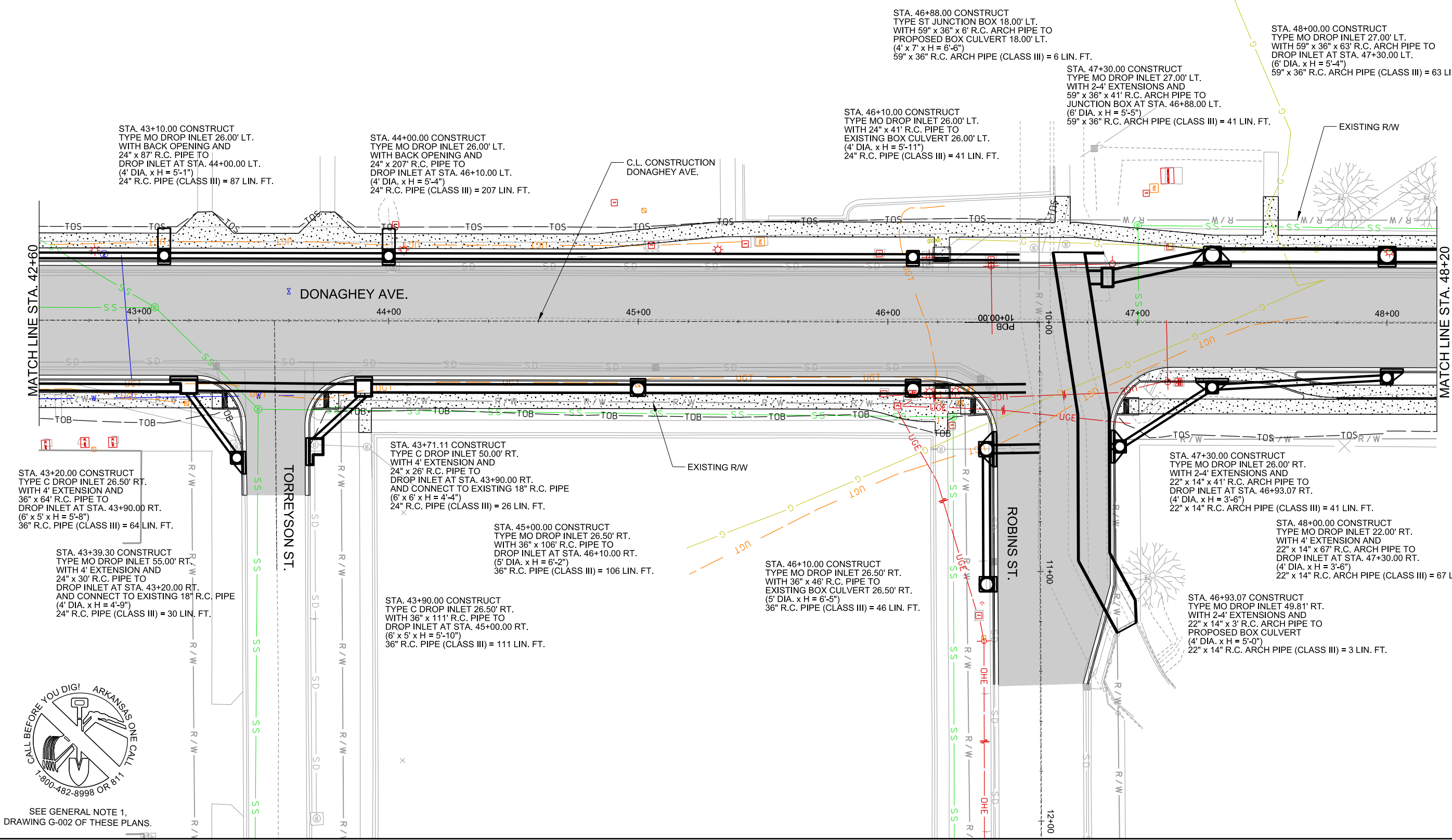
DRAINAGE PLAN AND PROFILE -
DONAGHEY AVE.
(SHEET 7 OF 11)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
C-707

SHEET NUMBER
88



STA. 43+10.00 CONSTRUCT TYPE MO DROP INLET 26.00' LT. WITH BACK OPENING AND 24" x 87" R.C. PIPE TO DROP INLET AT STA. 44+00.00 LT. (4' DIA. x H = 5'-1") 24" R.C. PIPE (CLASS III) = 87 LIN. FT.

STA. 44+00.00 CONSTRUCT TYPE MO DROP INLET 26.00' LT. WITH BACK OPENING AND 24" x 207" R.C. PIPE TO DROP INLET AT STA. 46+10.00 LT. (4' DIA. x H = 5'-4") 24" R.C. PIPE (CLASS III) = 207 LIN. FT.

STA. 46+10.00 CONSTRUCT TYPE MO DROP INLET 26.00' LT. WITH 24" x 41" R.C. PIPE TO EXISTING BOX CULVERT 26.00' LT. (4' DIA. x H = 5'-11") 24" R.C. PIPE (CLASS III) = 41 LIN. FT.

STA. 46+88.00 CONSTRUCT TYPE ST JUNCTION BOX 18.00' LT. WITH 59" x 36" x 6' R.C. ARCH PIPE TO PROPOSED BOX CULVERT 18.00' LT. (4' x 7' x H = 6'-6") 59" x 36" R.C. ARCH PIPE (CLASS III) = 6 LIN. FT.

STA. 47+30.00 CONSTRUCT TYPE MO DROP INLET 27.00' LT. WITH 2'-4" EXTENSIONS AND 59" x 36" x 41' R.C. ARCH PIPE TO JUNCTION BOX AT STA. 46+88.00 LT. (6' DIA. x H = 5'-5") 59" x 36" R.C. ARCH PIPE (CLASS III) = 41 LIN. FT.

STA. 48+00.00 CONSTRUCT TYPE MO DROP INLET 27.00' LT. WITH 59" x 36" x 63' R.C. ARCH PIPE TO DROP INLET AT STA. 47+30.00 LT. (6' DIA. x H = 5'-4") 59" x 36" R.C. ARCH PIPE (CLASS III) = 63 LIN. FT.

STA. 43+20.00 CONSTRUCT TYPE C DROP INLET 26.50' RT. WITH 4' EXTENSION AND 36" x 64" R.C. PIPE TO DROP INLET AT STA. 43+90.00 RT. (6' x 5' x H = 5'-8") 36" R.C. PIPE (CLASS III) = 64 LIN. FT.

STA. 43+39.30 CONSTRUCT TYPE MO DROP INLET 55.00' RT. WITH 4' EXTENSION AND 24" x 30' R.C. PIPE TO DROP INLET AT STA. 43+20.00 RT. AND CONNECT TO EXISTING 18" R.C. PIPE (4' DIA. x H = 4'-9") 24" R.C. PIPE (CLASS III) = 30 LIN. FT.

STA. 43+71.11 CONSTRUCT TYPE C DROP INLET 50.00' RT. WITH 4' EXTENSION AND 24" x 26' R.C. PIPE TO DROP INLET AT STA. 43+90.00 RT. AND CONNECT TO EXISTING 18" R.C. PIPE (6' x 6' x H = 4'-4") 24" R.C. PIPE (CLASS III) = 26 LIN. FT.

STA. 45+00.00 CONSTRUCT TYPE MO DROP INLET 26.50' RT. WITH 36" x 106' R.C. PIPE TO DROP INLET AT STA. 46+10.00 RT. (5' DIA. x H = 6'-2") 36" R.C. PIPE (CLASS III) = 106 LIN. FT.

STA. 43+90.00 CONSTRUCT TYPE C DROP INLET 26.50' RT. WITH 36" x 111' R.C. PIPE TO DROP INLET AT STA. 45+00.00 RT. (6' x 5' x H = 5'-10") 36" R.C. PIPE (CLASS III) = 111 LIN. FT.

STA. 46+10.00 CONSTRUCT TYPE MO DROP INLET 26.50' RT. WITH 36" x 46' R.C. PIPE TO EXISTING BOX CULVERT 26.50' RT. (5' DIA. x H = 6'-5") 36" R.C. PIPE (CLASS III) = 46 LIN. FT.

STA. 47+30.00 CONSTRUCT TYPE MO DROP INLET 26.00' RT. WITH 2'-4" EXTENSIONS AND 22" x 14" x 41' R.C. ARCH PIPE TO DROP INLET AT STA. 46+93.07 RT. (4' DIA. x H = 3'-6") 22" x 14" R.C. ARCH PIPE (CLASS III) = 41 LIN. FT.

STA. 48+00.00 CONSTRUCT TYPE MO DROP INLET 22.00' RT. WITH 4' EXTENSION AND 22" x 14" x 67' R.C. ARCH PIPE TO DROP INLET AT STA. 47+30.00 RT. (4' DIA. x H = 3'-6") 22" x 14" R.C. ARCH PIPE (CLASS III) = 67 LIN. FT.

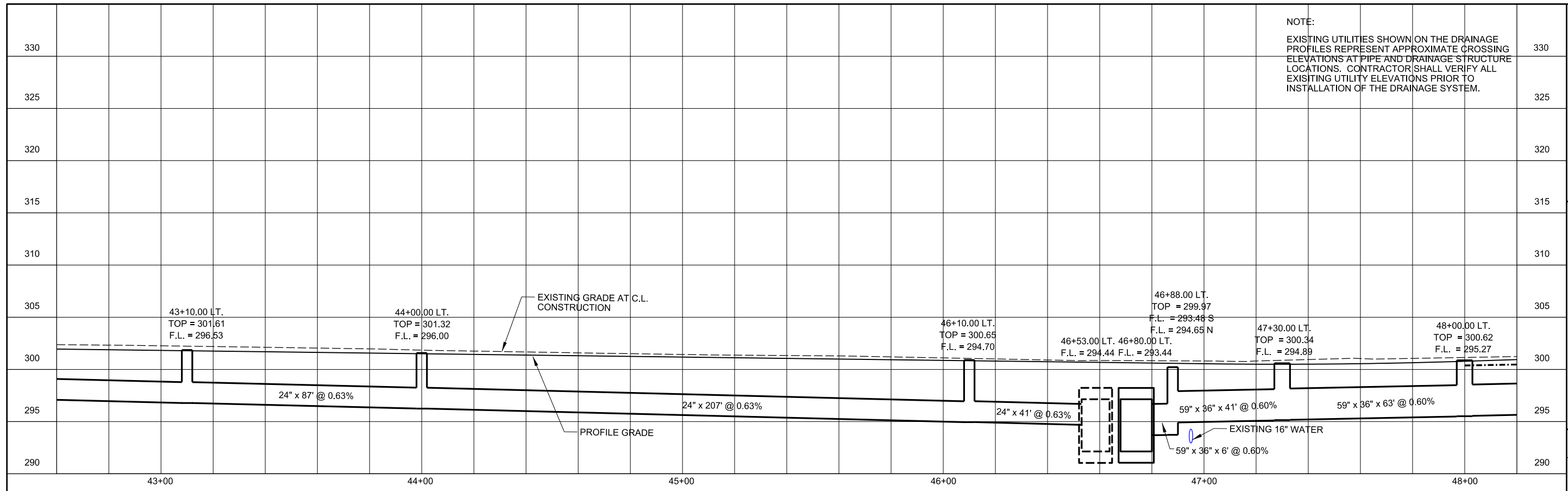
STA. 46+93.07 CONSTRUCT TYPE MO DROP INLET 49.81' RT. WITH 2'-4" EXTENSIONS AND 22" x 14" x 3' R.C. ARCH PIPE TO PROPOSED BOX CULVERT (4' DIA. x H = 5'-0") 22" x 14" R.C. ARCH PIPE (CLASS III) = 3 LIN. FT.

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SEE GENERAL NOTE 1,
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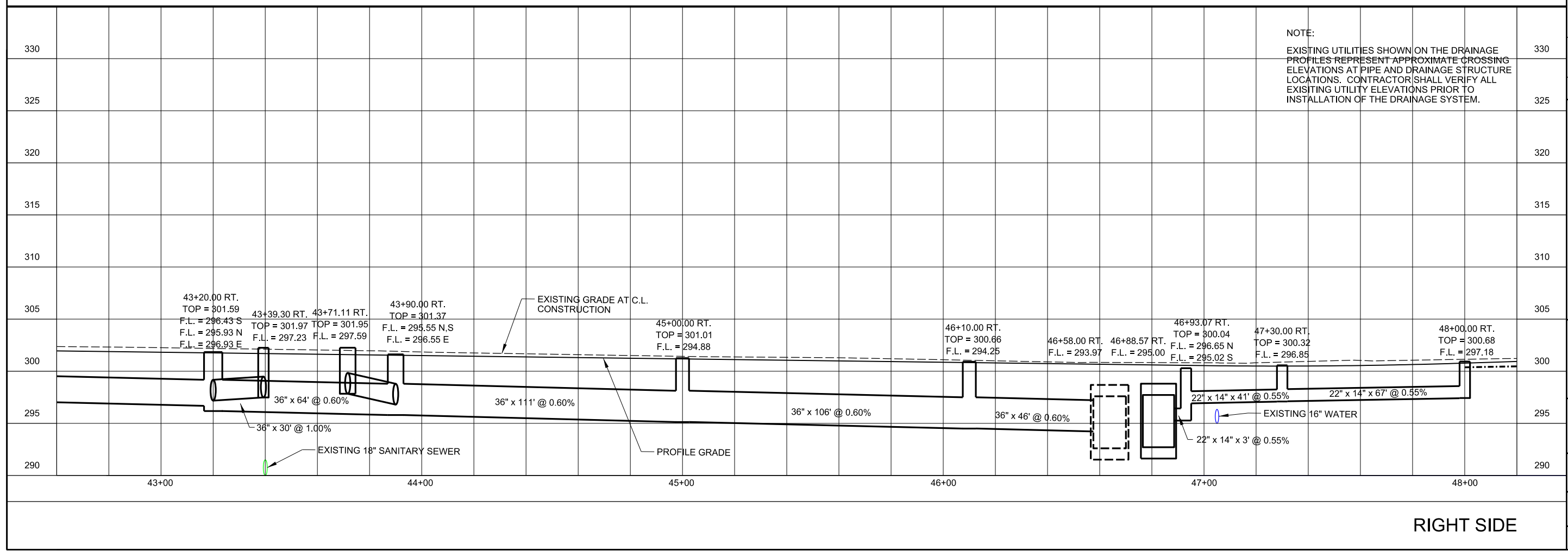
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REV.	DATE



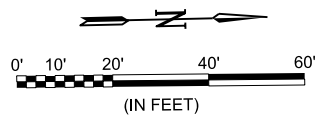
CITY OF CONWAY
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DRAINAGE PLAN AND PROFILE - DONAGHEY AVE. (SHEET 8 OF 11)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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C-708
 SHEET NUMBER
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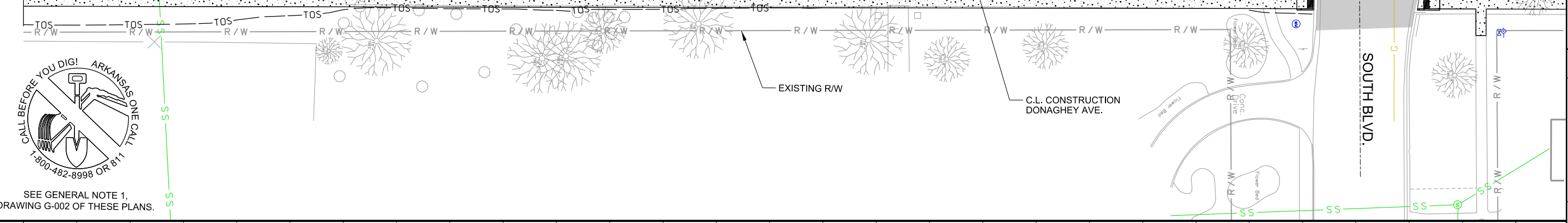


STA. 49+30.00 CONSTRUCT
TYPE MO DROP INLET 27.00' LT.
WITH 59" x 36" x 128' R.C. ARCH PIPE TO
DROP INLET AT STA. 47+30.00 LT.
(6' DIA. x H = 5'-2")
59" x 36" R.C. ARCH PIPE (CLASS III) = 128 LIN. FT.

STA. 50+20.00 CONSTRUCT
TYPE ST JUNCTION BOX
WITH 59" x 36" x 87' R.C. ARCH PIPE TO
DROP INLET AT STA. 49+20.00 LT.
AND CONNECT TO EXISTING STONE BOX CULVERT
AND CONNECT TO EXISTING DROP INLET LT.
(4' x 7' x H = 4'-9")
59" x 36" R.C. ARCH PIPE (CLASS III) = 87 LIN. FT.

FIELD VERIFY OFFSET
CENTER ON EXISTING
STONE BOX CULVERT

MATCH LINE STA. 48+20



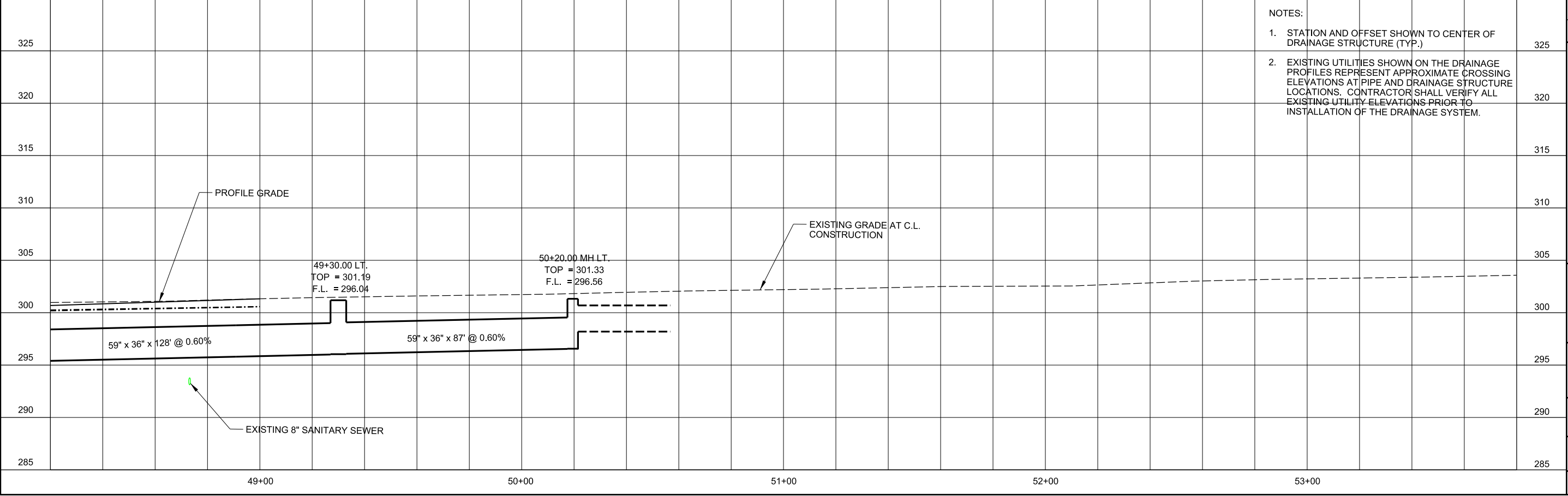
SEE GENERAL NOTE 1,
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REV.	DATE	DESCRIPTION	BY

- NOTES:
1. STATION AND OFFSET SHOWN TO CENTER OF DRAINAGE STRUCTURE (TYP.)
 2. EXISTING UTILITIES SHOWN ON THE DRAINAGE PROFILES REPRESENT APPROXIMATE CROSSING ELEVATIONS AT PIPE AND DRAINAGE STRUCTURE LOCATIONS. CONTRACTOR SHALL VERIFY ALL EXISTING UTILITY ELEVATIONS PRIOR TO INSTALLATION OF THE DRAINAGE SYSTEM.



CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

DRAINAGE PLAN AND
PROFILE -
DONAGHEY AVE.
(SHEET 9 OF 11)

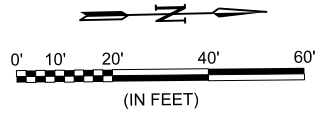
JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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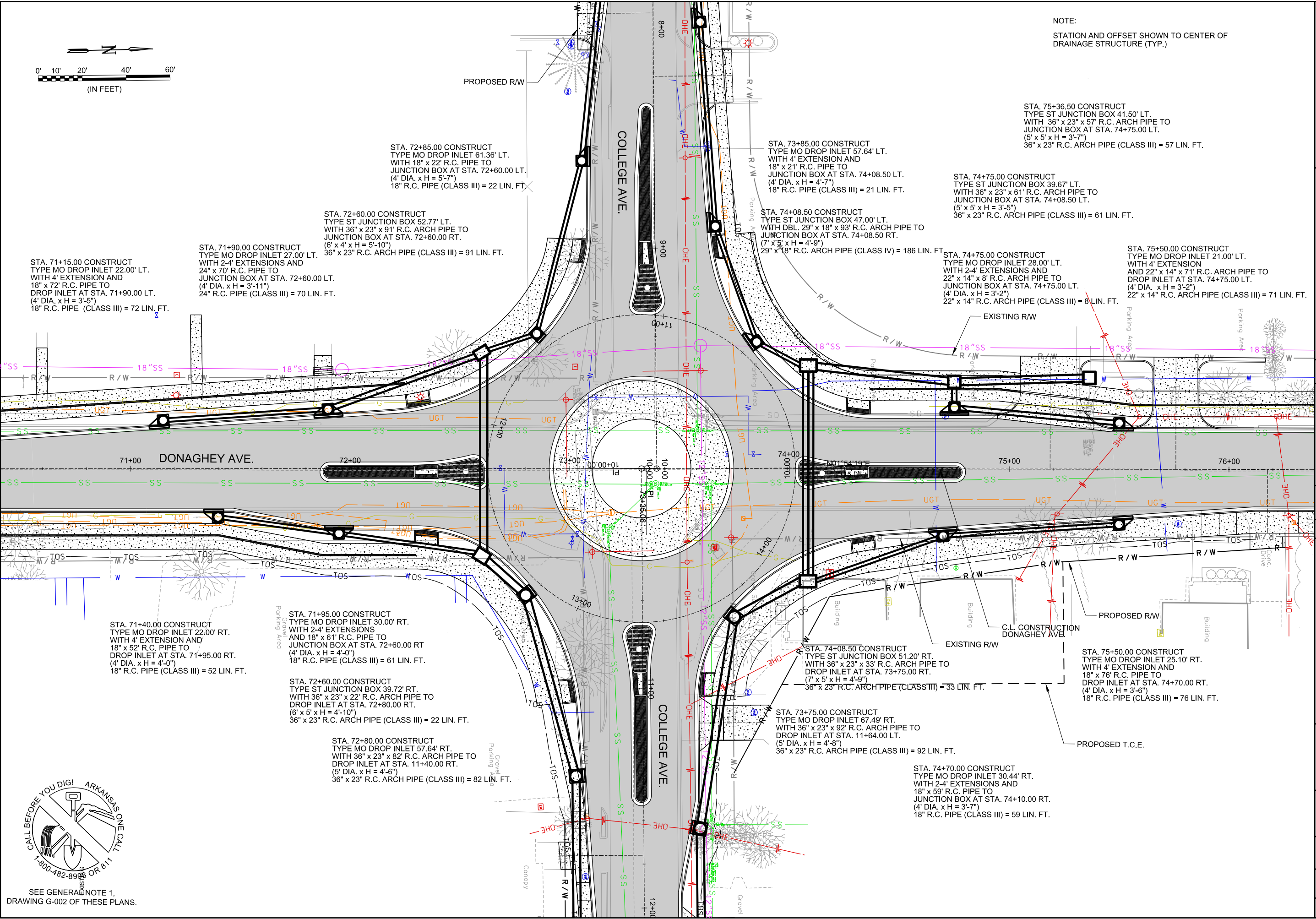
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SHEET NUMBER
90

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NOTE:
STATION AND OFFSET SHOWN TO CENTER OF DRAINAGE STRUCTURE (TYP.)



STA. 71+15.00 CONSTRUCT TYPE MO DROP INLET 22.00' LT. WITH 4' EXTENSION AND 18" x 72" R.C. PIPE TO DROP INLET AT STA. 71+90.00 LT. (4' DIA. x H = 3'-5") 18" R.C. PIPE (CLASS III) = 72 LIN. FT.

STA. 71+90.00 CONSTRUCT TYPE MO DROP INLET 27.00' LT. WITH 2-4' EXTENSIONS AND 24" x 70" R.C. PIPE TO JUNCTION BOX AT STA. 72+60.00 LT. (4' DIA. x H = 3'-11") 24" R.C. PIPE (CLASS III) = 70 LIN. FT.

STA. 71+95.00 CONSTRUCT TYPE MO DROP INLET 30.00' RT. WITH 2-4' EXTENSIONS AND 18" x 61" R.C. PIPE TO JUNCTION BOX AT STA. 72+60.00 RT (4' DIA. x H = 4'-0") 18" R.C. PIPE (CLASS III) = 61 LIN. FT.

STA. 71+40.00 CONSTRUCT TYPE MO DROP INLET 22.00' RT. WITH 4' EXTENSION AND 18" x 52" R.C. PIPE TO DROP INLET AT STA. 71+95.00 RT. (4' DIA. x H = 4'-0") 18" R.C. PIPE (CLASS III) = 52 LIN. FT.

STA. 72+60.00 CONSTRUCT TYPE ST JUNCTION BOX 39.72' RT. WITH 36" x 23" x 22' R.C. ARCH PIPE TO DROP INLET AT STA. 72+80.00 RT. (6' x 5' x H = 4'-10") 36" x 23" R.C. ARCH PIPE (CLASS III) = 22 LIN. FT.

STA. 72+80.00 CONSTRUCT TYPE MO DROP INLET 57.64' RT. WITH 36" x 23" x 82' R.C. ARCH PIPE TO DROP INLET AT STA. 11+40.00 RT. (5' DIA. x H = 4'-6") 36" x 23" R.C. ARCH PIPE (CLASS III) = 82 LIN. FT.

STA. 72+85.00 CONSTRUCT TYPE MO DROP INLET 61.36' LT. WITH 18" x 22' R.C. PIPE TO JUNCTION BOX AT STA. 72+60.00 LT. (4' DIA. x H = 5'-7") 18" R.C. PIPE (CLASS III) = 22 LIN. FT.

STA. 72+60.00 CONSTRUCT TYPE ST JUNCTION BOX 52.77' LT. WITH 36" x 23" x 91' R.C. ARCH PIPE TO JUNCTION BOX AT STA. 72+60.00 RT. (6' x 4' x H = 5'-10") 36" x 23" R.C. ARCH PIPE (CLASS III) = 91 LIN. FT.

STA. 73+85.00 CONSTRUCT TYPE MO DROP INLET 57.64' LT. WITH 4' EXTENSION AND 18" x 21' R.C. PIPE TO JUNCTION BOX AT STA. 74+08.50 LT. (4' DIA. x H = 4'-7") 18" R.C. PIPE (CLASS III) = 21 LIN. FT.

STA. 74+08.50 CONSTRUCT TYPE ST JUNCTION BOX 47.00' LT. WITH DBL. 29" x 18" x 93' R.C. ARCH PIPE TO JUNCTION BOX AT STA. 74+08.50 RT. (7' x 5' x H = 4'-9") 29" x 18" R.C. ARCH PIPE (CLASS IV) = 186 LIN. FT.

STA. 74+08.50 CONSTRUCT TYPE ST JUNCTION BOX 51.20' RT. WITH 36" x 23" x 33' R.C. ARCH PIPE TO DROP INLET AT STA. 73+75.00 RT. (7' x 5' x H = 4'-9") 36" x 23" R.C. ARCH PIPE (CLASS III) = 33 LIN. FT.

STA. 73+75.00 CONSTRUCT TYPE MO DROP INLET 67.49' RT. WITH 36" x 23" x 92' R.C. ARCH PIPE TO DROP INLET AT STA. 11+64.00 LT. (5' DIA. x H = 4'-8") 36" x 23" R.C. ARCH PIPE (CLASS III) = 92 LIN. FT.

STA. 74+70.00 CONSTRUCT TYPE MO DROP INLET 30.44' RT. WITH 2-4' EXTENSIONS AND 18" x 59" R.C. PIPE TO JUNCTION BOX AT STA. 74+10.00 RT. (4' DIA. x H = 3'-7") 18" R.C. PIPE (CLASS III) = 59 LIN. FT.

STA. 74+75.00 CONSTRUCT TYPE ST JUNCTION BOX 39.67' LT. WITH 36" x 23" x 61' R.C. ARCH PIPE TO JUNCTION BOX AT STA. 74+08.50 LT. (5' x 5' x H = 3'-5") 36" x 23" R.C. ARCH PIPE (CLASS III) = 61 LIN. FT.

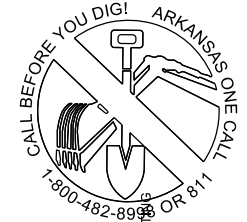
STA. 74+75.00 CONSTRUCT TYPE MO DROP INLET 28.00' LT. WITH 2-4' EXTENSIONS AND 22" x 14" x 8' R.C. ARCH PIPE TO JUNCTION BOX AT STA. 74+75.00 LT. (4' DIA. x H = 3'-2") 22" x 14" R.C. ARCH PIPE (CLASS III) = 8 LIN. FT.

STA. 75+36.50 CONSTRUCT TYPE ST JUNCTION BOX 41.50' LT. WITH 36" x 23" x 57' R.C. ARCH PIPE TO JUNCTION BOX AT STA. 74+75.00 LT. (5' x 5' x H = 3'-7") 36" x 23" R.C. ARCH PIPE (CLASS III) = 57 LIN. FT.

STA. 75+50.00 CONSTRUCT TYPE MO DROP INLET 21.00' LT. WITH 4' EXTENSION AND 22" x 14" x 71' R.C. ARCH PIPE TO DROP INLET AT STA. 74+75.00 LT. (4' DIA. x H = 3'-2") 22" x 14" R.C. ARCH PIPE (CLASS III) = 71 LIN. FT.

STA. 75+50.00 CONSTRUCT TYPE MO DROP INLET 25.10' RT. WITH 4' EXTENSION AND 18" x 76" R.C. PIPE TO DROP INLET AT STA. 74+70.00 RT. (4' DIA. x H = 3'-6") 18" R.C. PIPE (CLASS III) = 76 LIN. FT.

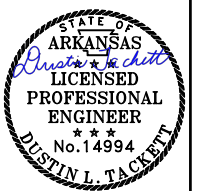
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CITY OF CONWAY
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DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

DRAINAGE PLAN AND PROFILE -
DONAGHEY AVE.
(SHEET 10 OF 11)

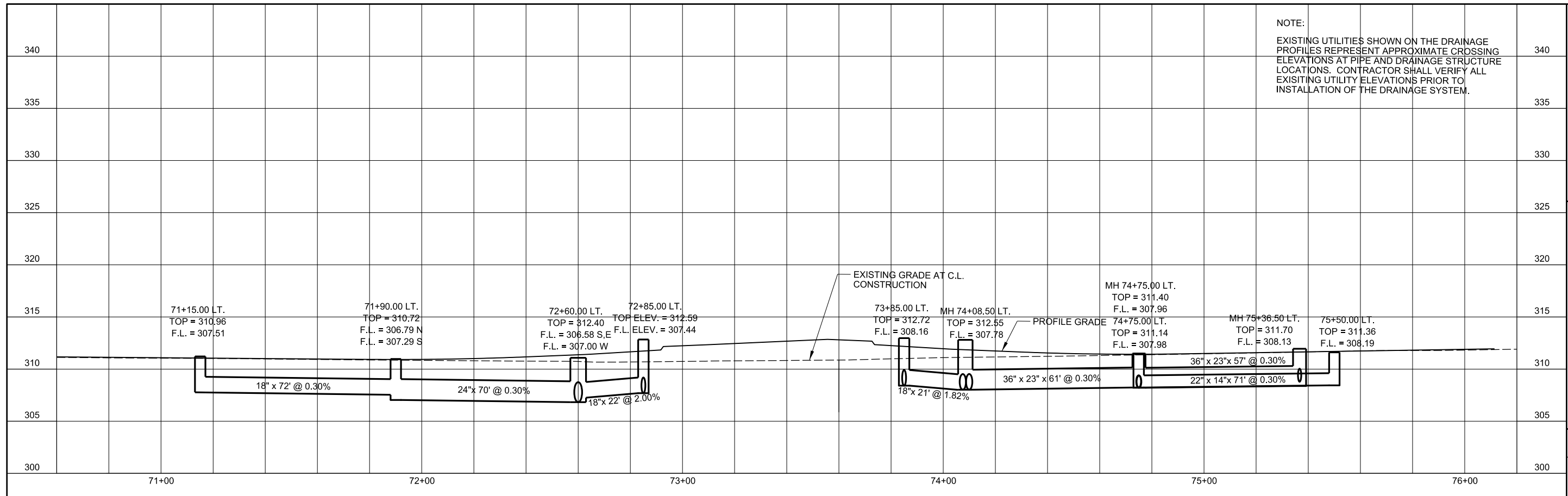
JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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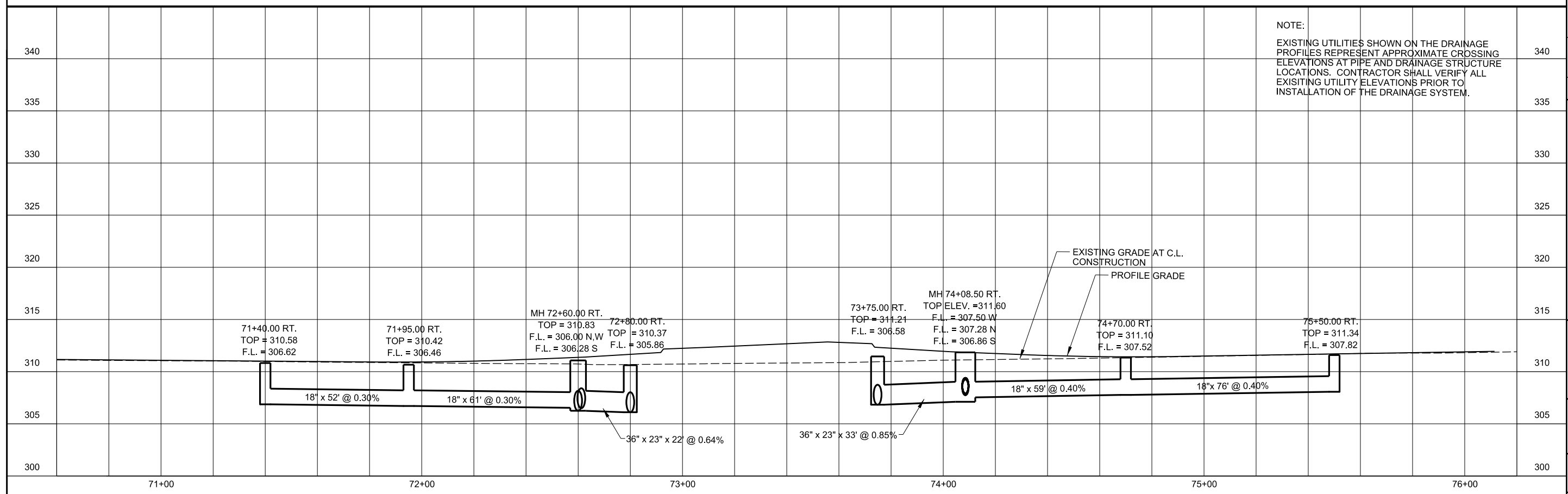
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REV.	DATE



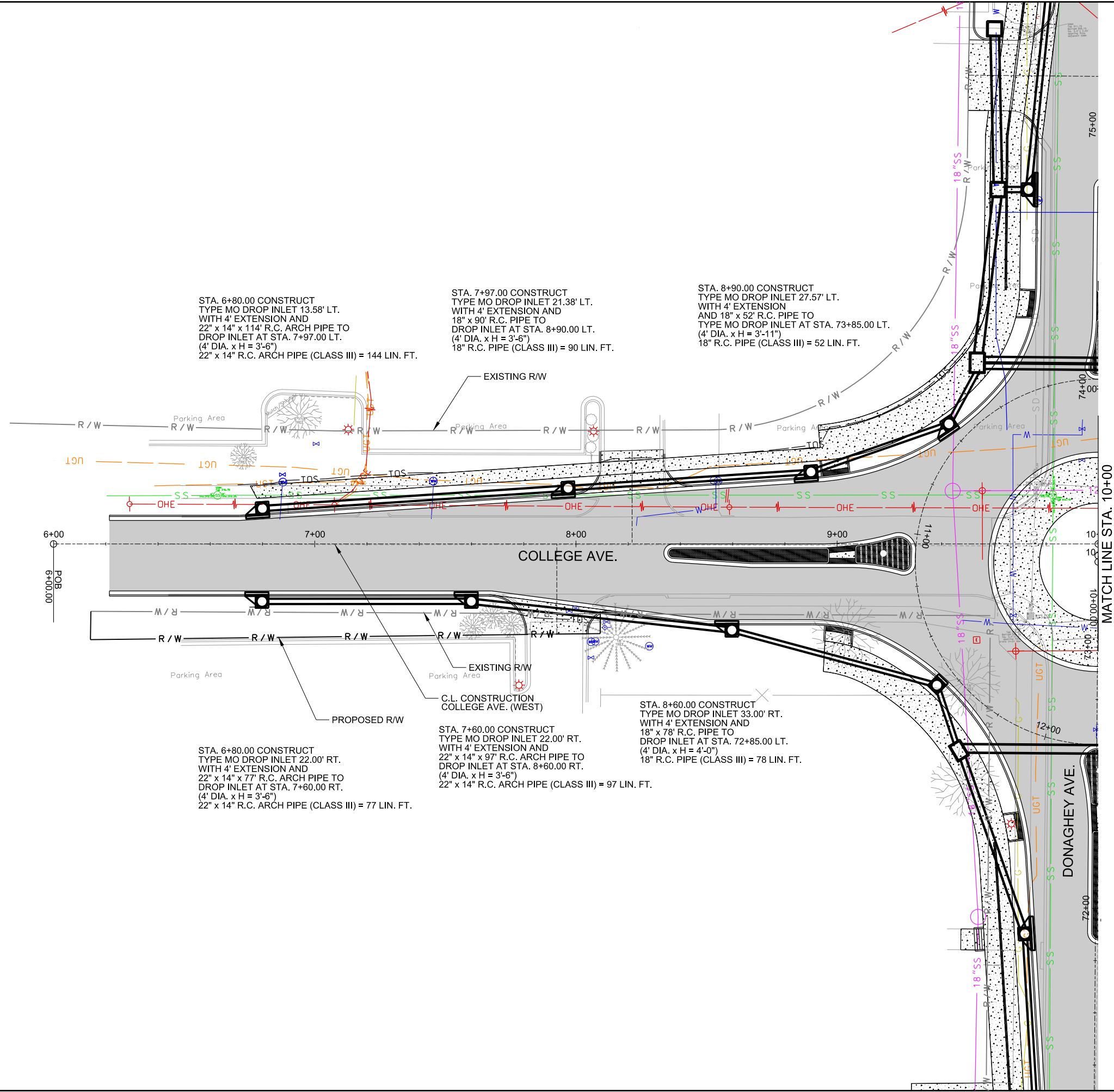
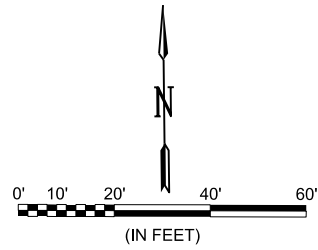
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

DRAINAGE PLAN AND PROFILE -
 DONAGHEY AVE.
 (SHEET 11 OF 11)

JOB NO.: 18147010
 DATE: MAY 2020
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C-711
 SHEET NUMBER
92



STA. 6+80.00 CONSTRUCT
TYPE MO DROP INLET 13.58' LT.
WITH 4' EXTENSION AND
22" x 14" x 114' R.C. ARCH PIPE TO
DROP INLET AT STA. 7+97.00 LT.
(4' DIA. x H = 3'-6")
22" x 14" R.C. ARCH PIPE (CLASS III) = 144 LIN. FT.

STA. 7+97.00 CONSTRUCT
TYPE MO DROP INLET 21.38' LT.
WITH 4' EXTENSION AND
18" x 90' R.C. PIPE TO
DROP INLET AT STA. 8+90.00 LT.
(4' DIA. x H = 3'-6")
18" R.C. PIPE (CLASS III) = 90 LIN. FT.

STA. 8+90.00 CONSTRUCT
TYPE MO DROP INLET 27.57' LT.
WITH 4' EXTENSION
AND 18" x 52' R.C. PIPE TO
TYPE MO DROP INLET AT STA. 73+85.00 LT.
(4' DIA. x H = 3'-11")
18" R.C. PIPE (CLASS III) = 52 LIN. FT.

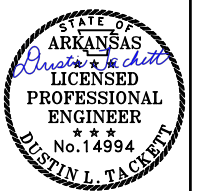
STA. 6+80.00 CONSTRUCT
TYPE MO DROP INLET 22.00' RT.
WITH 4' EXTENSION AND
22" x 14" x 77' R.C. ARCH PIPE TO
DROP INLET AT STA. 7+60.00 RT.
(4' DIA. x H = 3'-6")
22" x 14" R.C. ARCH PIPE (CLASS III) = 77 LIN. FT.

STA. 7+60.00 CONSTRUCT
TYPE MO DROP INLET 22.00' RT.
WITH 4' EXTENSION AND
22" x 14" x 97' R.C. ARCH PIPE TO
DROP INLET AT STA. 8+60.00 RT.
(4' DIA. x H = 3'-6")
22" x 14" R.C. ARCH PIPE (CLASS III) = 97 LIN. FT.

STA. 8+60.00 CONSTRUCT
TYPE MO DROP INLET 33.00' RT.
WITH 4' EXTENSION AND
18" x 78' R.C. PIPE TO
DROP INLET AT STA. 72+85.00 LT.
(4' DIA. x H = 4'-0")
18" R.C. PIPE (CLASS III) = 78 LIN. FT.



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CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

DRAINAGE PLAN AND
PROFILE -
COLLEGE AVE.
(SHEET 1 OF 4)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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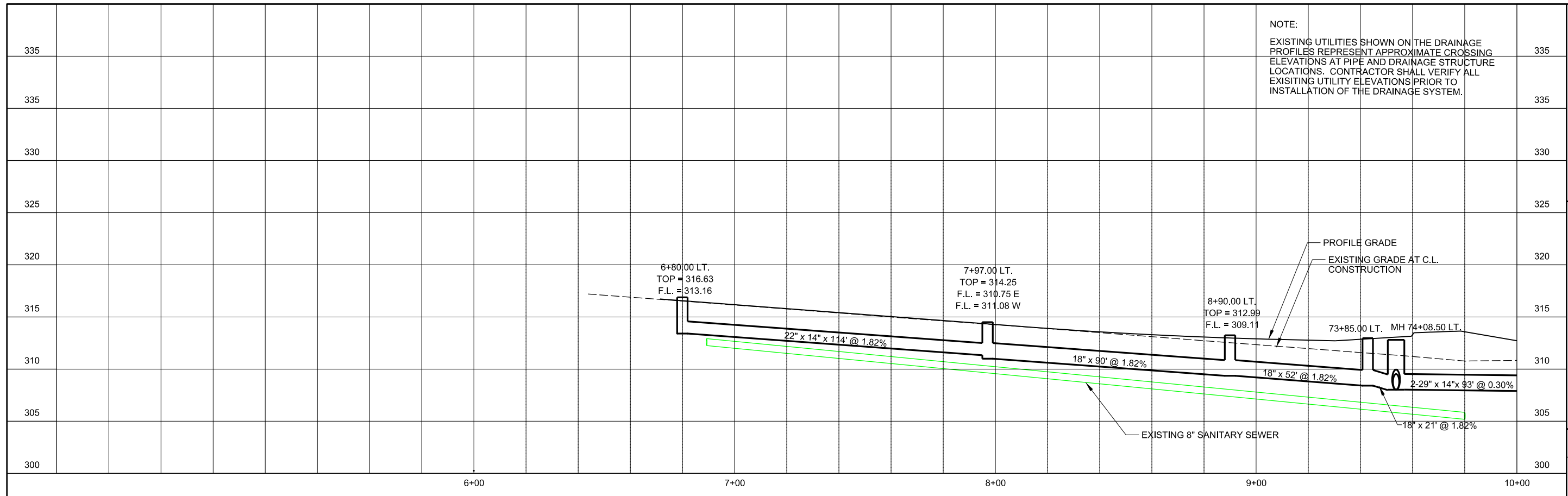
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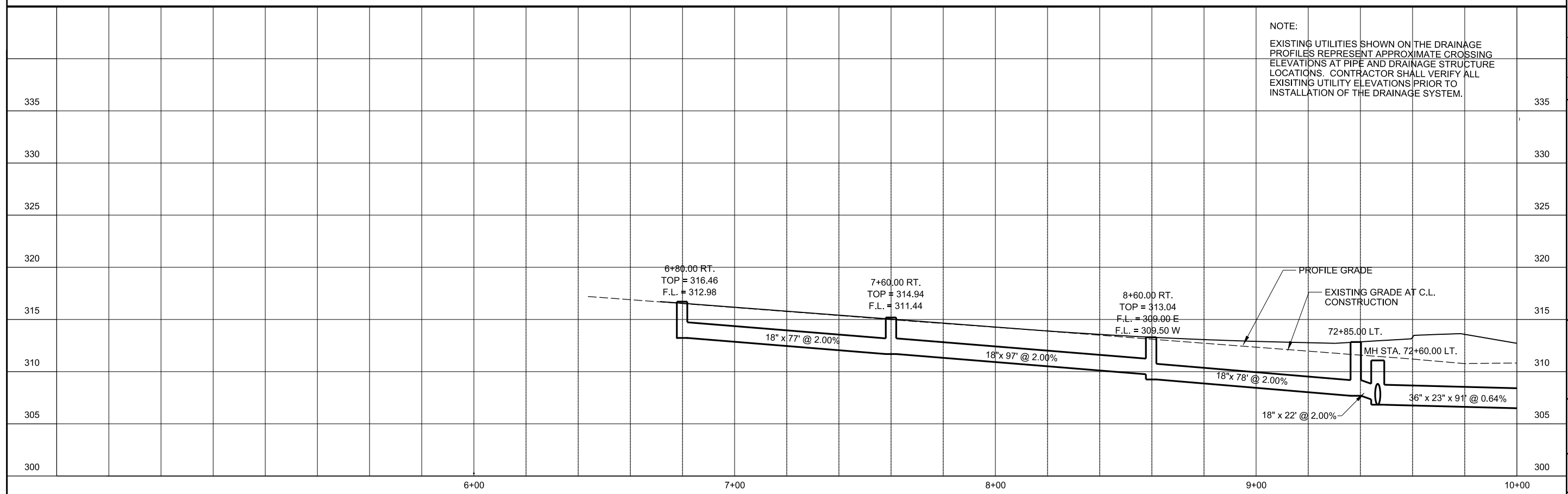


SEE GENERAL NOTE 1,
DRAWING G-002 OF THESE PLANS.


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CITY OF CONWAY
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**DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)**

DRAINAGE PLAN AND PROFILE - COLLEGE AVE. (SHEET 2 OF 4)

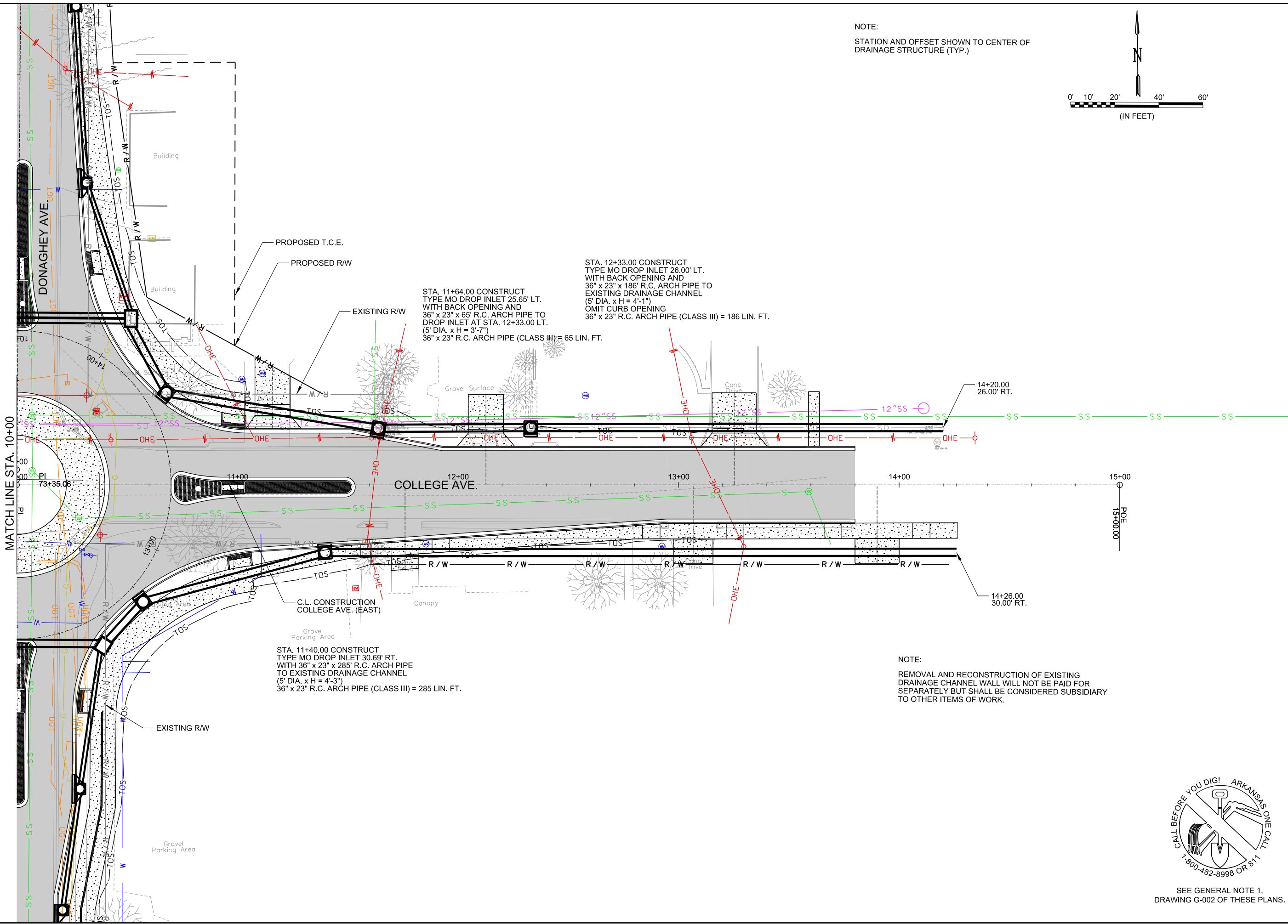
JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
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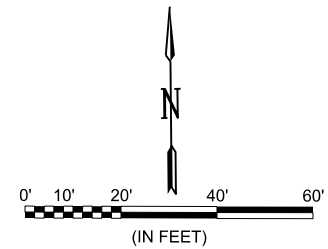
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SHEET NUMBER
94

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NOTE:
 STATION AND OFFSET SHOWN TO CENTER OF DRAINAGE STRUCTURE (TYP.)



STA. 11+64.00 CONSTRUCT TYPE MO DROP INLET 25.65' LT. WITH BACK OPENING AND 36" x 23" x 65' R.C. ARCH PIPE TO DROP INLET AT STA. 12+33.00 LT. (5' DIA. x H = 3'-7") 36" x 23" R.C. ARCH PIPE (CLASS III) = 65 LIN. FT.

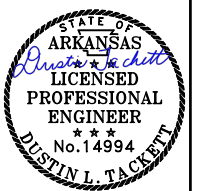
STA. 12+33.00 CONSTRUCT TYPE MO DROP INLET 26.00' LT. WITH BACK OPENING AND 36" x 23" x 186' R.C. ARCH PIPE TO EXISTING DRAINAGE CHANNEL (5' DIA. x H = 4'-1") OMIT CURB OPENING 36" x 23" R.C. ARCH PIPE (CLASS III) = 186 LIN. FT.

STA. 11+40.00 CONSTRUCT TYPE MO DROP INLET 30.69' RT. WITH 36" x 23" x 285' R.C. ARCH PIPE TO EXISTING DRAINAGE CHANNEL (5' DIA. x H = 4'-3") 36" x 23" R.C. ARCH PIPE (CLASS III) = 285 LIN. FT.

NOTE:
 REMOVAL AND RECONSTRUCTION OF EXISTING DRAINAGE CHANNEL WALL WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED SUBSIDIARY TO OTHER ITEMS OF WORK.



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CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

DRAINAGE PLAN AND PROFILE -
 COLLEGE AVE.
 (SHEET 3 OF 4)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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C-714
 SHEET NUMBER
95



SEE GENERAL NOTE 1,
 DRAWING G-002 OF THESE PLANS.

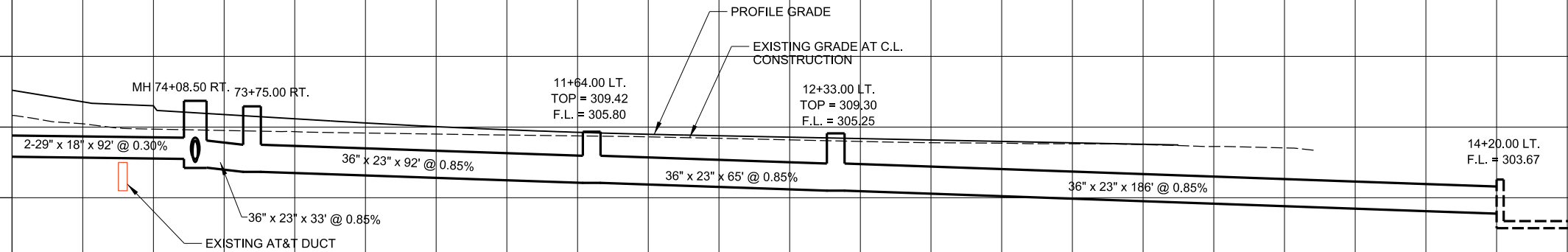
NOTE:
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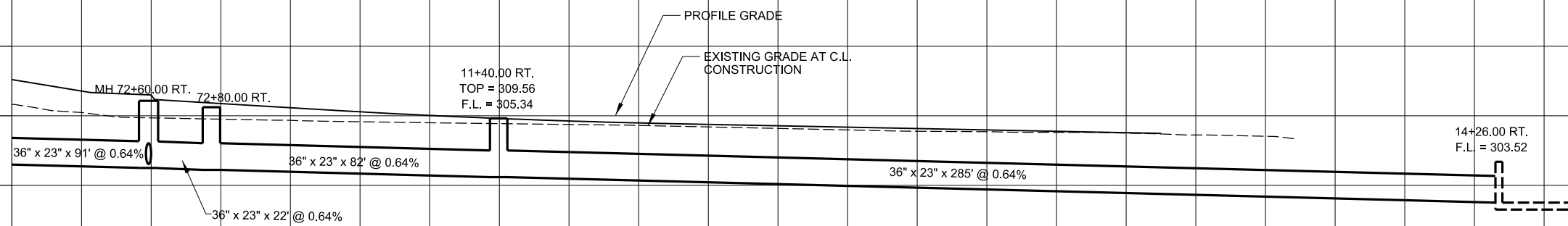


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LEFT SIDE

NOTE:
EXISTING UTILITIES SHOWN ON THE DRAINAGE PROFILES REPRESENT APPROXIMATE CROSSING ELEVATIONS AT PIPE AND DRAINAGE STRUCTURE LOCATIONS. CONTRACTOR SHALL VERIFY ALL EXISTING UTILITY ELEVATIONS PRIOR TO INSTALLATION OF THE DRAINAGE SYSTEM.



RIGHT SIDE

REV. DATE



CITY OF CONWAY
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DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

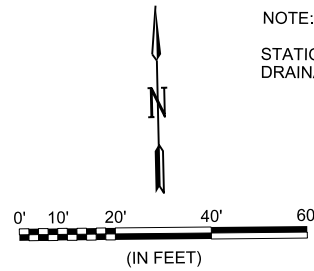
DRAINAGE PLAN AND PROFILE -
COLLEGE AVE.
(SHEET 4 OF 4)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

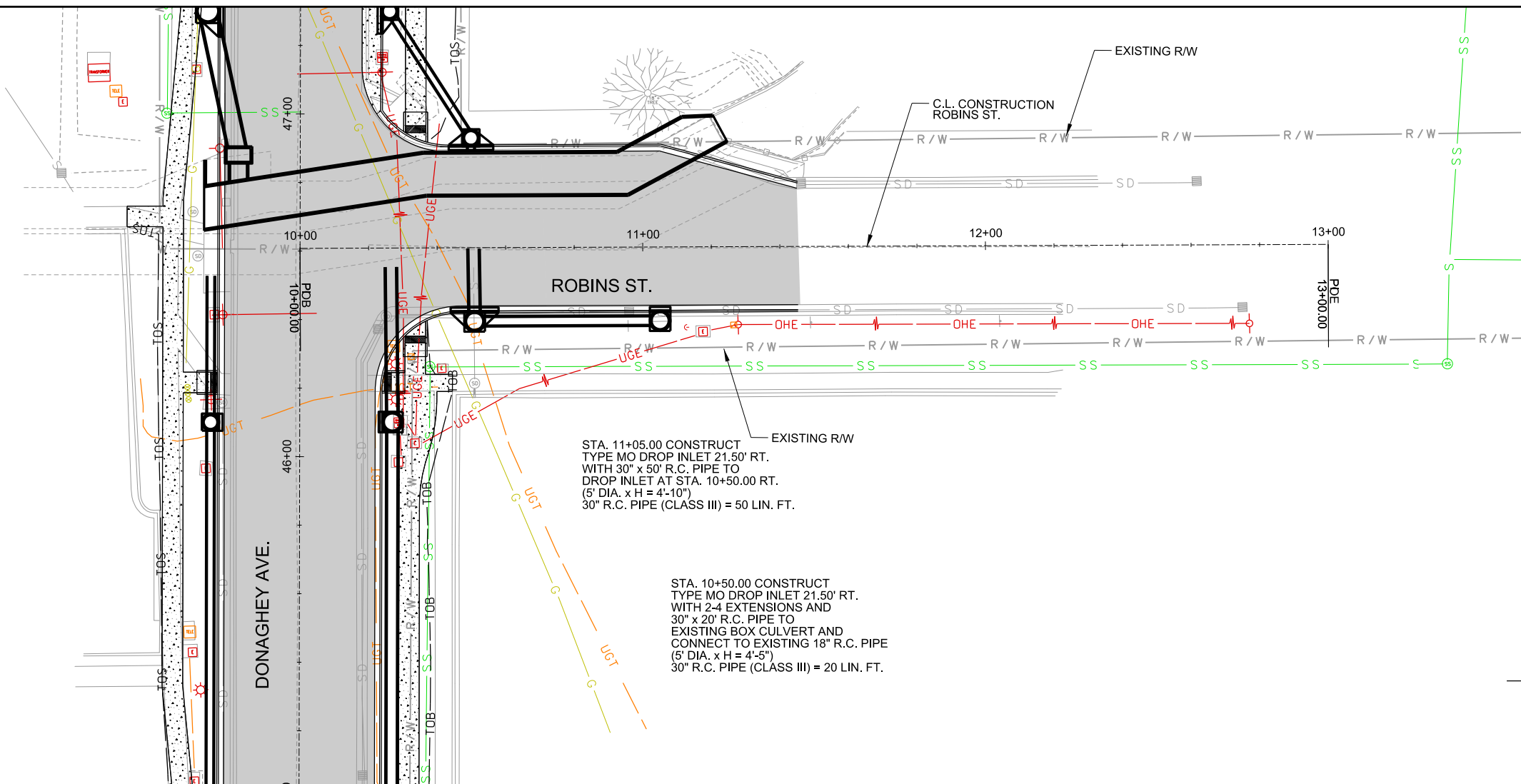
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DRAWING NUMBER
C-715

SHEET NUMBER
96



NOTE:
STATION AND OFFSET SHOWN TO CENTER OF DRAINAGE STRUCTURE (TYP.)



STA. 11+05.00 CONSTRUCT
TYPE MO DROP INLET 21.50' RT.
WITH 30" x 50' R.C. PIPE TO
DROP INLET AT STA. 10+50.00 RT.
(5' DIA. x H = 4'-10")
30" R.C. PIPE (CLASS III) = 50 LIN. FT.

STA. 10+50.00 CONSTRUCT
TYPE MO DROP INLET 21.50' RT.
WITH 2-4 EXTENSIONS AND
30" x 20' R.C. PIPE TO
EXISTING BOX CULVERT AND
CONNECT TO EXISTING 18" R.C. PIPE
(5' DIA. x H = 4'-5")
30" R.C. PIPE (CLASS III) = 20 LIN. FT.



SEE GENERAL NOTE 1,
DRAWING G-002 OF THESE PLANS.



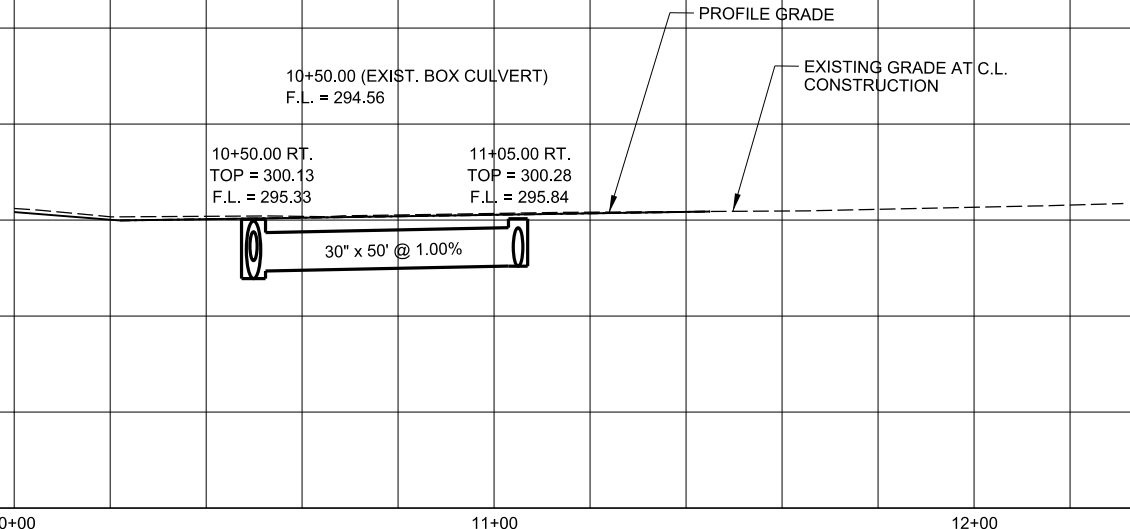
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REV.	DATE	DESCRIPTION	BY
325			
320			
315			
310			
305			
300			
295			
290			
285			

NOTE:
EXISTING UTILITIES SHOWN ON THE DRAINAGE
PROFILES REPRESENT APPROXIMATE CROSSING
ELEVATIONS AT PIPE AND DRAINAGE STRUCTURE
LOCATIONS. CONTRACTOR SHALL VERIFY ALL
EXISTING UTILITY ELEVATIONS PRIOR TO
INSTALLATION OF THE DRAINAGE SYSTEM.



CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

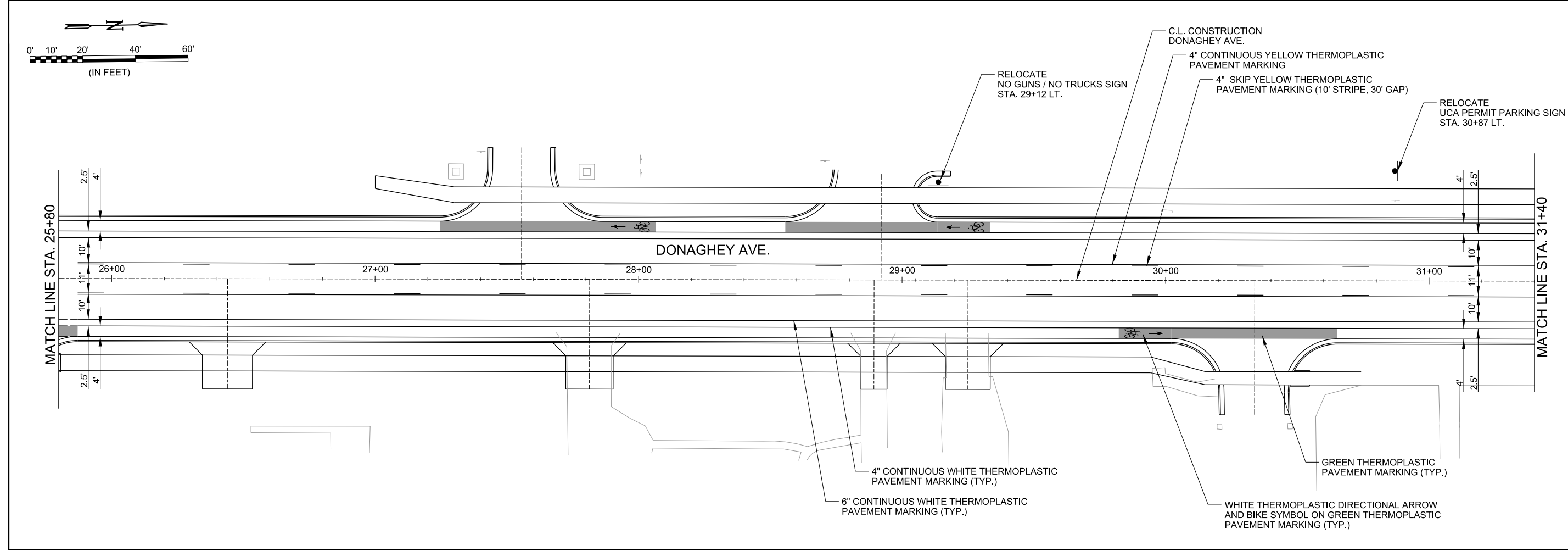
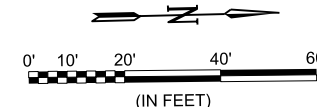
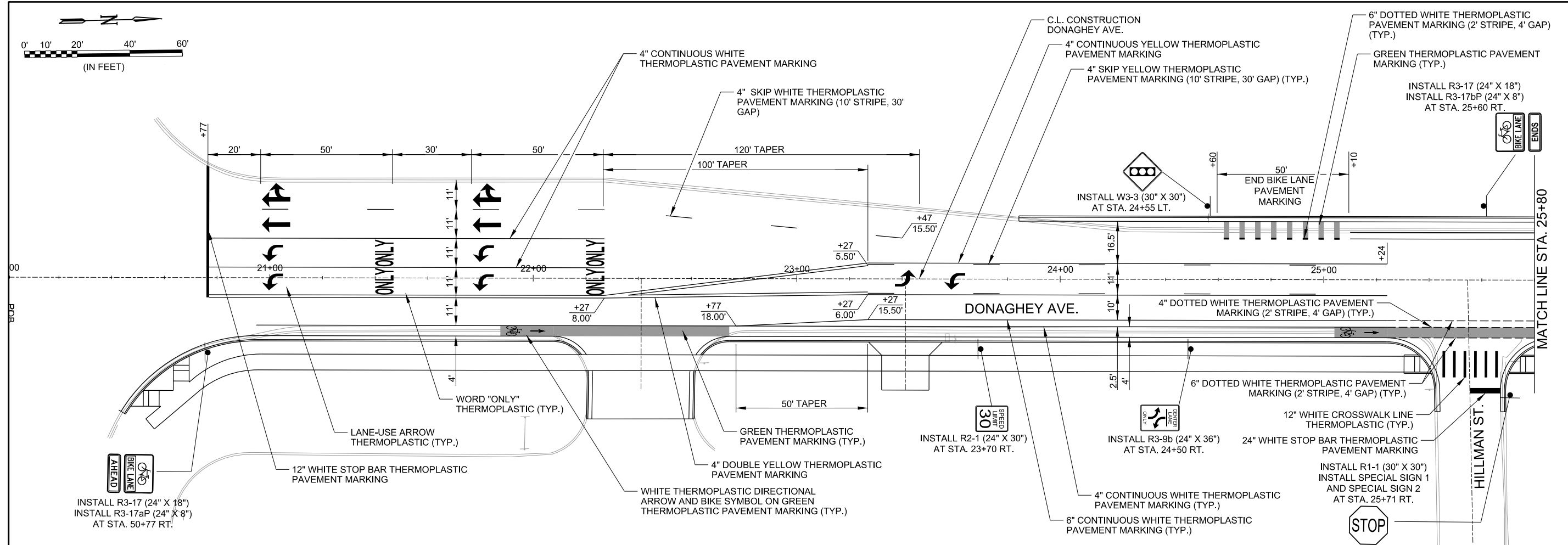
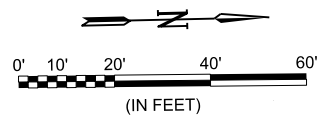
DRAINAGE PLAN AND
PROFILE -
ROBINS ST.

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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DRAWING NUMBER
C-716

SHEET
NUMBER **97**



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

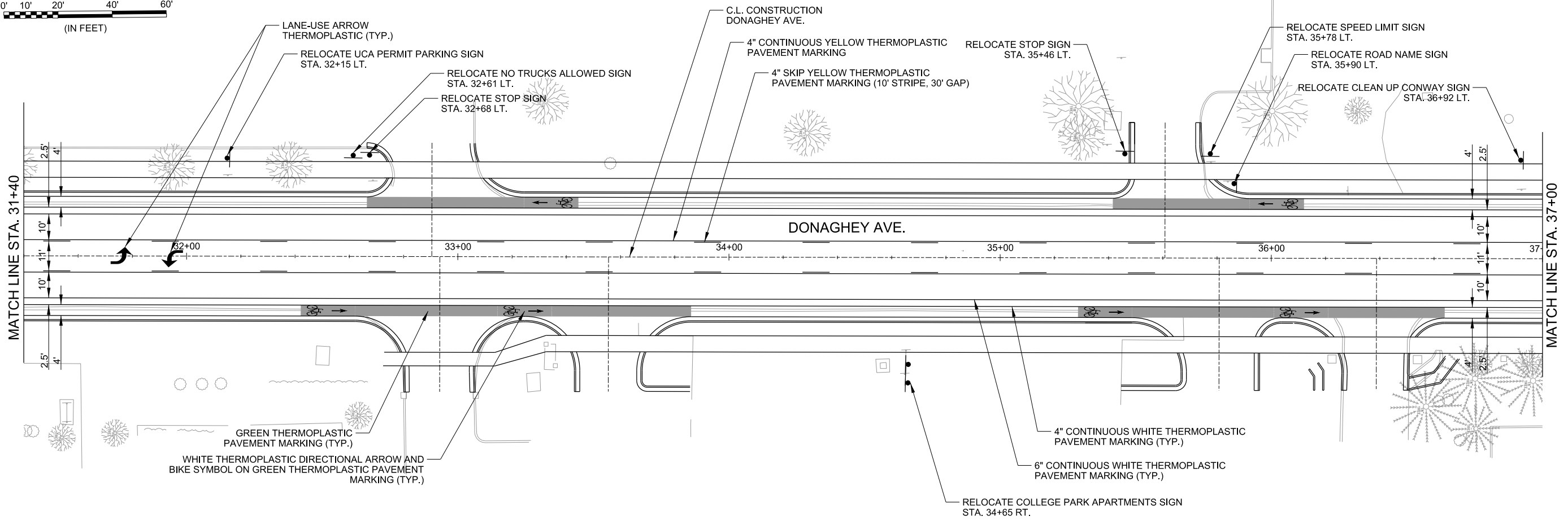
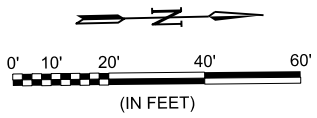
PAVEMENT MARKING AND SIGNING PLANS (SHEET 1 OF 9)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

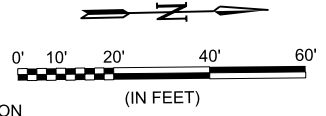
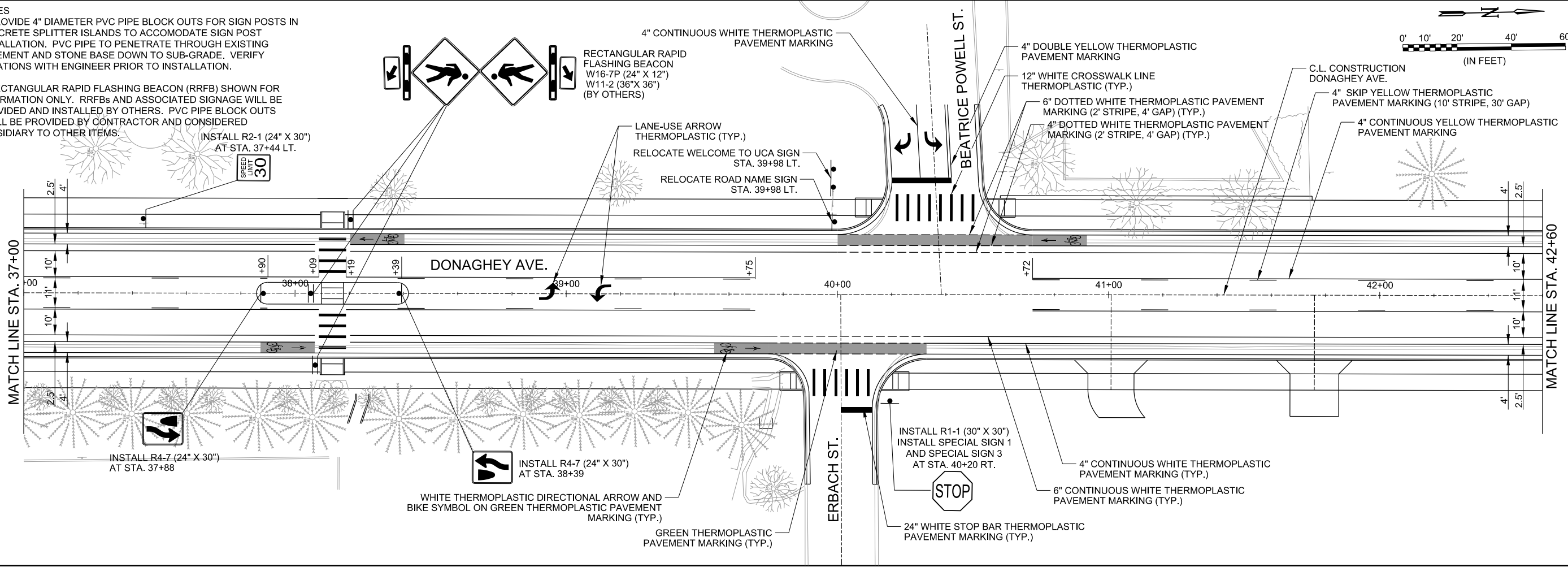
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DRAWING NUMBER
C-801
 SHEET NUMBER
98

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- NOTES**
1. PROVIDE 4" DIAMETER PVC PIPE BLOCK OUTS FOR SIGN POSTS IN CONCRETE SPLITTER ISLANDS TO ACCOMMODATE SIGN POST INSTALLATION. PVC PIPE TO PENETRATE THROUGH EXISTING PAVEMENT AND STONE BASE DOWN TO SUB-GRADE. VERIFY LOCATIONS WITH ENGINEER PRIOR TO INSTALLATION.
 2. RECTANGULAR RAPID FLASHING BEACON (RRFB) SHOWN FOR INFORMATION ONLY. RRFBS AND ASSOCIATED SIGNAGE WILL BE PROVIDED AND INSTALLED BY OTHERS. PVC PIPE BLOCK OUTS SHALL BE PROVIDED BY CONTRACTOR AND CONSIDERED SUBSIDIARY TO OTHER ITEMS.



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BY	DESCRIPTION	DATE	REV.



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

PAVEMENT MARKING AND SIGNING PLANS
 (SHEET 2 OF 9)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

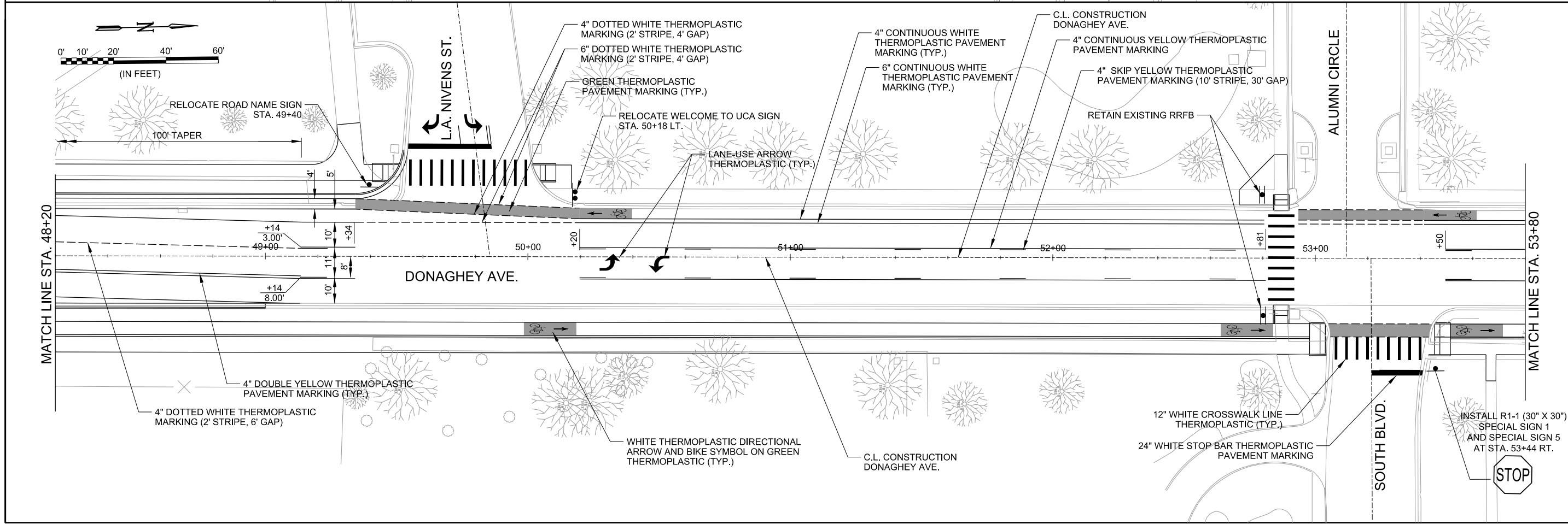
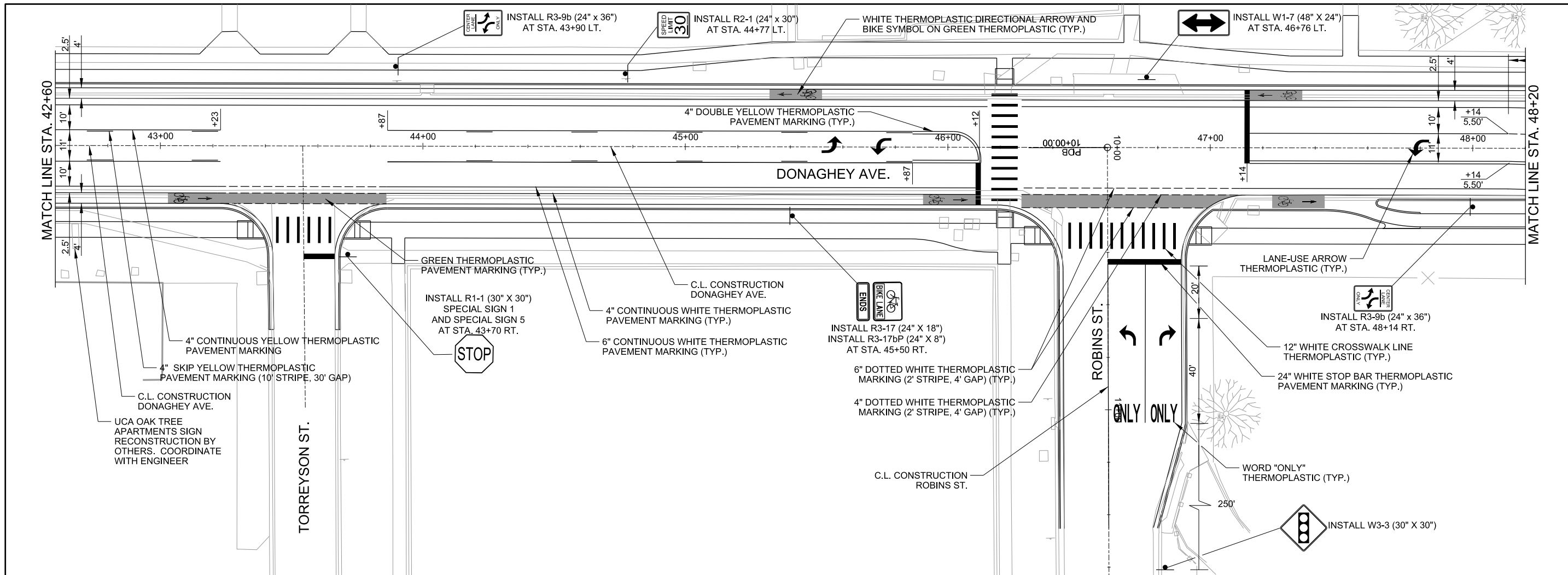
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DRAWING NUMBER
C-802

SHEET NUMBER
99

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HJBeck
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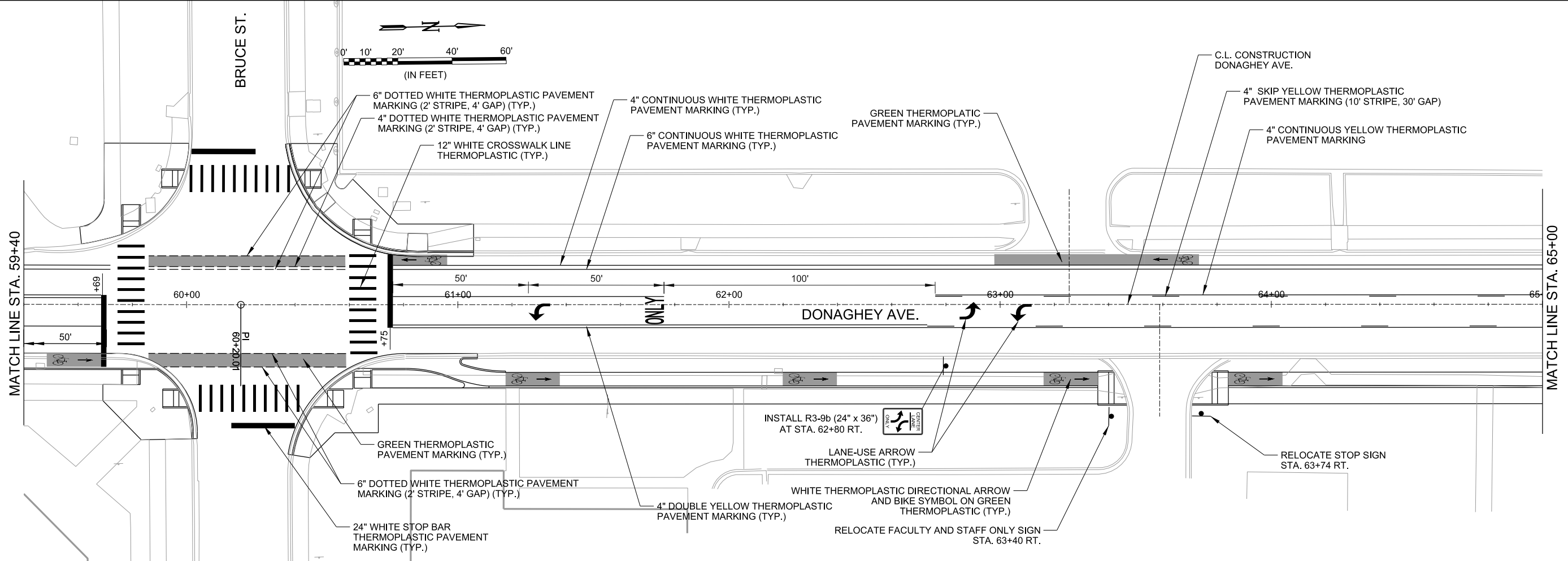
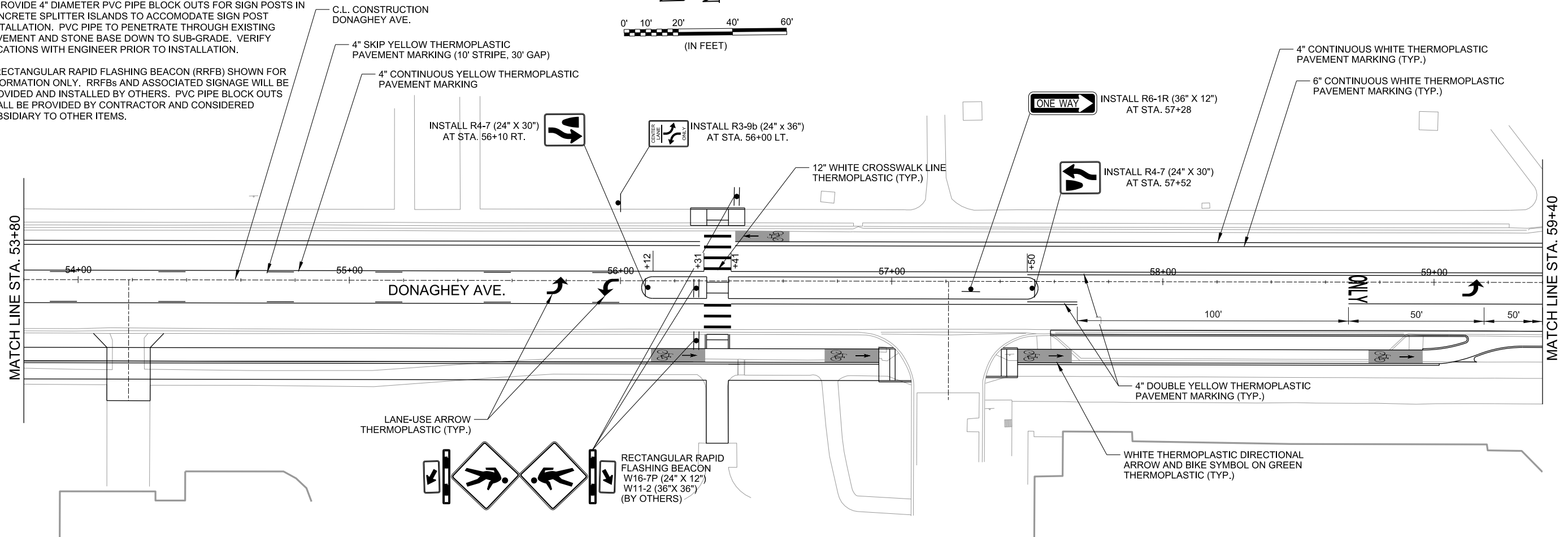
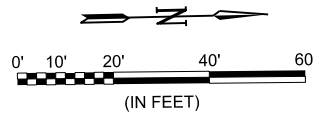


CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

PAVEMENT MARKING
 AND SIGNING PLANS
 (SHEET 3 OF 9)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB
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 DRAWING NUMBER
C-803
 SHEET NUMBER
100

NOTES
 1. PROVIDE 4" DIAMETER PVC PIPE BLOCK OUTS FOR SIGN POSTS IN CONCRETE SPLITTER ISLANDS TO ACCOMMODATE SIGN POST INSTALLATION. PVC PIPE TO PENETRATE THROUGH EXISTING PAVEMENT AND STONE BASE DOWN TO SUB-GRADE. VERIFY LOCATIONS WITH ENGINEER PRIOR TO INSTALLATION.
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CITY OF CONWAY
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 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

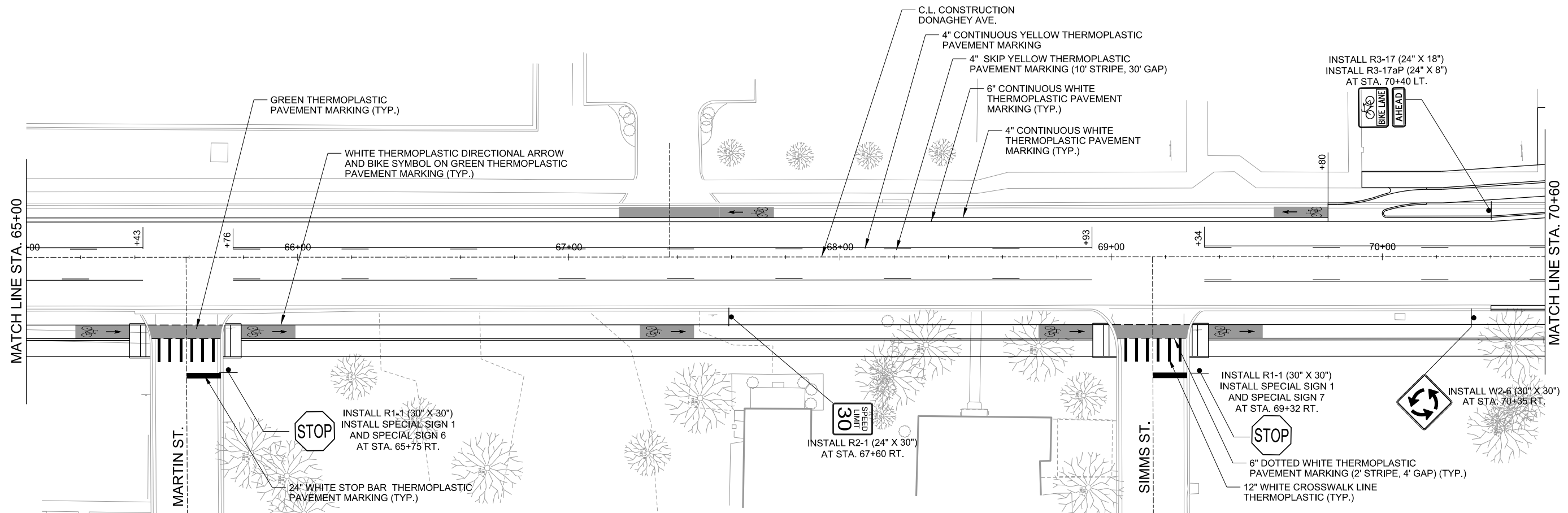
PAVEMENT MARKING AND SIGNING PLANS (SHEET 4 OF 9)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-804
 SHEET NUMBER
101

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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

PAVEMENT MARKING AND SIGNING PLANS
 (SHEET 5 OF 9)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

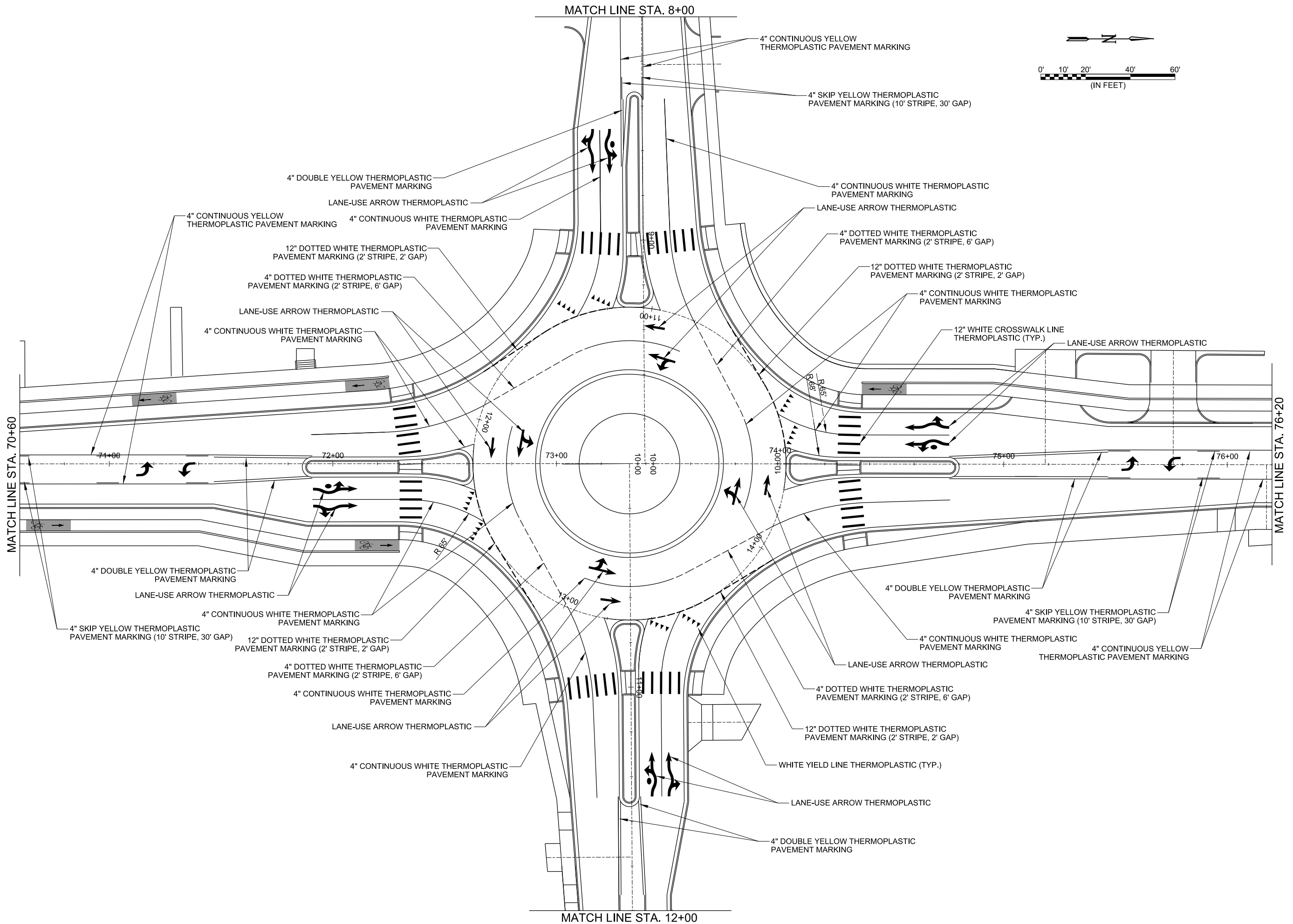
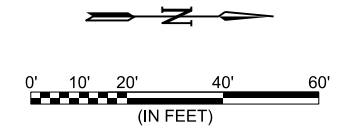
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DRAWING NUMBER
C-805

SHEET NUMBER
102

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MATCH LINE STA. 8+00



MATCH LINE STA. 12+00



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

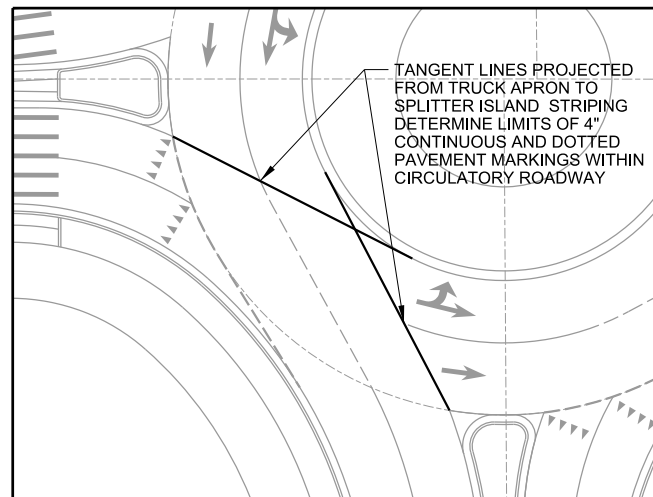
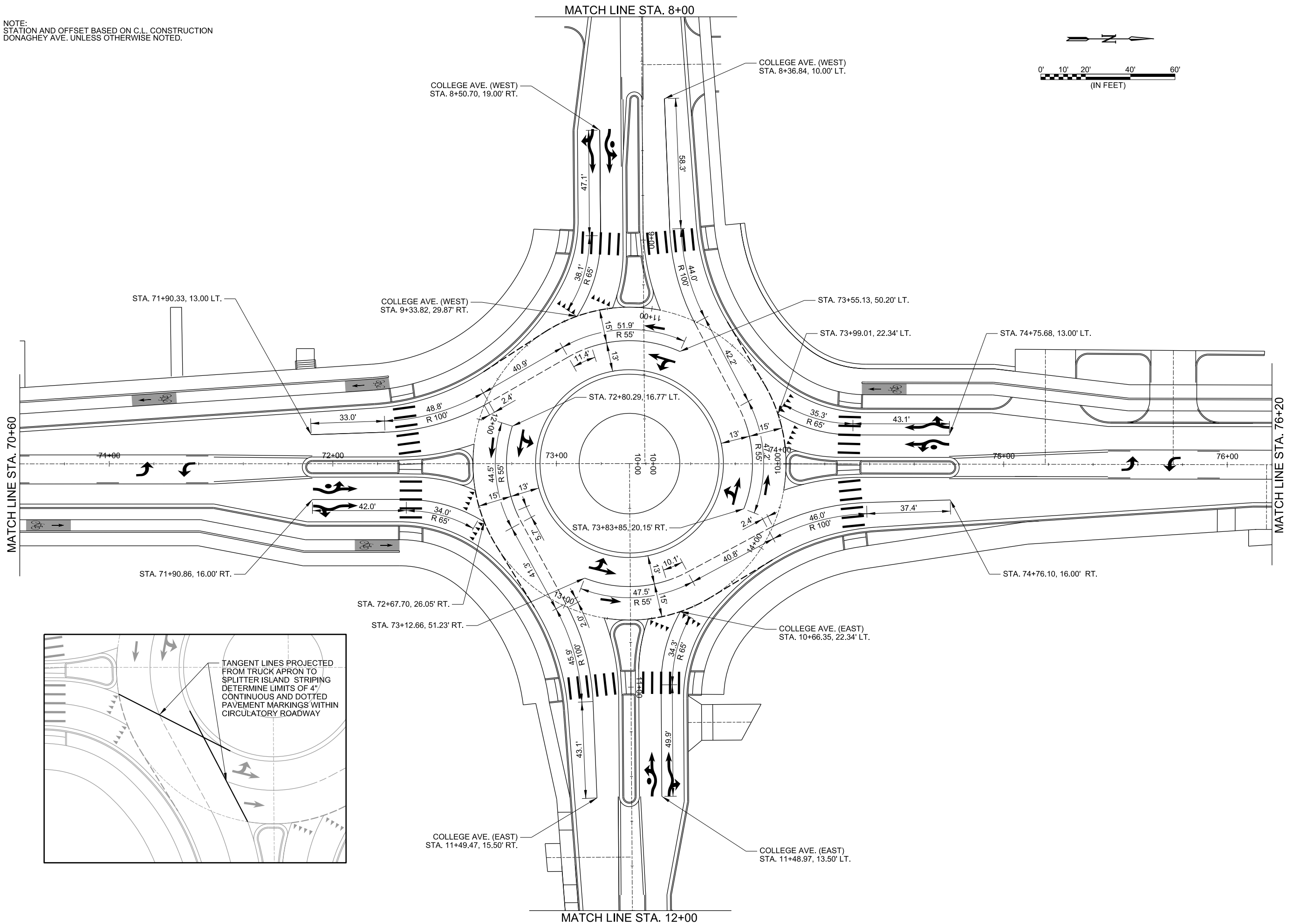
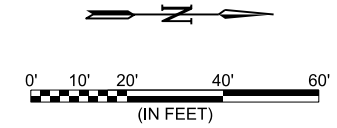
PAVEMENT MARKING
 AND SIGNING PLANS
 (SHEET 6 OF 9)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-806
 SHEET NUMBER
103

NOTE:
STATION AND OFFSET BASED ON C.L. CONSTRUCTION
DONAGHEY AVE. UNLESS OTHERWISE NOTED.



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CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

PAVEMENT MARKING AND SIGNING PLANS
(SHEET 7 OF 9)

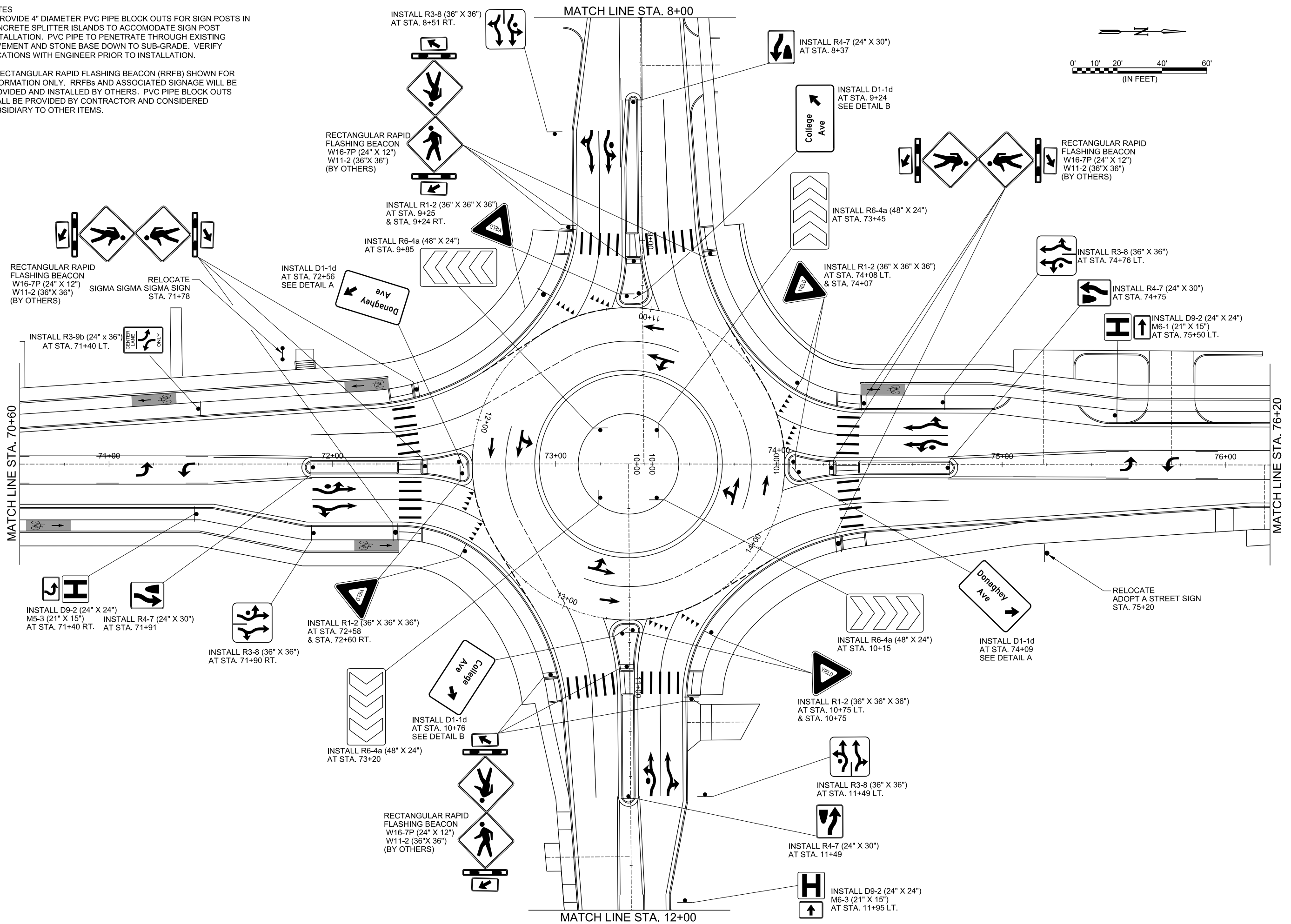
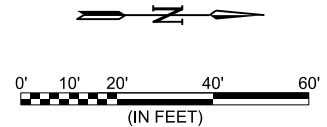
JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

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C-807
SHEET NUMBER
104

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 1. PROVIDE 4" DIAMETER PVC PIPE BLOCK OUTS FOR SIGN POSTS IN CONCRETE SPLITTER ISLANDS TO ACCOMMODATE SIGN POST INSTALLATION. PVC PIPE TO PENETRATE THROUGH EXISTING PAVEMENT AND STONE BASE DOWN TO SUB-GRADE. VERIFY LOCATIONS WITH ENGINEER PRIOR TO INSTALLATION.
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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

PAVEMENT MARKING AND SIGNING PLANS
 (SHEET 8 OF 9)

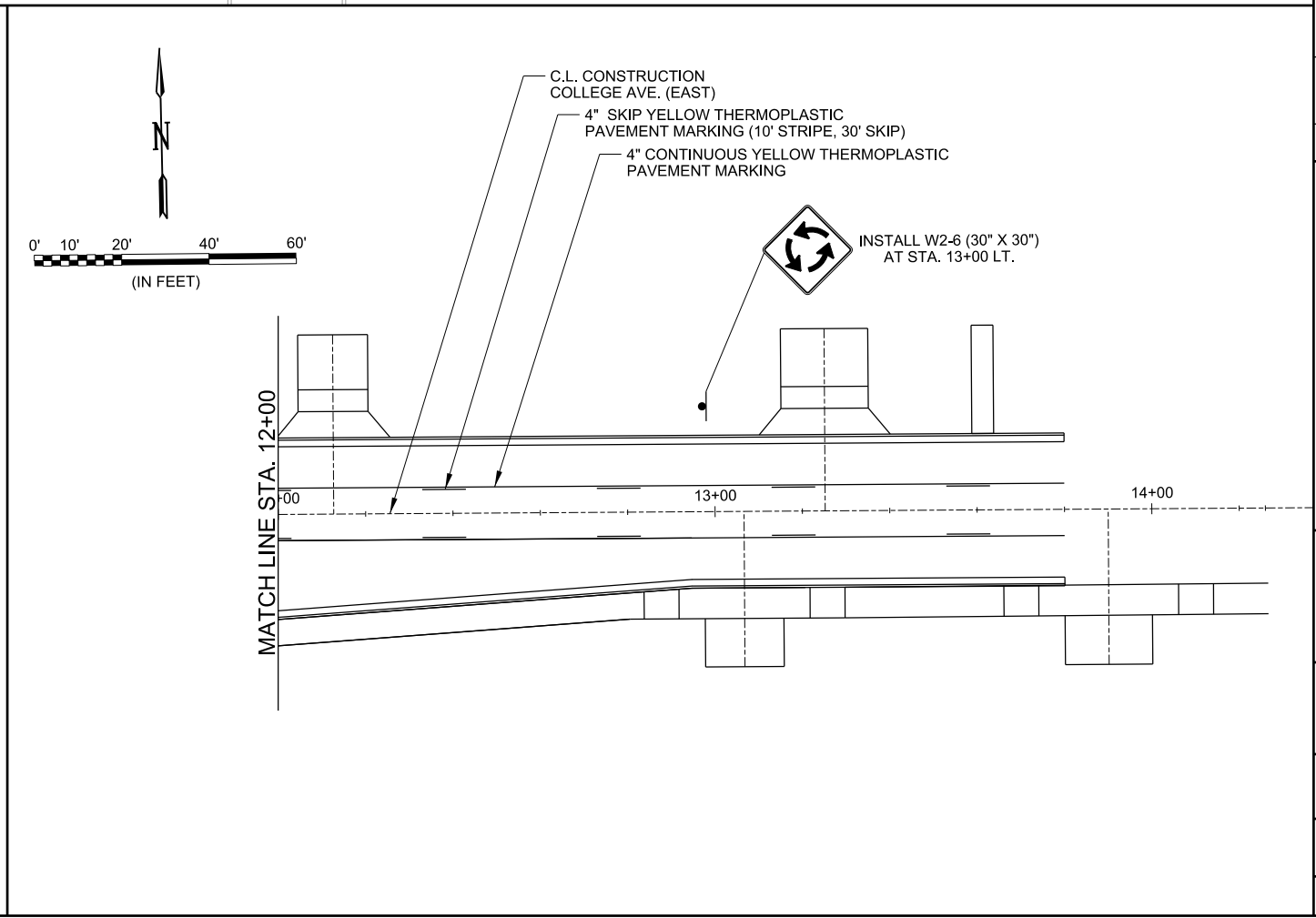
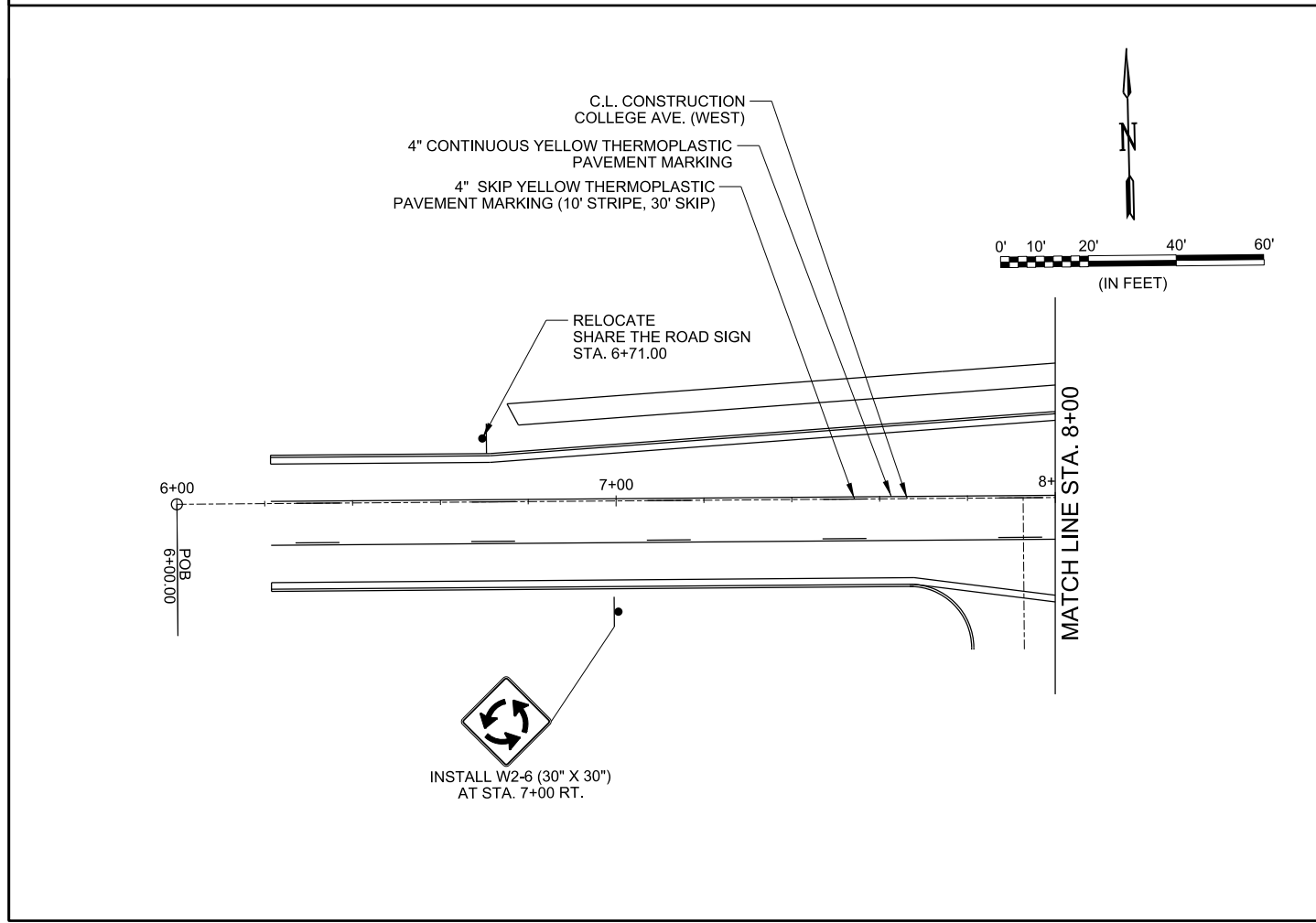
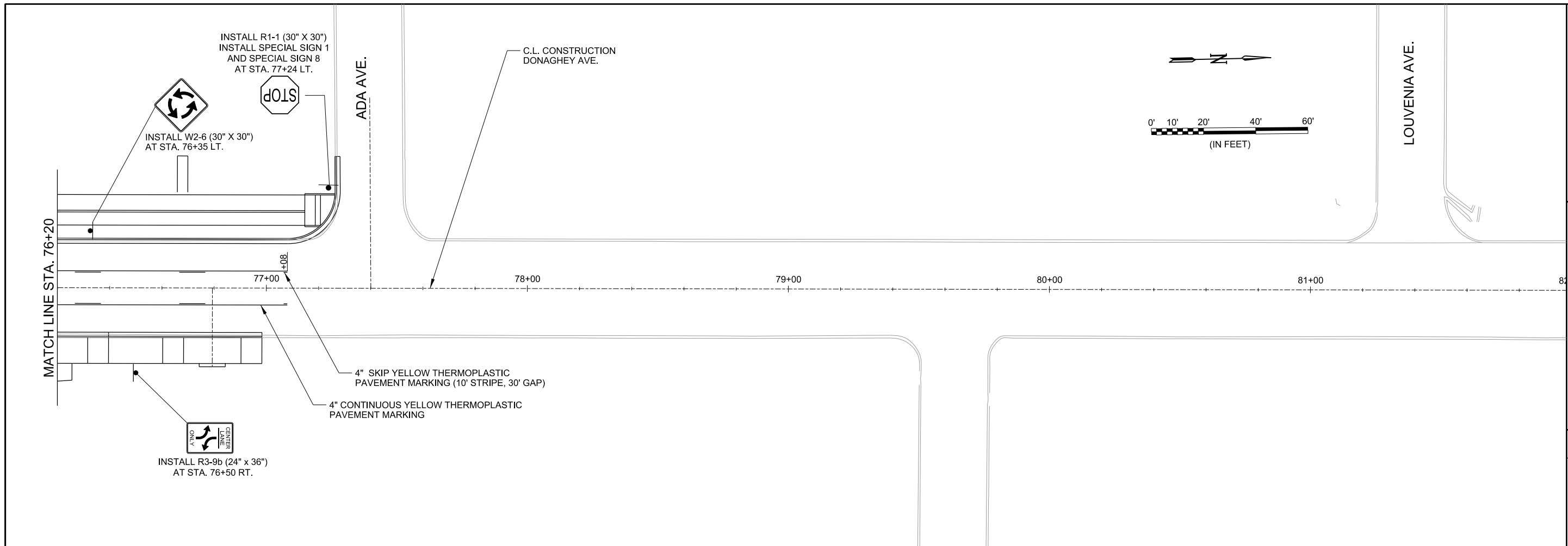
JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB


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C-808
 SHEET NUMBER
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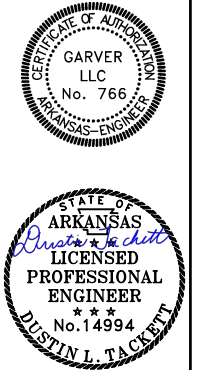
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STATE OF ARKANSAS
 Justin L. Tackett
 LICENSED
 PROFESSIONAL
 ENGINEER
 No. 14994
 JUSTIN L. TACKETT
 Digitally Signed 05/23/2020

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

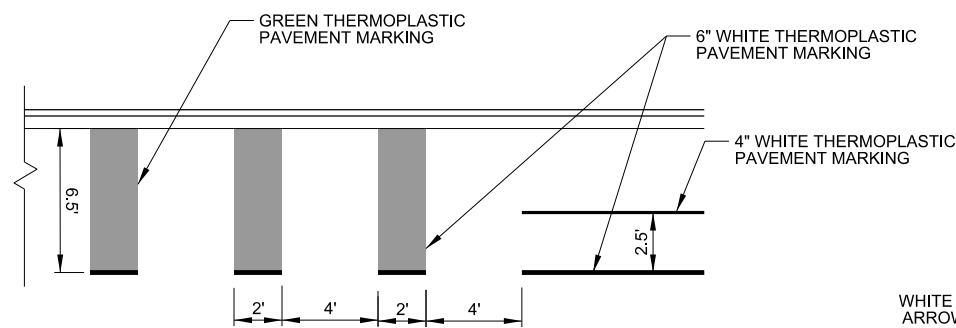
PAVEMENT MARKING
 AND SIGNING PLANS
 (SHEET 9 OF 9)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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DRAWING NUMBER
C-809

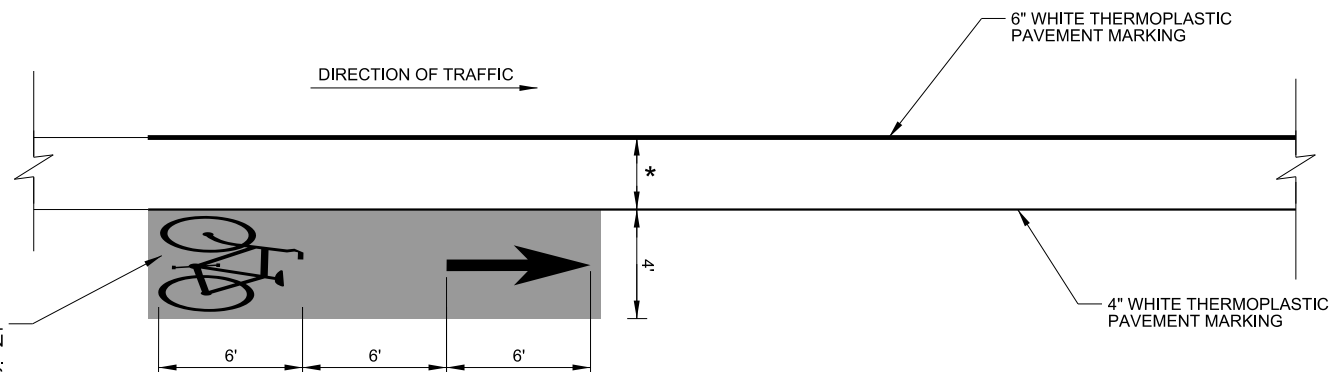
SHEET
 NUMBER **106**



END BIKE LANE PAVEMENT MARKING DETAIL

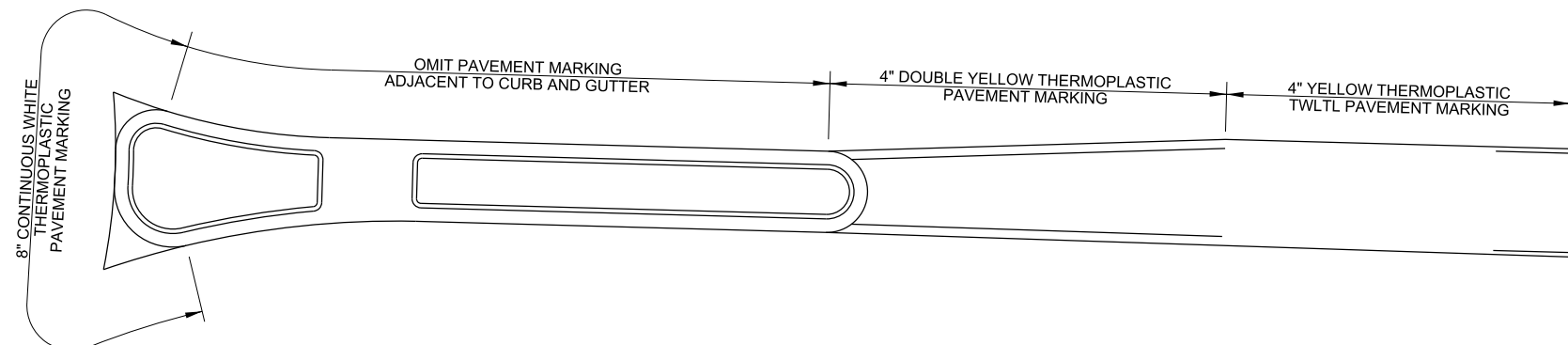
STA. 24+60.00 TO STA. 25+10.00 LT.

WHITE THERMOPLASTIC DIRECTIONAL ARROW AND BIKE SYMBOL ON GREEN THERMOPLASTIC PAVEMENT MARKING. SEE PLANS FOR LOCATIONS

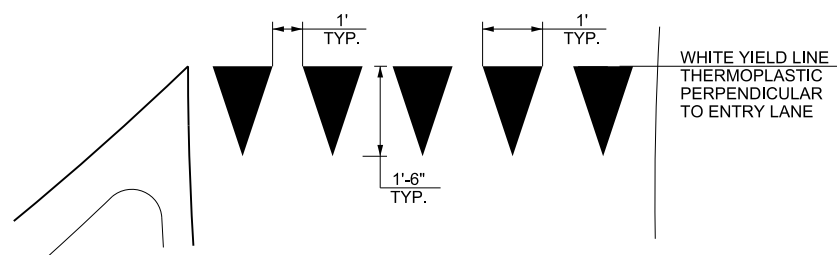


BIKE LANE BUFFER PAVEMENT MARKING DETAIL

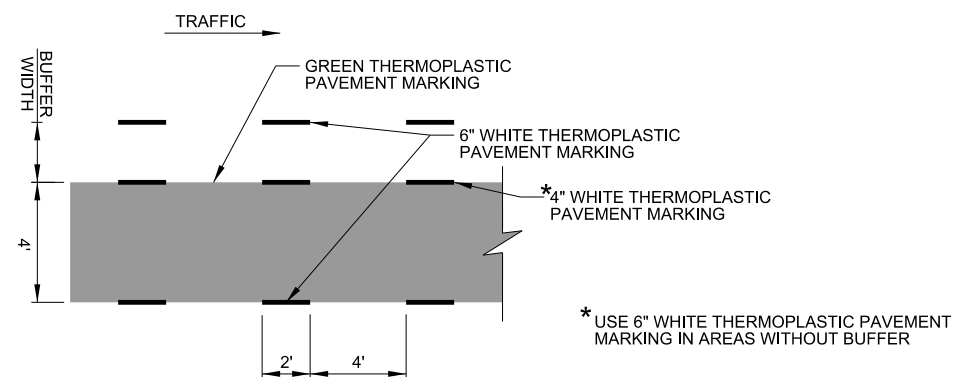
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 STA. 25+10.00 TO STA. 49+34.24 LT.
 *1'-6" STA. 50+19.72 TO STA. 69+80.00 LT.



TYPICAL SPLITTER ISLAND STRIPING



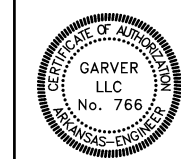
TYPICAL YIELD LINE STRIPING



BIKE LANE AT INTERSECTIONS PAVEMENT MARKING DETAILS



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REV.	DATE	DESCRIPTION	BY



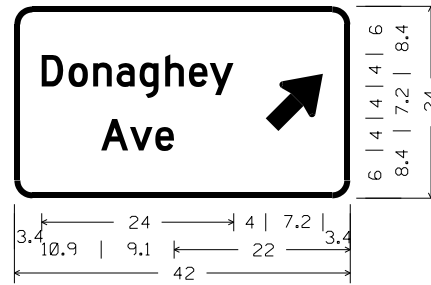
CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

PAVEMENT MARKING AND SIGNING DETAILS (SHEET 1 OF 2)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

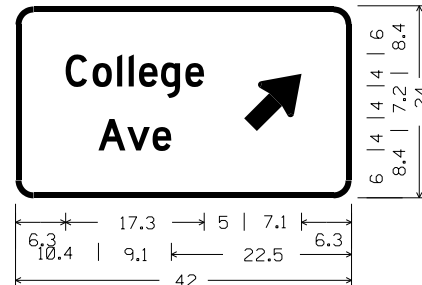
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DRAWING NUMBER
C-810
 SHEET NUMBER
107



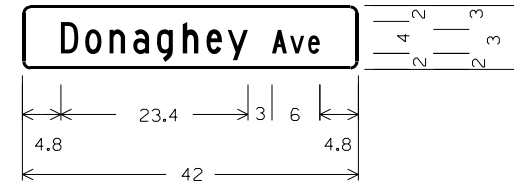
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2.3" Radius, 0.8" Border, White on, Green;
"Donaghey", D 2K;
"Ave", D 2K;
Standard Arrow Custom 9.0" X 6.0" 45';

DETAIL A



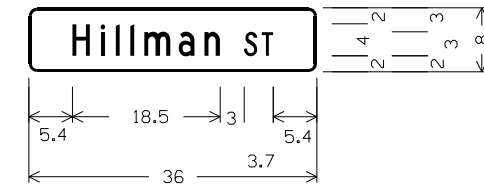
D3-1;
2.3" Radius, 0.8" Border, White on, Green;
"College", D 2K;
"Ave", D 2K;
Standard Arrow Custom 9.0" X 6.0" 45';

DETAIL B



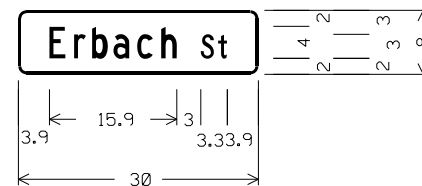
D3-1;
1.0" Radius, 0.4" Border, White on, Green;
"Donaghey", C;
"Ave", C;

SPECIAL SIGN 1



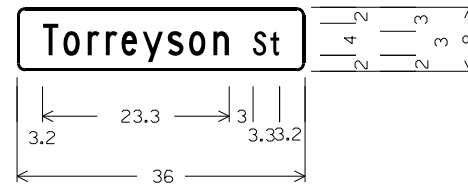
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"Hillman", C;
"St", C;

SPECIAL SIGN 2



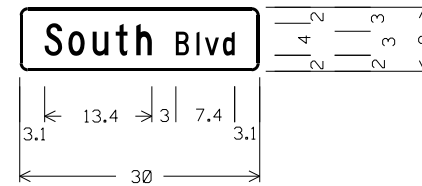
D3-1;
1.0" Radius, 0.4" Border, White on, Green;
"Erbach", C;
"St", C;

SPECIAL SIGN 3



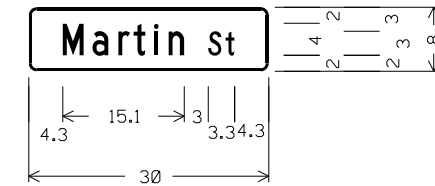
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"Torreyson", C;
"St", C;

SPECIAL SIGN 4



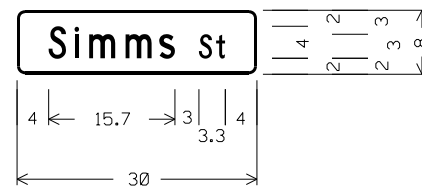
D3-1;
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"South", C;
"Blvd", C;

SPECIAL SIGN 5



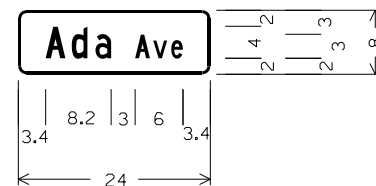
D3-1;
1.0" Radius, 0.4" Border, White on, Green;
"Martin", C;
"St", C;

SPECIAL SIGN 6



D3-1;
1.0" Radius, 0.4" Border, White on, Green;
"Simms", C;
"St", C;

SPECIAL SIGN 7



D3-1;
1.0" Radius, 0.4" Border, White on, Green;
"Ada", C;
"Ave", C;

SPECIAL SIGN 8



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Digitally Signed 05/23/2020

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

PAVEMENT MARKING
AND SIGNING DETAILS
(SHEET 2 OF 2)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DLT
DRAWN BY: HJB

BAR IS ONE INCH ON
ORIGINAL DRAWING
0 1"
IF NOT ONE INCH ON THIS SHEET,
ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER
C-811

SHEET
NUMBER **108**

TRAFFIC SIGNAL NOTES:

- ALL ELECTRICAL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE NFPA 70 (2017) NATIONAL ELECTRICAL CODE, NFPA 101 (CURRENT EDITION) LIFE SAFETY CODE, STATE ELECTRICAL CODE AND LOCAL ELECTRICAL CODE.
- EXTEND GREEN EQUIPMENT GROUNDING CONDUCTOR (E.G.C.) FROM GROUND BAR AT MAIN BREAKER TO CONTROL PANEL AND TO FIRST POLE. SOLIDLY BOND E.G.C. TO GROUND LUG OF CONTROL CABINET AND TO POLE GROUND. ENSURE THAT ONLY ONE NEUTRAL-TO-GROUND BOND EXISTS IN THE SYSTEM AND THAT IT IS AT THE MAIN BREAKER.
- ELECTRICAL SERVICE SHALL BE PROVIDED BY THE CITY/COUNTY TO A SERVICE POLE WITH EXTERNAL RAIN-TIGHT BREAKER (MAIN BREAKER), GALVANIZED STEEL SERVICE RISER, METER LOOP (IF REQUIRED), AND WEATHERHEAD AT A MUTUALLY ACCEPTABLE POINT WITHIN THE RIGHT-OF-WAY. IF THE SERVICE POINT IS OVER 10 FEET FROM THE CONTROLLER, THE CONTRACTOR SHALL PROVIDE AND INSTALL A SEPARATE TWO CIRCUIT EXTERNAL BREAKER (SECONDARY BREAKER) ON OR NEAR THE TRAFFIC SIGNAL CONTROLLER CABINET AND SHALL INSTALL CONDUIT, ELECTRICAL SERVICE WIRE (2c#6 A.W.G. USE RATED, WITH GROUND TYPICAL), AND PERFORM WIRING TO TAP INTO THE CITY'S/ COUNTY'S MAIN BREAKER AS PART OF THIS CONTRACT. CONDUIT IS PAID FOR AS A SEPARATE ITEM OF THIS CONTRACT. TWO CIRCUIT BREAKERS, CONSIDERED SUBSIDIARY TO THE CONTROL EQUIPMENT, ARE NEEDED WHERE STREET LIGHTING IS INCLUDED. AS PART OF THE SIGNAL INSTALLATION, STREET LIGHTING CIRCUIT (2c#12 A.W.G. UF RATED, TYPICAL) SHALL BE KEPT FROM THE CIRCUIT SERVING THE TRAFFIC SIGNAL CONTROL EQUIPMENT FROM THE POINT OF TIE-IN AT THE SECONDARY BREAKER PROVIDED BY THE CONTRACTOR.
- CONTRACTOR SHALL CONNECT A SEPARATE NEUTRAL FOR EACH LOAD SWITCH REPRESENTED ON EACH SIGNAL POLE.
- TRAFFIC CONTROLLER CABINET AND LAYOUT SHALL BE SUCH THAT IT IS NOT NECESSARY TO SHUT DOWN POWER OR REMOVE LOAD SWITCHES IN ORDER TO EASILY TEST OR MODIFY DETECTOR INPUTS TO THE CONTROLLER.
- CONTROLLER CABINET SHALL BE WIRED SUCH THAT DURING FLASH OPERATIONS POWER TO THE LOAD SWITCHES CANNOT BACKFEED TO LOAD SWITCH POWER BUSS.
- ALL PARTS OF THIS INSTALLATION SHALL BE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, STANDARD DRAWINGS AND WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, CURRENT EDITION.
- CONDUIT INSTALLED UNDER ROADWAY SURFACES SHALL BE INSTALLED BY PUSHING OR BORING METHODS. IF THE ENGINEER DETERMINES THIS IS NOT FEASIBLE, THEN A TRENCHING METHOD AS SHOWN IN THE STANDARD DRAWINGS MAY BE USED.
- TRAFFIC SIGNAL POLES SHALL BE GALVANIZED. BACKPLATES SHALL BE SUPPLIED FOR ALL SIGNAL HEADS.
- PAVEMENT MARKING SHOWN FOR REFERENCE ONLY. SEE PERMANENT PAVEMENT MARKING DETAILS.
- FOUNDATION FOR ALL POLES SHALL BE EXTENDED IF NECESSARY TO ACCOMMODATE THE REQUIREMENTS FOR SIGNAL HEAD CLEARANCE ABOVE ROADWAY ONLY AT LOCATIONS WHERE THE GROUND ELEVATION AT THE POLE IS BELOW THE ELEVATION OF THE ROADWAY (SEE NOTES ON STANDARD DRAWING). PAYMENT WILL BE INCLUDED IN SECTION 714 TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, CURRENT EDITION.
- ALL CONCRETE PULL BOXES SHALL BE (TYPE 2 HD) UNLESS OTHERWISE INDICATED. ALL CONDUIT SHALL BE THREE (3") INCH DIAMETER UNLESS SPECIFIED ON PLANS.
- CONTRACTOR SHALL NOTIFY ALL EXISTING UTILITY OWNERS BEFORE BEGINNING WORK ON THIS PROJECT.
- LUMINAIRE ASSEMBLIES SHALL BE OF THE FULL CUTOFF TYPE.
- HARDWARE INPUTS MAY BE DETERMINED BY SUPPLIER. EACH DETECTOR OUTPUT SHALL INPUT THE CONTROLLER THROUGH A SEPARATE INPUT UNLESS OTHERWISE NOTED AND BE PROGRAMMED TO ACTUATE THE ASSOCIATED PHASE. COMBINATION (COMB.) DETECTORS SHALL ALSO BE PROGRAMMED TO PROVIDE VEHICLE COUNT/OCCUPANCY DATA.
- THE LOCAL RADIO WITH ANTENNA SHALL BE COMPATIBLE WITH THE EXISTING CLOSED LOOP COORDINATION SYSTEM IN THE CITY OF SPRINGDALE.
- TO DETERMINE UTILITY CLEARANCES ABOVE THE TRAFFIC SIGNAL POLE, REFER TO THE POLE SCHEDULE FOR VERTICAL SHAFT HEIGHT. WHERE THE POLE SCHEDULE INDICATES THAT A LUMINAIRE ARM WILL BE USED, THIRTY-EIGHT (38') FEET SHOULD BE USED TO DETERMINE UTILITY CLEARANCE ABOVE THE LUMINAIRE ARM. WHERE THE POLE SCHEDULE INDICATES A TRAFFIC SIGNAL POLE WITHOUT A LUMINAIRE ARM, A HEIGHT OF TWENTY-ONE (21') FEET SHOULD BE USED TO DETERMINE UTILITY CLEARANCE ABOVE THE TRAFFIC SIGNAL MAST ARM. AN ADDITIONAL SIX (6') FEET SHOULD BE USED DIRECTLY ABOVE "VIDEO DETECTOR" AT LOCATIONS SHOWN ON THE SIGNAL PLANS.
- THE DESIRABLE MINIMUM DISTANCE FROM THE FACE OF ROADWAY CURB OR SHOULDER EDGE TO THE FACE OF NON-BREAKAWAY POLE OR OBSTRUCTION IS SIX (6') FEET. REFER TO TRAFFIC SIGNAL PLANS FOR SPECIFIC LOCATION OF POLES, CONTROLLER AND ANY OTHER NON-BREAKAWAY OBSTRUCTIONS. REFER TO "DESIGN PARAMETERS, MINIMUM CLEAR ZONE DISTANCE" FOR MINIMUM DISTANCE FROM THE EDGE OF TRAVELED WAY TO THE FACE OF A NON-BREAKAWAY POLE OR OBSTRUCTION. TRAFFIC SIGNAL POLES OR ANY OTHER NON-BREAKAWAY OBSTRUCTION SHALL NOT BE INSTALLED WITHIN THE CLEAR ZONE.
- AS DETERMINED BY THE ENGINEER, FOUNDATION EMBEDMENT MAY BE DECREASED BY A MAXIMUM OF TWO FEET IF COMPETENT ROCK IS ENCOUNTERED PRIOR TO ACHIEVING PLAN EMBEDMENT AND AT LEAST HALF OF THE REMAINING PLAN EMBEDMENT LENGTH IS KEYED INTO COMPETENT ROCK.

- CONNECTION OF TRAFFIC SIGNAL DISPLAY TO FIELD WIRING SHALL UTILIZE AN APPROVED TERMINAL STRIP BEHIND HAND-HOLE COVER AT BASE OF POLE. TERMINAL STRIP SHALL PROVIDE PROTECTION TO PREVENT EXPOSURE TO THE PUBLIC IN THE EVENT THAT POLE COVER IS MISSING. PAYMENT FOR TERMINAL STRIPS SHALL BE INCLUDED IN ITEM 714 TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, CURRENT EDITION.
- CONTROLLER CABINET LAYOUT AND ORIENTATION SHALL CONFORM TO IMSA STANDARDS.
- ONE VIDEO PROGRAMMING MODULE SHALL BE PROVIDED FOR AIMING AND SETUP OF DETECTORS IF THE VIDEO SYSTEM CANNOT BE ADJUSTED THROUGH HARDWARE AND SOFTWARE PROVIDED BY ITEMS WITHIN THE JOB.
- TRAFFIC SIGNAL CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER OR ASSIGNED DEPARTMENT PROJECT INSPECTOR EACH DAY PRIOR TO SIGNAL RELATED WORK. NO WORK ON TRAFFIC SIGNALS WILL BE ALLOWED OR APPROVED WITHOUT THIS PRIOR NOTIFICATION.
- ALL STEEL POLES SHALL BE DESIGNED TO MEET THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4th EDITION (2001) WITH 2003 AND 2006 INTERIMS.
- DOOR PANEL TEST PUSH BUTTONS SHALL ACTUATE INDICATED PHASES. DETECTOR ASSIGNMENTS AND/OR SIDE PANEL JUMPERS MAY REQUIRE MODIFICATION.
- ALL SYSTEM DETECTOR RACKS AND ASSOCIATED EQUIPMENT SHALL BE PROTECTED BY THE MAIN CONTROLLER CABINET POWER SURGE PROTECTION.
- IN PULL BOXES, POLE BASES, JUNCTION BOXES AND CONTROLLER CABINETS, THE DIRECTION OF EACH CABLE RUN SHALL BE INDICATED BY ATTACHING A PERMANENT TAG OF RIGID PLASTIC OR NON-FERROUS METAL TO THE CONDUIT. TAGS SHALL BE EMBOSSED, STAMPED OR ENGRAVED WITH LETTERS 1/4" OR GREATER IN HEIGHT AND SECURED TO THE CONDUIT WITH NYLON OR PLASTIC TIES. IN INSTANCES WHERE THE CONDUIT OR CONDUIT ENTRANCES ARE NOT VISIBLE OR ACCESSIBLE, A DIRECTION TAG SHALL BE ATTACHED TO EACH CABLE.
- THE CONTRACTOR SHALL PERFORM ALL WORK POSSIBLE THAT WILL MINIMIZE THE TIME THAT THE TRAFFIC SIGNAL IS OUT OF OPERATION. IF, IN THE OPINION OF THE ENGINEER, TRAFFIC CONDITIONS WARRANT THE CONTRACTOR SHALL PROVIDE FLAGMEN TO DIRECT TRAFFIC WHILE THE TRAFFIC SIGNAL IS OUT OF OPERATION.
- ALL NON-METALLIC CONDUIT RUNS SHALL HAVE BELL RING FITTINGS INSTALLED ON THE TERMINATING ENDS OF THE CONDUIT. THIS INCLUDES PULL BOXES, POLE BASES, AND TRAFFIC SIGNAL CABINETS.
- ALL CONCRETE PULL BOXES SHALL BE SET ON A GRAVEL OR CRUSHED STONE BEDDING AS SPECIFIED IN SECTION 711, CONCRETE PULL BOX, OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014.

ADDITIONAL TRAFFIC SIGNAL NOTES:

- VIDEO DETECTION ZONES WILL BE USED TO ACTUATE BICYCLE LANES.
- BICYCLE SIGNAL HEADS SHALL BE 8 INCH MODULES DISPLAYING SYMBOLIC BICYCLE WITH BCKPLATE.
- AT THE INTERSECTION OF ROBINS ST. THE EXISTING PAN TILT ZOOM SYSTEM WILL BE RELOCATED PER PLANS.
- AT THE INTERSECTION OF BRUCE ST. THE REMOVAL OF TRAFFIC SIGNAL EQUIPMENT WILL BE FOR THE EXISTING PEDESTRIAN HEADS AND PUSH BUTTONS.
- AT THE INTERSECTION OF COLLEGE AVE. THE REMOVAL OF TRAFFIC SIGNAL EQUIPMENT WILL BE FOR BOTH EXISTING PERMANENT AND PROPOSED TEMPORARY SPAN WIRE SIGNALS.

GOVERNING SPECIFICATIONS

ARKANSAS HIGHWAY COMMISSION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EDITION OF 2014, AND THE FOLLOWING SPECIAL PROVISIONS.

- | | |
|-------|--|
| SP-1 | ARDOT SPECIFICATIONS |
| SP-2 | TEMPORARY TRAFFIC SIGNAL CONSTRUCTION |
| SP-3 | SYSTEM LOCAL CONTROLLER |
| SP-4 | ELECTRICAL CONDUCTORS FOR LUMINAIRES |
| SP-5 | ELECTRICAL CONDUCTORS-IN-CONDUIT |
| SP-6 | LED COUNTDOWN PEDESTRIAN SIGNAL HEAD |
| SP-7 | LED TRAFFIC SIGNAL HEAD |
| SP-8 | LED LUMINAIRE ASSEMBLY (BUG U0 TYPE) |
| SP-9 | STREET NAME SIGN (MAST ARM MOUNTED) |
| SP-10 | SERVICE POINT ASSEMBLY (TRAFFIC CONTROL DEVICES) |
| SP-11 | PEDESTRIAN SIGNAL HEAD RELOCATION |
| SP-12 | TRAFFIC SIGNAL CONTROLLER (MODIFICATION) |
| SP-13 | REMOVAL OF TRAFFIC SIGNAL EQUIPMENT |
| SP-14 | VIDEO DETECTOR (COLOR) |
| SP-15 | EDGE CARD VIDEO PROCESSOR |
| SP-16 | EMERGENCY BATTERY BACKUP SYSTEM INSTALLATION |
| SP-17 | BICYCLE SIGNAL HEAD, 8 INCH LED |
| SP-18 | RELOCATION OF PAN TILT ZOOM CAMERA SYSTEM |



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Digitally Signed 05/29/2020

REV.	DATE	DESCRIPTION	BY
Δ	5/29/2020	ADDENDUM NO. 1 - ADD NOTES	DPP



CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

TRAFFIC SIGNAL
NOTES

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DPP
DRAWN BY: ARM

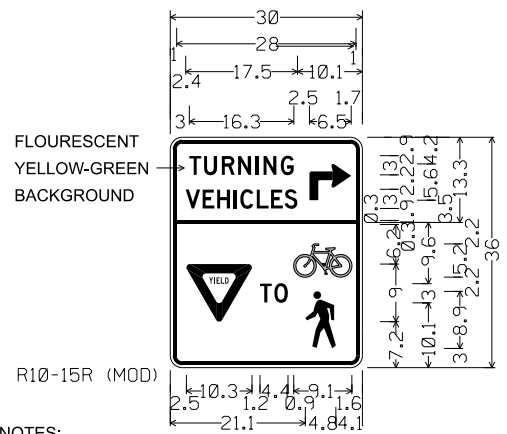
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IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

DRAWING NUMBER
SG-101
SHEET NUMBER **109**

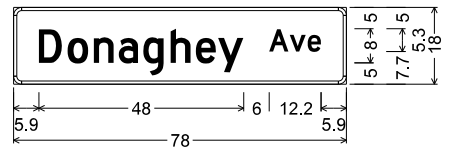
SUMMARY OF TRAFFIC SIGNAL QUANTITIES

ITEM NUMBER	ITEM	DONAGHEY AVE AT ROBINS ST	DONAGHEY AVE AT BRUCE ST	DONAGHEY AVE AT COLLEGE AVE	QUANTITY	UNIT
** SP & 701	SYSTEM LOCAL CONTROLLER TS2-TYPE 2 (8 PHASES)			1	1	EACH
SP & 701	SYSTEM LOCAL CONTROLLER TS2-TYPE 2, E-NET (8 PHASES)	1			1	EACH
SP	ETHERNET SWITCH, T100 HARDENED (8-PORT)	1			1	EACH
SP	E-NET CABLE (EXTERIOR CAT 5E)	314			314	LIN. FT.
SP	LOCAL RADIO (E-NET 5.8) WITH ANTENNA	1			1	EACH
SP	LOCAL REPEATER RADIO WITH ANTENNA	1			1	EACH
SP	BATTERY BACKUP SYSTEM	1			1	EACH
SP	RELOCATE PTZ CAMERA SYSTEM	1			1	EACH
SP & 706	TRAFFIC SIGNAL HEAD, LED, (3 SECTION, 1 WAY)	5		6	11	EACH
SP	TRAFFIC SIGNAL HEAD, LED, (3 SECTION, 1 WAY, 8 INCH, BICYCLE)	2	2		4	EACH
SP & 706	TRAFFIC SIGNAL HEAD, LED, (4 SECTION, 1 WAY)	1			1	EACH
SP & 706	TRAFFIC SIGNAL HEAD, LED, (5 SECTION, 1 WAY)	1		3	4	EACH
SP	TRAFFIC SIGNAL MODIFICATION		1		1	LUMP SUM
DELETED						
SP	PEDESTRIAN SIGNAL HEAD RELOCATION			3	3	EACH
SP & 707	COUNTDOWN PEDESTRIAN SIGNAL HEAD, LED	4	8		12	EACH
708	TRAFFIC SIGNAL CABLE (5C/14 A.W.G.)	821	1372	3167	5360	LIN. FT.
708	TRAFFIC SIGNAL CABLE (7C/14 A.W.G.)	151		620	771	LIN. FT.
708	TRAFFIC SIGNAL CABLE (12C/14 A.W.G.)	188			188	LIN. FT.
708	TRAFFIC SIGNAL CABLE (20C/14 A.W.G.)	239			239	LIN. FT.
SP	ELECTRICAL CONDUCTORS-IN-CONDUIT (1C/8 A.W.G., E.G.C.)	569	204		773	LIN. FT.
SP	ELECTRICAL CONDUCTORS-IN-CONDUIT (1C/12 A.W.G., E.G.C.)	150			150	LIN. FT.
SP	ELECTRICAL CONDUCTORS-IN-CONDUIT (2C/6 A.W.G.)	41		26	67	LIN. FT.
SP	ELECTRICAL CONDUCTORS FOR LUMINAIRES	622			622	LIN. FT.
709	GALVANIZED STEEL CONDUIT (2")	14		16	30	LIN. FT.
710	NON-METALLIC CONDUIT (2")	164	139		303	LIN. FT.
710	NON-METALLIC CONDUIT (3")	350			350	LIN. FT.
711	CONCRETE PULL BOX (TYPE 2)		2		2	EACH
711	CONCRETE PULL BOX (TYPE 2 HD)	7	2		9	EACH
713	SPAN WIRE ASSEMBLY (TEMPORARY)			1	1	EACH
SS & 714	TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION (26')	1			1	EACH
SS & 714	TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION (34')	2			2	EACH
SP	LED LUMINAIRE ASSEMBLY	3			3	EACH
SS & 715	TRAFFIC SIGNAL PEDESTAL POLE WITH FOUNDATION		4		4	EACH
SP	SERVICE POINT ASSEMBLY (2 CIRCUITS)	1		1	2	EACH
SP	REMOVAL OF TRAFFIC SIGNAL EQUIPMENT	0.33	0.33	0.33	1.00	LUMP SUM
716	TREATED WOOD POLE (CLASS 2, 40')			2	2	EACH
716	TREATED WOOD POLE (CLASS 6, 20')			2	2	EACH
SP & 726	STANDARD SIGN	15	30		45	SQ. FT.
SP	18" STREET NAME SIGN	3			3	EACH
733	VIDEO DETECTOR RELOCATION			3	3	EACH
* SP & 733	VIDEO DETECTOR (CLR)	3			3	EACH
733	VIDEO CABLE	560		517	1077	LIN. FT.
733	VIDEO MONITOR (CLR)	1		1	2	EACH
* SP & 733	VIDEO PROCESSOR, EDGE CARD (2 CAMERA)	2		3	5	EACH
SP & 733	VIDEO EDGE CARD EXTENDER	1			1	EACH
SP & 733	VEHICLE DETECTOR RACK (16 CHANNEL)	1		1	2	EACH

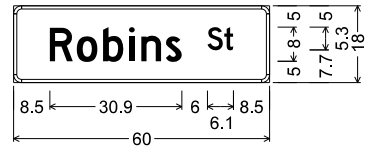
* ONE SPARE VIDEO DETECTOR AND ONE SPARE VIDEO PROCESSOR SHALL BE SUPPLIED.
 ** SUPPLIED BY THE CITY OF CONTRACTOR INSTALLED BY CONTRACTOR



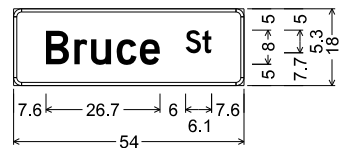
- NOTES:
 1. R10-15R (MOD) USED PER DIRECTION OF CITY OF CONWAY.
 2. FLOURESCENT YELLOW-GREEN BACKGROUND UTILIZED BEHIND "TURNING VEHICLES".



D3-1(1)_VARx8;
 1.9" Radius, 0.8" Border, White on, Green;
 "Donaghey", D 2K; "Ave", D 2K;



D3-1(1)_VARx8;
 1.9" Radius, 0.8" Border, White on, Green;
 "Robins", D 2K;
 "St", D 2K;



D3-1(1)_VARx8;
 1.9" Radius, 0.8" Border, White on, Green;
 "Bruce", D 2K;
 "St", D 2K;

- NOTES:
 1. REFLECTIVE SHEETING SHALL COMPLY WITH ASTM 4956 TYPE 8 OR 9 REFLECTIVE SHEETING. SHEETING AND LEGEND SHALL BE APPLIED IN SUCH A MANNER TO PROVIDE WRINKLE AND BUBBLE FREE SURFACES. APPLICATION OF SHEETING IS CAUSE FOR REJECTION OF MATERIALS DUE TO WORKMANSHIP.
 2. ALUMINUM SIGN BLANK SHALL BE ALLOY 6061-T6 OR 5052-H38. THE ALUMINUM SIGN SHALL BE ALSO ALODIZED. THE ALUMINUM SHEETING SHALL BE 0.100 INCH NOMINAL THICKNESS AND OF THE SIZE SHOWN WITH 1.5" CORNER RADII. PRIOR TO FABRICATION OF THE SIGNS, THE LAYOUT SHALL FIRST BE APPROVED BY AN AGENT OF THE CITY/ COUNTY.
 3. WHEN CROSSROAD HAS TWO NAMES, THE SIGN FOR THE CROSSROAD TO THE LEFT MAY BE INSTALLED ON THE BACKSIDE OF THE MAST ARM ON THE NEAR SIDE LEFT POLE. SEE STANDARD DRAWING SHEET FOR MORE INFORMATION FOR MOUNTING ON MAST ARM ASSEMBLY.
 4. THE SERIES C 2000 STANDARD ALPHABET SHALL BE USED FOR ALL LETTERS.



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Digitally Signed 05/29/2020

REV.	DATE	DESCRIPTION	BY
Δ	5/29/2020	ADDENDUM NO. 1 - REVISE QUANTITIES	DPP

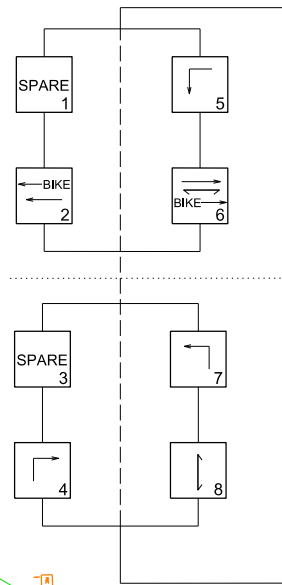


CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

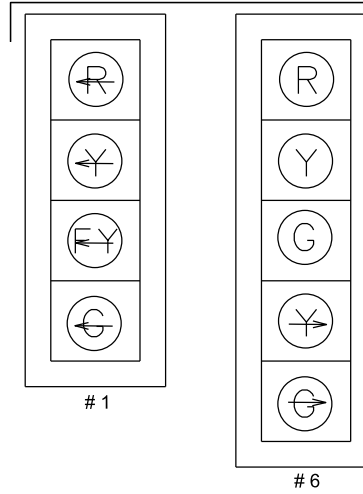
TRAFFIC SIGNAL QUANTITIES

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DPP
 DRAWN BY: ARM
 BAR IS ONE INCH ON ORIGINAL DRAWING
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.
 DRAWING NUMBER
SG-102
 SHEET NUMBER
110

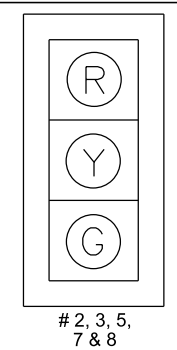
PHASING DIAGRAM



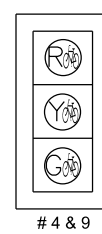
12" LENSES



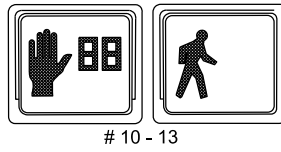
SIGNAL FACES



8" LENSES

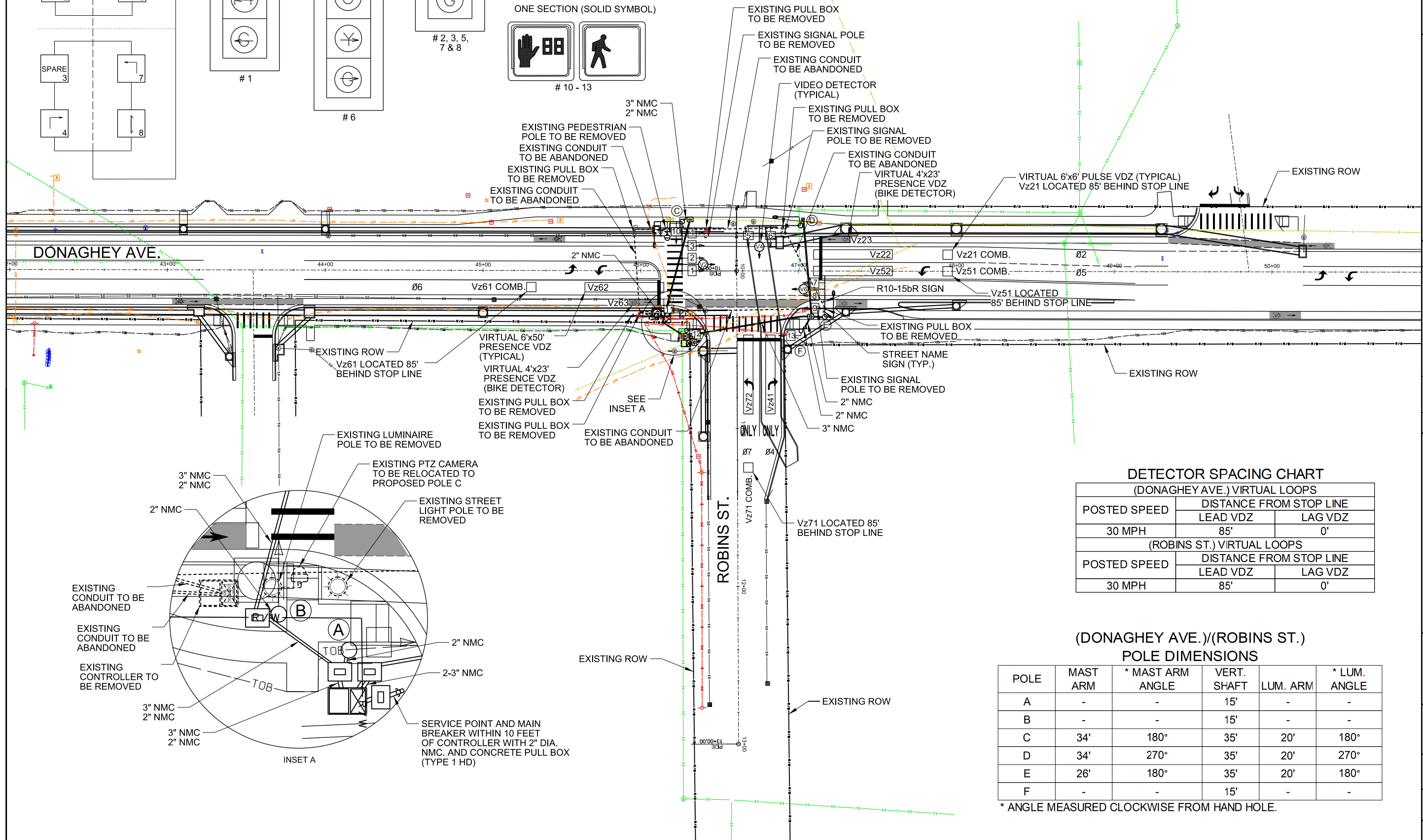
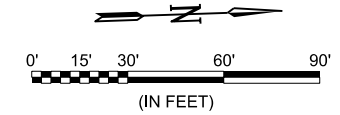


ONE SECTION (SOLID SYMBOL)



NOTES:

1. ALL SIGNAL HEADS SHALL HAVE BACKPLATES.
2. REFER TO SPECIAL PROVISION "RETROREFLECTIVE BACKPLATES" FOR DETAILS ON REQUIREMENTS FOR BACKPLATES.
3. REFER TO SPECIAL PROVISIONS FOR DETAILS ON REQUIREMENTS FOR PEDESTRIAN SIGNAL HEADS.
4. ALL PEDESTRIAN SIGNAL HEADS CAN BE PLACED INTO OPERATION IF THERE ARE BOTH WHEELCHAIR RAMPS AND A CROSSWALK THAT MEET A.D.A.S. STANDARDS.



DETECTOR SPACING CHART
(DONAGHEY AVE.) VIRTUAL LOOPS

POSTED SPEED	DISTANCE FROM STOP LINE	
	LEAD VDZ	LAG VDZ
30 MPH	85'	0'

(ROBINS ST.) VIRTUAL LOOPS

POSTED SPEED	DISTANCE FROM STOP LINE	
	LEAD VDZ	LAG VDZ
30 MPH	85'	0'

**(DONAGHEY AVE.)/(ROBINS ST.)
POLE DIMENSIONS**

POLE	MAST ARM	* MAST ARM ANGLE	VERT. SHAFT	LUM. ARM	* LUM. ANGLE
A	-	-	15'	-	-
B	-	-	15'	-	-
C	34'	180°	35'	20'	180°
D	34'	270°	35'	20'	270°
E	26'	180°	35'	20'	180°
F	-	-	15'	-	-

* ANGLE MEASURED CLOCKWISE FROM HAND HOLE.



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BY	DESCRIPTION	DATE	REV.



CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

TRAFFIC SIGNAL PLAN -
ROBINS ST.
SHEET (1 OF 2)

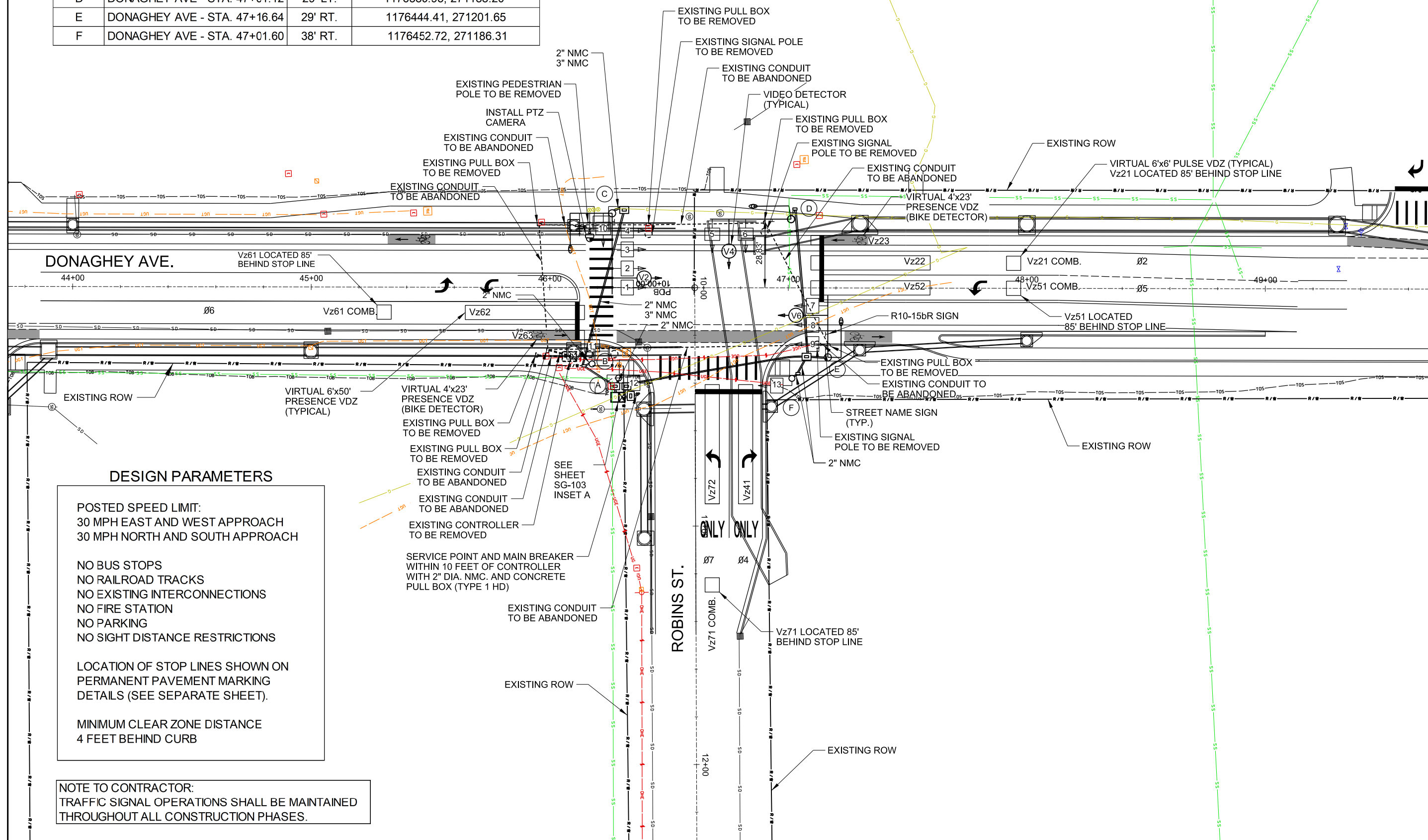
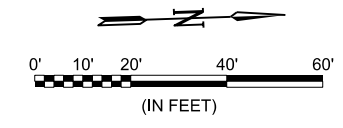
JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DPP
DRAWN BY: ARM

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DRAWING NUMBER
SG-103
SHEET NUMBER
111

(DONAGHEY AVE.)/(ROBINS ST.)
POLE LOCATIONS

POLE	LOCATION & STATION	OFFSET	X, Y COORDINATES
A	DONAGHEY AVE - STA. 46+28.95	38' RT.	1176450.11, 271113.70
B	DONAGHEY AVE - STA. 46+17.81	32' RT.	1176443.82, 271102.78
C	DONAGHEY AVE - STA. 46+26.16	31' LT.	1176380.98, 271113.31
D	DONAGHEY AVE - STA. 47+01.12	29' LT.	1176385.95, 271188.20
E	DONAGHEY AVE - STA. 47+16.64	29' RT.	1176444.41, 271201.65
F	DONAGHEY AVE - STA. 47+01.60	38' RT.	1176452.72, 271186.31



DESIGN PARAMETERS

POSTED SPEED LIMIT:
30 MPH EAST AND WEST APPROACH
30 MPH NORTH AND SOUTH APPROACH

NO BUS STOPS
NO RAILROAD TRACKS
NO EXISTING INTERCONNECTIONS
NO FIRE STATION
NO PARKING
NO SIGHT DISTANCE RESTRICTIONS

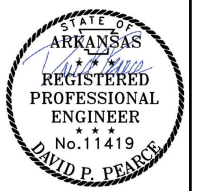
LOCATION OF STOP LINES SHOWN ON
PERMANENT PAVEMENT MARKING
DETAILS (SEE SEPARATE SHEET).

MINIMUM CLEAR ZONE DISTANCE
4 FEET BEHIND CURB

NOTE TO CONTRACTOR:
TRAFFIC SIGNAL OPERATIONS SHALL BE MAINTAINED
THROUGHOUT ALL CONSTRUCTION PHASES.



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CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

TRAFFIC SIGNAL PLAN -
ROBINS ST.
SHEET (2 OF 2)

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DPP
DRAWN BY: ARM

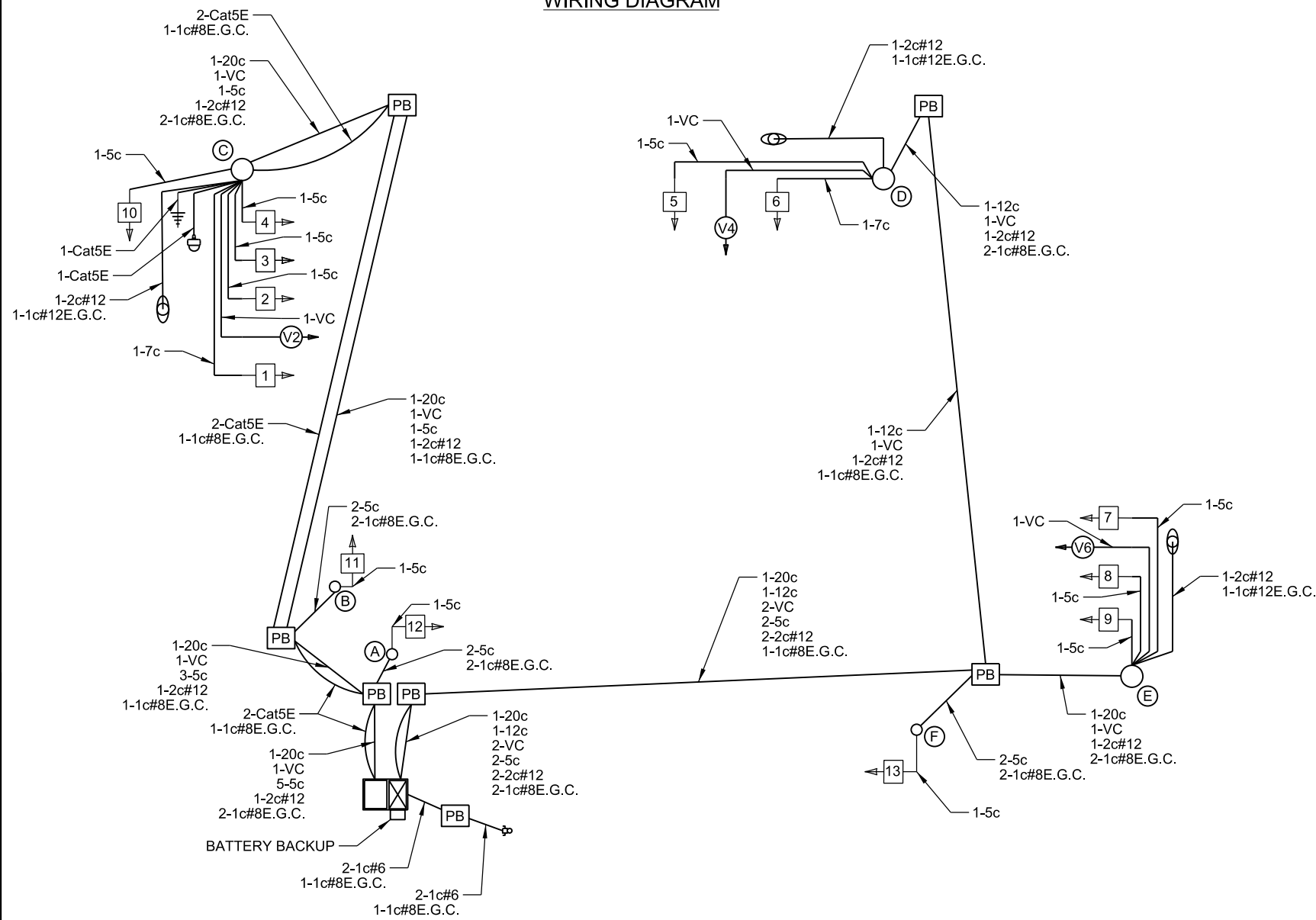
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SHEET NUMBER
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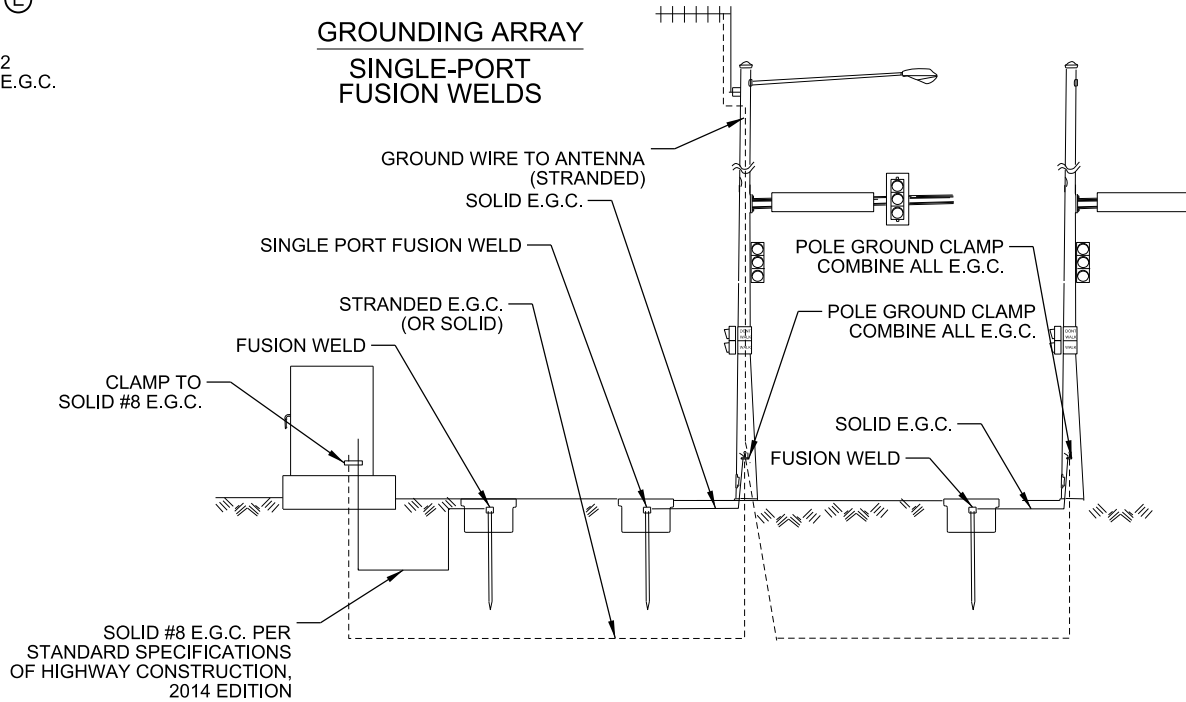
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WIRING DIAGRAM



**GROUNDING ARRAY
 SINGLE-PORT FUSION WELDS**



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CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TRAFFIC SIGNAL
 WIRING DIAGRAM -
 ROBINS ST.

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DPP
 DRAWN BY: ARM

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DRAWING NUMBER
SG-105
 SHEET NUMBER
113

DETECTOR CHART

DETECTOR SYSTEM DESCRIPTION: JOB 18147010											
(DONAGHEY AVE)/(ROBINS ST.) DETECTOR ASSIGNMENTS			HARDWARE INPUTS BY SUPPLIER				PROGRAM ASSIGNMENTS		COMMENTS	TUBE LENGTHS	
DET. ID #	LOCATION DIRECTION	TYPE	DET. #	CAB. TRM. #	AMP CHN. #	CON. IMP. #	PHS	SYSTEM DET. #			MASTER SYSTEM DETECTOR NUMBERS
Vz21	SB ADVANCE	LOCAL			1	V2	2	1		CAMERA V2	37"
Vz22	SB NEAR	COMB.			2	V10	2			CAMERA V2	37"
Vz23	SB BIKE	LOCAL			3	V3	2			CAMERA V2	
Vz41	WB ADVANCE	LOCAL			9	V4	4			CAMERA V4	37"
Vz51	SB LEFT TURN FAR	COMB.			5	V13	5	5		CAMERA V2	37"
Vz52	SB LEFT TURN	LOCAL			6	V5	5			CAMERA V2	37"
Vz61	NB ADVANCE	LOCAL			13	V14	6			CAMERA V6	37"
Vz62	NB NEAR	COMB.			14	V6	6	6		CAMERA V6	37"
Vz63	NB BIKE	COMB.			15	V8	6			CAMERA V6	37"
Vz71	WB LEFT TURN FAR	COMB.			10	V15	7	7		CAMERA V4	37"
Vz72	WB LEFT TURN	LOCAL			11	V7	7			CAMERA V4	37"
PB6 A&B	(DONAGHEY AVE) E. LEG	PED.				P6	6				
PB8 A&B	(ROBINS ST.) S. LEG	PED.				P8	8				
SPARE: 4, 7, 8, 12, & 16											

CONTROLLER INPUT ABBREVIATIONS:

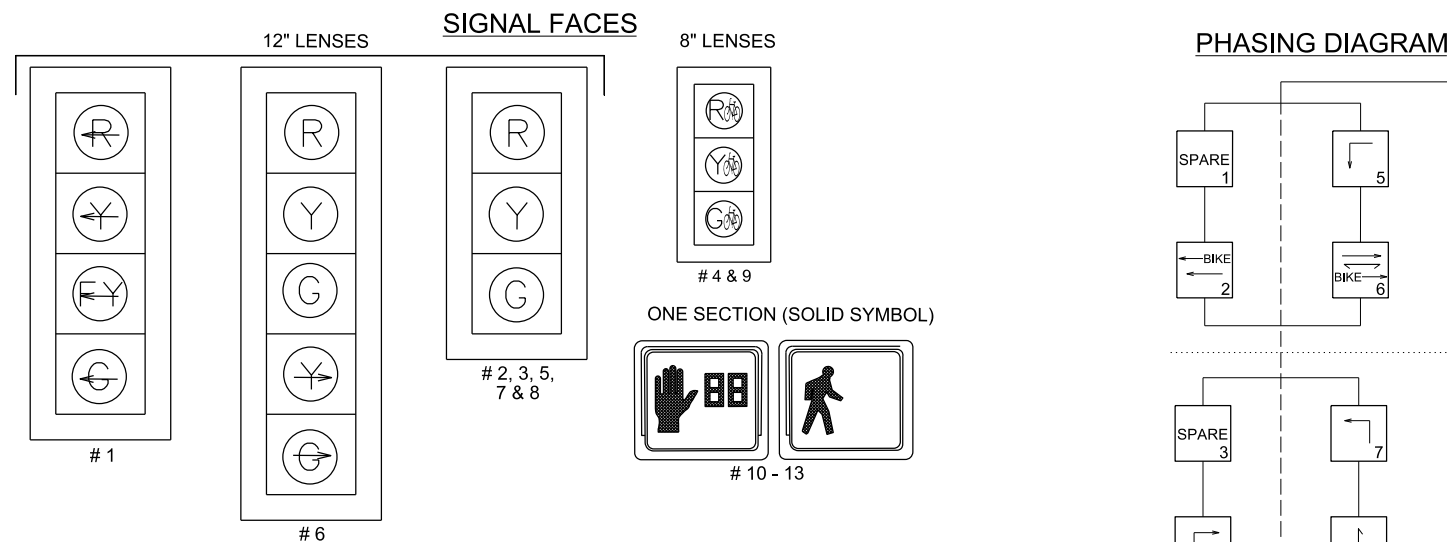
- V = VEHICLE INPUT
- D = SYSTEM OR AUXILIARY INPUT
- P = PEDESTRIAN INPUT

NOTE: "AMP CHN =" REFERS TO THE RACK OUTPUT POSITION.
THIS IS WIRED TO CONTROLLER INPUT DETECTOR NUMBER WHICH IS PROGRAMMED TO ACTUATE THE DESIGNATED PHASE.
EXAMPLE: V9 = SYSTEM DETECTOR 1, V10 = SYSTEM DETECTOR 2

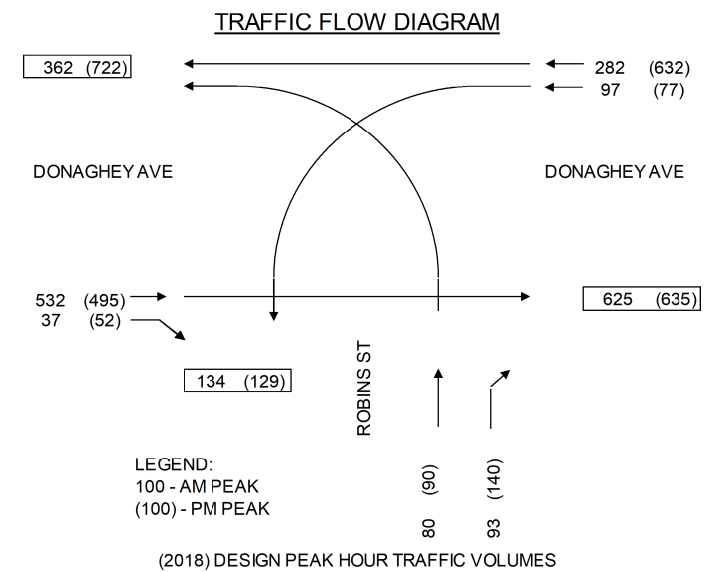
INTERVAL CHART

SIGNAL FACES	DONAGHEY AVE./ROBINS ST.								FLASH SEQUENCE
	2+5	CLR.	2+6	CLR.	4+7	CLR.	4+8	CLR.	
1	<G	***	FY	***	<R	<R	<R	<R	<R
2 & 3	G	**	G	**	R	R	R	R	R
4	G	****	G	****	R	R	R	R	R
5	R	R	R	R	G	Y	R	R	R
6	R	R	R	R	G	*G	R	R	R
7 & 8	R	R	G	Y	R	R	R	R	R
9	R	R	G	****	R	R	R	R	R
10 & 11	DW	DW	DW	DW	DW	DW	W	FDW	BLK
12 & 13	DW	DW	W	FDW	DW	DW	DW	DW	BLK

- * DENOTES GREEN OR YELLOW ARROW DEPENDING ON NEXT PHASE
- ** DENOTES GREEN OR YELLOW BALL DEPENDING ON NEXT PHASE
- *** DENOTES FLASHING YELLOW ARROW OR YELLOW ARROW DEPENDING ON NEXT PHASE
- **** DENOTES GREEN OR YELLOW BIKE DEPENDING ON NEXT PHASE
- NOTE: BIKE SIGNAL HEADS #4 AND #9



- NOTES:
- ALL SIGNAL HEADS SHALL HAVE BACKPLATES.
 - REFER TO SPECIAL PROVISION "RETROREFLECTIVE BACKPLATES" FOR DETAILS ON REQUIREMENTS FOR BACKPLATES.
 - REFER TO SPECIAL PROVISIONS FOR DETAILS ON REQUIREMENTS FOR PEDESTRIAN SIGNAL HEADS.
 - ALL PEDETRIAN SIGNAL HEADS CAN BE PLACED INTO OPERATION IF THERE ARE BOTH WHEELCHAIR RAMPS AND A CROSSWALK THAT MEET A.D.A.S. STANDARDS.



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

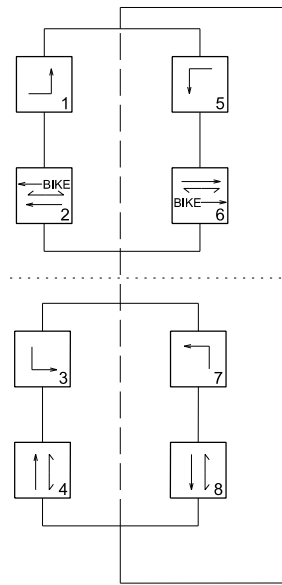
TRAFFIC SIGNAL CHARTS PLAN - ROBINS ST.

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DPP
DRAWN BY: ARM

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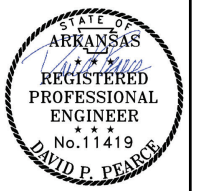
DRAWING NUMBER
SG-106
SHEET NUMBER
114

PHASING DIAGRAM

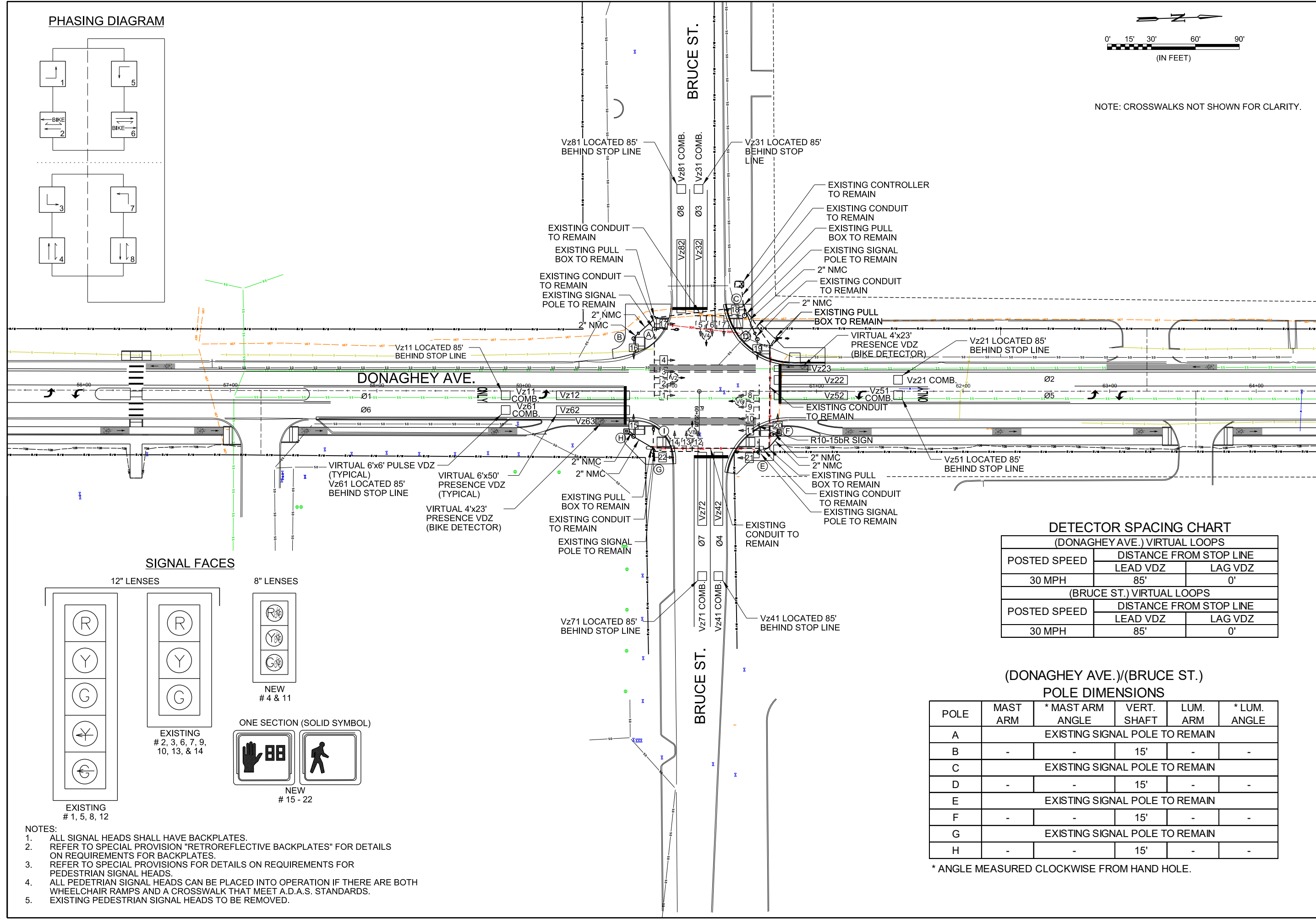


NOTE: CROSSWALKS NOT SHOWN FOR CLARITY.

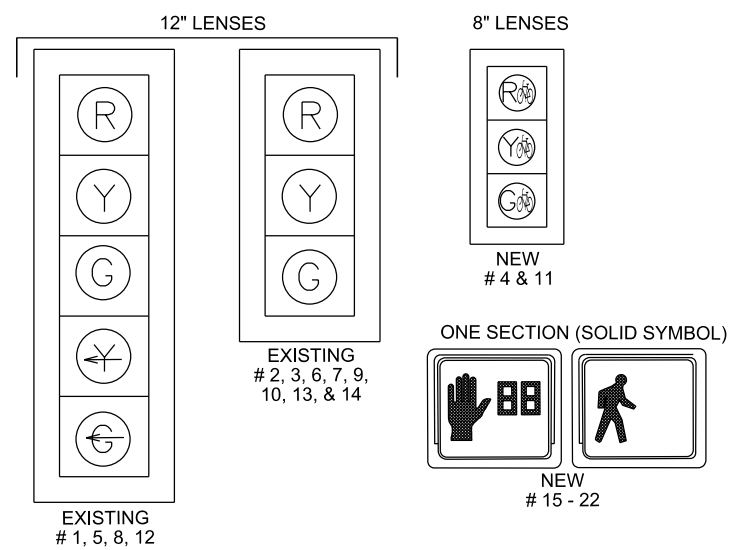
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SIGNAL FACES



- NOTES:
- ALL SIGNAL HEADS SHALL HAVE BACKPLATES. REFER TO SPECIAL PROVISION "RETROREFLECTIVE BACKPLATES" FOR DETAILS ON REQUIREMENTS FOR BACKPLATES.
 - REFER TO SPECIAL PROVISIONS FOR DETAILS ON REQUIREMENTS FOR PEDESTRIAN SIGNAL HEADS.
 - ALL PEDESTRIAN SIGNAL HEADS CAN BE PLACED INTO OPERATION IF THERE ARE BOTH WHEELCHAIR RAMPS AND A CROSSWALK THAT MEET A.D.A.S. STANDARDS.
 - EXISTING PEDESTRIAN SIGNAL HEADS TO BE REMOVED.

DETECTOR SPACING CHART

(DONAGHEY AVE.) VIRTUAL LOOPS		
POSTED SPEED	DISTANCE FROM STOP LINE	
	LEAD VDZ	LAG VDZ
30 MPH	85'	0'
(BRUCE ST.) VIRTUAL LOOPS		
POSTED SPEED	DISTANCE FROM STOP LINE	
	LEAD VDZ	LAG VDZ
30 MPH	85'	0'

(DONAGHEY AVE.)/(BRUCE ST.) POLE DIMENSIONS

POLE	MAST ARM	* MAST ARM ANGLE	VERT. SHAFT	LUM. ARM	* LUM. ANGLE
A	-	-	15'	-	-
B	-	-	15'	-	-
C	-	-	15'	-	-
D	-	-	15'	-	-
E	-	-	15'	-	-
F	-	-	15'	-	-
G	-	-	15'	-	-
H	-	-	15'	-	-

* ANGLE MEASURED CLOCKWISE FROM HAND HOLE.

REV.	DATE	DESCRIPTION



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TRAFFIC SIGNAL PLAN -
 BRUCE ST.
 SHEET (1 OF 2)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DPP
 DRAWN BY: ARM

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SG-107
 SHEET NUMBER
115

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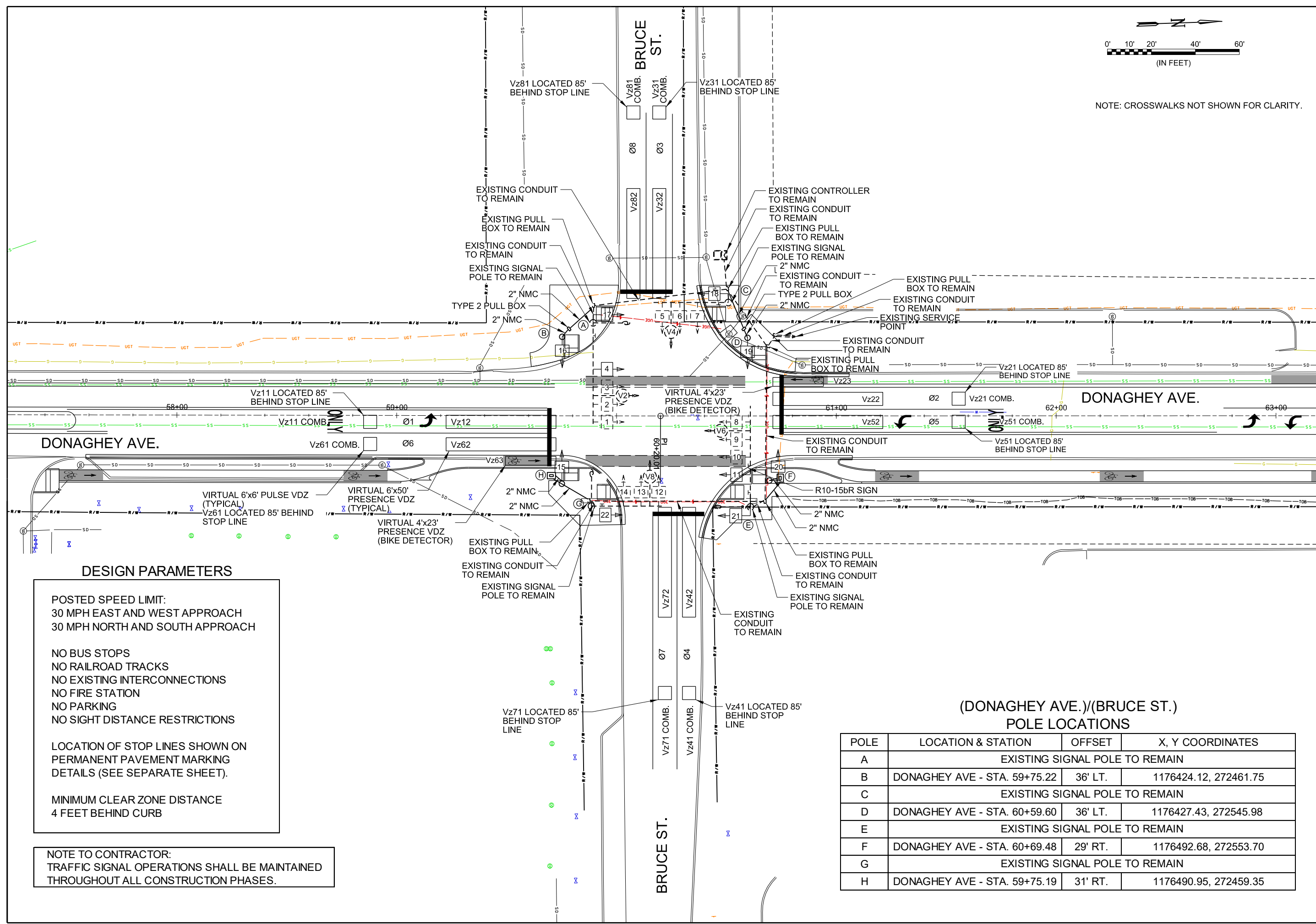
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DESIGN PARAMETERS

POSTED SPEED LIMIT:
 30 MPH EAST AND WEST APPROACH
 30 MPH NORTH AND SOUTH APPROACH

NO BUS STOPS
 NO RAILROAD TRACKS
 NO EXISTING INTERCONNECTIONS
 NO FIRE STATION
 NO PARKING
 NO SIGHT DISTANCE RESTRICTIONS

LOCATION OF STOP LINES SHOWN ON PERMANENT PAVEMENT MARKING DETAILS (SEE SEPARATE SHEET).

MINIMUM CLEAR ZONE DISTANCE
 4 FEET BEHIND CURB

NOTE TO CONTRACTOR:
 TRAFFIC SIGNAL OPERATIONS SHALL BE MAINTAINED THROUGHOUT ALL CONSTRUCTION PHASES.

(DONAGHEY AVE.)/(BRUCE ST.)
 POLE LOCATIONS

POLE	LOCATION & STATION	OFFSET	X, Y COORDINATES
A	EXISTING SIGNAL POLE TO REMAIN		
B	DONAGHEY AVE - STA. 59+75.22	36' LT.	1176424.12, 272461.75
C	EXISTING SIGNAL POLE TO REMAIN		
D	DONAGHEY AVE - STA. 60+59.60	36' LT.	1176427.43, 272545.98
E	EXISTING SIGNAL POLE TO REMAIN		
F	DONAGHEY AVE - STA. 60+69.48	29' RT.	1176492.68, 272553.70
G	EXISTING SIGNAL POLE TO REMAIN		
H	DONAGHEY AVE - STA. 59+75.19	31' RT.	1176490.95, 272459.35

REV.	DATE	DESCRIPTION



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TRAFFIC SIGNAL PLAN -
 BRUCE ST.
 SHEET (2 OF 2)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DPP
 DRAWN BY: ARM

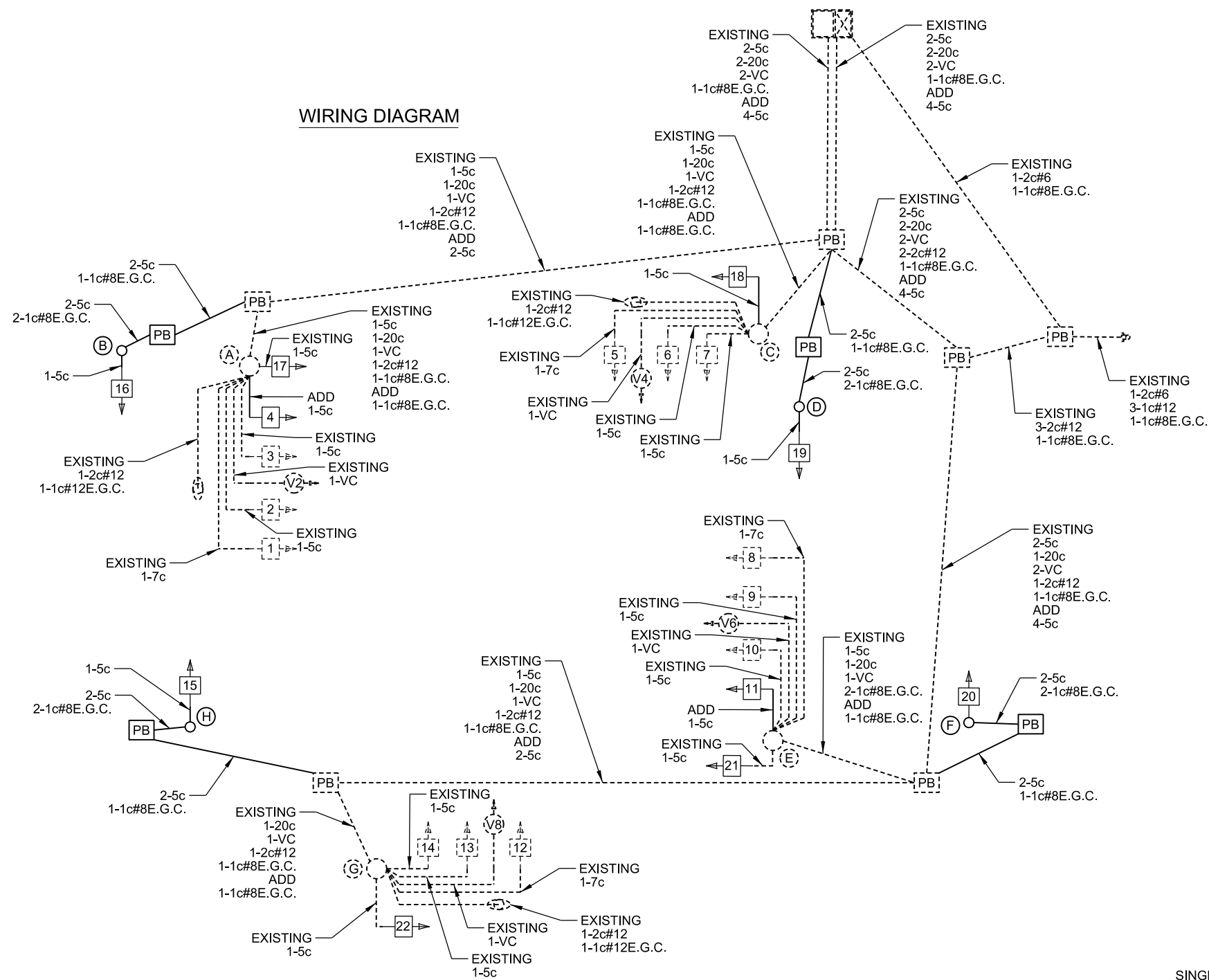
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SG-108

SHEET NUMBER
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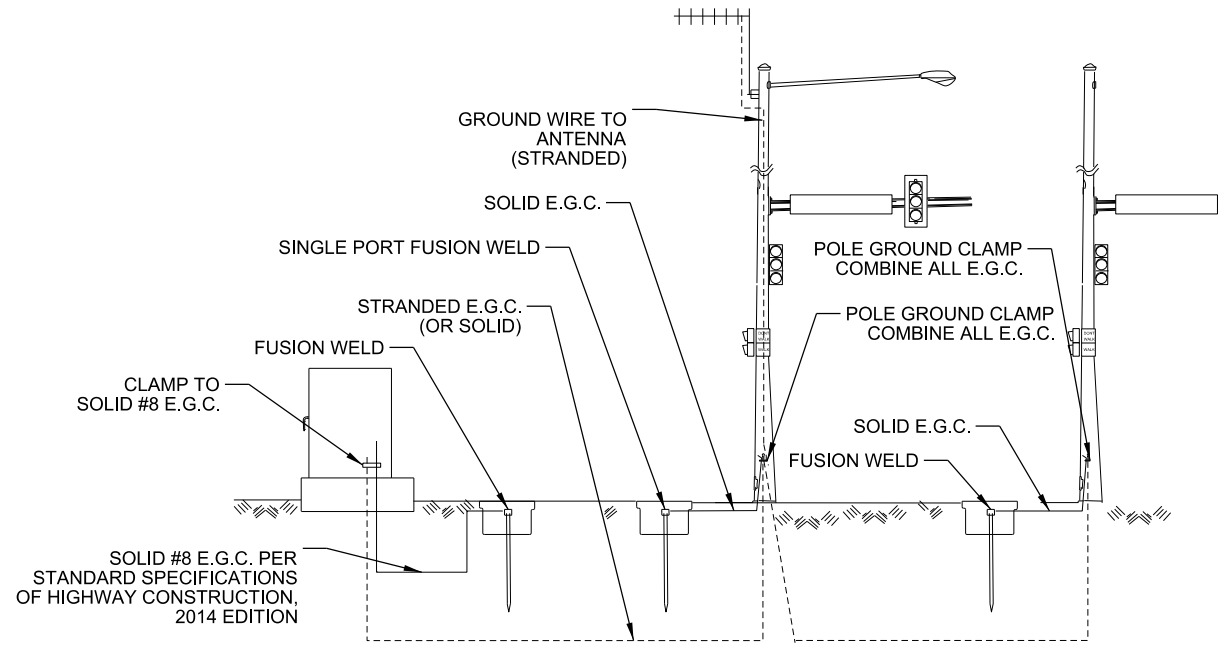
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WIRING DIAGRAM

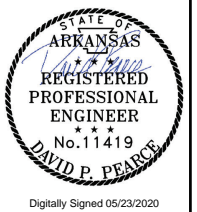


- NOTES TO CONTRACTOR:
- ONE SEPARATE 1-5c IS RUN TO EACH POLE FOR THE PEDESTRIAN PUSH BUTTON.
 - ALL DETECTOR RACK CHANNELS, INCLUDING UNUSED, SHALL BE BROUGHT TO TERMINAL STRIP IN DETECTOR AREA OF CABINET.
 - THE LOCAL GOVERNMENT SHALL BE RESPONSIBLE FOR PROVIDING POWER TO THE SERVICE POINT.

GROUNDING ARRAY SINGLE-PORT FUSION WELDS



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CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

TRAFFIC SIGNAL
WIRING DIAGRAM -
BRUCE ST.

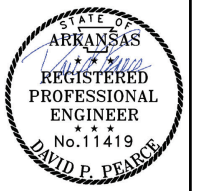
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DRAWING NUMBER
SG-109
SHEET NUMBER
117



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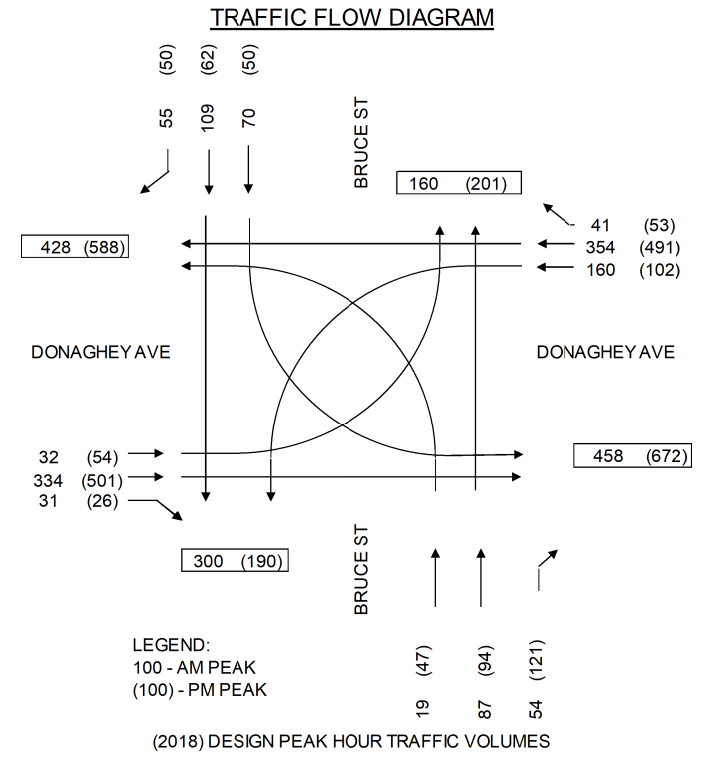


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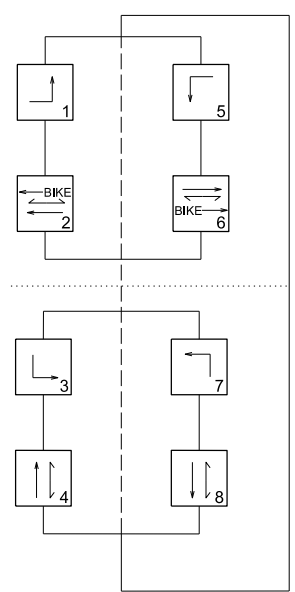
INTERVAL CHART

SIGNAL FACES	DONAGHEY AVE./BRUCE ST.																FLASH SEQUENCE	
	1+5	CLR	1+6	CLR	2+5	CLR	2+6	CLR	3+7	CLR	3+8	CLR	4+7	CLR	4+8	CLR		
1	G	←	*	**	R	R	G	*	G	*	R	R	R	R	R	R	R	R
2 & 3	R	R	R	R	G	←	*	**	G	*	R	R	R	R	R	R	R	R
4	R	R	R	R	G	←	****	G	****	R	R	R	R	R	R	R	R	R
5	R	R	R	R	R	R	R	R	R	R	R	R	G	←	*	**	G	*
6 & 7	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
8	G	←	*	**	G	←	*	**	R	R	G	*	R	R	R	R	R	R
9 & 10	R	R	G	*	R	R	G	*	R	R	R	R	R	R	R	R	R	R
11	R	R	G	****	R	R	G	****	R	R	R	R	R	R	R	R	R	R
12	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	**	R
13 & 14	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	**	R
15 & 16	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW
17 & 18	DW	DW	DW	DW	W	FDW	W	FDW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW
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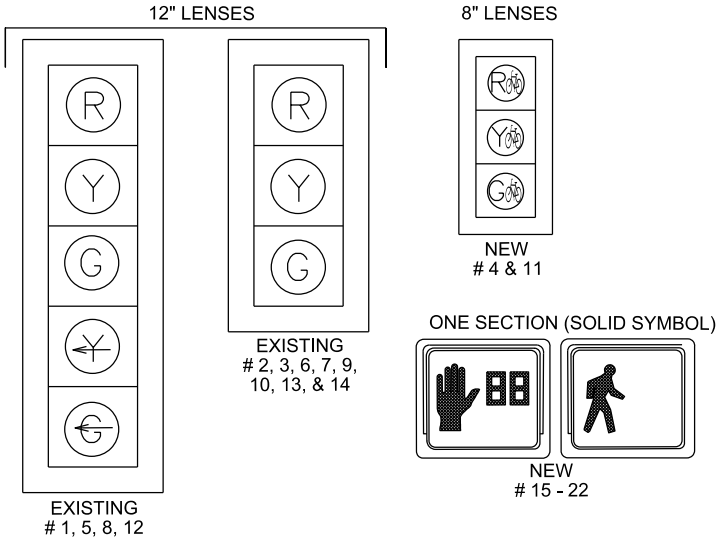
* DENOTES GREEN OR YELLOW ARROW DEPENDING ON NEXT PHASE
 ** DENOTES GREEN OR YELLOW BALL DEPENDING ON NEXT PHASE
 *** DENOTES FLASHING YELLOW ARROW OR YELLOW ARROW DEPENDING ON NEXT PHASE
 **** DENOTES GREEN OR YELLOW BIKE DEPENDING ON NEXT PHASE
 NOTE: BIKE SIGNAL HEADS #4 AND #11



PHASING DIAGRAM



SIGNAL FACES



- NOTES:
- ALL SIGNAL HEADS SHALL HAVE BACKPLATES.
 - REFER TO SPECIAL PROVISION "RETROREFLECTIVE BACKPLATES" FOR DETAILS ON REQUIREMENTS FOR BACKPLATES.
 - REFER TO SPECIAL PROVISIONS FOR DETAILS ON REQUIREMENTS FOR PEDESTRIAN SIGNAL HEADS.
 - ALL PEDESTRIAN SIGNAL HEADS CAN BE PLACED INTO OPERATION IF THERE ARE BOTH WHEELCHAIR RAMPS AND A CROSSWALK THAT MEET A.D.A.S. STANDARDS.
 - EXISTING PEDESTRIAN SIGNAL HEADS TO BE REMOVED.

DETECTOR CHART

DETECTOR SYSTEM DESCRIPTION: JOB 18147010

DET. ID #	LOCATION DIRECTION	TYPE	DET. #	HARDWARE INPUTS BY SUPPLIER			PROGRAM ASSIGNMENTS		COMMENTS	TUBE LENGTHS		
				CAB. TRM. #	AMP CHN. #	CON. IMP. #	PHS	SYSTEM DET. #			MASTER SYSTEM DETECTOR NUMBERS	
Vz11	NB LEFT TURN FAR	COMB.			1	V9	1	1	CAMERA V6	EXIST.		
Vz12	NB LEFT TURN	LOCAL			2	V1	1		CAMERA V6	EXIST.		
Vz21	SB ADVANCE	LOCAL			9	V2	2		CAMERA V2	EXIST.		
Vz22	SB NEAR	COMB.			10	V10	2	2	CAMERA V2	EXIST.		
Vz23	SB BIKE	LOCAL			11	V17	2		CAMERA V2			
Vz31	WB LEFT TURN FAR	COMB.			17	V11	3	3	CAMERA V8	EXIST.		
Vz32	WB LEFT TURN	LOCAL			18	V3	3		CAMERA V8	EXIST.		
Vz41	EB ADVANCE	LOCAL			21	V4	4		CAMERA V4	EXIST.		
Vz42	EB NEAR	COMB.			22	V12	4	4	CAMERA V4	EXIST.		
Vz51	SB LEFT TURN FAR	LOCAL			13	V13	5		CAMERA V2	EXIST.		
Vz52	SB LEFT TURN	COMB.			14	V5	5	5	CAMERA V2	EXIST.		
Vz61	NB ADVANCE	LOCAL			5	V6	6		CAMERA V6	EXIST.		
Vz62	NB NEAR	COMB.			6	V14	6	6	CAMERA V6	EXIST.		
Vz63	NB BIKE	LOCAL			7	V18	6		CAMERA V6			
Vz71	EB LEFT TURN FAR	COMB.			23	V15	7	7	CAMERA V4	EXIST.		
Vz72	EB LEFT TURN	LOCAL			24	V7	7		CAMERA V4	EXIST.		
Vz81	WB ADVANCE	LOCAL			19	V8	8		CAMERA V8	EXIST.		
Vz82	WB NEAR	COMB.			20	V16	8	8	CAMERA V8	EXIST.		
PB2 A&B	(BRUCE ST.) W. LEG	PED.				P2	2					
PB4 A&B	(DONAGHEY AVE.) S. LEG	PED.				P4	4					
PB6 A&B	(BRUCE ST.) E. LEG	PED.				P6	6					
PB8 A&B	(DONAGHEY AVE.) N. LEG	PED.				P8	8					
				SPARE: 3, 4, 8, 12, 15, 16								

CONTROLLER INPUT ABBREVIATIONS:
 V = VEHICLE INPUT
 D = SYSTEM OR AUXILIARY INPUT
 P = PEDESTRIAN INPUT

NOTE: "AMP CHN =" REFERS TO THE RACK OUTPUT POSITION.
 THIS IS WIRED TO CONTROLLER INPUT DETECTOR NUMBER WHICH IS PROGRAMMED TO ACTUATE THE DESIGNATED PHASE.
 EXAMPLE: V9 = SYSTEM DETECTOR 1, V10 = SYSTEM DETECTOR 2

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BY	DATE	DESCRIPTION

CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TRAFFIC SIGNAL CHARTS SHEET - BRUCE ST.

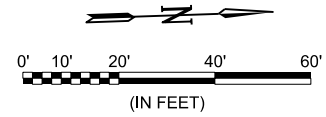
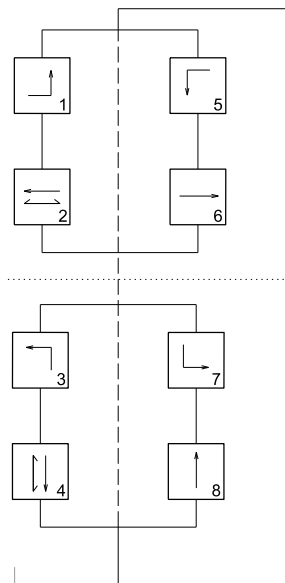
JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DPP
 DRAWN BY: ARM

BAR IS ONE INCH ON ORIGINAL DRAWING
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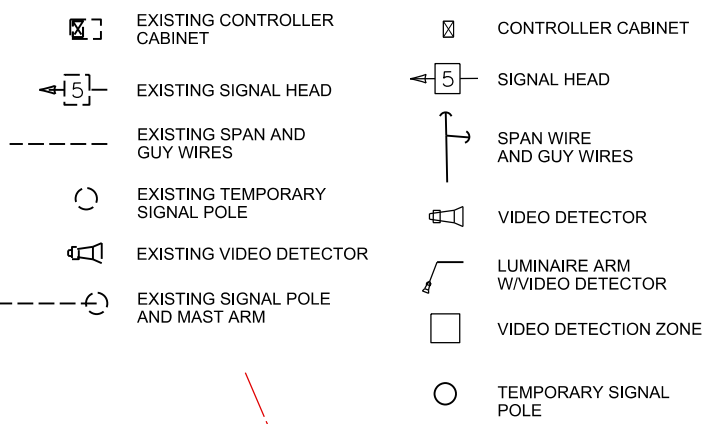
DRAWING NUMBER
SG-110

SHEET NUMBER
118

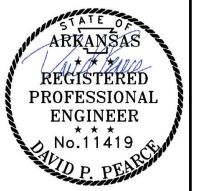
PHASING DIAGRAM



LEGEND



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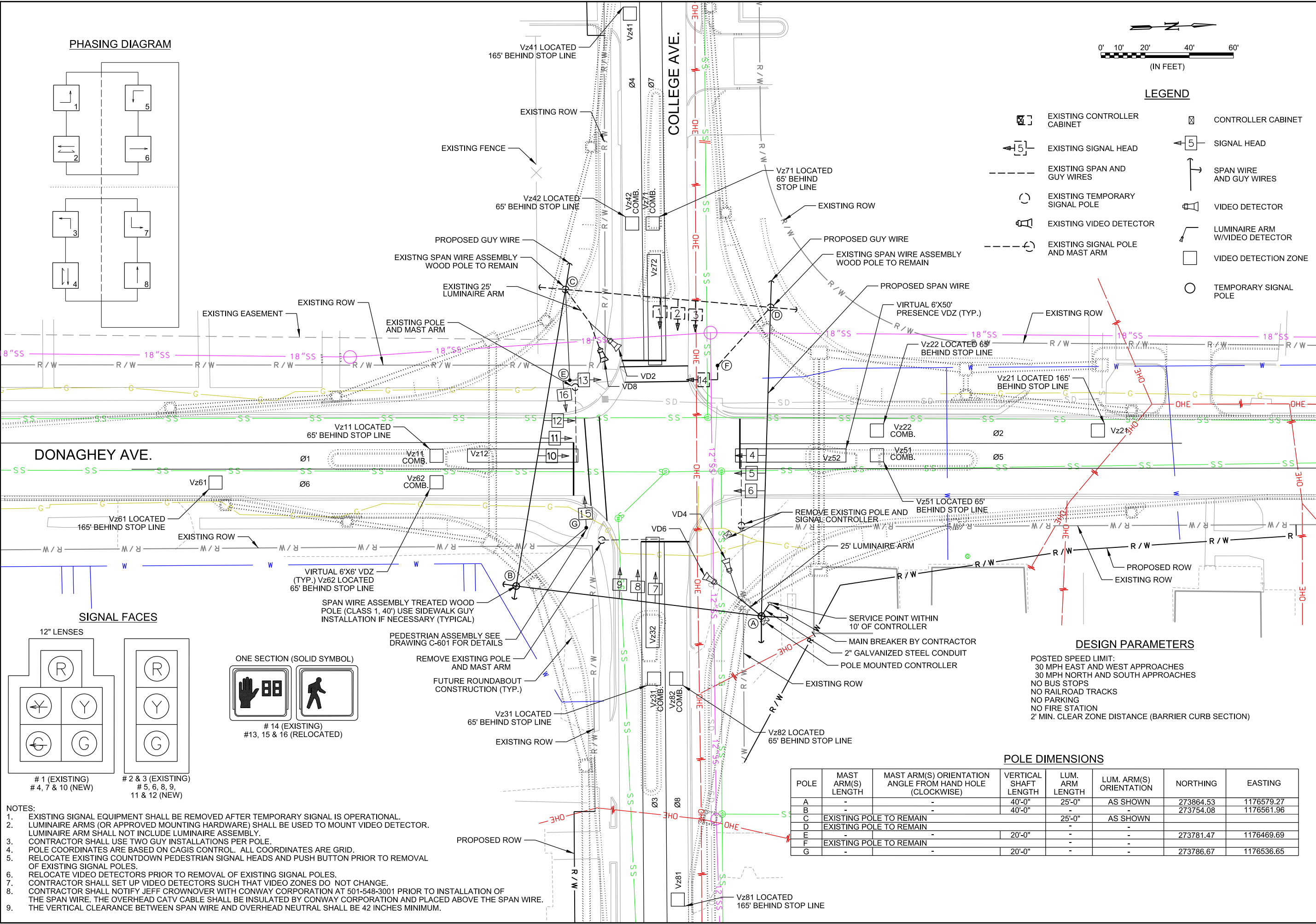
BY	DESCRIPTION	DATE	REV.



CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1
(DAVE WARD DR. TO ADA AVE.)

TEMPORARY TRAFFIC SIGNAL PLAN - COLLEGE AVE.

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: DPP
DRAWN BY: ARM
DRAWING NUMBER
SG-111
SHEET NUMBER
119



DONAGHEY AVE.

COLLEGE AVE.

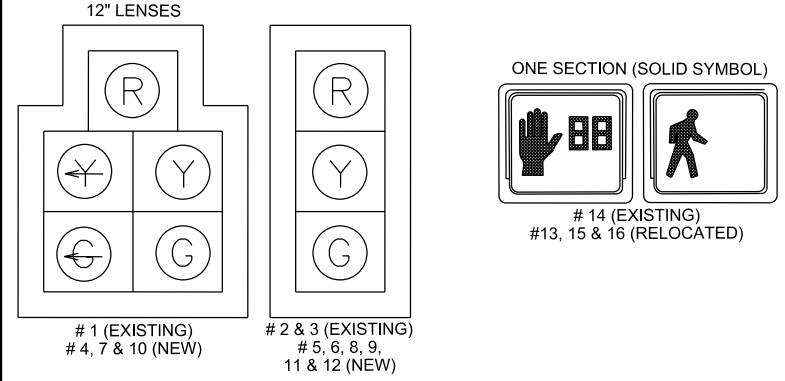
DESIGN PARAMETERS

POSTED SPEED LIMIT:
30 MPH EAST AND WEST APPROACHES
30 MPH NORTH AND SOUTH APPROACHES
NO BUS STOPS
NO RAILROAD TRACKS
NO PARKING
NO FIRE STATION
2' MIN. CLEAR ZONE DISTANCE (BARRIER CURB SECTION)

POLE DIMENSIONS

POLE	MAST ARM(S) LENGTH	MAST ARM(S) ORIENTATION ANGLE FROM HAND HOLE (CLOCKWISE)	VERTICAL SHAFT LENGTH	LUM. ARM LENGTH	LUM. ARM(S) ORIENTATION	NORTHING	EASTING
A	-	-	40'-0"	25'-0"	AS SHOWN	273864.53	1176579.27
B	-	-	40'-0"	-	AS SHOWN	273754.08	1176561.96
C	EXISTING POLE TO REMAIN	-	-	25'-0"	-	-	-
D	EXISTING POLE TO REMAIN	-	-	-	-	-	-
E	-	-	20'-0"	-	-	273781.47	1176469.69
F	EXISTING POLE TO REMAIN	-	-	-	-	-	-
G	-	-	20'-0"	-	-	273786.67	1176536.65

SIGNAL FACES



- NOTES:
- EXISTING SIGNAL EQUIPMENT SHALL BE REMOVED AFTER TEMPORARY SIGNAL IS OPERATIONAL.
 - LUMINAIRE ARMS (OR APPROVED MOUNTING HARDWARE) SHALL BE USED TO MOUNT VIDEO DETECTOR. LUMINAIRE ARM SHALL NOT INCLUDE LUMINAIRE ASSEMBLY.
 - CONTRACTOR SHALL USE TWO GUY INSTALLATIONS PER POLE.
 - POLE COORDINATES ARE BASED ON CAGIS CONTROL. ALL COORDINATES ARE GRID.
 - RELOCATE EXISTING COUNTDOWN PEDESTRIAN SIGNAL HEADS AND PUSH BUTTON PRIOR TO REMOVAL OF EXISTING SIGNAL POLES.
 - RELOCATE VIDEO DETECTORS PRIOR TO REMOVAL OF EXISTING SIGNAL POLES.
 - CONTRACTOR SHALL SET UP VIDEO DETECTORS SUCH THAT VIDEO ZONES DO NOT CHANGE.
 - CONTRACTOR SHALL NOTIFY JEFF CROWNOVER WITH CONWAY CORPORATION AT 501-548-3001 PRIOR TO INSTALLATION OF THE SPAN WIRE. THE OVERHEAD CATV CABLE SHALL BE INSULATED BY CONWAY CORPORATION AND PLACED ABOVE THE SPAN WIRE. THE VERTICAL CLEARANCE BETWEEN SPAN WIRE AND OVERHEAD NEUTRAL SHALL BE 42 INCHES MINIMUM.

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INTERVAL CHART

SIGNAL FACES	INTERSECTION INTERVALS																FLASH SEQ.
	1+5	CLR.	1+6	CLR.	2+5	CLR.	2+6	CLR.	3+7	CLR.	3+8	CLR.	4+7	CLR.	4+8	CLR.	
1	R	R	R	R	R	R	R	R	G	**	G	**	G	*	G	*	⚡
2 & 3	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R
4	G	**	G	**	G	*	G	*	R	R	R	R	R	R	R	R	⚡
5 & 6	R	R	G	*	R	*	G	**	R	R	R	R	R	R	R	R	R
7	R	R	R	R	R	R	R	R	G	**	G	*	G	**	G	*	⚡
8 & 9	R	R	R	R	R	R	R	R	R	R	R	R	G	*	R	R	R
10	G	**	G	*	G	**	G	*	R	R	R	R	R	R	R	R	⚡
11 & 12	R	R	R	R	G	*	G	*	R	R	R	R	R	R	R	R	R
13 & 14	DW	DW	DW	DW	W	FDW	W	FDW	DW	DW	DW	DW	DW	DW	DW	DW	BLK
15 & 16	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	DW	W	FDW	W	FDW	BLK

* DENOTES GREEN OR YELLOW BALL DEPENDING ON NEXT PHASE
 ** DENOTES GREEN OR YELLOW ARROW DEPENDING ON NEXT PHASE

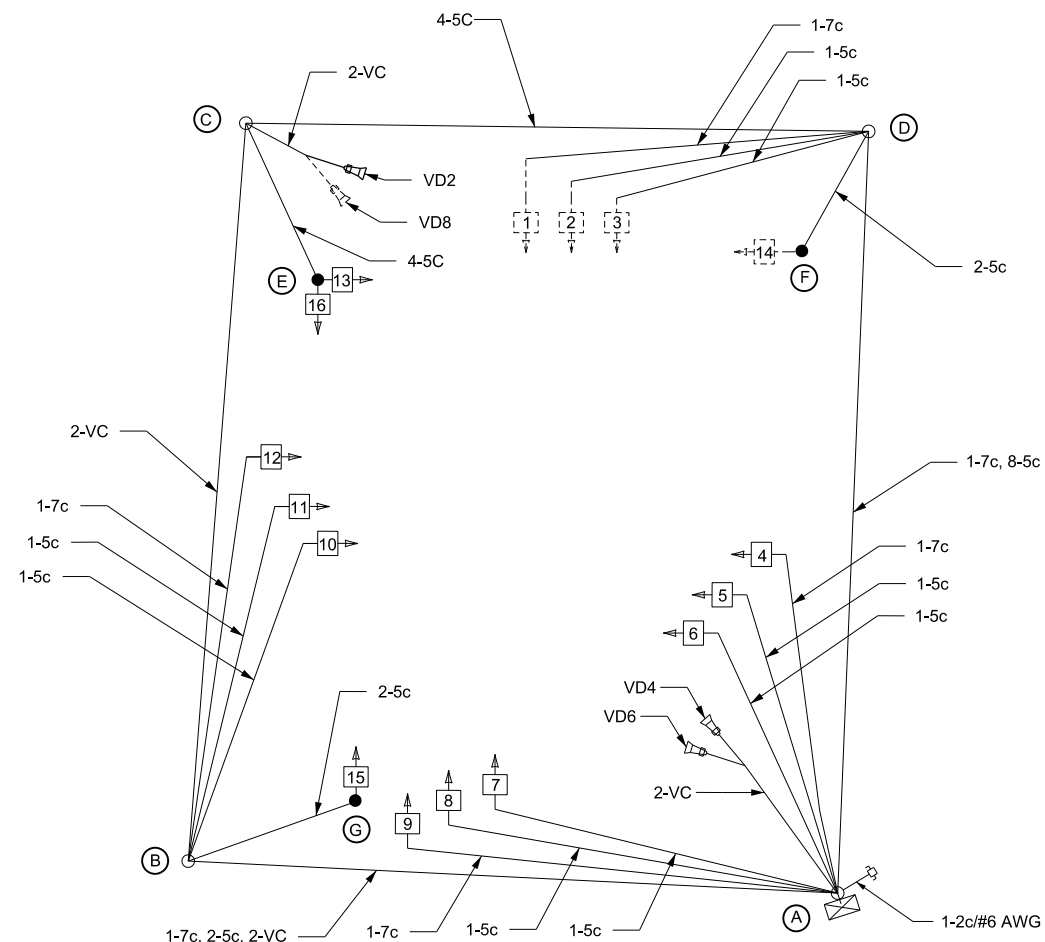
DETECTOR CHART

DETECTOR I.D. #	DIRECTION & LOCATION	TYPE	DET. #	HARDWARE INPUTS BY SUPPLIER			PROGRAM ASSIGNMENTS			VIDEO DET. TUBE LENGTH	COMMENT
				CAB. TRM. #	AMP. CHN. #	CON. INP. #	LOCAL		MSTR. SYS. DET. #		
							PHS. DET. #	SYS. DET. #			
Vz11	NB LEFT FAR	COMB.			9	V9	1	1		EXIST.	VD6
Vz12	NB LEFT NEAR	LOCAL			10	V1	1			EXIST.	VD6
Vz21	SB FAR	LOCAL			1	V2	2			EXIST.	VD2
Vz22	SB NEAR	COMB.			2	V10	2	2		EXIST.	VD2
Vz31	WB LEFT FAR	COMB.			13	V11	3	3		EXIST.	VD8
Vz32	WB LEFT NEAR	LOCAL			14	V3	3			EXIST.	VD8
Vz41	EB FAR	LOCAL			5	V4	4			EXIST.	VD4
Vz42	EB NEAR	COMB.			6	V12	4	4		EXIST.	VD4
Vz51	SB LEFT FAR	COMB.			3	V13	5	5		EXIST.	VD2
Vz52	SB LEFT NEAR	LOCAL			4	V5	5			EXIST.	VD2
Vz61	NB FAR	LOCAL			11	V6	6			EXIST.	VD6
Vz62	NB NEAR	COMB.			12	V14	6	6		EXIST.	VD6
Vz71	EB LEFT FAR	COMB.			7	V15	7	7		EXIST.	VD4
Vz72	EB LEFT NEAR	LOCAL			8	V7	7			EXIST.	VD4
Vz81	WB FAR	LOCAL			15	V8	8			EXIST.	VD8
Vz82	WB NEAR	COMB.			16	V16	8	8		EXIST.	VD8
P2	(COLLEGE AVE.) W. LEG	PED.					P2	2			
P4	(DONAGHEY AVE.) S. LEG	PED.					P4	4			

CONTROLLER INPUT ABBREVIATIONS:

V = VEHICLE INPUT
 P = PEDESTRIAN INPUT

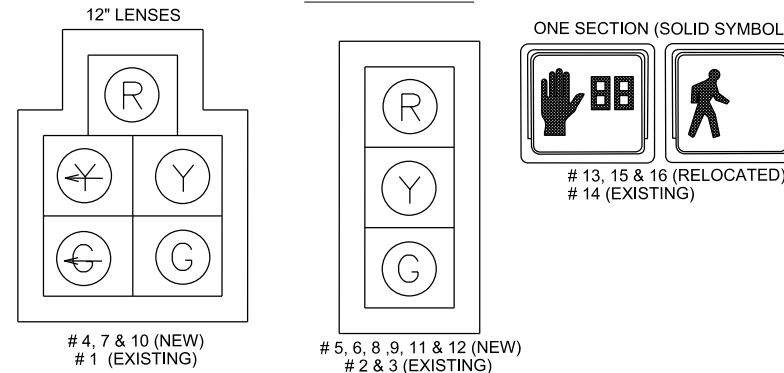
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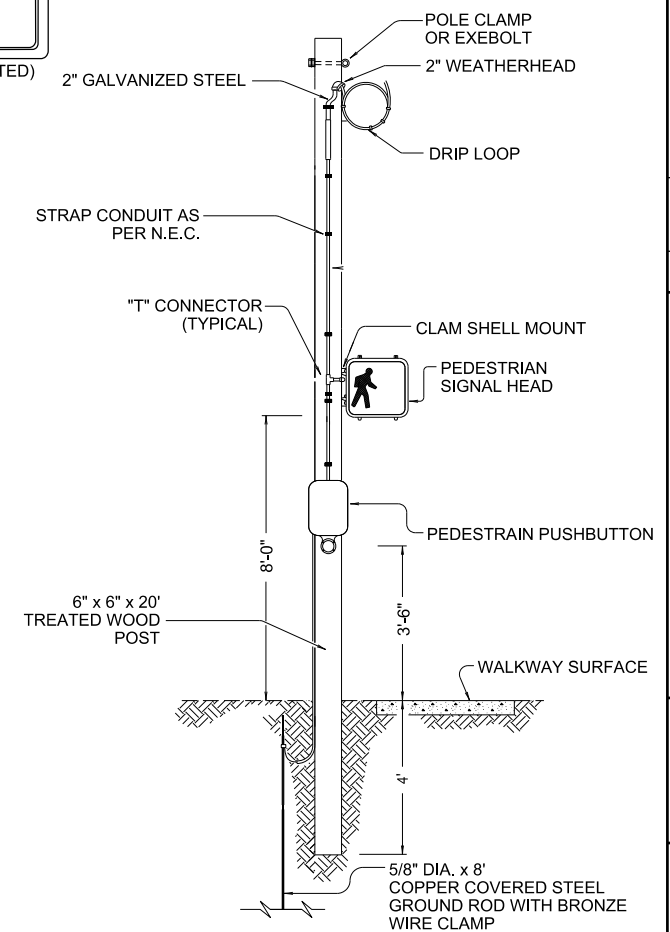
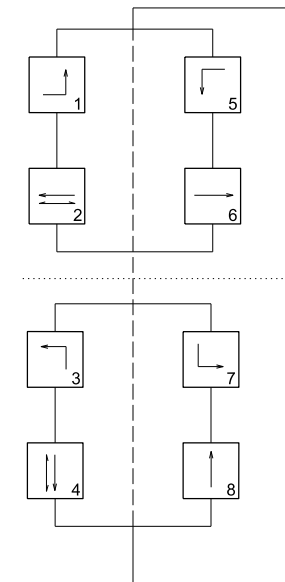
WIRING DIAGRAM

- TYPICAL WIRING INCLUDES:
1. SEPARATE 5c/#14 AWG FROM EACH 3 SEC SIGNAL HEAD TO CONTROLLER.
 2. SEPARATE 7c/#14 AWG FROM EACH 5 SEC SIGNAL HEAD TO CONTROLLER.

SIGNAL FACES



PHASING DIAGRAM



TYPICAL PEDESTRIAN WOOD POST INSTALLATION OVERHEAD CONDUCTORS

NOTE: COST FOR FURNISHING AND ERECTING THE ASSEMBLY; FOR ALL FITTINGS, CLAMPS, AND INSULATORS, AND FOR ALL MATERIALS LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK, TO BE INCLUDED IN ITEM SP2-4.1 TEMPORARY TRAFFIC SIGNAL CONSTRUCTION.



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BY	DATE	DESCRIPTION



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

TEMPORARY TRAFFIC SIGNAL WIRING DIAGRAM - COLLEGE AVE.

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DPP
 DRAWN BY: ARM

BAR IS ONE INCH ON ORIGINAL DRAWING
 0 1" = 10' IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.
 DRAWING NUMBER
SG-112
 SHEET NUMBER
120

GENERAL IRRIGATION NOTES

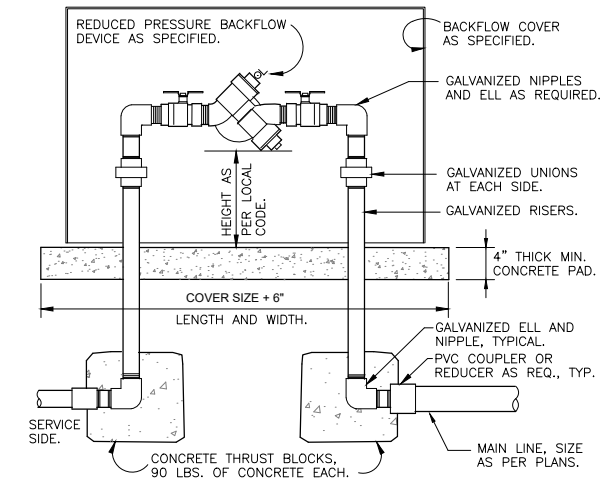
- ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE PROJECT STANDARD SPECIFICATIONS.
- THE CONTRACTOR SHALL PROVIDE SHOP DRAWINGS TO THE LANDSCAPE ARCHITECT AND THE OWNER FOR APPROVAL PRIOR TO COMMENCEMENT OF CONSTRUCTION. THE CONTRACTOR IS TO INSTALL EQUIPMENT NECESSARY TO PROVIDE A COMPLETE AND FUNCTIONAL SYSTEM THAT IS IN COMPLIANCE WITH THE PLANS, SPECIFICATIONS, ALL APPLICABLE CODES, AND REGULATIONS.
- CONTRACTOR SHALL BE RESPONSIBLE TO FIELD STAKE SYSTEM PRIOR TO DIGGING/ TRENCHING WITH FINAL APPROVAL BY LANDSCAPE ARCHITECT OR DESIGNATED REPRESENTATIVE.
- THE IRRIGATION CONTRACTOR SHALL BE RESPONSIBLE FOR DAMAGES TO THE GROUNDS, PAVEMENT, SIDEWALKS, CURBS, ELECTRICAL SYSTEMS, AND OTHER SITE ELEMENTS CAUSED BY LEAKS IN PIPE ALREADY INSTALLED OR PIPE/ OTHER SYSTEM ELEMENTS BEING INSTALLED. REPAIR TO ALL DAMAGE CAUSED SHALL BE MADE AT IRRIGATION CONTRACTOR'S EXPENSE.
- SUBMIT ALL PRODUCT INFORMATION AND SHOP DRAWINGS TO THE LANDSCAPE ARCHITECT PRIOR TO COMMENCEMENT OF CONSTRUCTION.
- UPON COMPLETION OF INSTALLATION AND SYSTEM ACCEPTANCE, CONTRACTOR SHALL PROVIDE THE OWNER WITH A 3-RING BINDER CONTAINING ONE (1) COPY OF AS-BUILT DRAWINGS OF THE IRRIGATION SYSTEM, CUT SHEETS FROM IRRIGATION PRODUCTS USED, AND INFORMATION ON CONTROLLER(S), INCLUDING IRRIGATION SCHEDULE PROGRAMMED INTO THE CONTROLLER.
- SYSTEM SHALL BE DESIGNED TO IRRIGATE SHRUB BEDS AND TURF AS SHOWN ON THE DRAWINGS.
- CONTROLLER SHALL BE RAINBIRD ESP-LXME CONTROLLER, MOUNTED IN A LOCATION OF THE OWNER/ ARCHITECT'S CHOOSING. POWER (120V) SHALL BE LOCATED WITHIN 5-FEET OF CONTROLLER. INSTALL RAIN / FREEZE SENSOR WITH CONTROLLER.
- IRRIGATION CONTRACTOR SHALL USE NETAFIM TECHLINE CV DRIPLINE (TLCV-06-18) FOR SHRUB AREAS, AS SHOWN. RAINBIRD AND TORO DRIP IRRIGATION EQUIPMENT ARE APPROVED EQUALS.
- RAINBIRD, HUNTER AND TORO VALVES AND SPRAY HEADS ARE ALLOWED MANUFACTURERS FOR TURF AREAS.
- RAINBIRD, HUNTER AND WEATHERMATIC VALVES ARE ALLOWABLE. VALVES SHALL BE INSTALLED IN VALVE BOXES SIZED FOR THE VALVE AND ALLOW FOR MANUAL OPERATION, REMOVAL OF SOLENOID AND/ OR VALVE COVER WITHOUT THE NEED FOR EXCAVATION..
- EACH ZONE SHALL HAVE AUTOMATIC DRAIN VALVES INSTALLED.
- IRRIGATION CONTRACTOR WILL VERIFY STATIC PRESSURE AND VOLUME OF IRRIGATION WATER SUPPLY AT LEAST 10 WORKING DAYS PRIOR TO BEGINNING CONSTRUCTION. NOTIFY THE LANDSCAPE ARCHITECT IF STATIC PRESSURE IS LESS THAN 55 P.S.I. NO WORK WILL PROCEED UNTIL DIRECTED BY OWNER.

SLEEVING

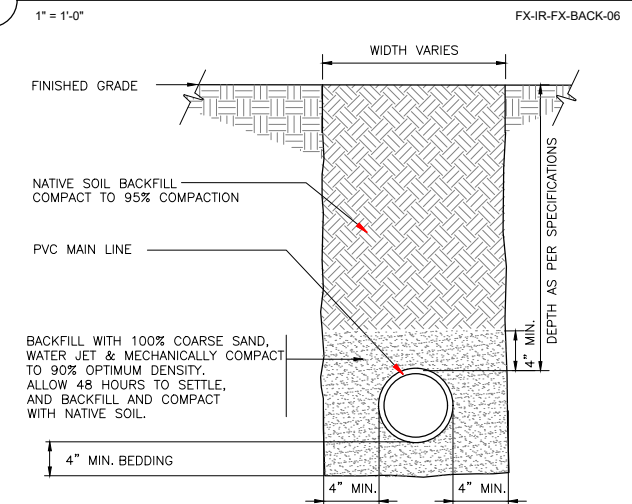
- SLEEVES SHALL BE FURNISHED AND INSTALLED BY THE GENERAL CONTRACTOR.
- SLEEVE MATERIAL SHALL BE SCHEDULE 40 PIPE, AT THE SIZE INDICATED ON THE PLAN SHEETS.
- CONTRACTOR SHALL LAY SLEEVED AND CONDUITS AT 24-INCHES BELOW FINISH GRADE, AS MEASURED FROM THE TOP OF PAVEMENT.
- CONTRACTOR SHALL EXTEND PIPE SLEEVES 1-FOOT BEYOND THE EDGE OF ALL PAVEMENT.
- CONTRACTOR SHALL CAP ALL PIPE ENDS WITH PVC PIPE CAPS. MARK END OF SLEEVES WITH T-POSTS OR VERTICAL PIPES.

LATERALS AND MAINLINE

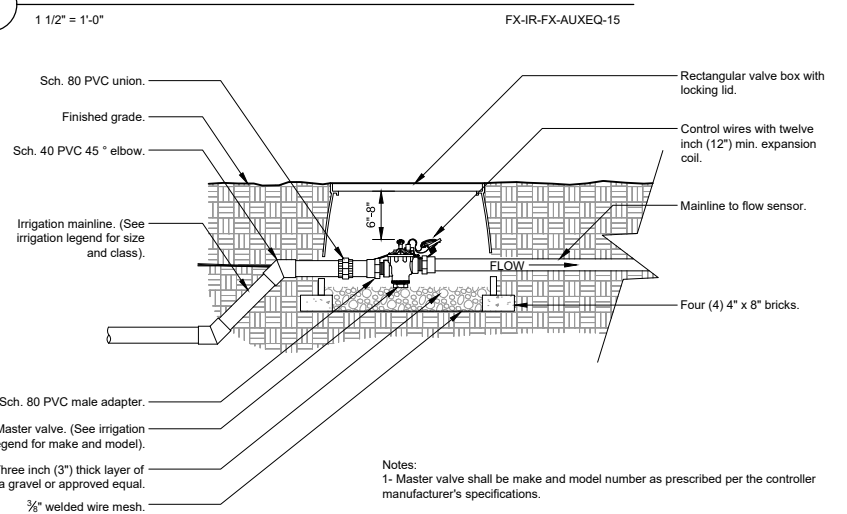
- ALL LATERALS TO BE SIZED AS REQUIRED TO MAINTAIN ADEQUATE PRESSURE BASED UPON STANDARD FLOW/ PIPE SIZING. ALL LATERALS UP TO DRIP LINE ZONE VALVES TO BE SCHEDULE 40 PVC.
- PVC LATERALS SHALL NOT BE BURIED UNTIL PIPE/ VALVE CONNECTIONS AND JOINTS ARE INSPECTED AND APPROVED FOR PROPER INSTALLATION AND USE OF PURPLE-DYED PRIMER AND APPROPRIATE ADHESIVE.
- WHEN INSTALLING IRRIGATION PIPE ALONG CURBS OR SIDEWALKS, PLACE PIPE AS CLOSE TO PAVEMENT AS POSSIBLE TO ALLOW FOR PLANTING OF TREES, SHRUBS, AND GROUNDCOVERS.
- COMBINE MAINLINE, LATERALS, AND CONTROL WIRES INTO SAME TRENCH WHEREVER POSSIBLE.
- CONTRACTOR SHALL INSTALL A TRACER WIRE ON TOP OF IRRIGATION MAINLINE BEFORE BACKFILL OF MAINLINE TRENCH.
- MAINLINE TRENCH SHALL NOT BE BURIED UNTIL JOINTS AND PRESENCE OF TRACER WIRE HAS BEEN INSPECTED AND APPROVED.
- ELECTRICAL CONTROL AND GROUND WIRE TO BE USED FOR CONNECTING REMOTE CONTROL VALVES TO THE AUTOMATIC CONTROLLER SHALL BE 14 AWG MINIMUM.
 - CONTROL WIRE SHALL BE ONE COLOR AND GROUND WIRE SHALL BE ANOTHER COLOR.
 - UNUSED WIRES BURIED IN THE GROUND FOR FUTURE USE SHALL BE A THIRD COLOR.
 - WIRE CONNECTIONS IN THE FIELD SHALL BE MADE USING APPROVED WIRE CONNECTORS UTILIZING A SEALING CEMENT TO INSURE A WATERTIGHT CONNECTION.
 - PROVIDE (2) ADDITIONAL WIRES FOR FUTURE USE IN MAINLINE TRENCH.
 - WIRE SPLICES SHALL ONLY OCCUR AT VALVE BOXES AND CONTROLLER ONLY.



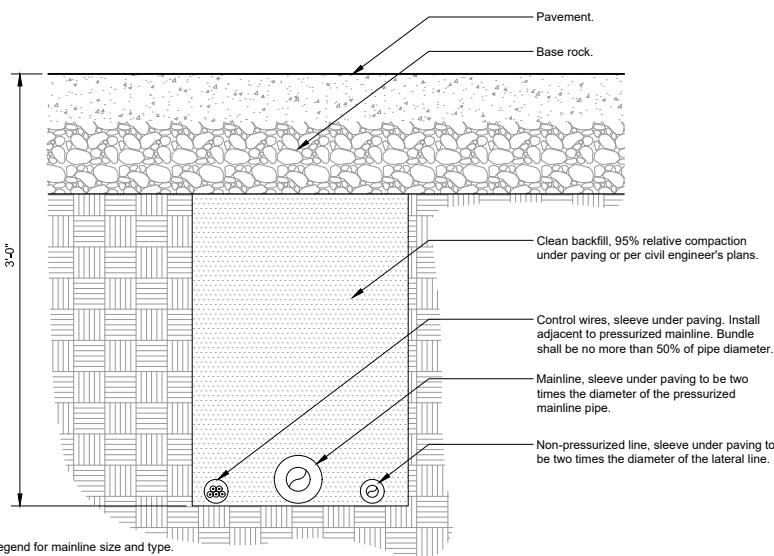
1 RP BACKFLOW W/ENCLOSURE



2 SLEEVE AT ROAD



- Notes:
- Master valve shall be make and model number as prescribed per the controller manufacturer's specifications.
 - Install master valve per manufacturer's specifications and recommendations.
 - Master valve wire shall be 14 AWG or larger. Color: black.
 - All wire runs shall be continuous without any splices. Wire connections shall be made using DBR-Y/6 connectors or approved equal.
 - Wrap valve box with min. 3 mil. thick plastic and secure it to the valve box using duct tape or electrical tape.
 - See master valve/ flow sensor detail for linear dimensions.
 - Valve boxes shall be located in planting areas.

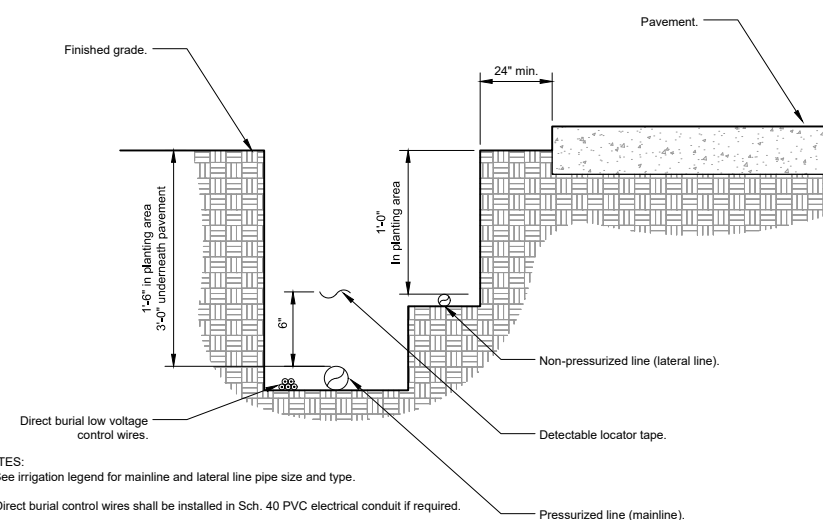


- Notes:
- See irrigation legend for mainline size and type.
 - All sleeves shall be Sch. 40 PVC pipe.
 - All sleeves shall extend 12\"/>

3 PIPE BENEATH PAVEMENT

1 1/2\"/>

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FX-IR-FX-AUXEQ-05



- NOTES:
- See irrigation legend for mainline and lateral line pipe size and type.
 - Direct burial control wires shall be installed in Sch. 40 PVC electrical conduit if required.
 - 2-wire irrigation wire shall be installed in Sch. 40 PVC electrical conduit.
 - Detectable locator tape shall be located six inches (6\"/>

4 IRRIGATION TRENCHING

1 1/2\"/>

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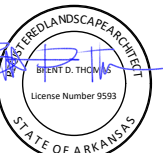
5 MASTER VALVE

1\"/>

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REV.	DATE	DESCRIPTION	BY

CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1

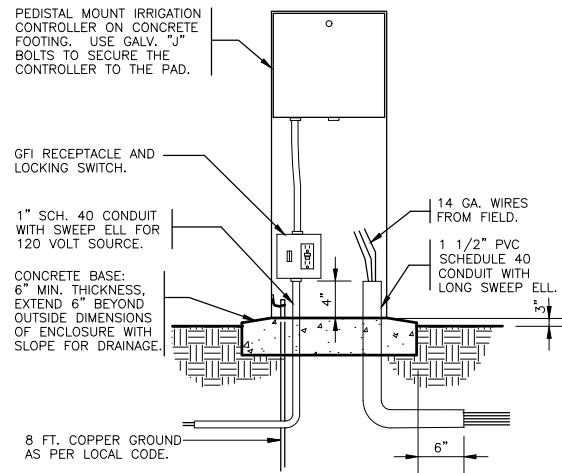
IRRIGATION NOTES AND DETAILS

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: BDT
DRAWN BY: BDT

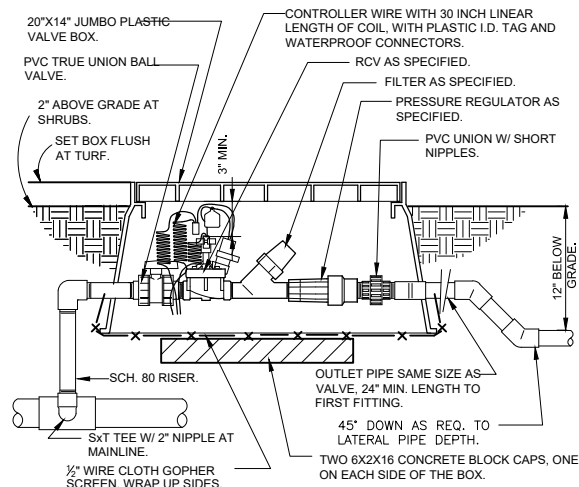
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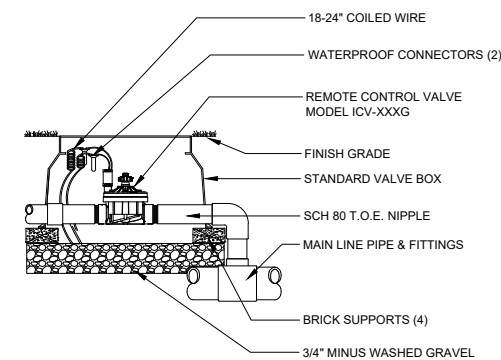
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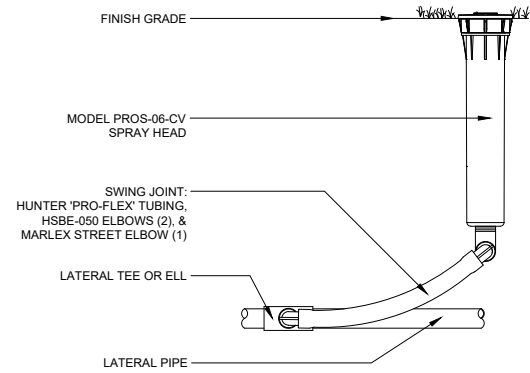
1 PEDESTAL MOUNT CONTROLLER
1" = 1'-0" FX-IR-FX-CONT-09



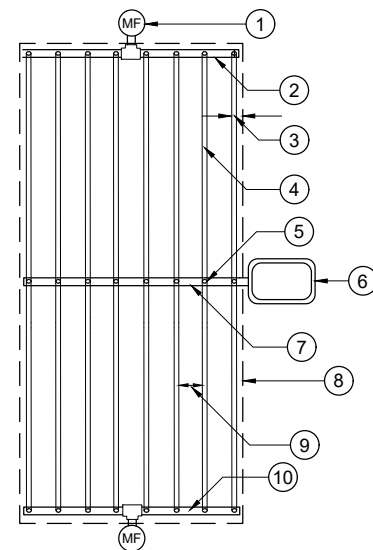
2 1" DRIP VALVE/FILTER/REGULATOR
1 1/2" = 1'-0" FX-IR-FX-DRIP-12



3 ICV GLOBE VALVE
1" = 1'-0" FX-IR-HUNT-VALV-20



4 PROS-06-CV SPRAY HEAD WITH PRO-FLEX TUBING
3" = 1'-0" FX-IR-HUNT-SPRA-40

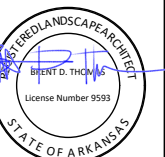


- NOTE:
1. RECOMMENDED MINIMUM FILTRATION: 120 MESH
 2. PRESSURE AT FLUSH VALVE SHALL BE MIN 21.8 PSI
 3. HIGH CHECK VALVE (MAX 8.5' OF WATER (ELEVATION CHANGE)) REFER TO MAXIMUM LENGTH OF A SINGLE LATERAL CHART
- 1) MANUAL FLUSH VALVE PLUMBED TO PVC OR POLY (TYP)
 - 2) PVC OR POLY EXHAUST HEADER
 - 3) PERIMETER LATERALS 2"-4" FROM EDGE
 - 4) TECHLINE HCVXR DRIPLINE
 - 5) TECHLINE START CONNECTION
 - 6) REMOTE CONTROL VALVE WITH DISC FILTER AND PRV
 - 7) PVC OR POLY SUPPLY HEADER
 - 8) AREA PERIMETER
 - 9) SEE SPECIFICATIONS FOR ROW SPACING
 - 10) PVC OR POLY EXHAUST HEADER

5 TECHLINE HCVXR CENTER FEED LAYOUT
Not to scale FX-IR-NETA-DRIP-28



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CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1

IRRIGATION NOTES AND DETAILS

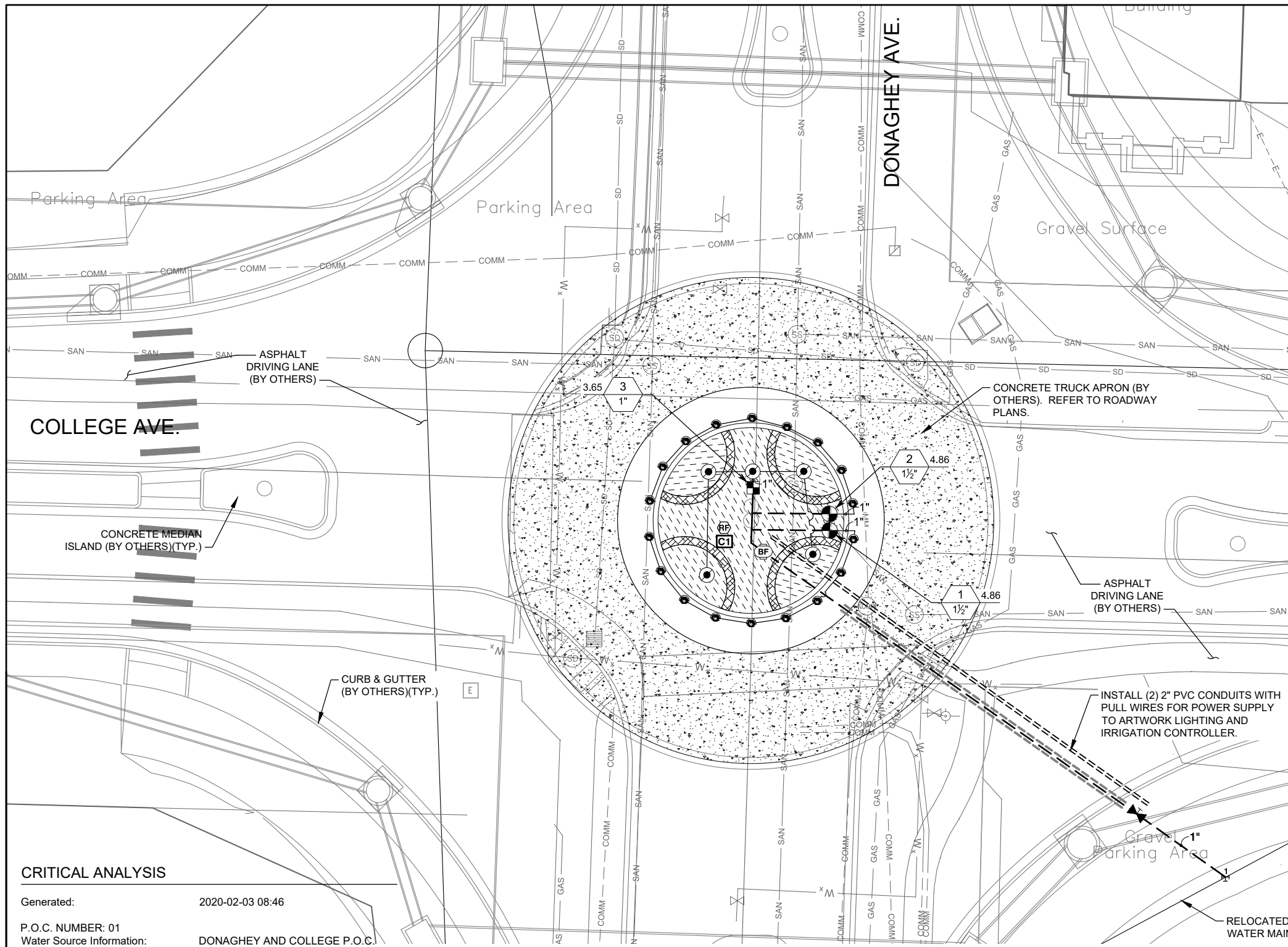
JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: BDT
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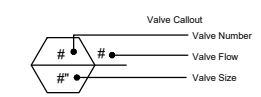
SHEET NUMBER
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IRRIGATION SCHEDULE- COLLEGE AVE. ROUNDABOUT

SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY	PSI
20 2H 40 4H 60 6H	Hunter PROS-06-CV short radius nozzles Turf Spray, 6.0" Pop-Up. With Drain Check Valve. Co-molded wiper seal with UV Resistant Material.	18	40
■	Netafim LVCZS8010075-HF 1" Pre-Assembled Control Zone Kit, with 1" Series 80 Control Valve, 3/4" Disc Filter, and High Flow Pressure Regulator 4.5GPM to 17.6GPM.	1	
●	Pipe Transition Point in Drip Box Pipe transition point from PVC lateral to drip tubing with riser in 6" (150mm) drip box.	5	
[Hatched Box]	Area to Receive Dripline Netafim TLHCVXR-033-12 Techline HCVXR Pressure Compensating Landscape Dripline with Check Valve and Anti-Siphon feature. 0.33 GPH emitters at 12" O.C. Dripline laterals spaced at 12" apart, with emitters offset for triangular pattern. 17mm.	662.8 l.f.	
SYMBOL	MANUFACTURER/MODEL/DESCRIPTION	QTY	
⊕	Hunter ICV-G 1-1/2" 1", 1-1/2", 2", and 3" Plastic Electric Remote Control Valves, Globe Configuration, with NPT Threaded Inlet/Outlet, for Commercial/Municipal Use.	2	
⊘	Shut Off Valve	1	
⊕	Zurn 975XL 2" Reduced Pressure Backflow device	1	
C1	Rain Bird ESP8LXMEF-LXMM-LXMMPED 8 Station Capable Commercial Controller. Mounted on a Powder-Coated Metal Pedestal. Flow Sensing and Water Management Capabilities. Flow sensing.	1	
RF	Rain Bird WR2-RFC Wireless Rain and Freeze Sensor Combo, includes 1 receiver and 1 rain/freeze sensor transmitter.	1	
⊕	Point of Connection 1" DONAGHEY AND COLLEGE P.O.C.	1	
—	Irrigation Lateral Line: PVC Schedule 40 1"	154.9 l.f.	
---	Irrigation Mainline: PVC Schedule 40 1"	133.4 l.f.	
----	Pipe Sleeve: PVC Schedule 40	57.9 l.f.	



CRITICAL ANALYSIS

Generated: 2020-02-03 08:46

P.O.C. NUMBER: 01
 Water Source Information: DONAGHEY AND COLLEGE P.O.C.

FLOW AVAILABLE
 Point of Connection Size: 1"
 Flow Available: 20.24 gpm

PRESSURE AVAILABLE
 Static Pressure at POC: 70.00 psi
 Pressure Available: 70.00 psi

DESIGN ANALYSIS
 Maximum Station Flow: 4.86 gpm
 Flow Available at POC: 20.24 gpm
 Residual Flow Available: 15.38 gpm

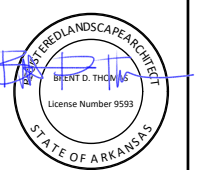
Pressure Req. at Critical Station: 47.22 psi
 Loss for Fittings: 0.04 psi
 Loss for Main Line: 0.43 psi
 Loss for POC to Valve Elevation: 0.00 psi
 Loss for Backflow: 13.50 psi
 Critical Station Pressure at POC: 61.19 psi
 Pressure Available: 70.00 psi
 Residual Pressure Available: 8.81 psi

VALVE SCHEDULE

NUMBER	MODEL	SIZE	TYPE	GPM	HEADS	WIRE	DESIGN PSI	PSI	PSI @ POC	PRECIP
1	Hunter ICV-G	1-1/2"	Turf Spray	4.86	9	19.9	40	41.66	56.01	1.28 in/h
2	Hunter ICV-G	1-1/2"	Turf Spray	4.86	9	22.6	40	41.66	56.03	1.29 in/h
3	Netafim LVCZS8010075-HF Common Wire	1"	Area for Dripline	3.65	662.8 l.f.	13.8	40	47.22	61.19	0.53 in/h



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**DONAGHEY AVE. IMPROVEMENTS
 PHASE 1**

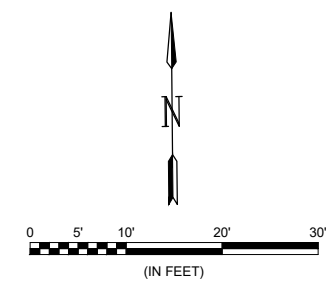
IRRIGATION PLAN

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: BDT
 DRAWN BY: BDT

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SHEET NUMBER
123



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GENERAL PLANTING NOTES

1. PRIOR TO BEGINNING ANY WORK ON THE SITE, THE CONTRACTOR SHALL CONTACT THE LANDSCAPE ARCHITECT TO ADDRESS ANY QUESTIONS.
2. LANDSCAPE CONTRACTOR SHALL PROVIDE LABOR, MATERIALS AND SERVICE NECESSARY TO FURNISH AND INSTALL MATERIALS AS SPECIFIED HEREIN AND SHOWN ON THE PLANS.
3. ALL PLANT MATERIAL SELECTIONS WILL BE REVIEWED AND APPROVED BY OWNERS' REPRESENTATIVES PRIOR TO INSTALLATION.
4. NO MATERIAL SUBSTITUTIONS SHALL BE MADE WITHOUT LANDSCAPE ARCHITECT'S WRITTEN APPROVAL. ALTERNATE MATERIALS OF SIMILAR SIZE AND CHARACTER MAY BE CONSIDERED IF SPECIFIED PLANT MATERIALS CANNOT BE OBTAINED. LANDSCAPE ARCHITECT RESERVES THE RIGHT TO REVISE THE PLANT LIST AS DEEMED NECESSARY.
5. QUANTITIES OF PLANT MATERIALS SHOWN ON THE PLAN TAKE PRECEDENCE OVER THE QUANTITIES SHOWN ON THE PLANT SCHEDULE. LANDSCAPE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL QUANTITIES ON THE PLANTING PLAN.
6. REPORT DISCREPANCIES IN THE PLANTING PLAN TO THE LANDSCAPE ARCHITECT PRIOR TO PURCHASING MATERIALS OR COMMENCING CONSTRUCTION.
7. REVIEW THE LANDSCAPE SPECIFICATION SECTION LOCATED IN THE PROJECT MANUAL FOR ADDITIONAL PROJECT RESPONSIBILITIES AND INSTRUCTIONS.

SITE PREPARATION AND EARTHWORK

8. TOPSOIL HAULED TO THE SITE SHALL BE FERTILE, FRIABLE, NATURAL LOAM SOIL OF UNIFORM QUALITY CHARACTERISTIC OF REPRESENTATIVE LOCAL SOILS WHICH PRODUCE HEAVY GROWTH OF CROP GRASSES, OR OTHER VEGETATION. SOIL SHALL BE FREE OF SUBSOIL, CLAY LUMPS, BRUSH WEEDS ROOTS, STONES, TRASH, OR ANY OTHER DELETERIOUS MATERIALS.
9. TOPSOIL SHALL BE DELIVERED IN AN UNFROZEN AND NON-MUDDY CONDITION AND SHALL BE SUBJECT TO APPROVAL OF THE LANDSCAPE ARCHITECT. SOLUBLE SALTS SHALL NOT EXCEED 500 ppm AND ORGANIC MATTER SHALL BE NO LESS THAN 1.5% BY WEIGHT. pH SHALL RANGE BETWEEN 6.0 AND 7.5.
10. LANDSCAPE CONTRACTOR SHALL HAVE TOPSOIL TESTED BY A CERTIFIED TESTING LABORATORY AND OBTAIN RECOMMENDATIONS FOR SOIL AMENDMENT TYPE(S) AND QUANTITIES. SUBMIT A COPY OF THIS REPORT TO THE LANDSCAPE ARCHITECT FOR THEIR RECORDS. RECOMMENDATIONS SHALL BE SPECIFIC TO THE TOPSOIL USED AND THE PLANT MATERIALS SPECIFIED IN THE PLANS. A SAMPLE OF THE TOPSOIL TO BE USED SHALL BE SUBMITTED TO THE LANDSCAPE ARCHITECT FOR APPROVAL PRIOR TO INSTALLATION.

PLANTING PREPARATION

11. CULTIVATE PLANTING BEDS TO A DEPTH OF 12". TILL ORGANIC SOIL AMENDMENTS

INTO THE PLANTING BEDS AT THE RATIOS SPECIFIED.

12. BACKFILL: FOR PLANT EXCAVATIONS, BACKFILL SHALL BE CLEAN, NATURAL TOPSOIL, MIXED WITH AMENDMENTS AT THE RATIOS SPECIFIED.

PLANTING MATERIALS

13. PLANT MATERIALS SHALL BE WELL FORMED AND DEVELOPED IN GOOD CONDITIONS, HEALTHY AND DISEASE-FREE, AND BE TYPICAL OF THE SPECIES. PLANTS SHALL COMPLY IN ALL APPLICABLE RESPECTS WITH ACCEPTABLE STANDARDS AS SET FORTH IN THE AMERICAN ASSOCIATION OF NURSERYMAN'S "AMERICAN STANDARD OF NURSERY STOCK". HEIGHT OF PLANT MATERIALS SHALL BE MEASURED FROM EXISTING SOIL LINE AT TOP OF ROOTBALL TO TOP OF CROWN.
14. PLANT MATERIALS SHALL BE PROTECTED BY THE DRYING ACTION OF THE SUN AND WIND AFTER BEING DUG, WHILE BEING TRANSPORTED, AND WHILE AWAITING PLANTING. BALLS OF PLANTS WHICH CANNOT BE PLANTED IMMEDIATELY SHALL BE PROTECTED FROM DRYING ACTION BY COVERING THEM WITH MOIST MULCH. PERIODICALLY, APPLY WATER TO MULCH-COVERED ROOT BALLS TO KEEP MOIST. IF PLANTING SHOULD OCCUR DURING GROWING SEASON, APPLY ANTI-DESSICANT BEFORE TRANSPORT TO REDUCE THE LIKELIHOOD OF WIND BURN. REAPPLY ANTI-DESSICANT AFTER PLANTING TO REDUCE TRANSPIRATION.
15. PLANTS DESIGNATED "CONTAINER GROWN" SHALL HAVE BEEN GROWN IN POTS, CANS OR BOXES FOR A MINIMUM OF SIX MONTHS AND A MAXIMUM OF TWO YEARS. THESE PLANTS SHALL BE REMOVED FROM CONTAINERS BEFORE PLANTING. PLANTS THAT APPEAR ROOT-BOUND SHALL BE REJECTED.
16. PLANT LOCATIONS ARE APPROXIMATE. ADJUST AS NECESSARY TO AVOID CONFLICTS.
17. USE TRIANGULAR SPACING ON ALL NATIVE FORBES, PERENNIALS, AND ANNUALS.
18. PLANT SUBSTITUTIONS WILL ONLY BE ALLOWED UNDER THE FOLLOWING CIRCUMSTANCES: LANDSCAPE CONTRACTOR SHALL SUBMIT A WRITTEN SUBSTITUTION REQUEST TO THE LANDSCAPE ARCHITECT STATING WHAT PLANTS TO BE SUBSTITUTED AND THE REQUESTED SUBSTITUTION PLANT ALONG WITH EXPLANATION OF SUBSTITUTION REQUEST. NO SUBSTITUTION SHALL CONSTITUTE AN INCREASE IN THE COST FROM THE ORIGINAL CONTRACT AMOUNT. ANY PLANT SUBSTITUTIONS MADE WITHOUT APPROVAL SHALL BE REPLACED BY THE LANDSCAPE CONTRACTOR AT THE TIME OF INSPECTION AT NO COST TO THE PROJECT.
19. PLACE 3" OF PINE STRAW MULCH IN ALL PLANTING BEDS SHOWN ON THE PLAN.
20. PLANTING BEDS RECEIVING MULCH SHALL BE FREE OF WEEDS, GRASS AND DEBRIS. TREAT BEDS WITH A PRE-EMERGENT WITH TREFLAN, SUCH AS PREEN, PRIOR TO PLANTING AND MULCH PLACEMENT. A SECOND APPLICATION SHOULD BE APPLIED IF WEEDS EMERGE PRIOR TO COMPLETION OF WORK. APPLY IN ACCORDANCE WITH STANDARD TRADE PRACTICE AND MANUFACTURER'S PRODUCT LABELING.

MAINTENANCE AND CLEAN-UP

21. PROVIDE TEMPORARY WATER FOR PLANT MATERIALS DURING THE ESTABLISHMENT PERIOD.
22. REMOVE ALL RUBBISH, EQUIPMENT, AND MATERIAL AND LEAVE THE AREA IN A NEAT, CLEAN CONDITION EACH DAY. MAINTAIN PAVED AREAS UTILIZED FOR HAULING EQUIPMENT AND MATERIALS BY OTHER TRADES IN A CLEAN AND UNOBSTRUCTED CONDITION AT ALL TIMES.
23. REMOVE SOIL OR DIRT THAT HAS ACCUMULATED ON PAVED SURFACES DURING OR AS A RESULT OF PLANTING OPERATIONS EACH DAY.
24. FERTILIZE ALL PLANTS WITH A 10-20-10 COMMERCIAL, SLOW-RELEASE FERTILIZER AS DIRECTED BY INSTRUCTIONS ON PRODUCT LABEL.
25. LIMIT AMOUNT OF PRUNING TO A MINIMUM NECESSARY TO REMOVE DEAD OR INJURED TWIGS AND BRANCHES. PRUNE IN SUCH A MANNER AS NOT TO CHANGE NATURAL HABIT OR SHAPE OF PLANT. MAKE CUTS FLUSH, LEAVING NO STUBS. CENTRAL LEADERS SHALL NOT BE REMOVED.
26. LANDSCAPE CONTRACTOR TO REMOVE TREE STAKES, GUYS, AND ALL DEAD WOOD ON TREES AND SHRUBS ONE YEAR AFTER PROVISIONAL ACCEPTANCE.
27. CONTRACTOR IS RESPONSIBLE TO MOW ALL SEEDED AND SODDED AREAS A MINIMUM OF TWO (2) TIMES AT A HEIGHT OF NO MORE THAN 1-1/2" PRIOR TO TURNING OVER TO THE CITY.

INCIDENTAL IRRIGATION

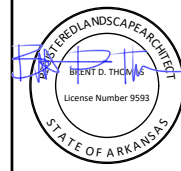
1. CERTAIN PROPERTIES IN THE CORRIDOR HAVE AUTOMATIC IRRIGATION SYSTEMS. CONTRACTOR SHALL PROPERLY CAP AND/ OR TEMPORARILY SHUT OFF IRRIGATION IN AREAS IMPACTED BY CONSTRUCTION. UPON COMPLETION OF CONSTRUCTION ACTIVITIES, CONTRACTOR SHALL RETURN SPRINKLER SYSTEMS TO WORKING ORDER, ENSURING THAT ADJUSTED VALVE BOXES, SPRINKLERS AND OTHER EQUIPMENT IS FLUSH WITH NEW FINISH GRADES AND SPRINKLERS PROVIDE ADEQUATE COVERAGE OF THE PLANT MATERIALS THEY ARE WATERING.

TURF RESTORATION

1. REFER TO TYPICAL ROADWAY CROSS SECTIONS FOR SOD AND SEED LIMITS THROUGHOUT CORRIDOR.



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CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1

LANDSCAPE NOTES

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: BDT
DRAWN BY: BDT

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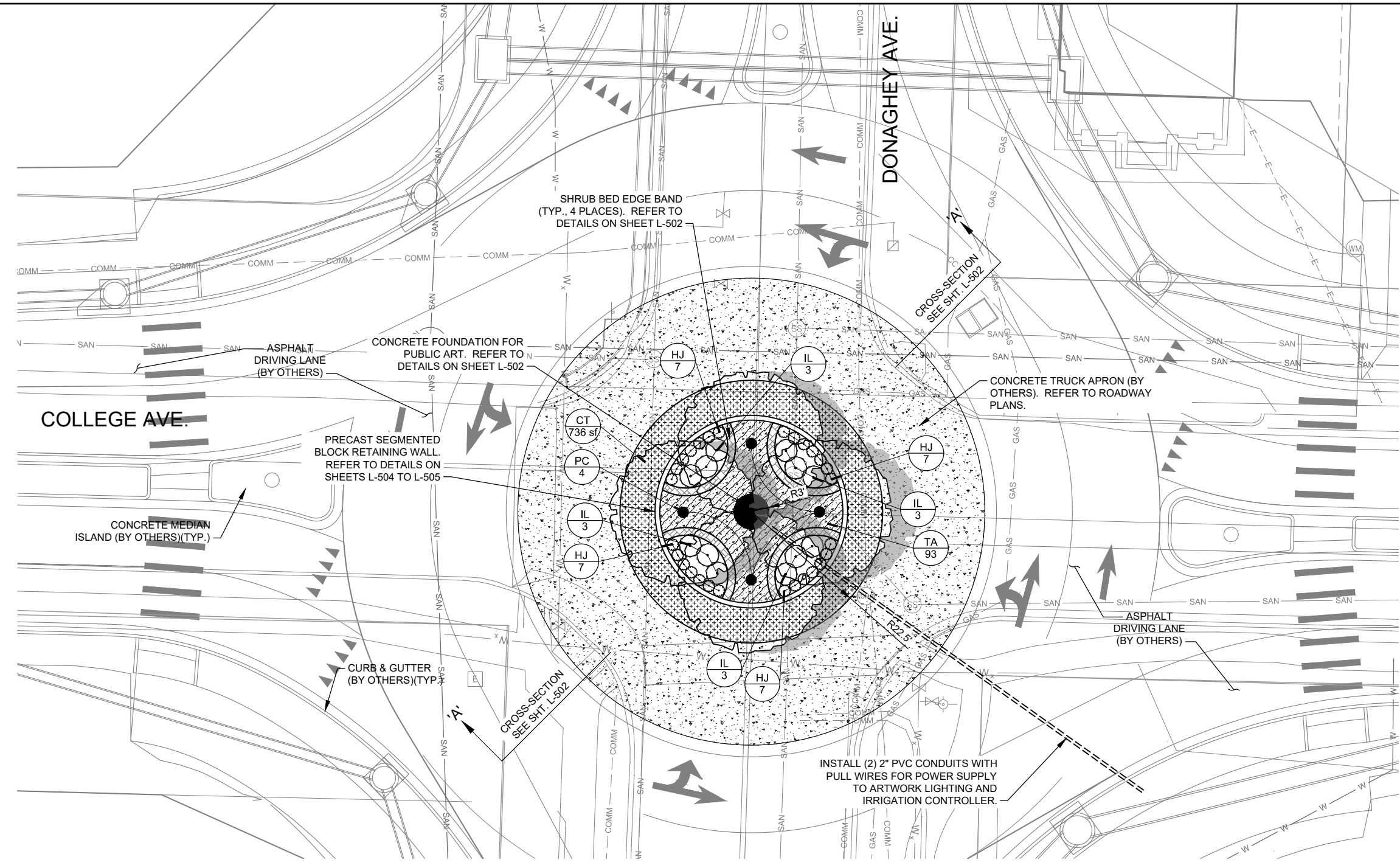
LANDSCAPE PLAN

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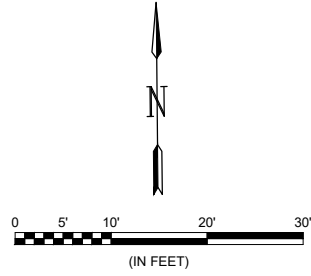
PLANT SCHEDULE- COLLEGE AVE. ROUNDABOUT

TREES	QTY	BOTANICAL NAME	COMMON NAME	CONT	CAL	
PC	4	<i>Pistacia chinensis</i>	Chinese Pistache	B & B	3"Cal	

SHRUBS	QTY	BOTANICAL NAME	COMMON NAME	SIZE	FIELD2	SPACING
HJ	28	<i>Hypericum kalmianum</i>	Kalm St. Johnswort	3 gal		24" o.c.
IL	12	<i>Itea virginica</i> 'Little Henry' TM	Little Henry Sweetspire	5 gal		48" o.c.

GROUND COVERS	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	FIELD2	SPACING
	TA	93	<i>Trachelospermum asiaticum</i>	Asian Jasmine	1 gal		24" o.c.

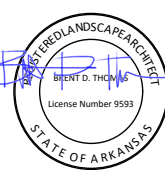
TURF	CODE	QTY	BOTANICAL NAME	COMMON NAME	CONT	FIELD2	SPACING
	CT	736 sf	<i>Cynodon dactylon</i> 'Tif 419'	Tif 419 Bermuda Grass	sod		



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 \$\$\$WORKSPACES\$\$\$
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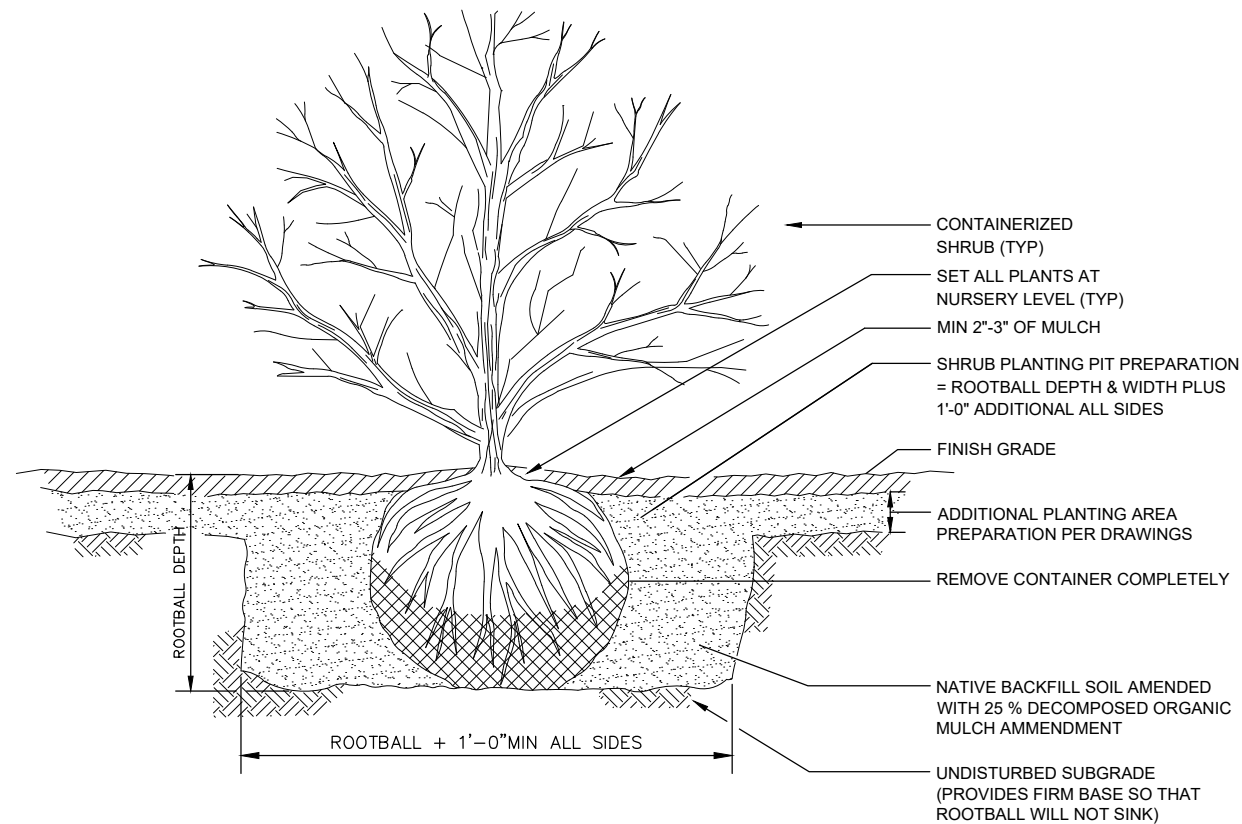
LANDSCAPE DETAILS

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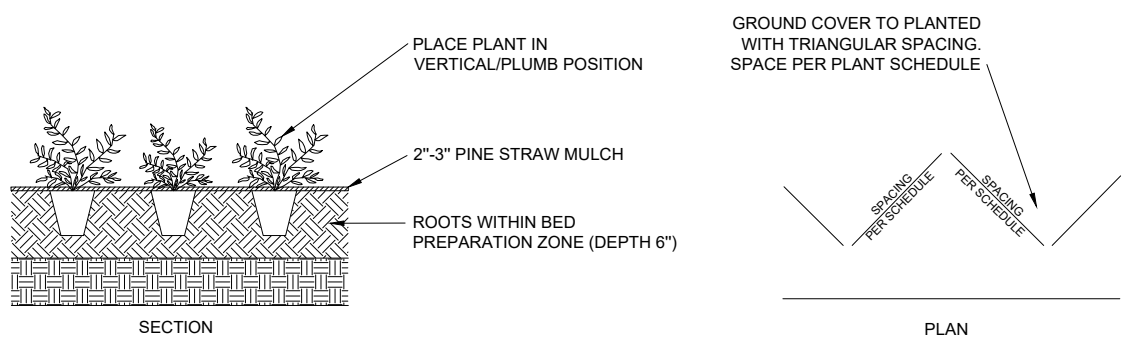
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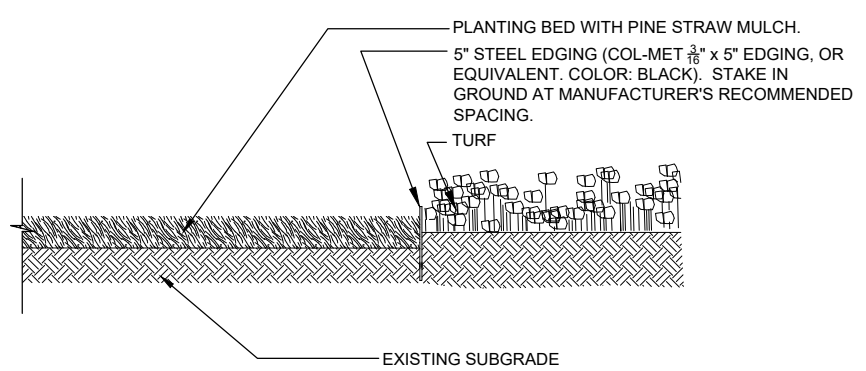
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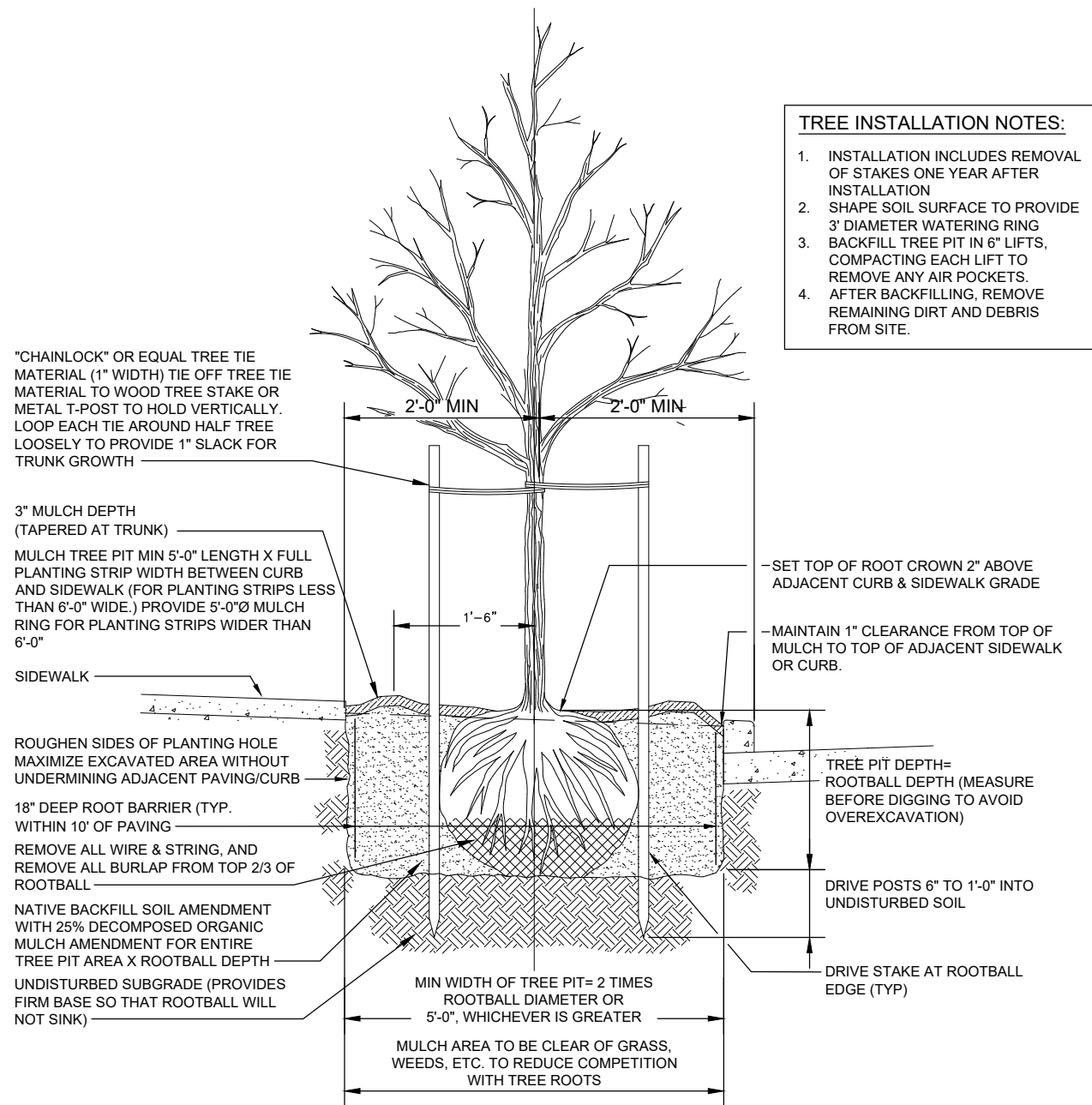
1 SHRUB PLANTING DETAIL
SCALE: NONE



3 GROUND COVER PLANTING DETAIL
SCALE: NONE



4 LANDSCAPE EDGING
SCALE: NONE

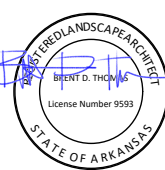


2 DECIDUOUS TREE PLANTING DETAIL
SCALE: NONE

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127

PRECAST CONCRETE BLOCK WALL NOTES

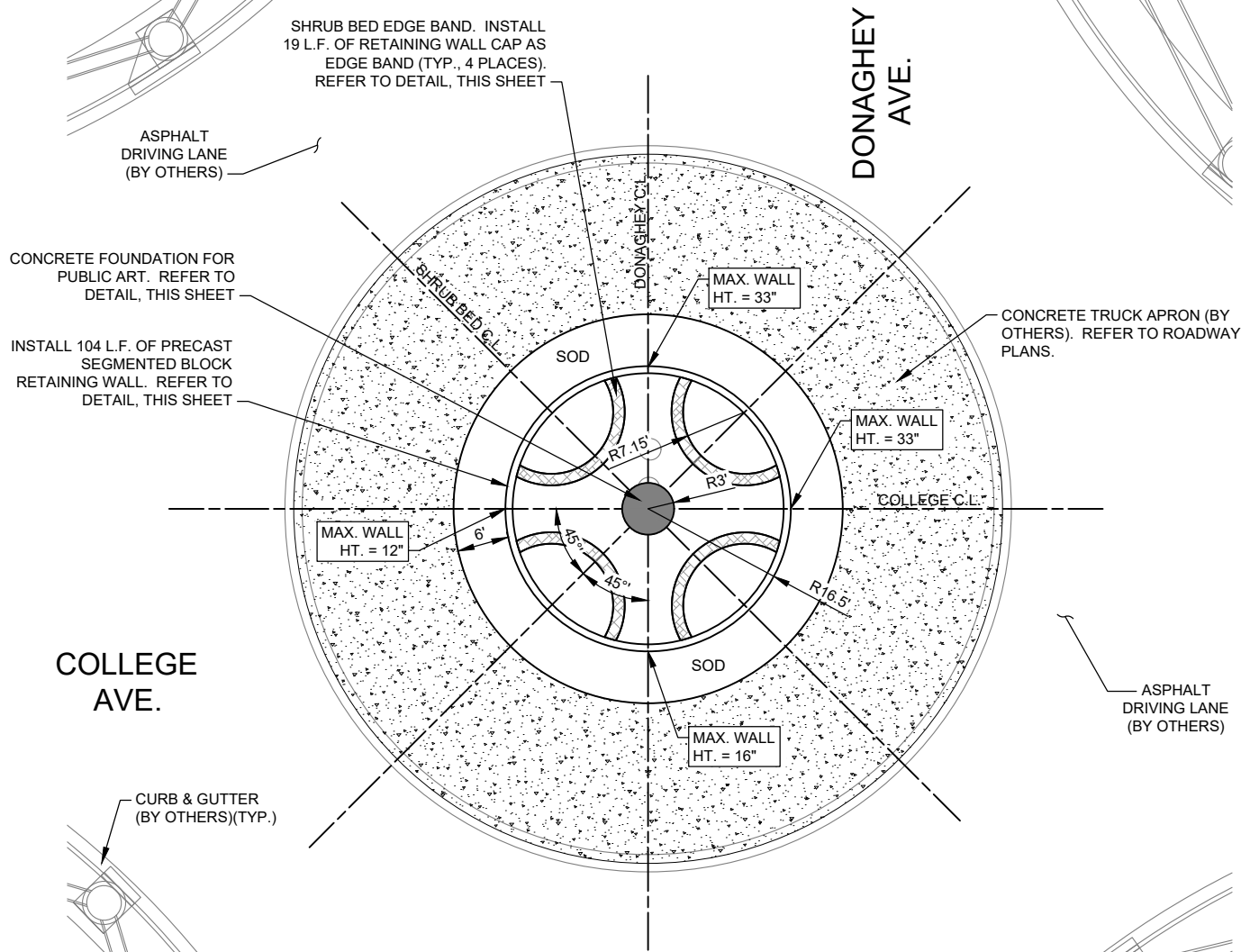
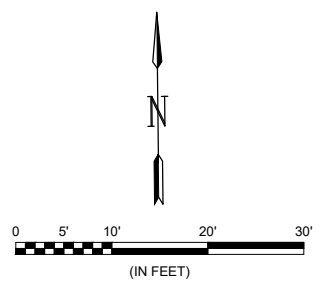
1. BASE COURSE OF WALL SHALL BE LEVEL. STEP DOWN BASE COURSE WHERE GRADES DICTATE, FOLLOWING MANUFACTURER'S RECOMMENDATIONS FOR DOING SO.
2. PROVIDE A MINIMUM OF 1 FULL COURSE OF BLOCK ABOVE FINISH GRADE OF CONCRETE APRON.
3. MAXIMUM WALL HEIGHT ALLOWED ABOVE NEARBY CONCRETE APRON IS INDICATED ON DETAIL 1 ON THIS SHEET. ALLOWABLE HEIGHTS INDICATED ARE BASED ON SIGHT DISTANCE LINES FOR EACH APPROACH OF THE ROUNDABOUT.
4. FOLLOW MANUFACTURER'S RECOMMENDATIONS FOR WALL SYSTEM INSTALLATION, AS FOUND ON DETAIL SHEETS L-504 AND L-505.

BLOCK WALL MATERIALS

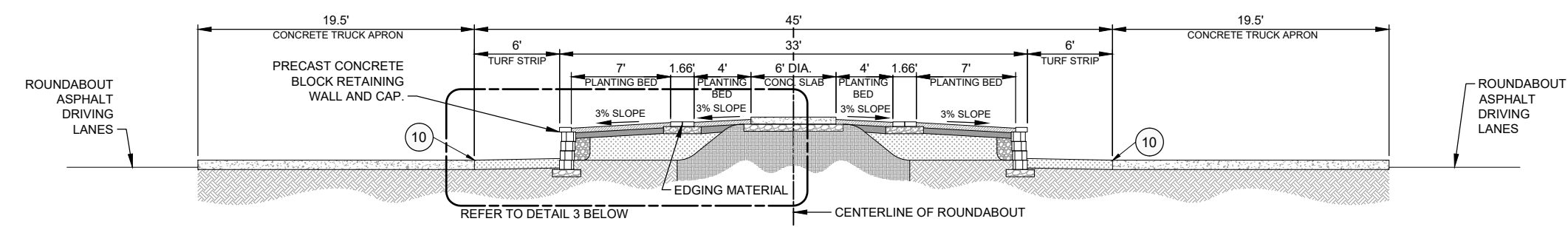
BLOCKS:
 ROCKWOOD CLASSIC 8 BEVELED RETAINING WALL BLOCK
 COLOR: AUTUMN BLEND

CAPS:
 ROCKWOOD CLASSIC 8 UNIVERSAL CAP
 COLOR: AUTUMN BLEND
 ADHERE CAP STONES TO BLOCK WALL USING CONSTRUCTION ADHESIVE.

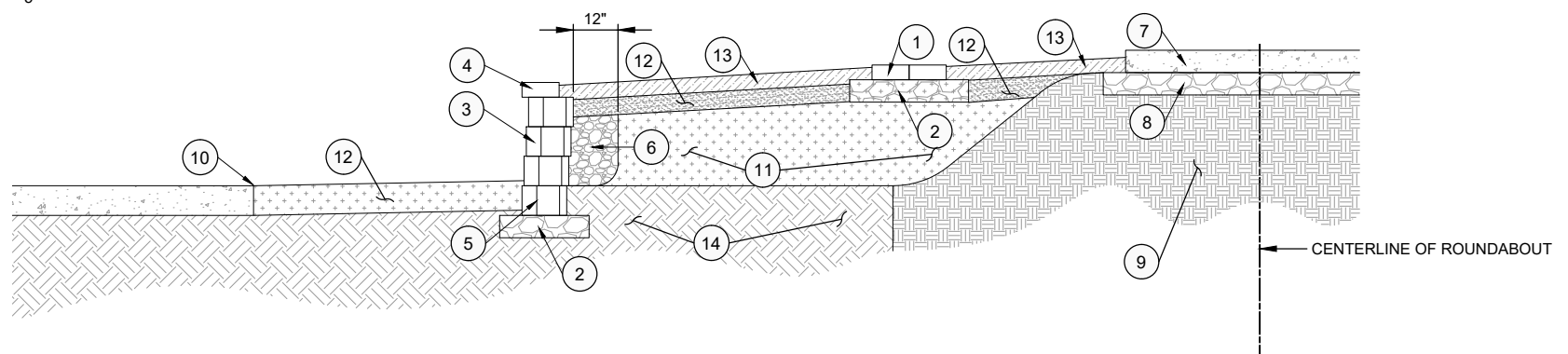
ALTERNATIVE MATERIALS SHALL BE SUBMITTED TO THE LANDSCAPE ARCHITECT FOR APPROVAL.



1 COLLEGE AVE. ROUNDABOUT LANDSCAPE LAYOUT
 SCALE: 1" = 10'



2 COLLEGE AVE. ROUNDABOUT LANDSCAPE CROSS SECTION (SECTION 'A')
 SCALE: 1" = 5'

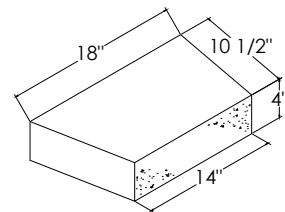


3 COLLEGE AVE. ROUNDABOUT LANDSCAPE SECTION DETAIL
 SCALE: NONE

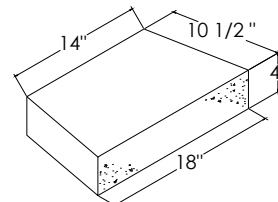
\$\$\$DATESS \$\$\$\$TIMESS
 \$\$\$ORSPACE\$\$\$WORKSPACES
 \$\$\$FILESS \$\$\$REVISED DATE:
 \$\$\$

Typical Details Classic 8

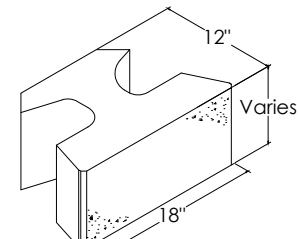
(Dimensions may vary by region)



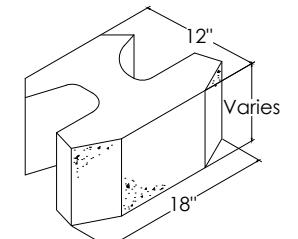
Universal Cap - back



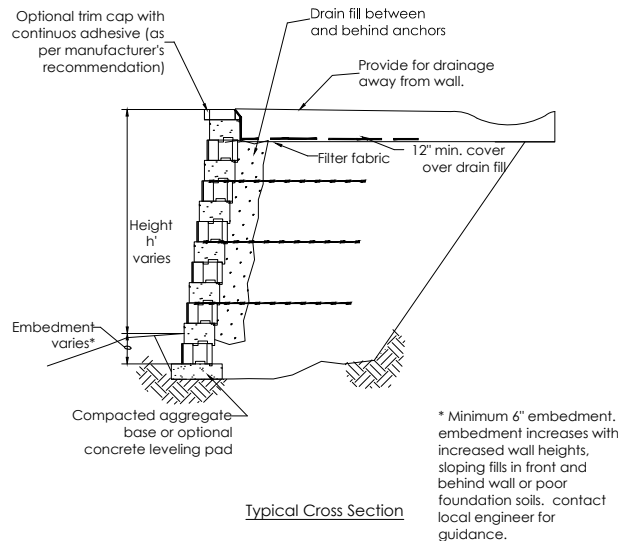
Universal Cap - front



Classic 8 unit
(Straight)

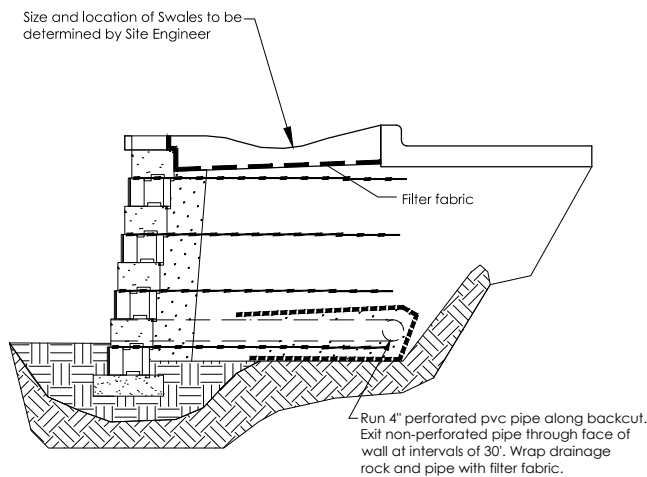


Classic 8 unit
(Beveled)

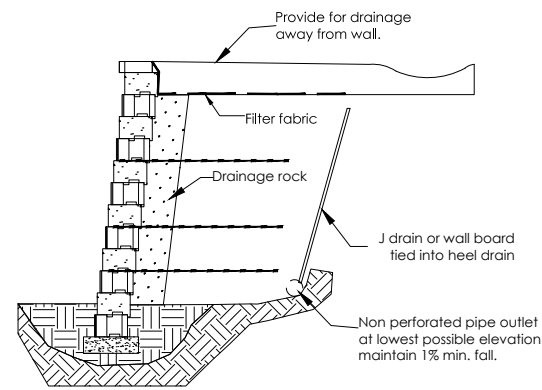


Typical Cross Section

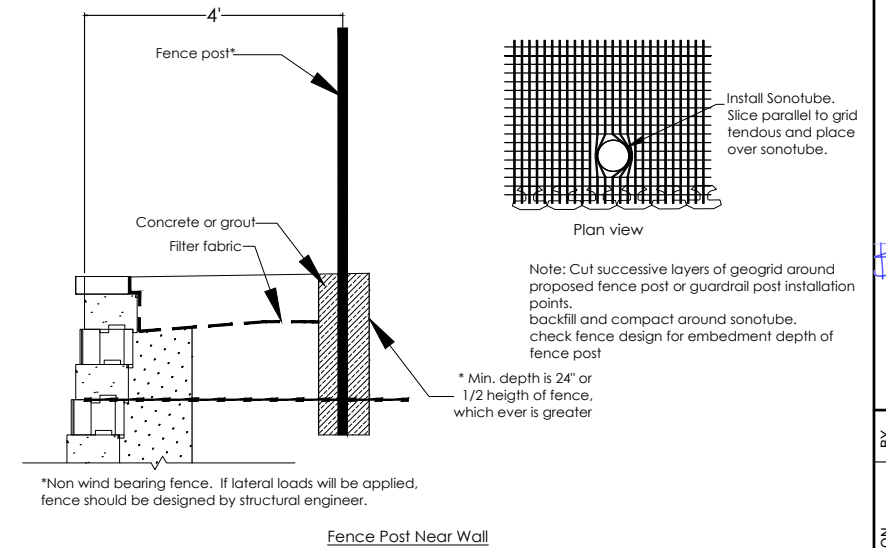
* Minimum 6" embedment. embedment increases with increased wall heights, sloping fills in front and behind wall or poor foundation soils. contact local engineer for guidance.



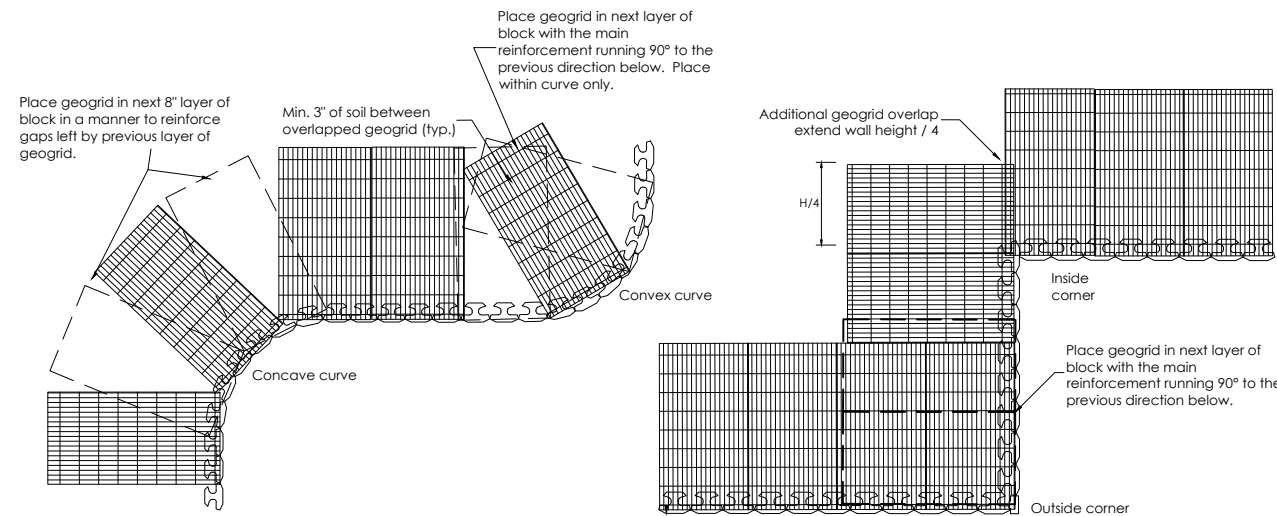
Typical Heel Drain Option
(Seepage / potential seepage)



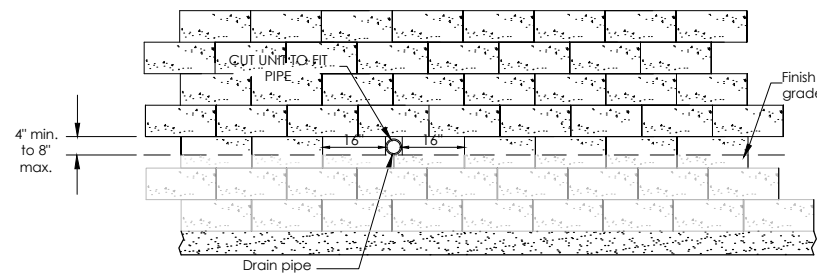
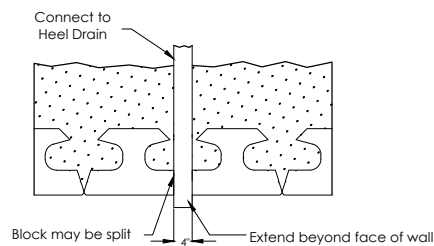
Typical J Drain/Wall Board Option
(Seepage / potential seepage)



Fence Post Near Wall

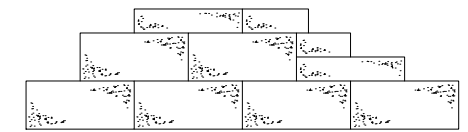


Geogrid Placement

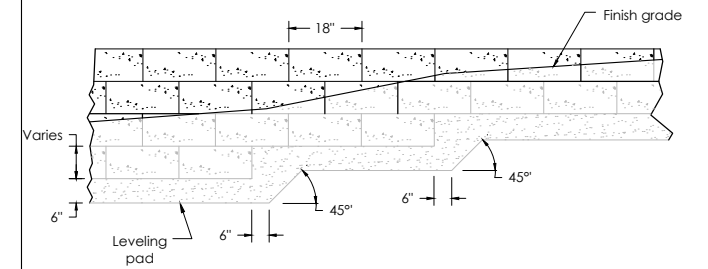


Note: Locate drain tile at lowest elevation above adjacent finish grade.

Drain tile outlet



Universal cap step detail

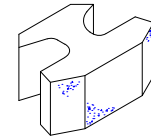


Leveling pad step detail

This drawing is furnished for preliminary design purposes only, and should not be used for final design drawings or construction drawings without the certification of a professional engineer registered in the state in which the wall will be constructed

No.	Date.	Revision.	By.

Prepared by:



ROCKWOOD
RETAINING WALLS
A better way.

Legend

	Compacted Fill		Crushed Rock
	Native Soil		Leveling Pad
	Geogrid		

Project Name:	XX	Prepared For:	CCC
Project Location:	YY	Drawn By:	DDD
Drawing Description:	Classic 8 Typical Details		
Project Number:	WWWPRO#		

Job No.:	18147010
Date:	MAY 2020
Designed By:	BDT
Drawn By:	BDT
Sheet # of \$	
Date:	MM/DD/YYYY
Project Number:	WWWPRO#



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REV.	DATE	DESCRIPTION	BY

CITY OF CONWAY
CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
PHASE 1

LANDSCAPE DETAILS

JOB NO.:	18147010
DATE:	MAY 2020
DESIGNED BY:	BDT
DRAWN BY:	BDT
BAR IS ONE INCH ON ORIGINAL DRAWING	
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DRAWING NUMBER	L-503
SHEET NUMBER	128

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 \$\$\$ORFESS\$ \$ORFACES\$ \$NORFACES\$
 \$\$\$RFILES\$ \$RFILES\$ \$RFILES\$
 \$\$\$REVISED DATE:

Wall Construction Sequence with Classic™

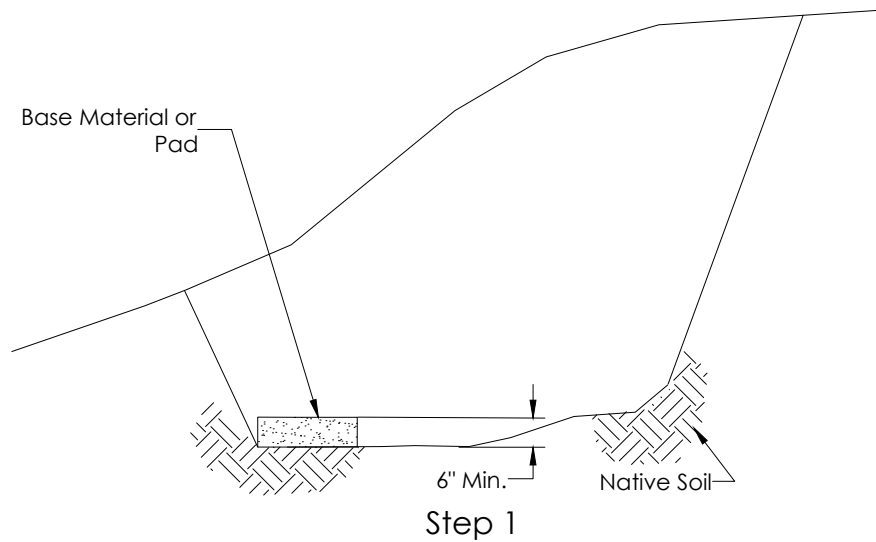


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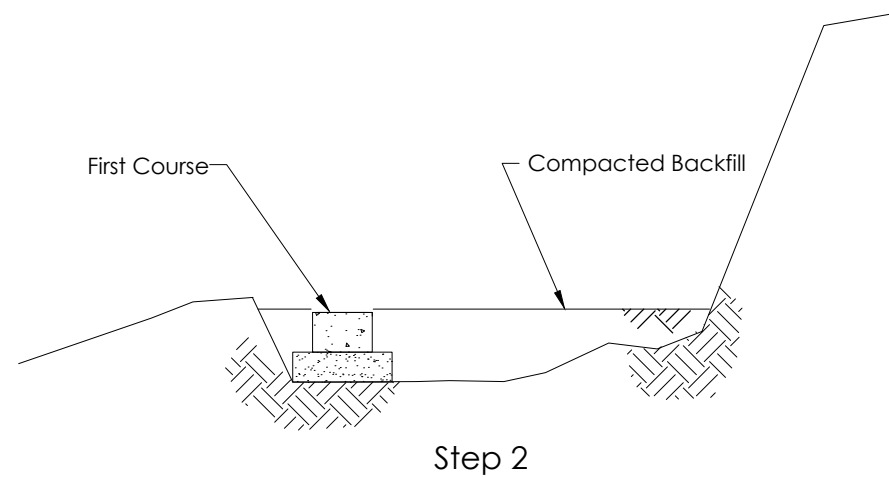


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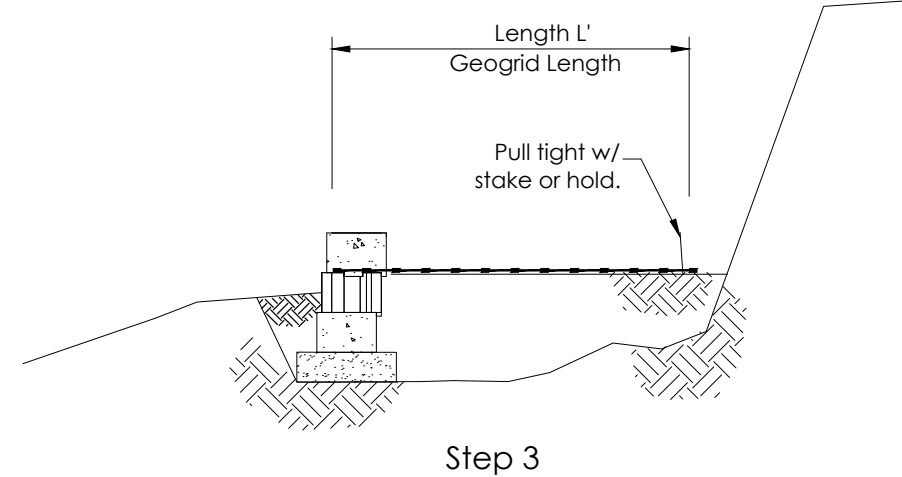
- Excavate trench for a level base, remove all organic and unsuitable soils and compact.
- Install compacted aggregate base material or concrete leveling pad.
- Check levelness of base material or leveling pad.



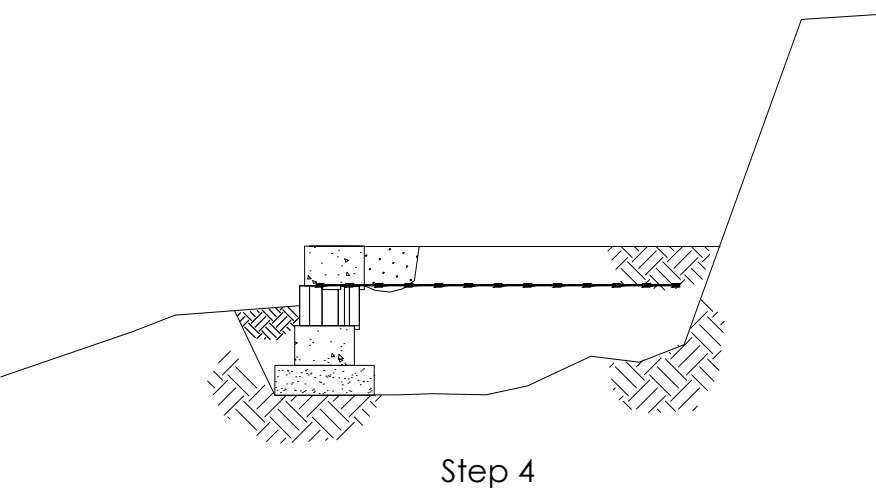
- Check all line grade and curves.
- Install first course insuring all blocks are level; both side to side and front to back.
- Align back side of block to insure a straight installation.
- Set units side by side, so they are touching.
- Place and compact backfill behind and in front of the first course.
- Sweep top of block.
- Re-check levelness.



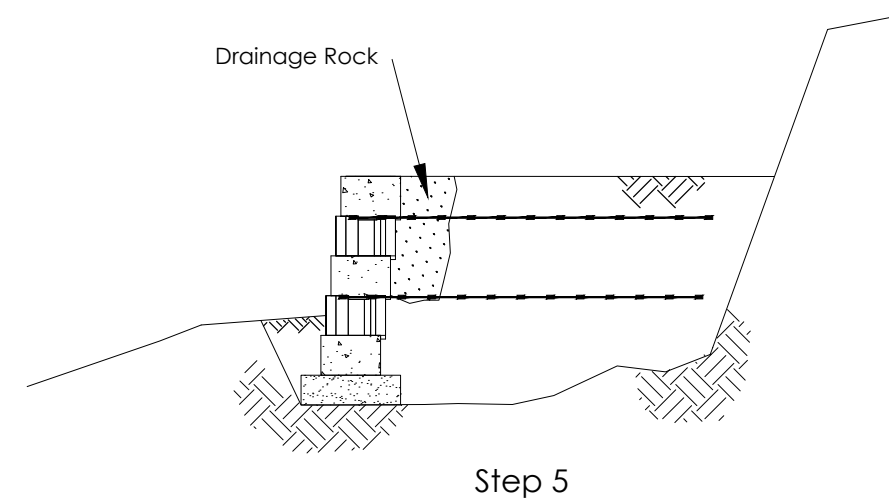
- Install next course (anchor bar down) by offsetting the center of block over the seams of previous course.
- Continue placing courses until geogrid placement is required.
- place and compact 8" max. lifts.
- Place geogrid over block and lay over compacted backfill.
- place next course of block over geogrid.
- Pull geogrid tight, keep tension applied until backfill is placed; staples or stakes may be used to maintain tension.



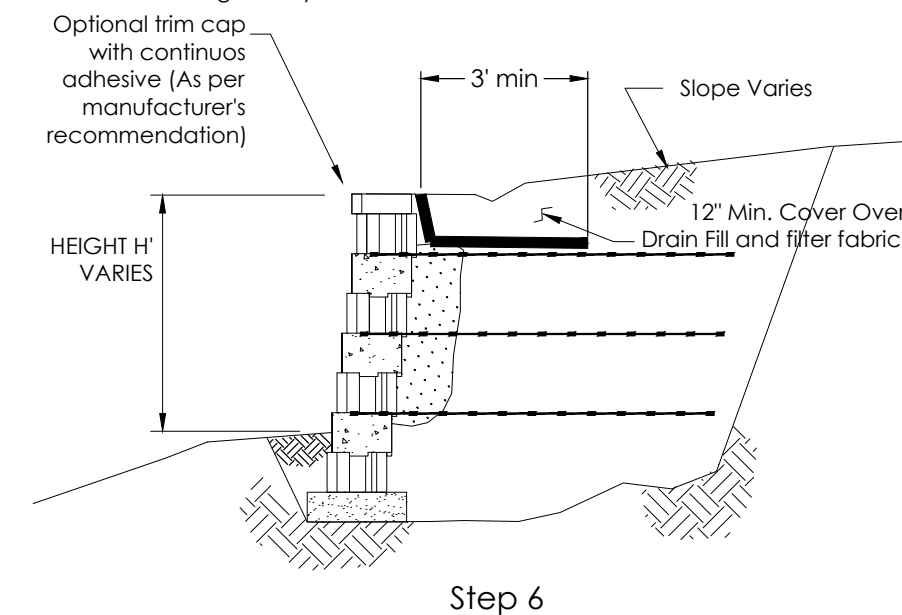
- Place a minimum of 12" of drainage rock above the finish grade in front of wall.
- Place compacted backfill behind drainage rock.
- Place additional block courses by repeating step 3.



- Continue wall construction to full height.
- Place additional block course by repeating steps 3 and 4.
- Place geogrid at required heights and lengths by repeating steps 2 thru 4.



- Repeat steps 3 thru 5 until wall is at required height.
- Install cap units, filter fabric, and final lift of backfill.
- Provide for drainage away from wall.



This drawing is furnished for preliminary design purposes only, and should not be used for final design drawings or construction drawings without the certification of a professional engineer registered in the state in which the wall will be constructed

No.	Date.	Revision.	By.

PREPARED BY:

Legend	
	Compacted Fill
	Crushed Rock
	Native Soil
	Leveling Pad
	Geogrid

Project Name:	XX
Project Location:	YY
Drawing Description:	Wall Construction Sequence

Prepared For:	CCC DDD
Drawn By:	
Checked By:	
Sheet # or \$	
Date:	MM/DD/YYYY
Project Number:	WWWPRO#

BY	DESCRIPTION	DATE	REV.

CITY OF CONWAY
CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
PHASE 1

LANDSCAPE DETAILS

JOB NO.: 18147010
DATE: MAY 2020
DESIGNED BY: BDT
DRAWN BY: BDT

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DRAWING NUMBER
L-504

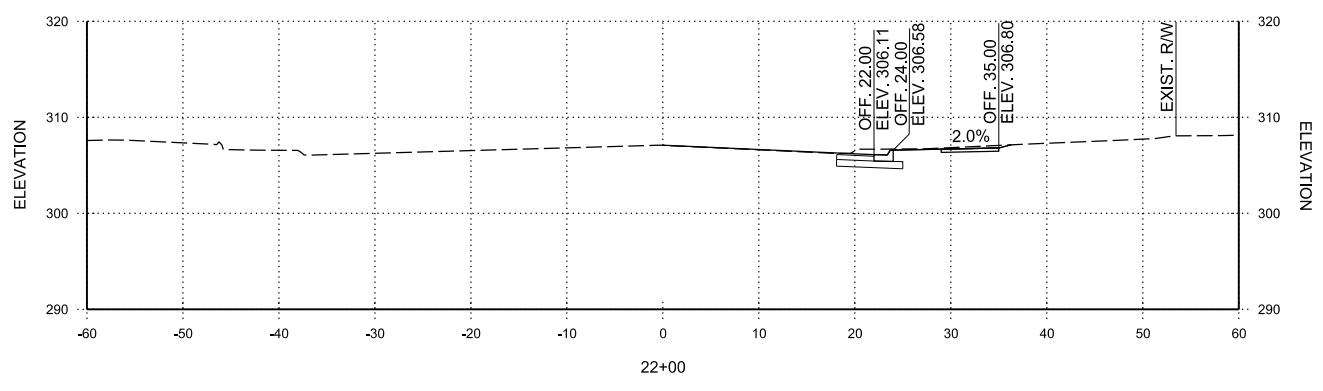
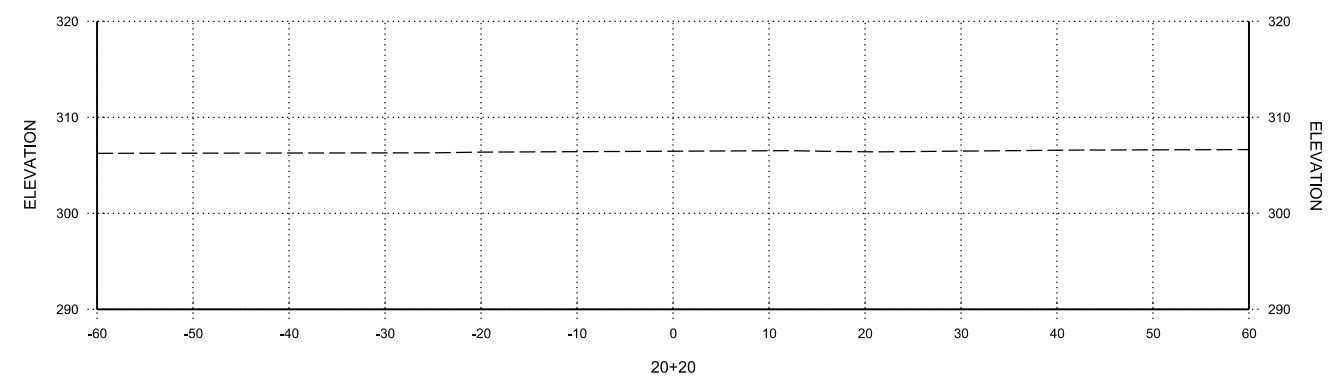
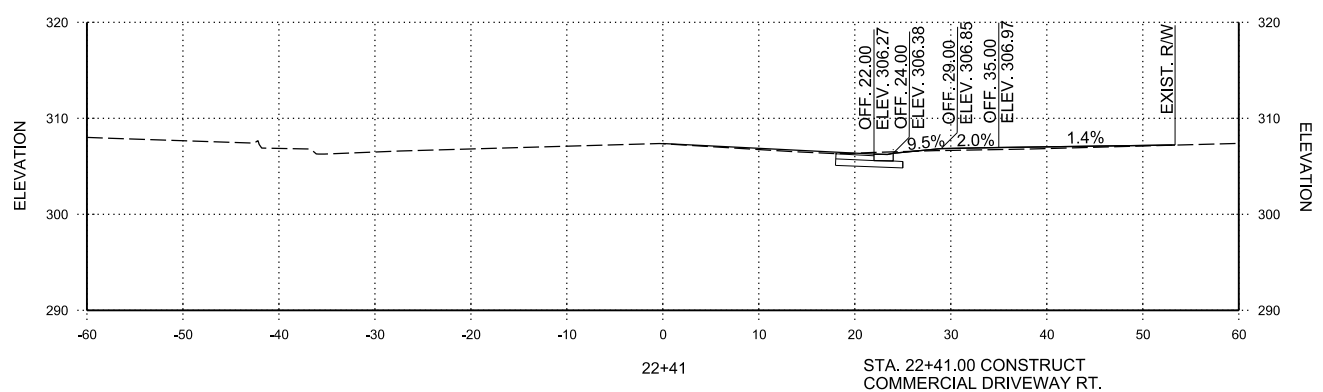
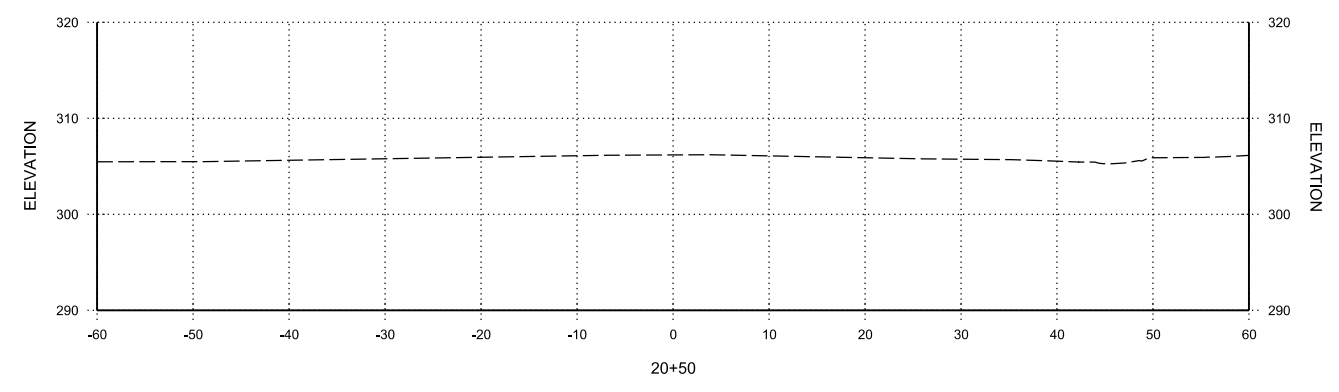
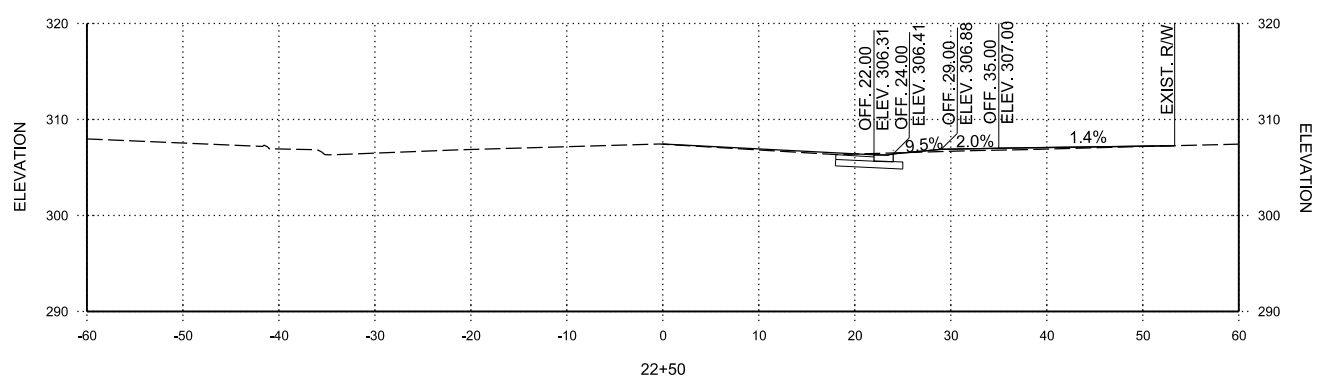
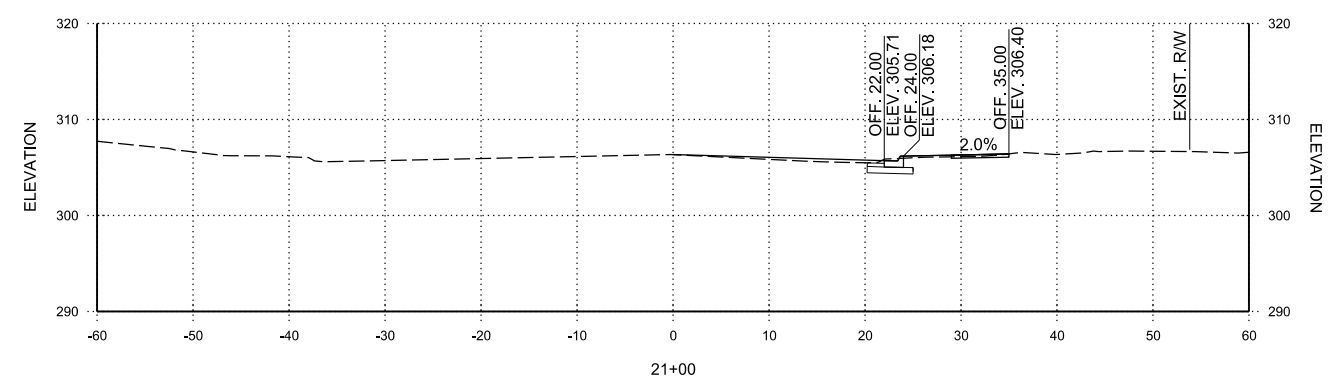
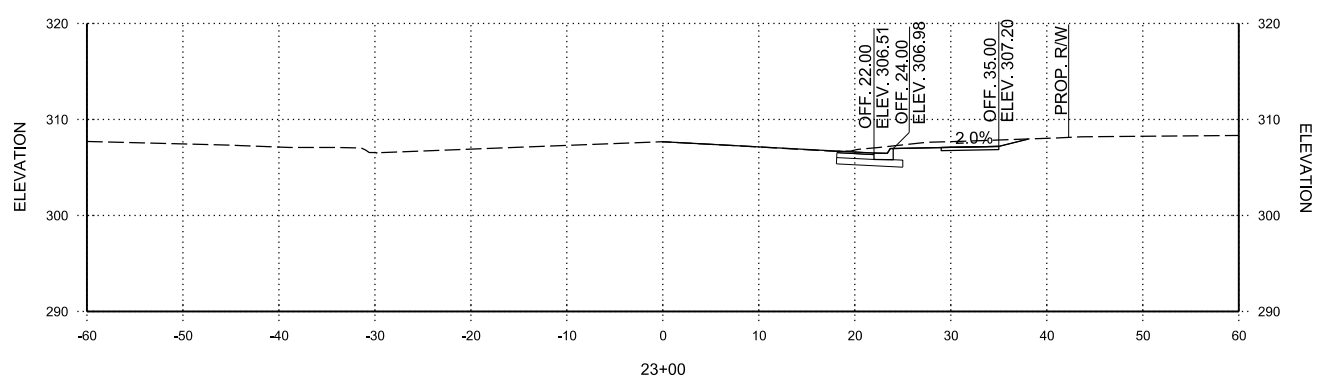
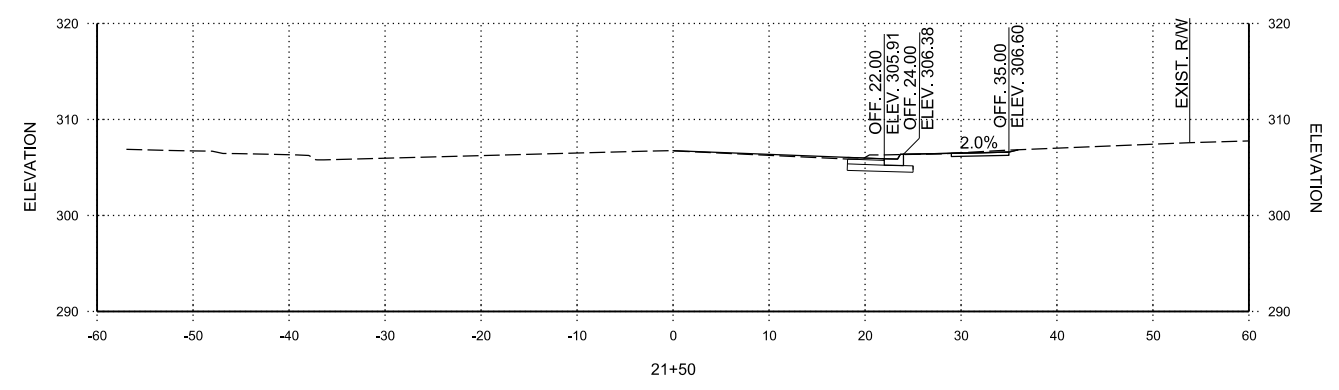
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 \$\$\$WORKSPACE\$\$\$
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STA. 22+41.00 CONSTRUCT COMMERCIAL DRIVEWAY RT.

**DONAGHEY AVE.
 STA. 20+20 TO STA. 23+00**

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
**DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)**

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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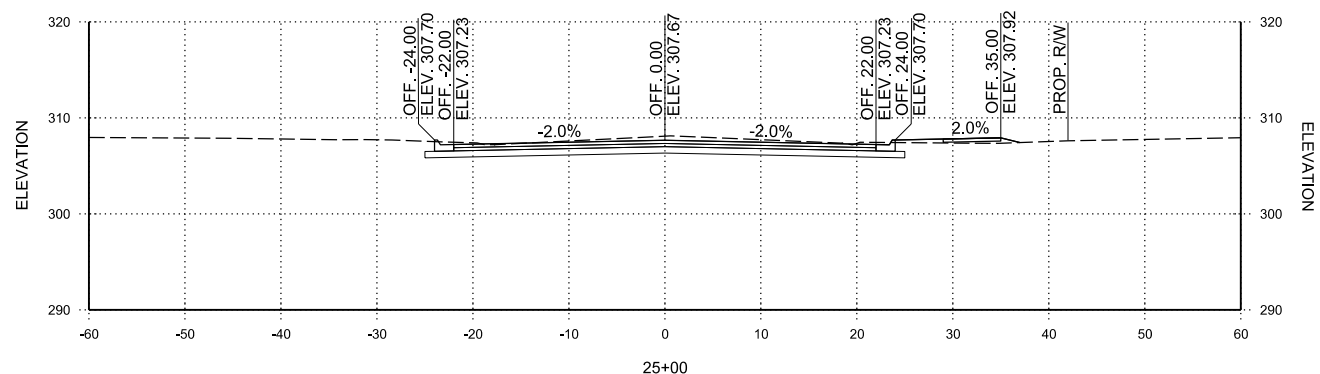
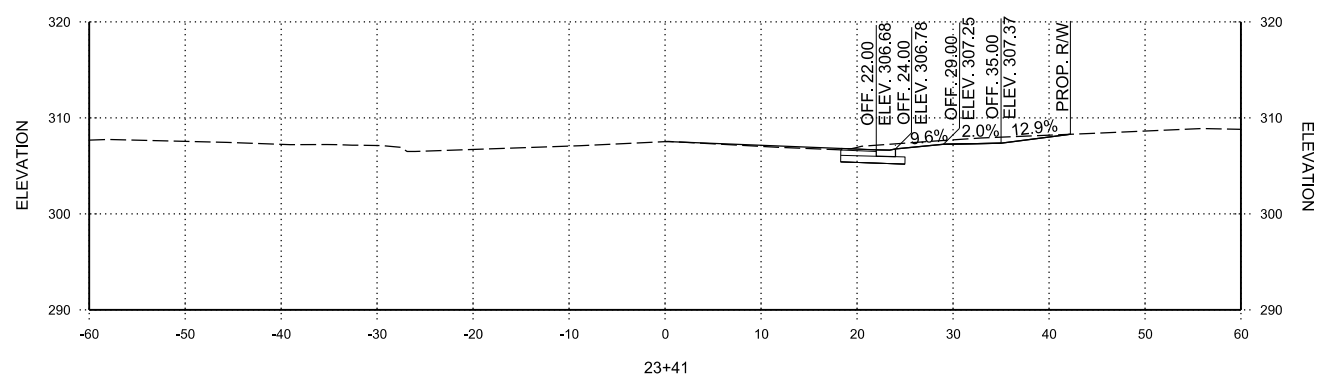
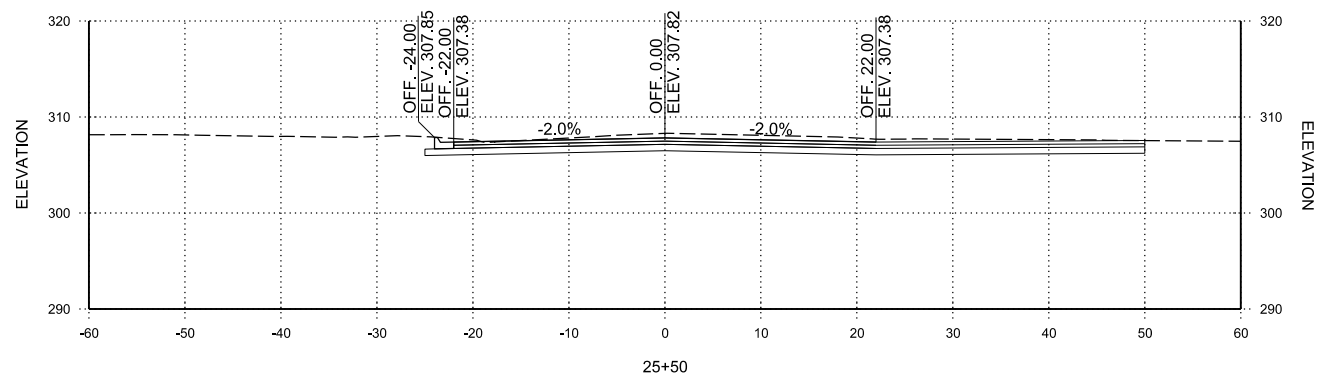
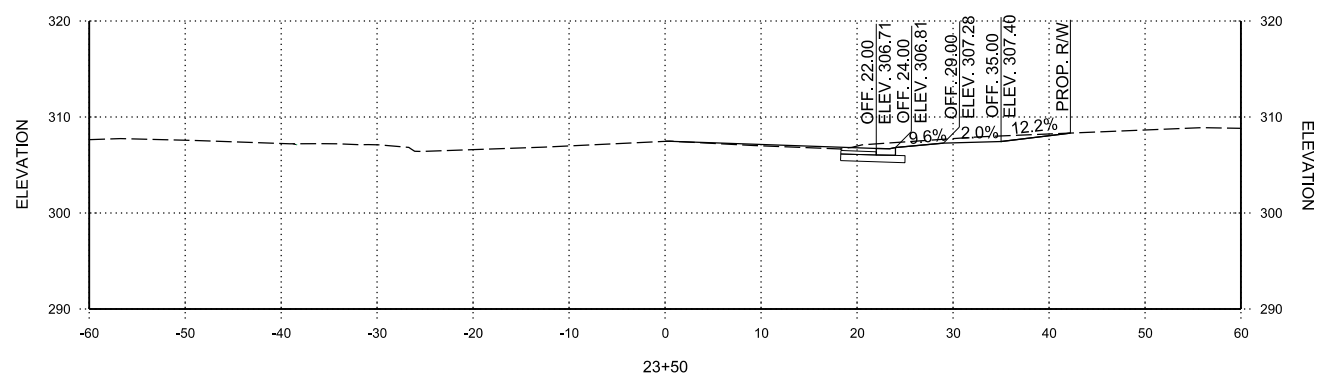
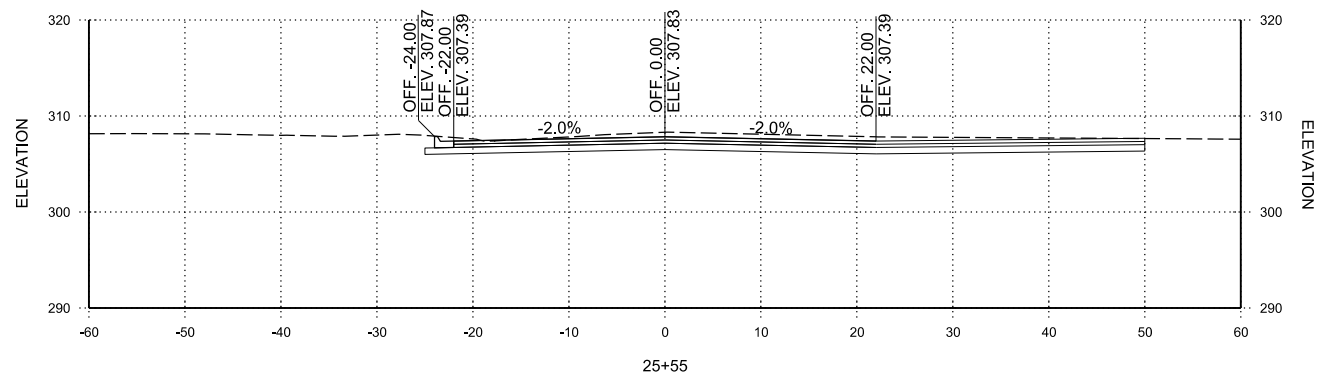
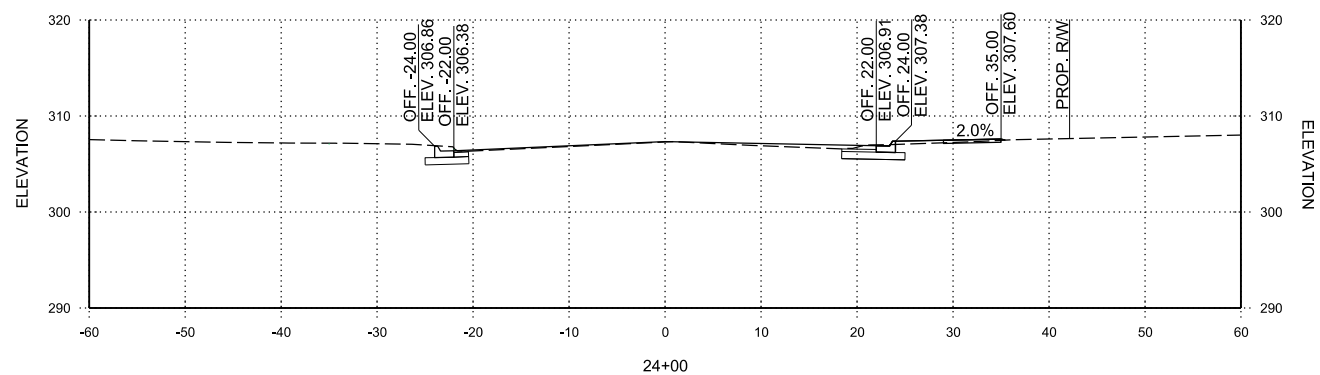
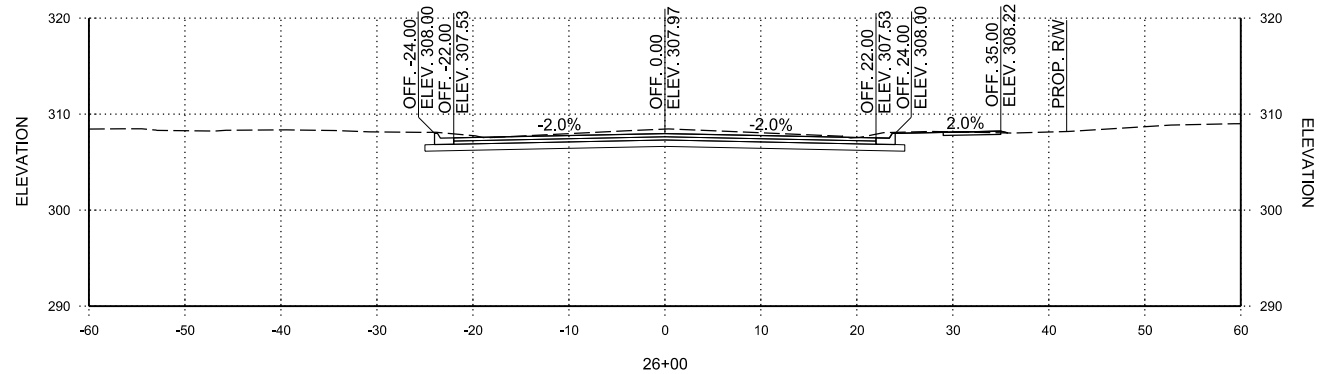
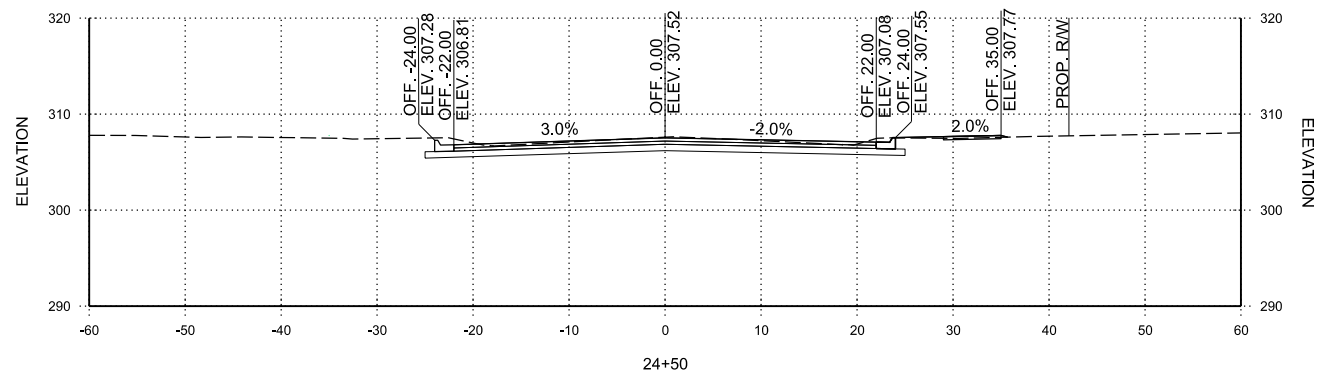
DRAWING NUMBER
CX-1

SHEET NUMBER
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STA. 23+41.30 CONSTRUCT
 RESIDENTIAL DRIVEWAY RT.

DONAGHEY AVE.
 STA. 23+41 TO STA. 26+00

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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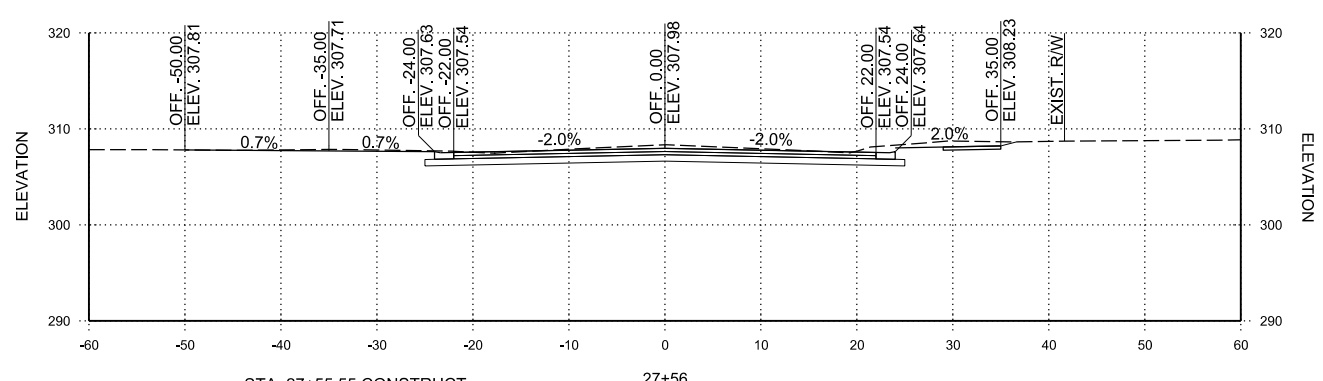
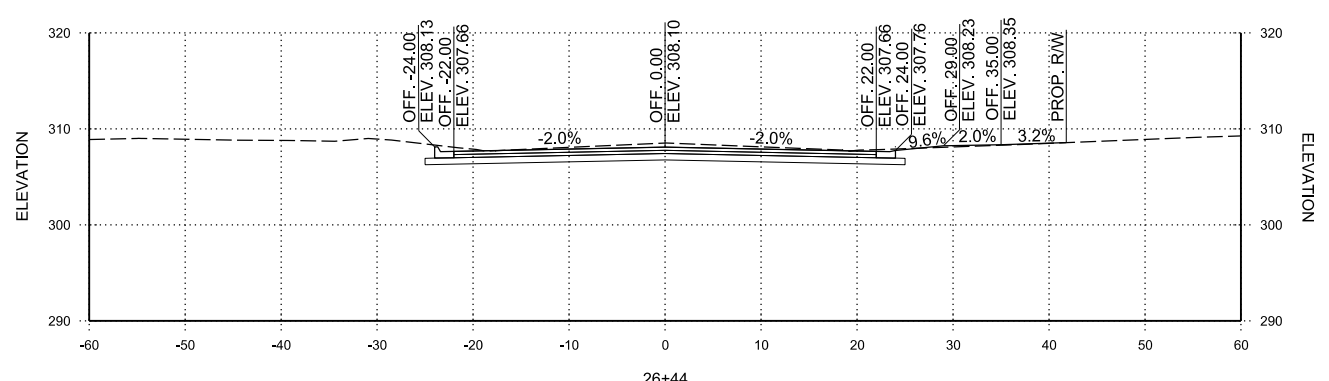
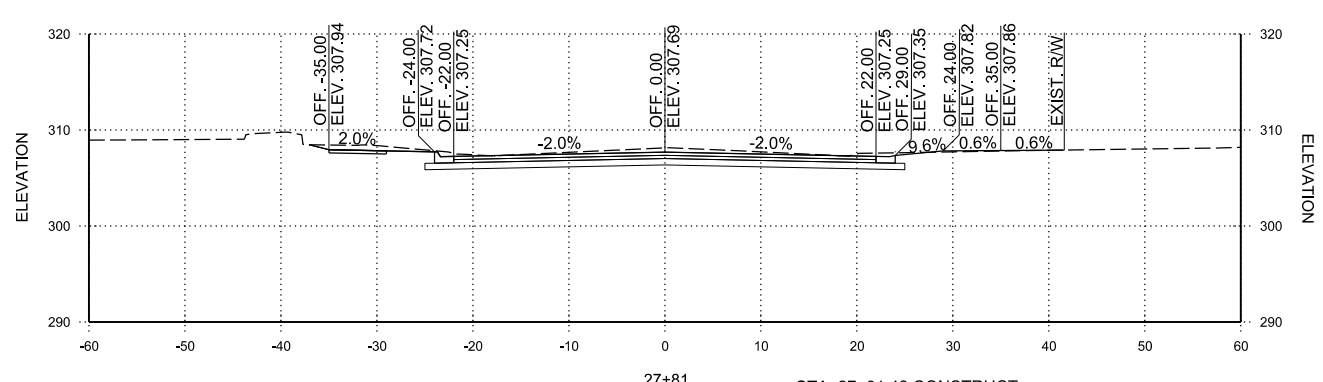
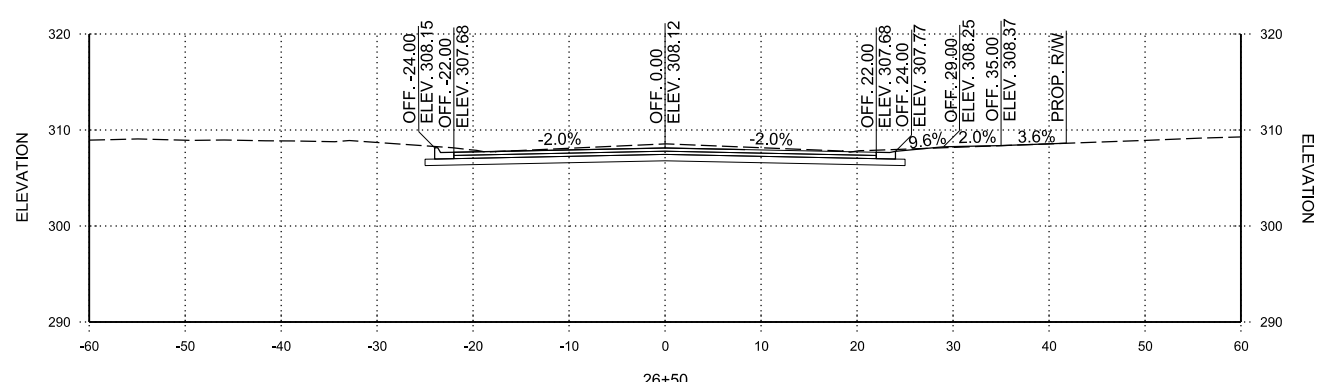
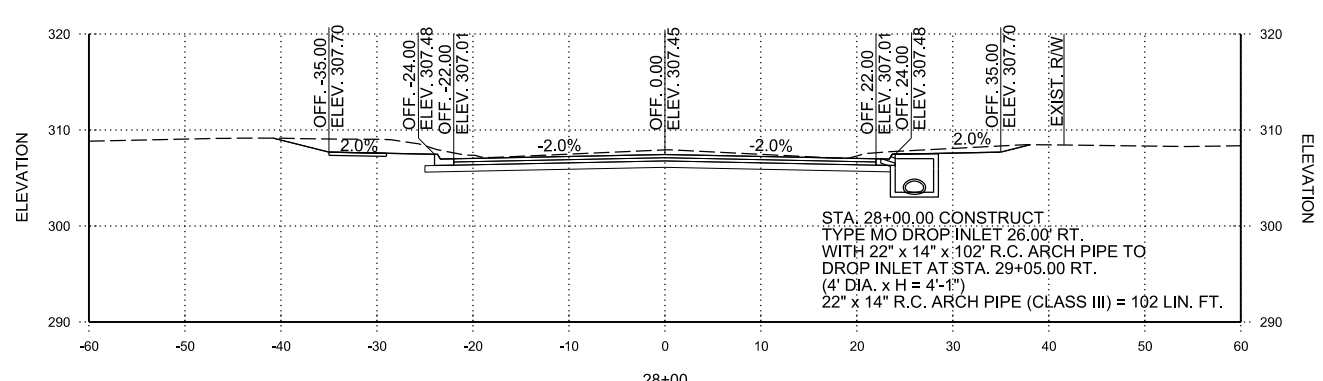
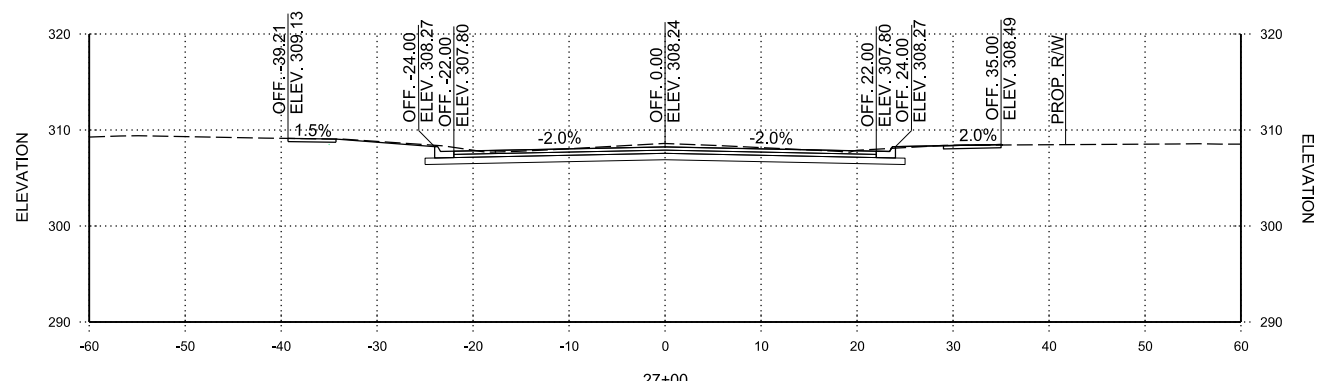
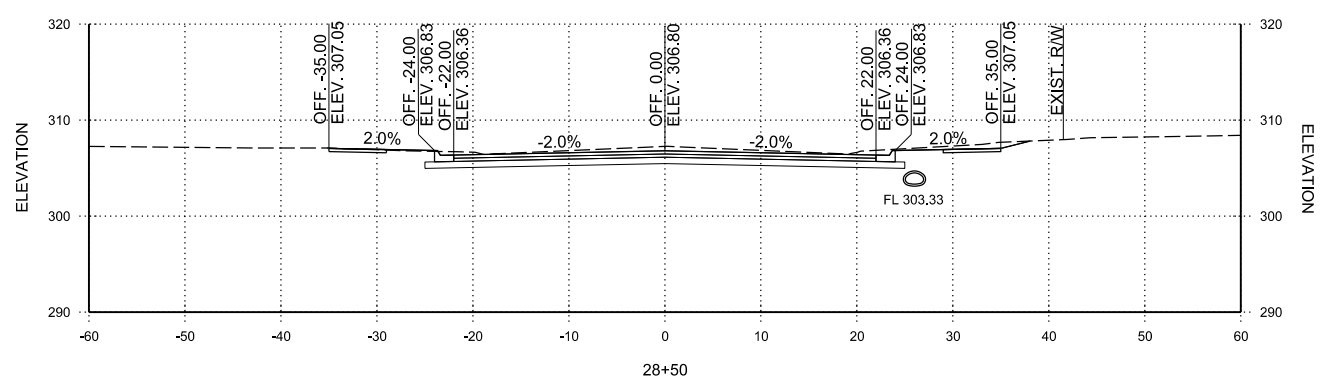
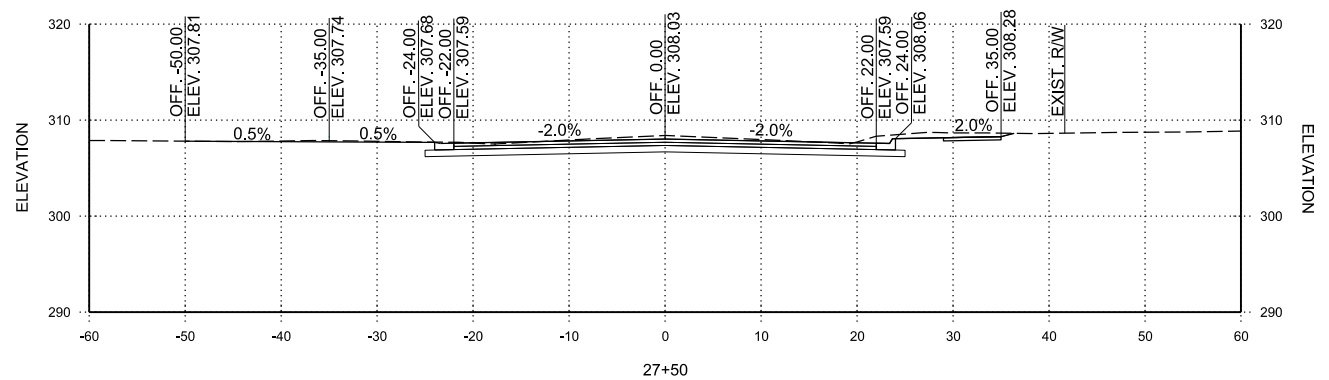
DRAWING NUMBER
CX-2

SHEET NUMBER
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**DONAGHEY AVE.
 STA. 26+44 TO STA. 28+50**

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

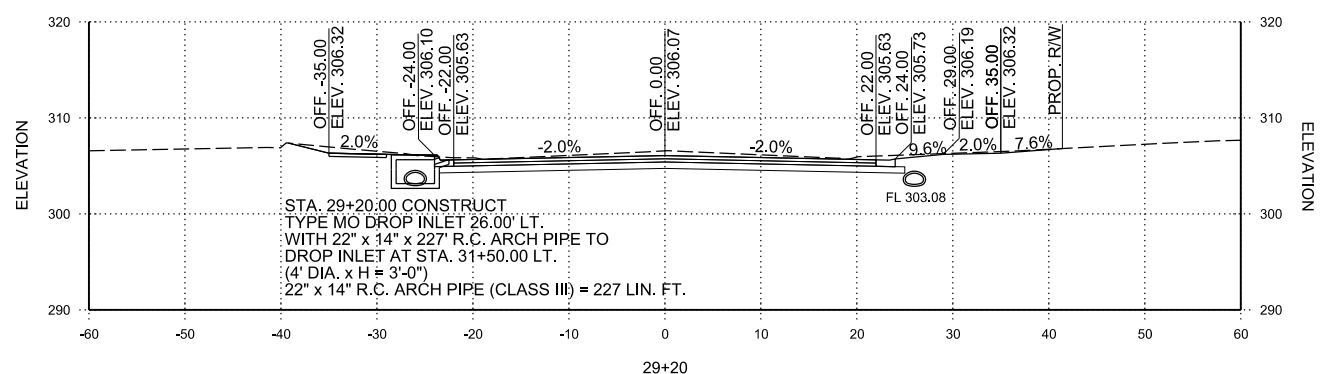
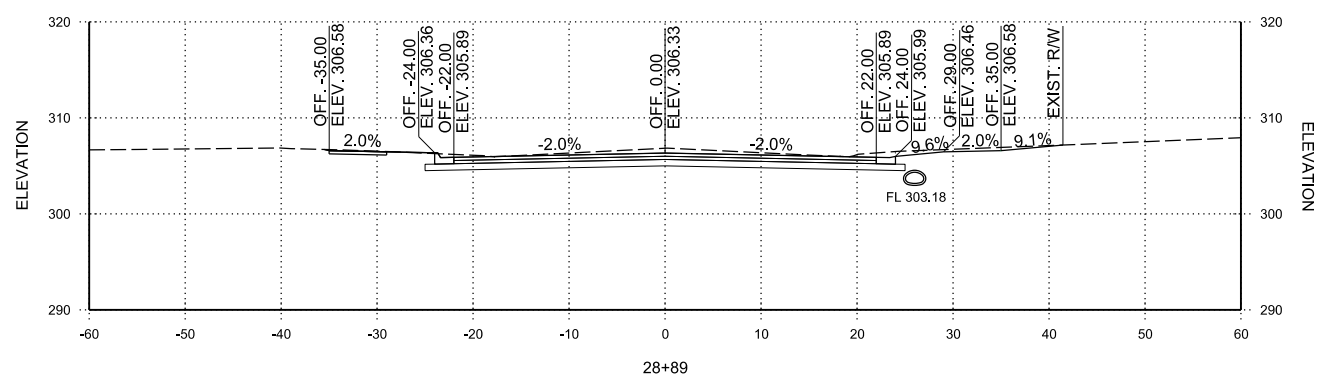
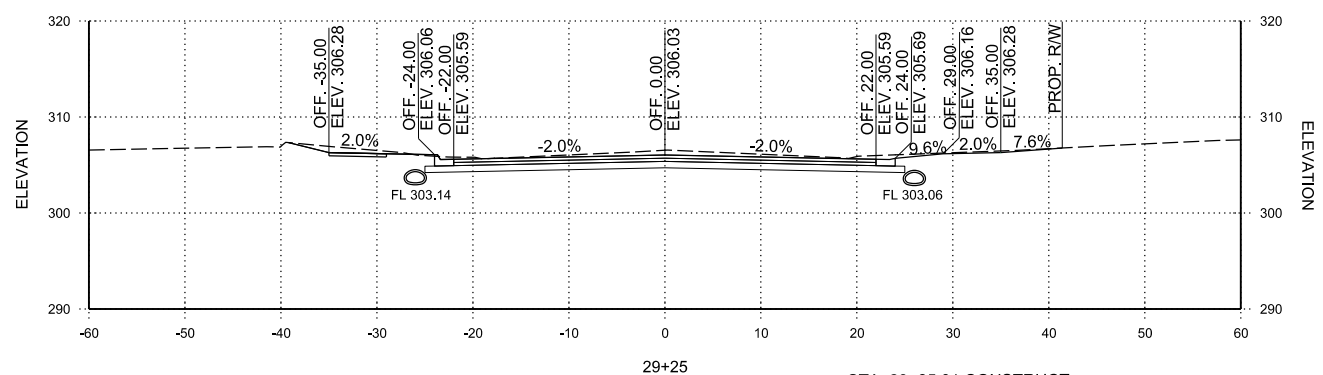
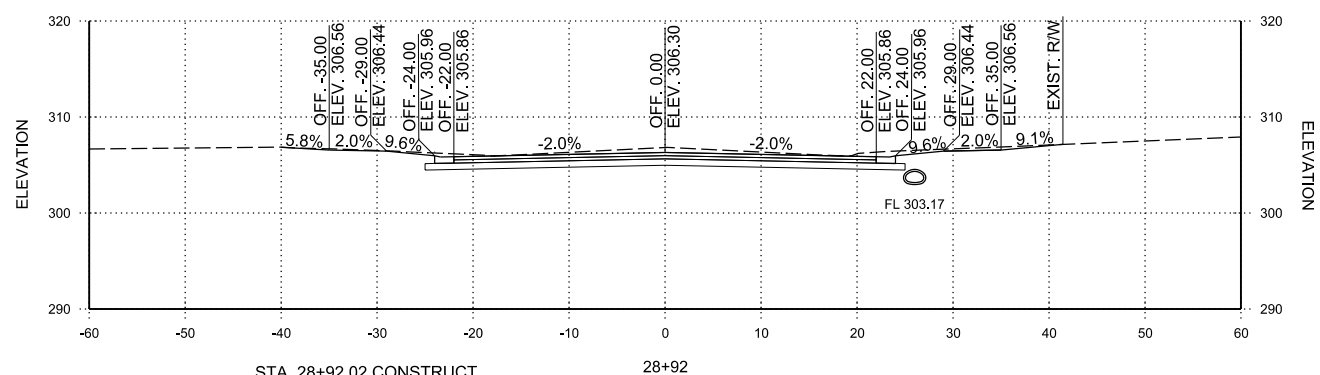
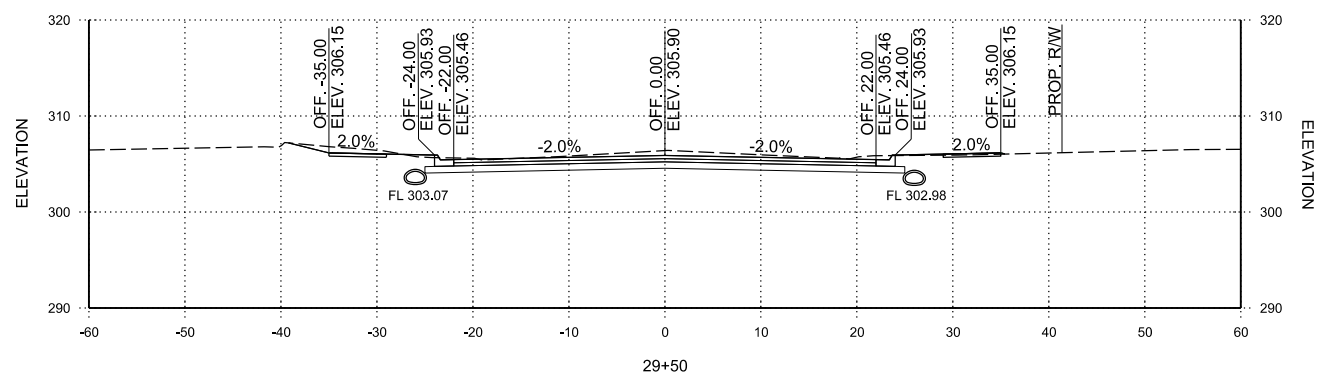
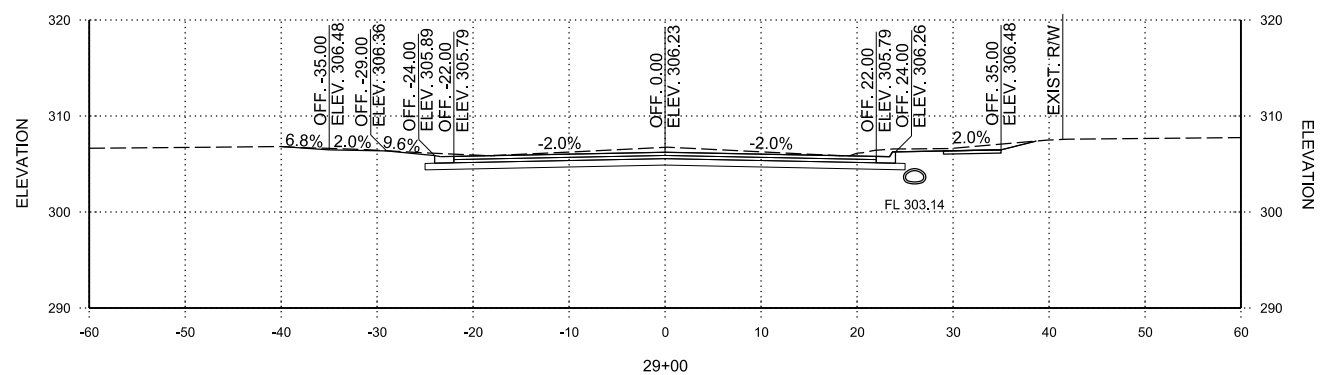
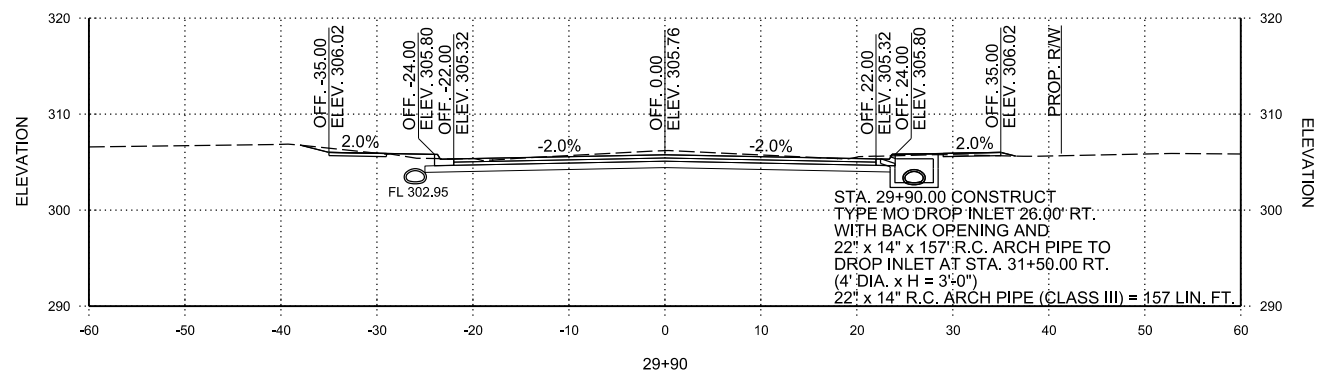
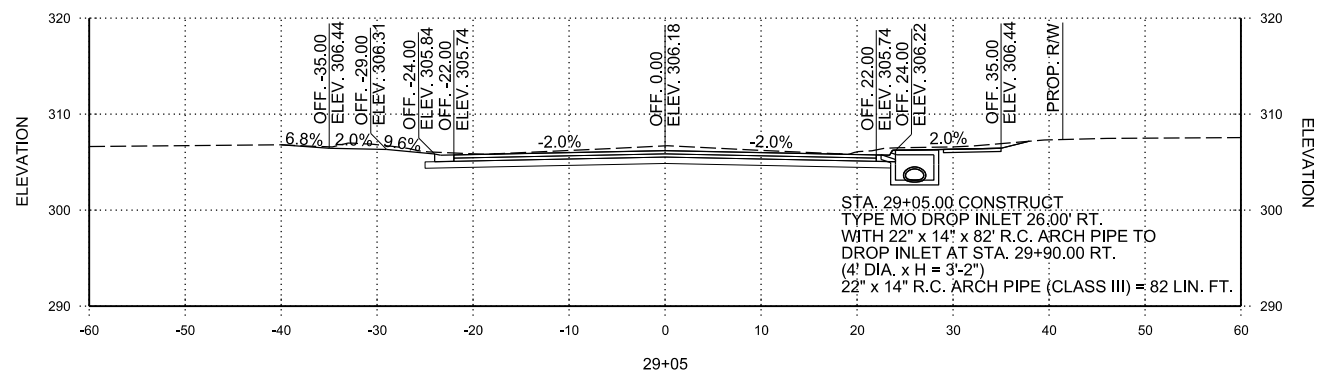
JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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 SHEET NUMBER
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**DONAGHEY AVE.
 STA. 28+89 TO STA. 29+90**

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

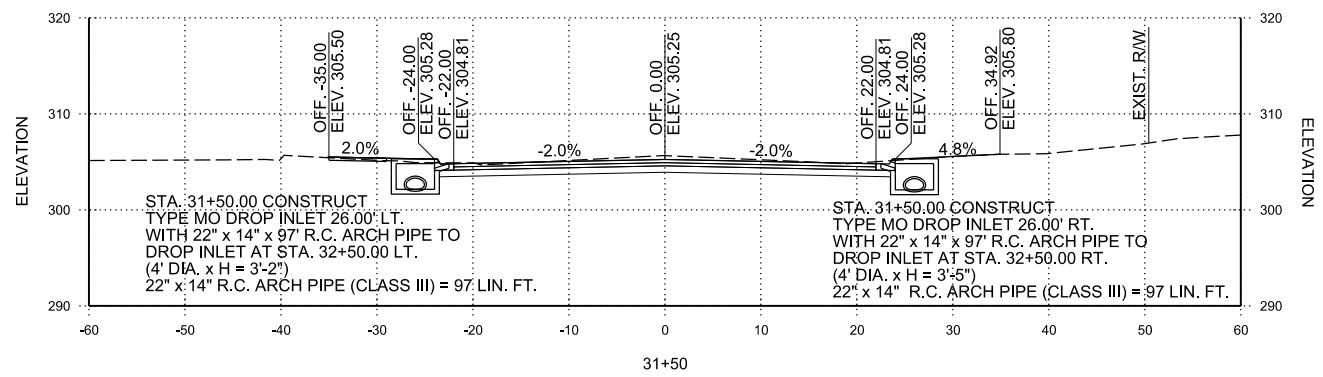
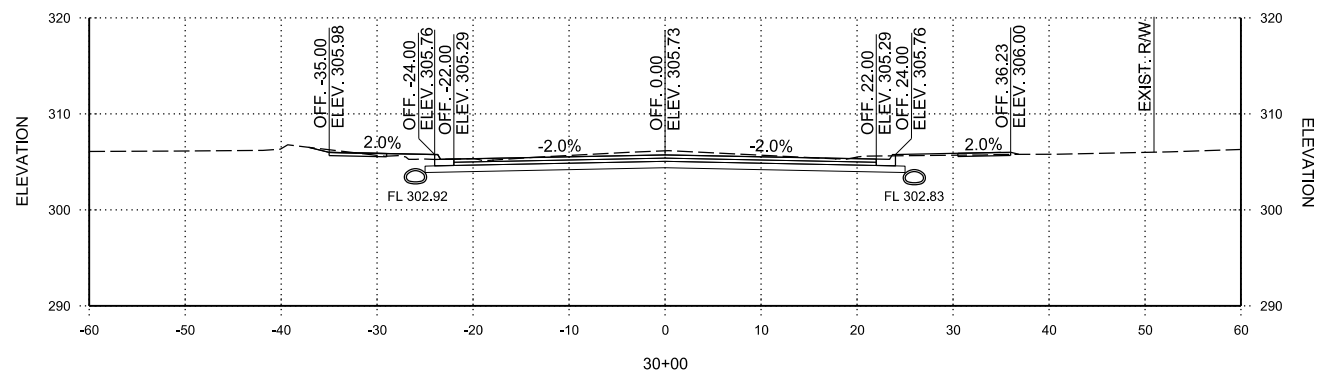
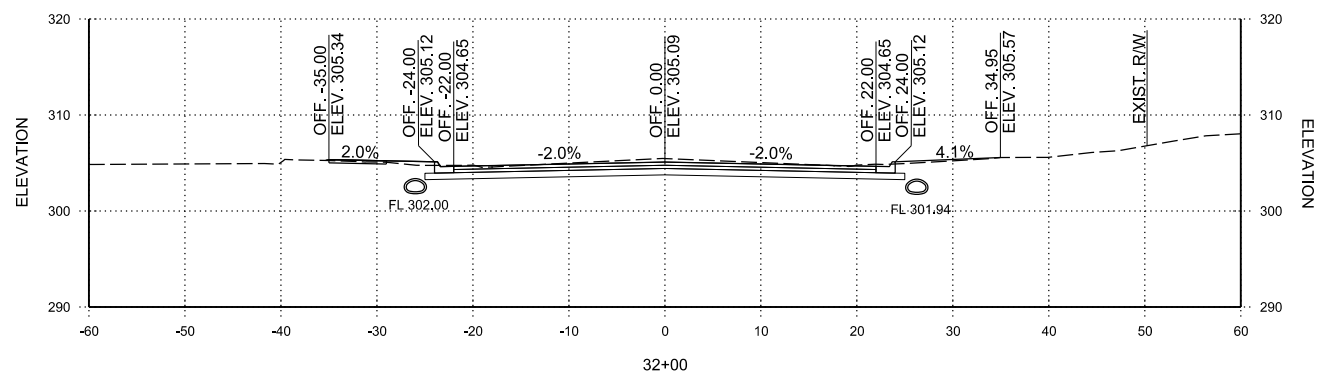
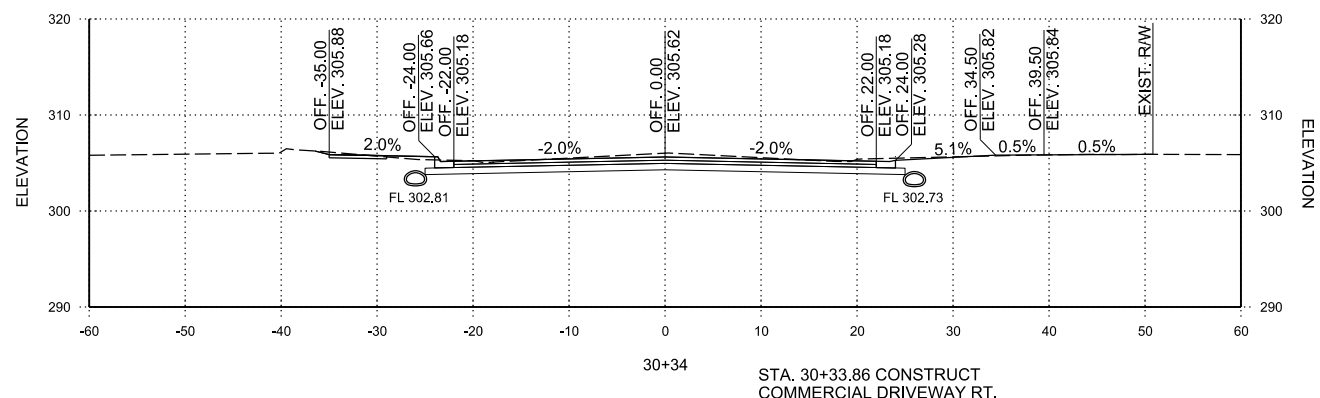
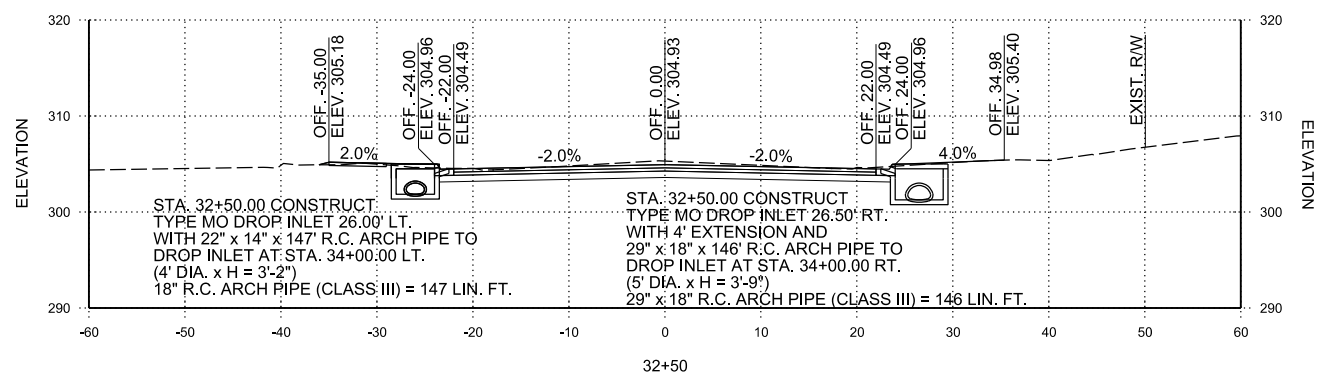
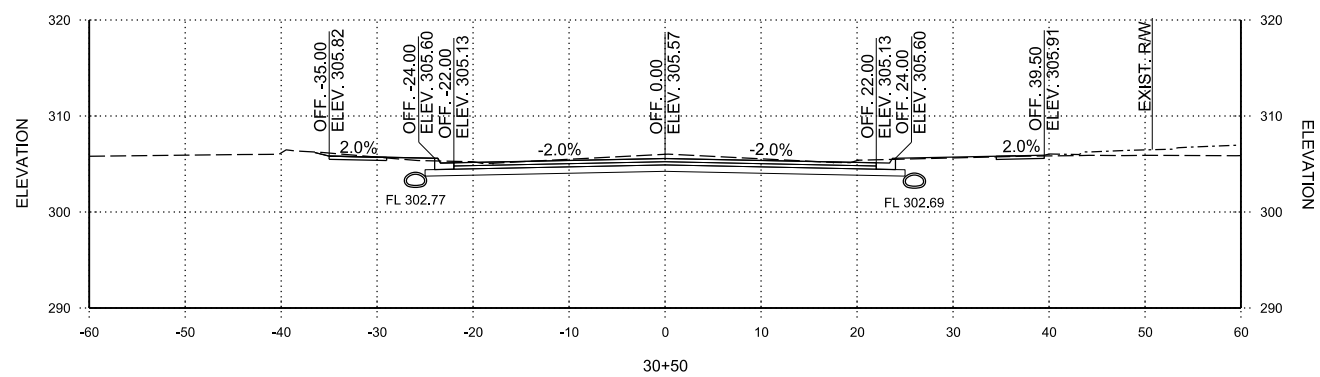
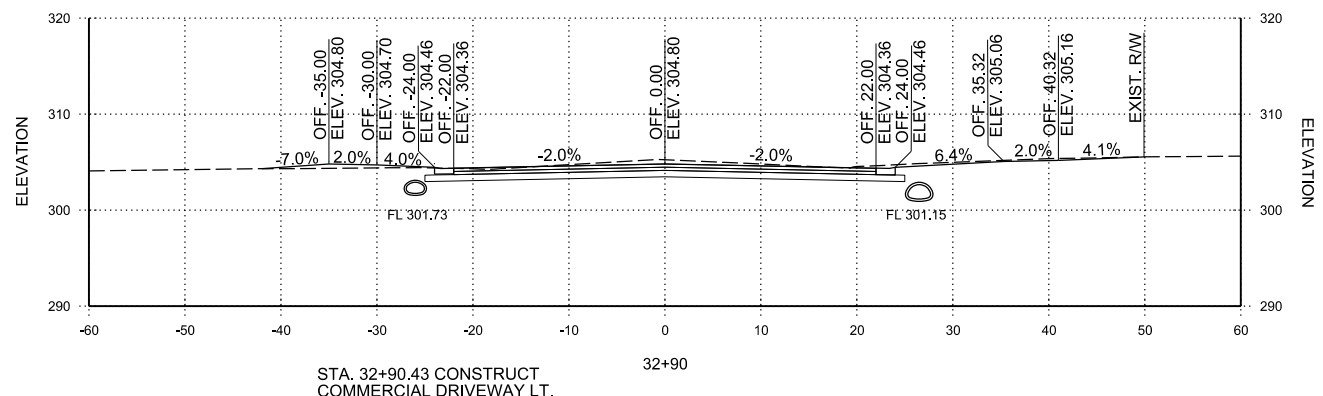
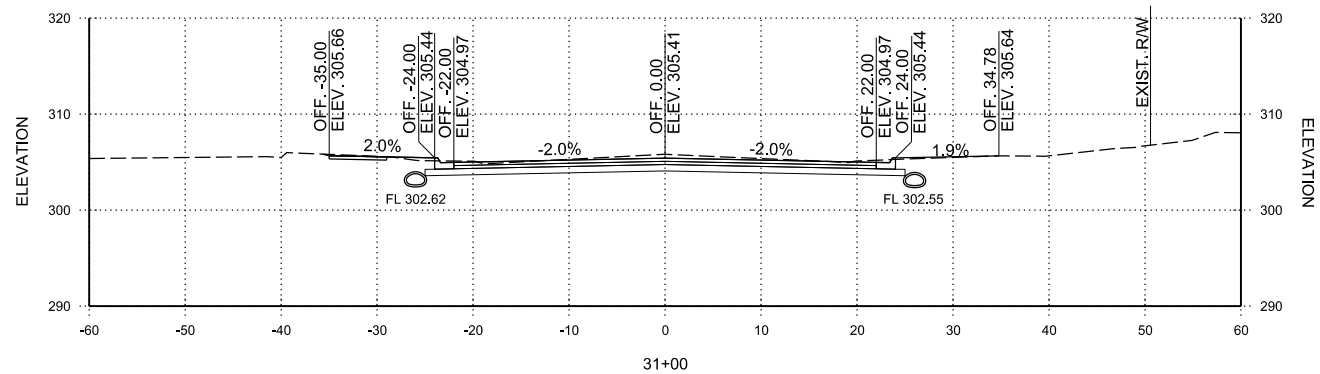
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DRAWING NUMBER
CX-4

SHEET NUMBER
133



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DONAGHEY AVE.
 STA. 30+00 TO STA. 32+90

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

BAR IS ONE INCH ON ORIGINAL DRAWING
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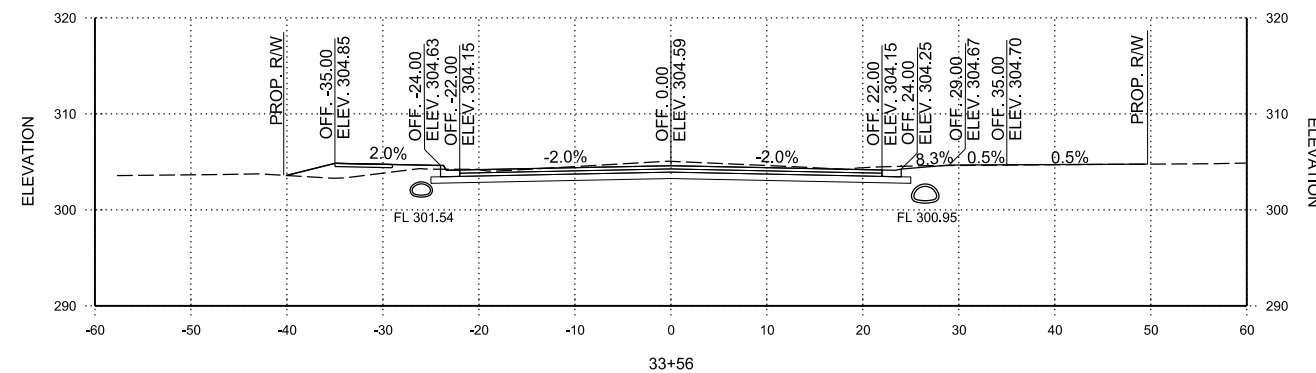
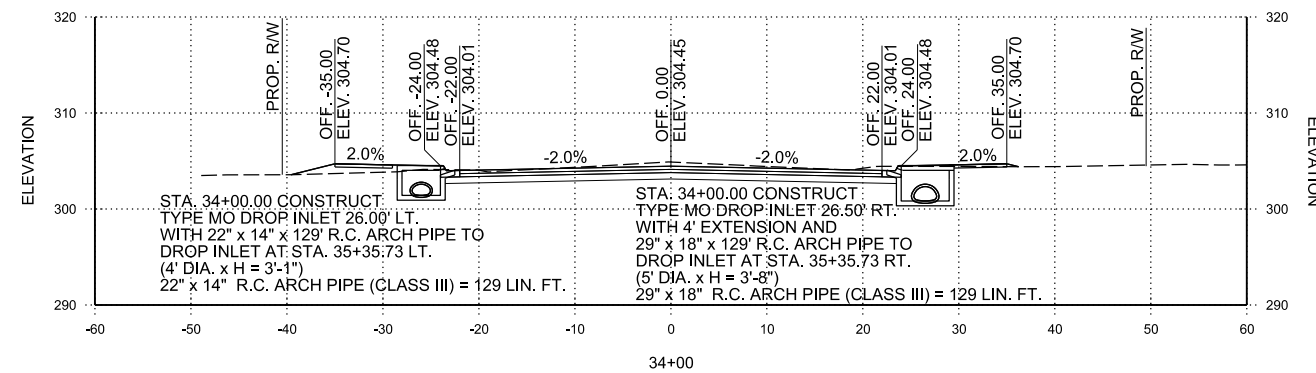
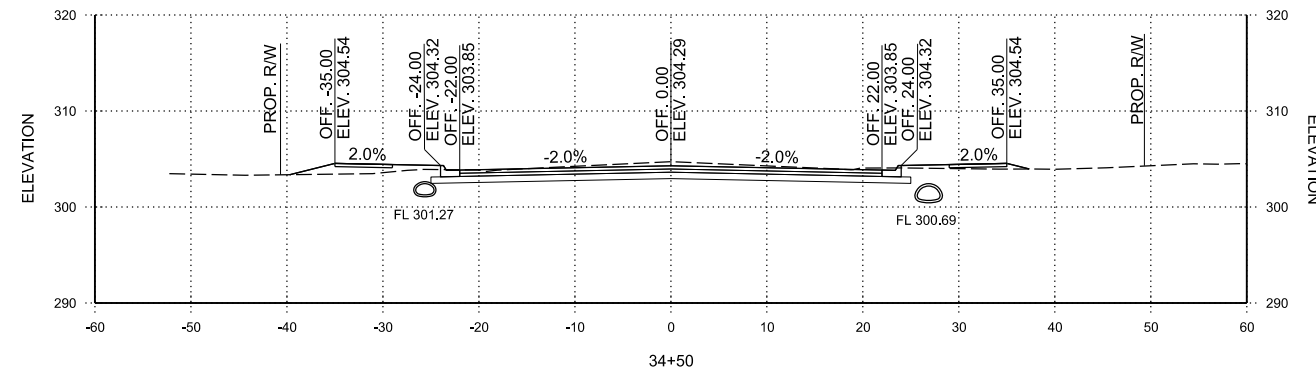
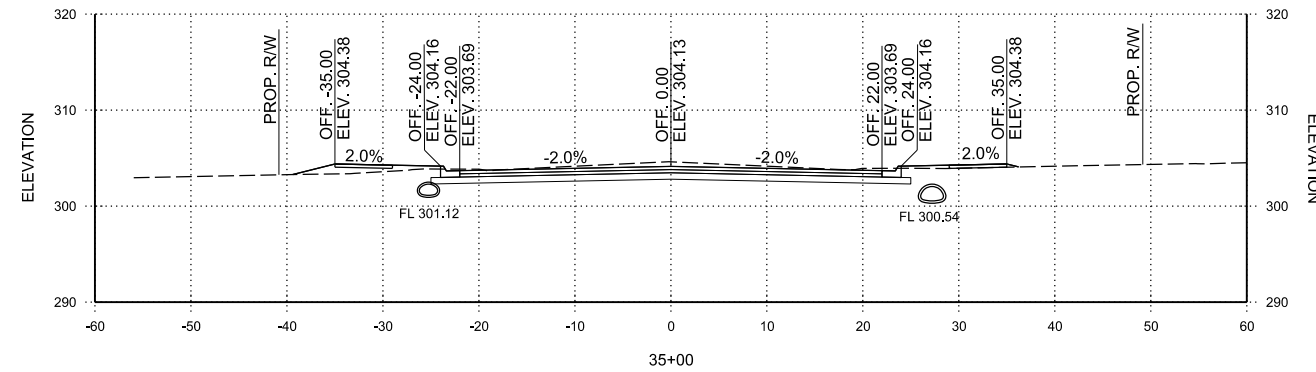
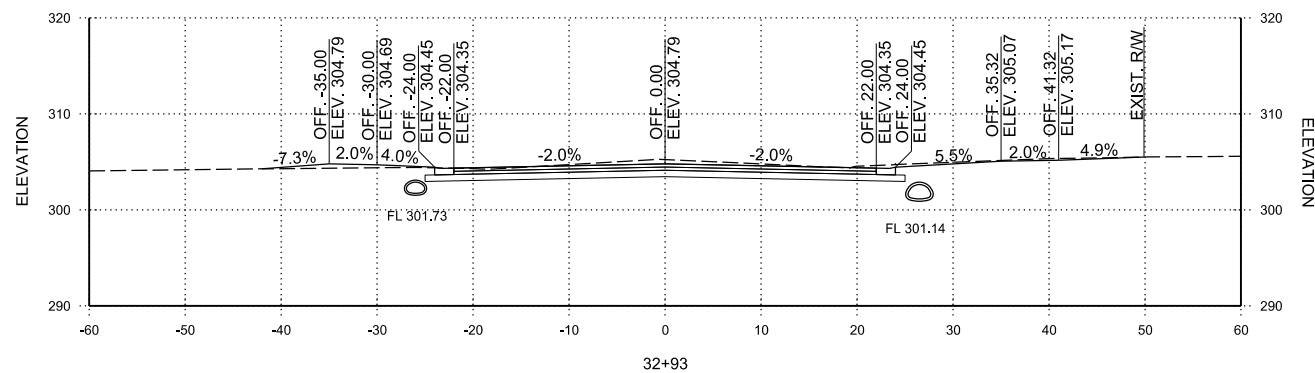
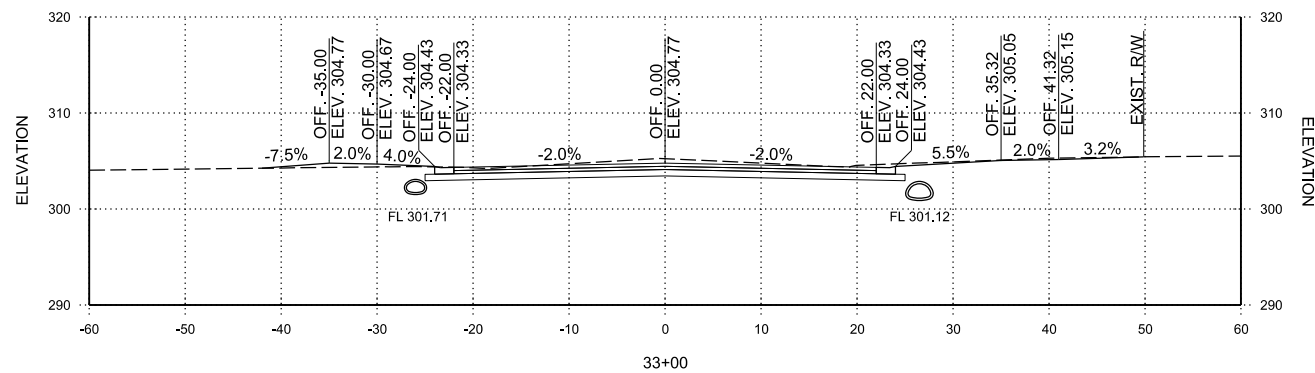
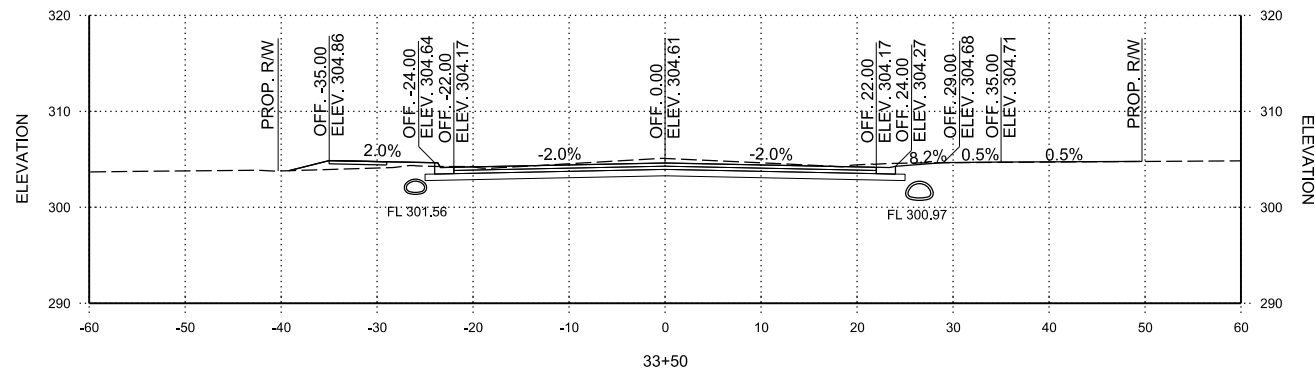
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 SHEET NUMBER
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**DONAGHEY AVE.
 STA. 32+93 TO STA. 35+00**

STA. 32+93.40 CONSTRUCT
 COMMERCIAL DRIVEWAY RT.

STA. 33+55.59 CONSTRUCT
 COMMERCIAL DRIVEWAY RT.



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

**DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)**

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

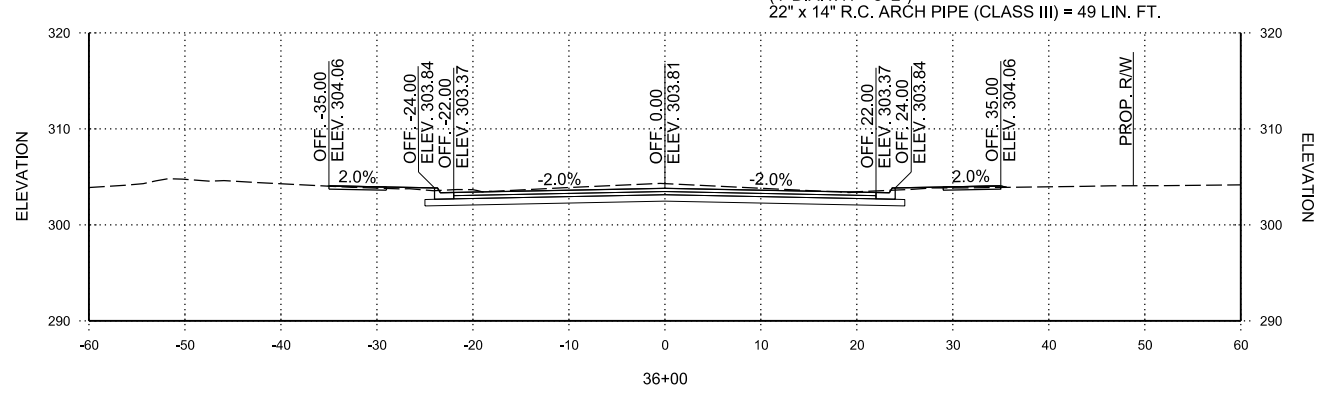
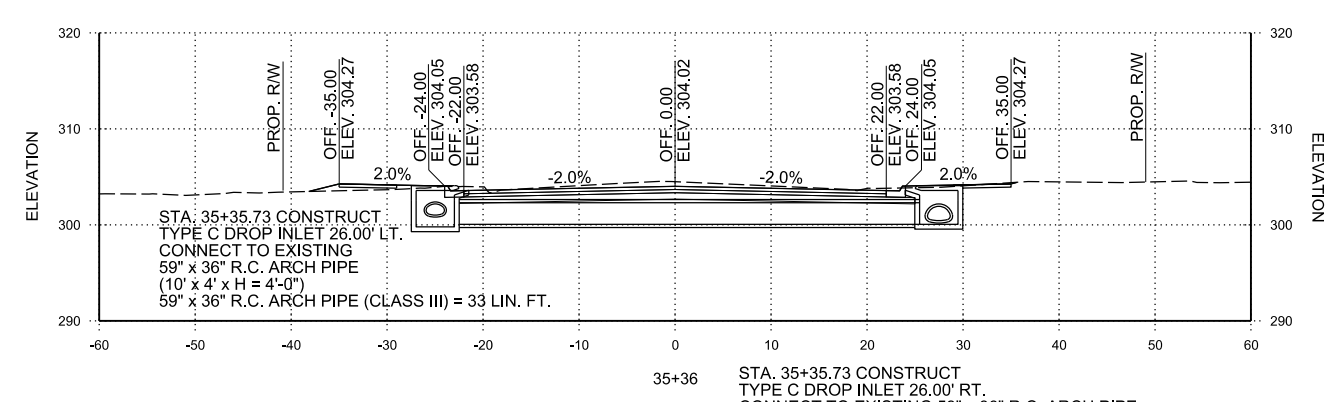
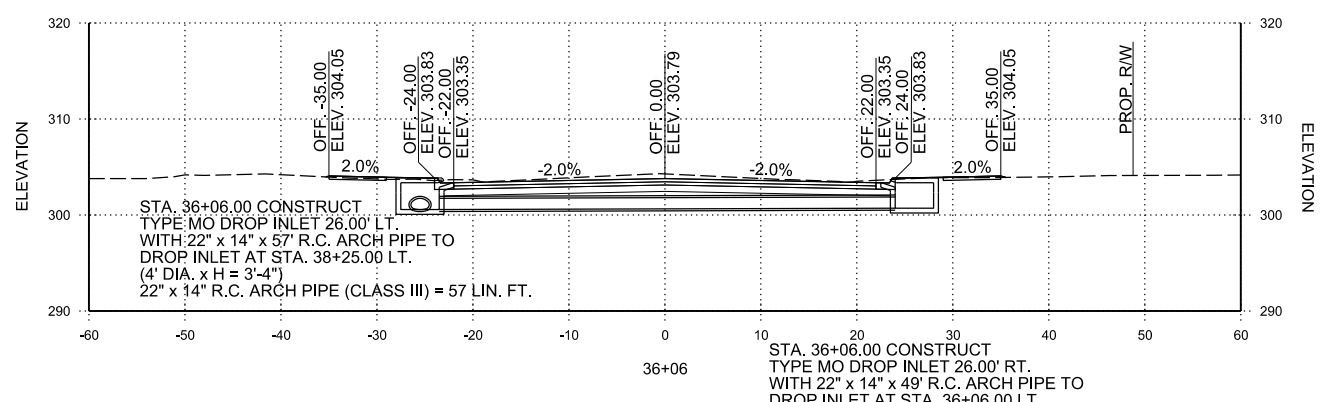
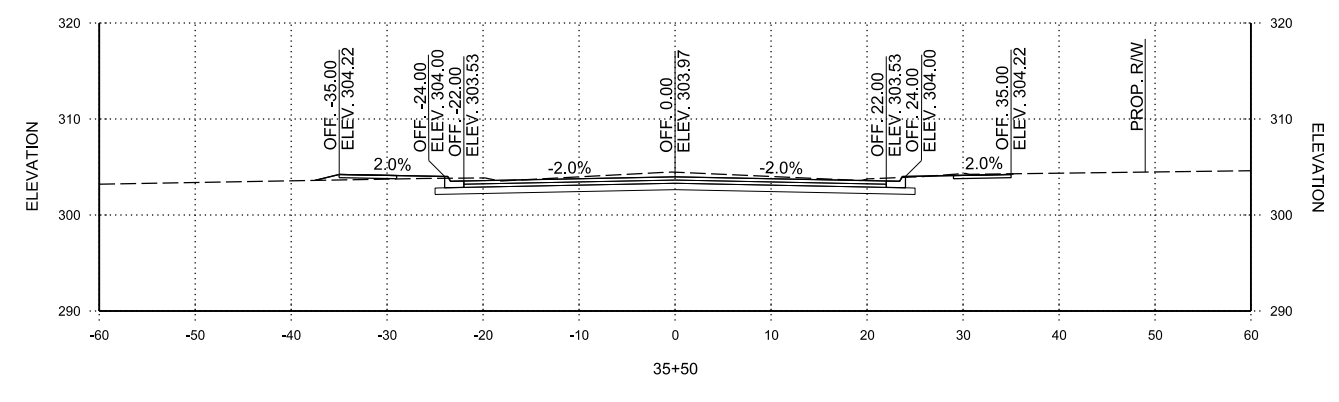
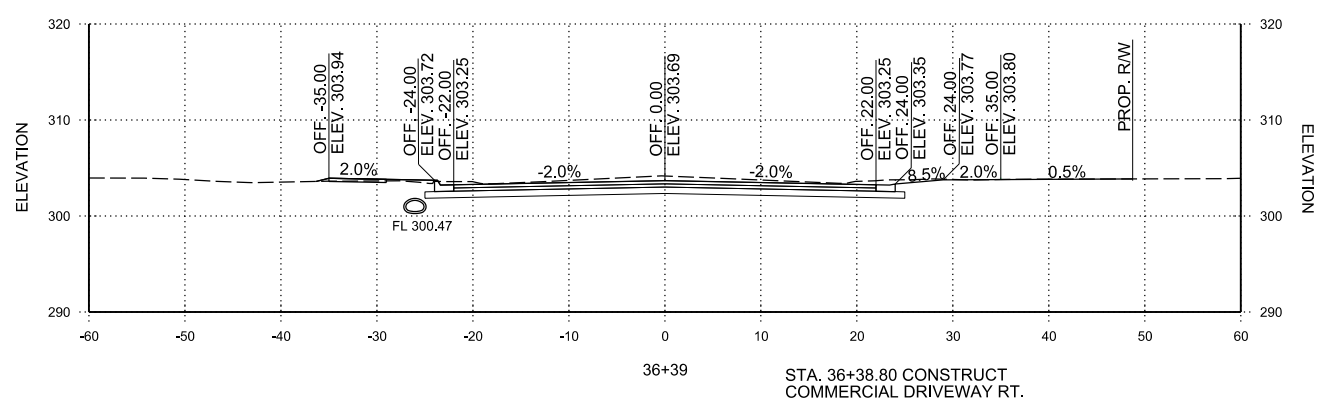
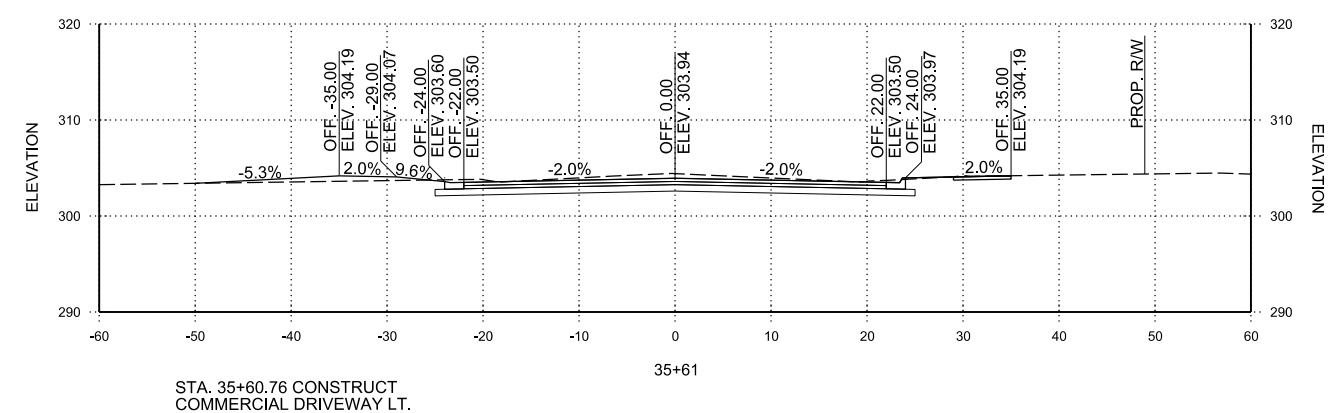
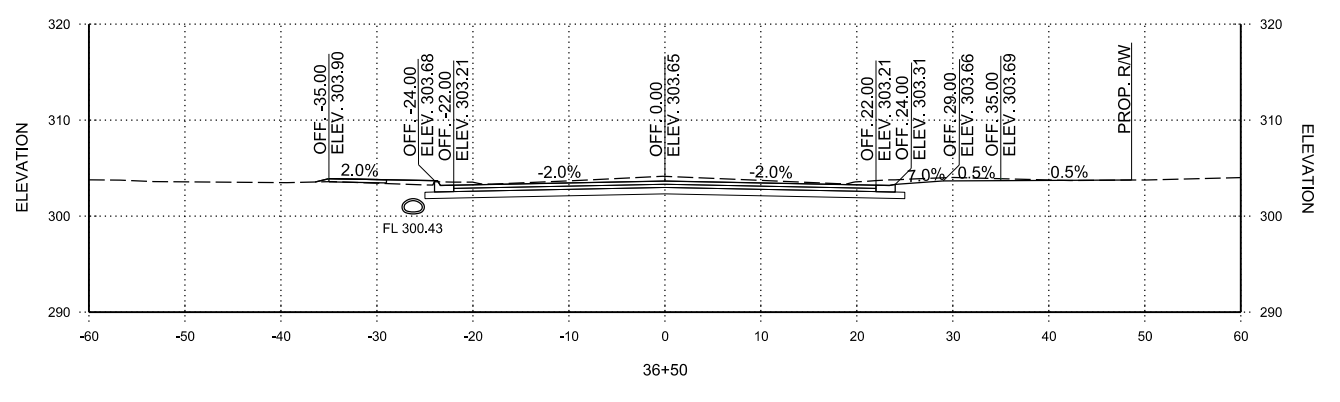
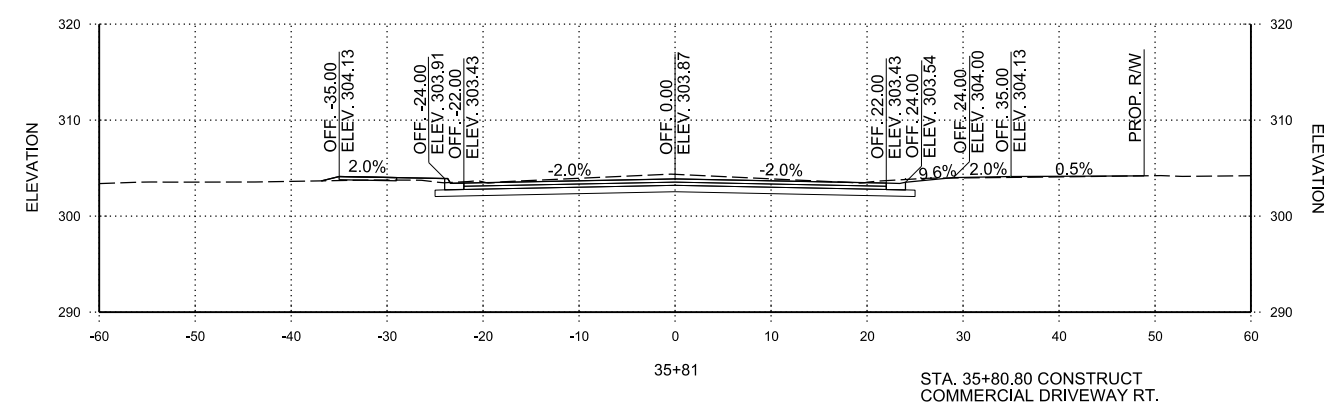
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SHEET NUMBER
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STA. 35+35.73 CONSTRUCT TYPE C DROP INLET 26.00' RT. CONNECT TO EXISTING 59" x 36" R.C. ARCH PIPE WITH DBL. 44" x 27" x 48' R.C. ARCH PIPES TO DROP INLET AT STA. 35+35.73 LT. (10' x 4' x H = 3'-11") 44" x 27" R.C. ARCH PIPE (CLASS III) = 96 LIN. FT.

**DONAGHEY AVE.
 STA. 35+36 TO STA. 36+50**

REV.	DATE	DESCRIPTION	BY

CITY OF CONWAY
 CONWAY, ARKANSAS

**DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)**

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

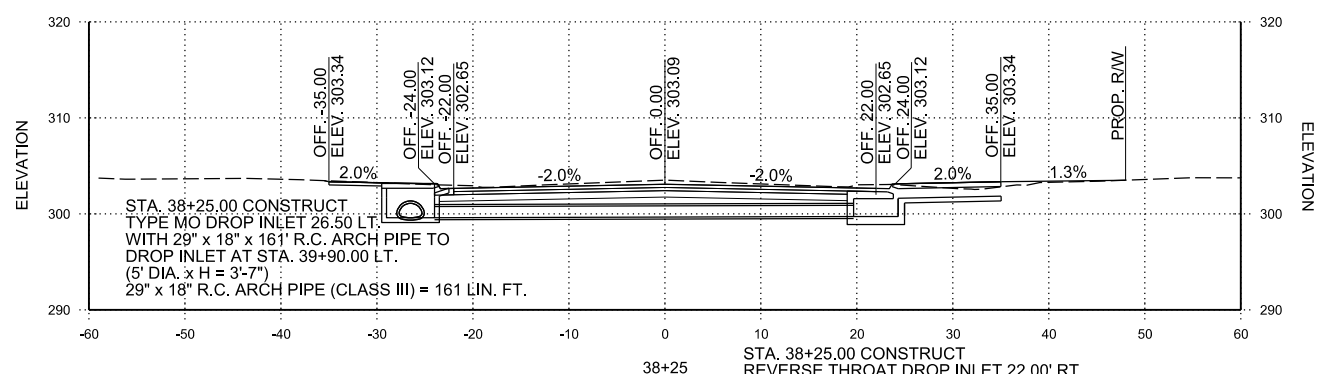
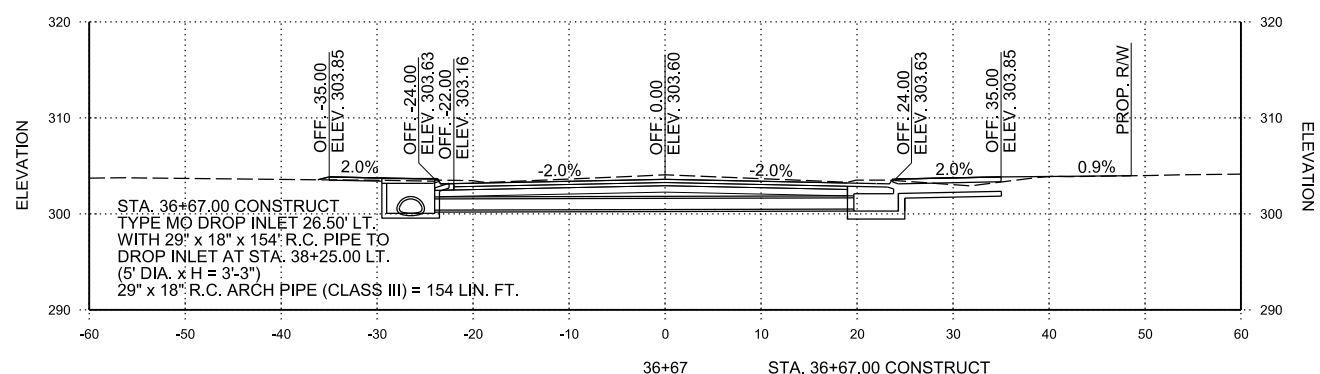
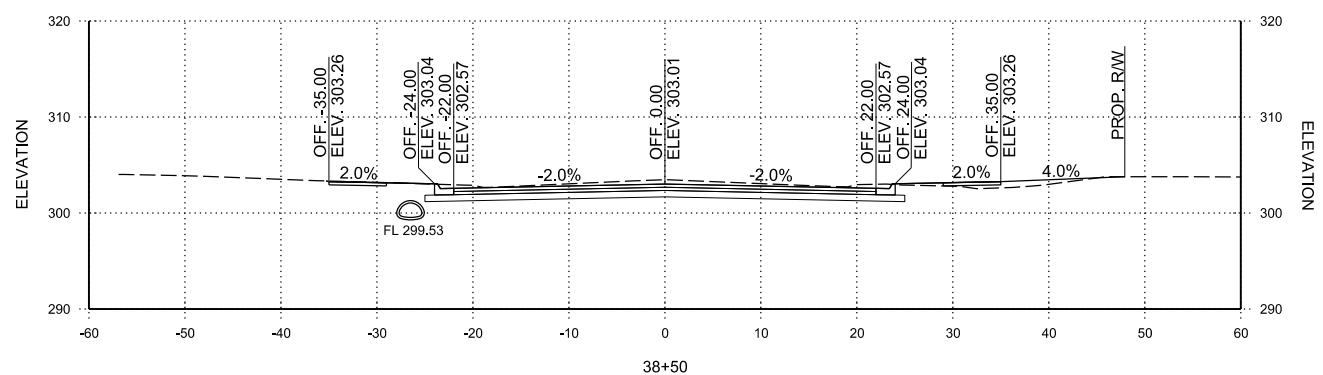
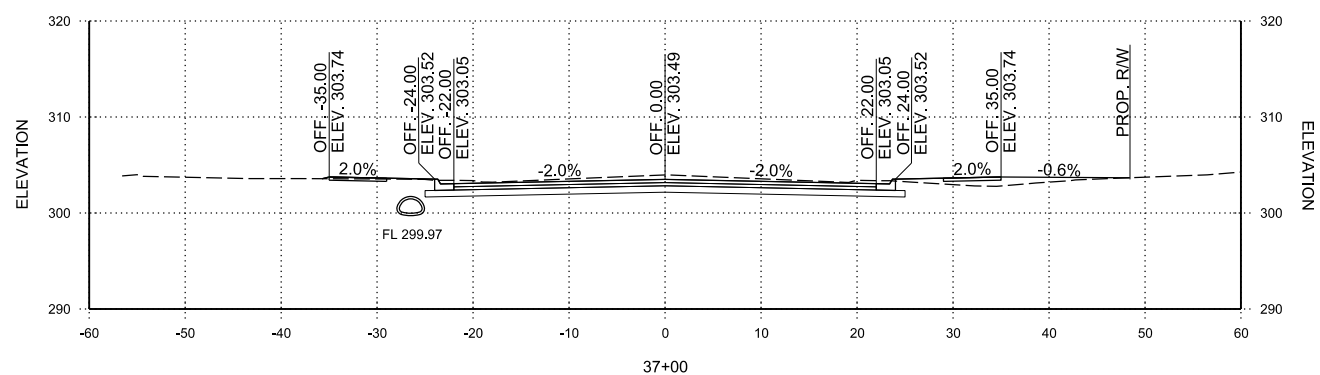
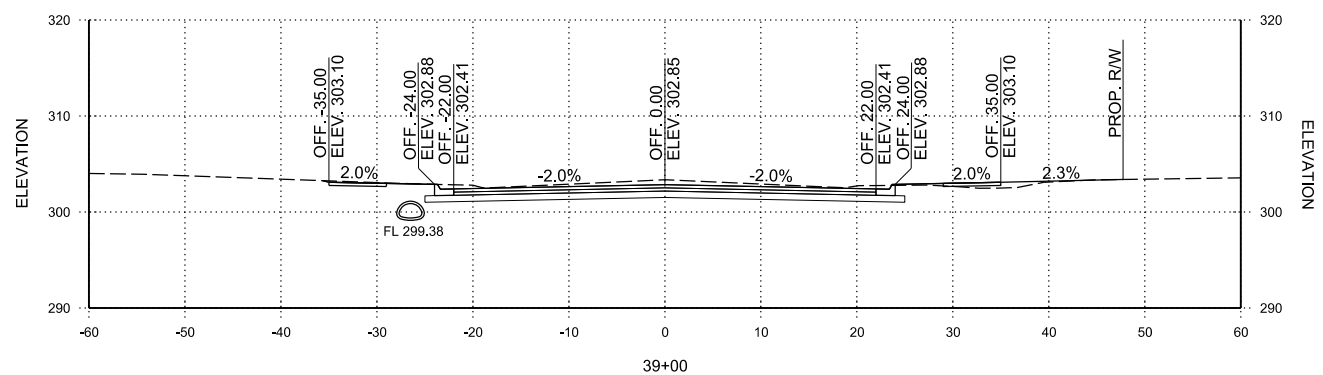
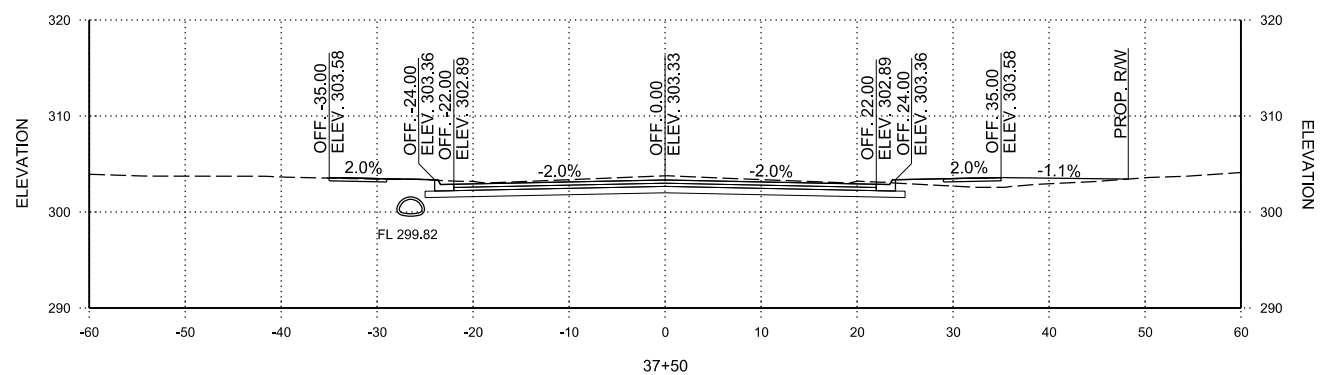
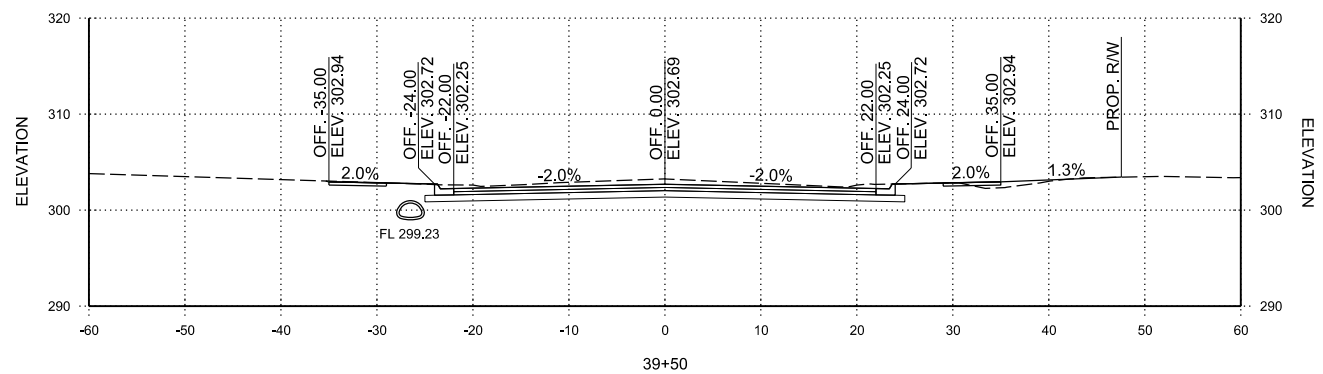
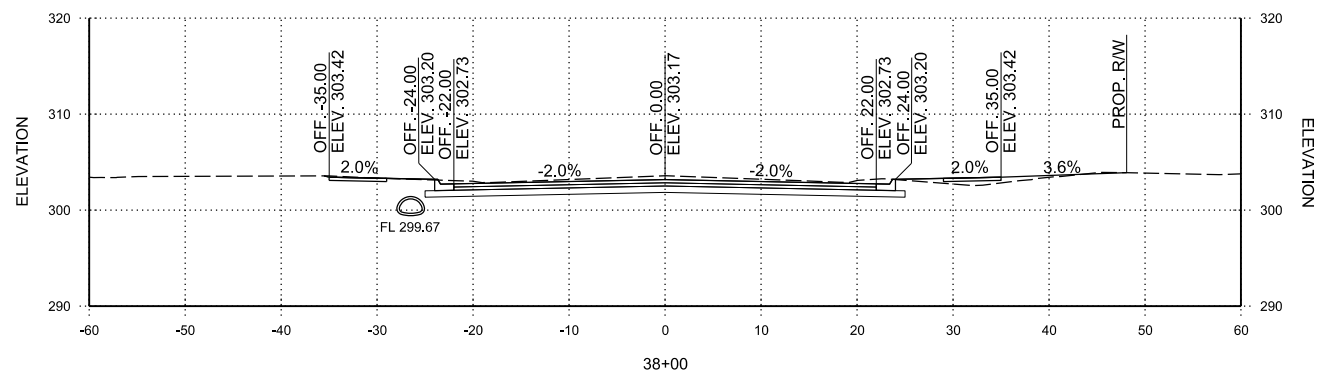
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CX-7

SHEET NUMBER
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**DONAGHEY AVE.
 STA. 36+67 TO STA. 39+50**

STA. 36+67.00 CONSTRUCT REVERSE THROAT DROP INLET 22.00' RT. WITH BACK OPENING AND 22" x 14" x 44' R.C. PIPE TO DROP INLET AT STA. 36+67.00 LT. (4' x 5' x H = 2'-8") 22" x 14" R.C. ARCH PIPE (CLASS III) = 44 LIN. FT.

STA. 38+25.00 CONSTRUCT REVERSE THROAT DROP INLET 22.00' RT. WITH 22" x 14" x 44' R.C. PIPE TO DROP INLET AT STA. 38+25.00 LT. (4' x 5' x H = 2'-11") 22" x 14" R.C. ARCH PIPE (CLASS III) = 44 LIN. FT.

STA. 36+67.00 CONSTRUCT TYPE MO DROP INLET 26.50' LT. WITH 29" x 18" x 154' R.C. PIPE TO DROP INLET AT STA. 38+25.00 LT. (5' DIA. x H = 3'-3") 29" x 18" R.C. ARCH PIPE (CLASS III) = 154 LIN. FT.

STA. 38+25.00 CONSTRUCT TYPE MO DROP INLET 26.50' LT. WITH 29" x 18" x 161' R.C. ARCH PIPE TO DROP INLET AT STA. 39+00.00 LT. (5' DIA. x H = 3'-7") 29" x 18" R.C. ARCH PIPE (CLASS III) = 161 LIN. FT.

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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
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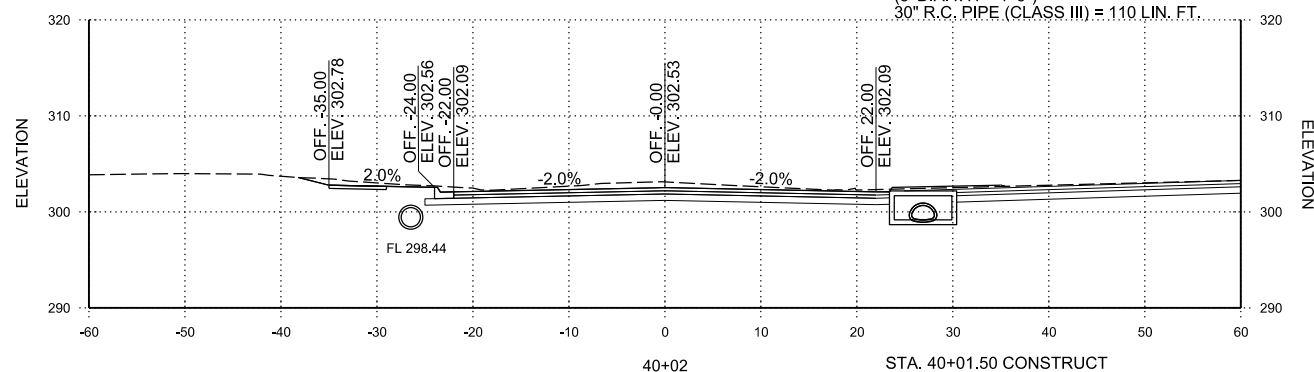
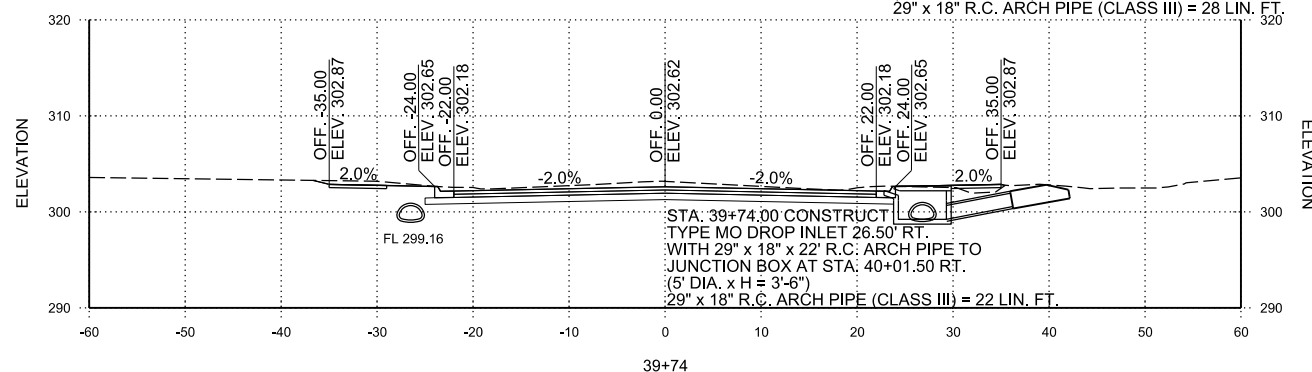
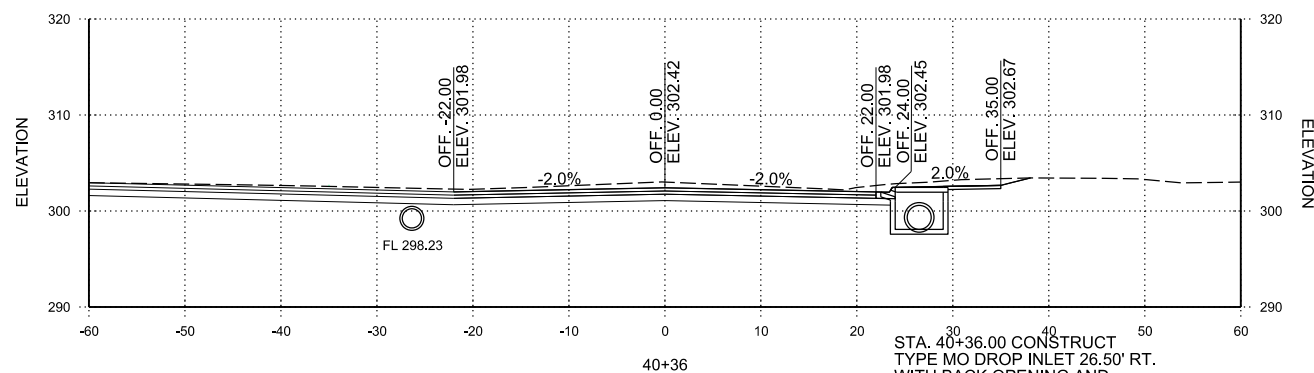
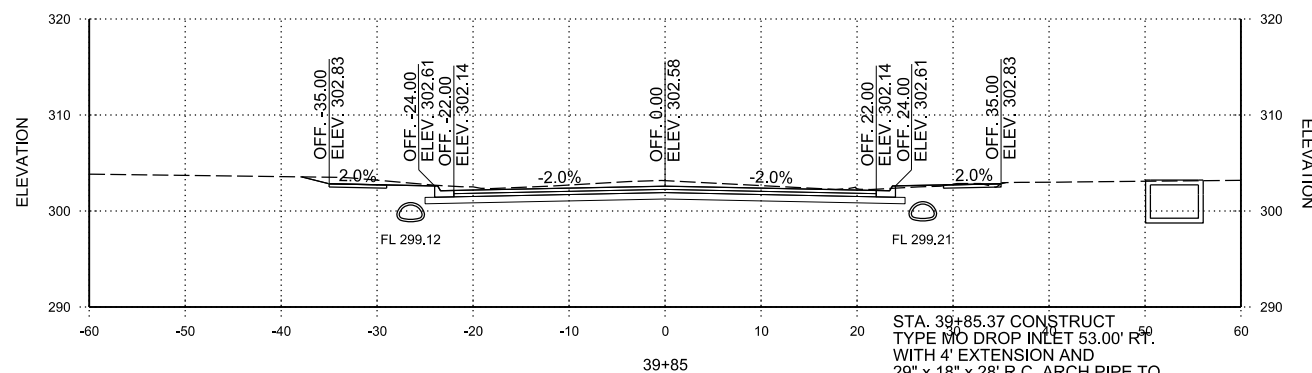
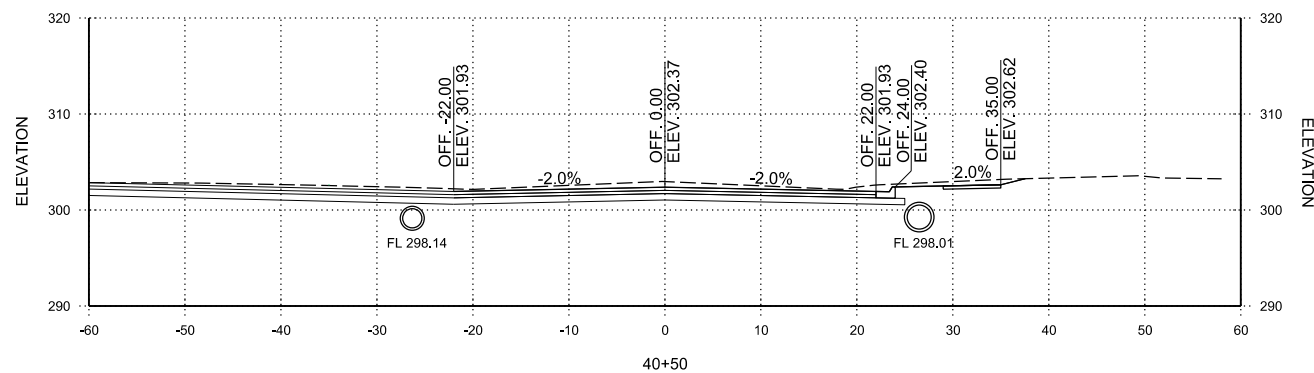
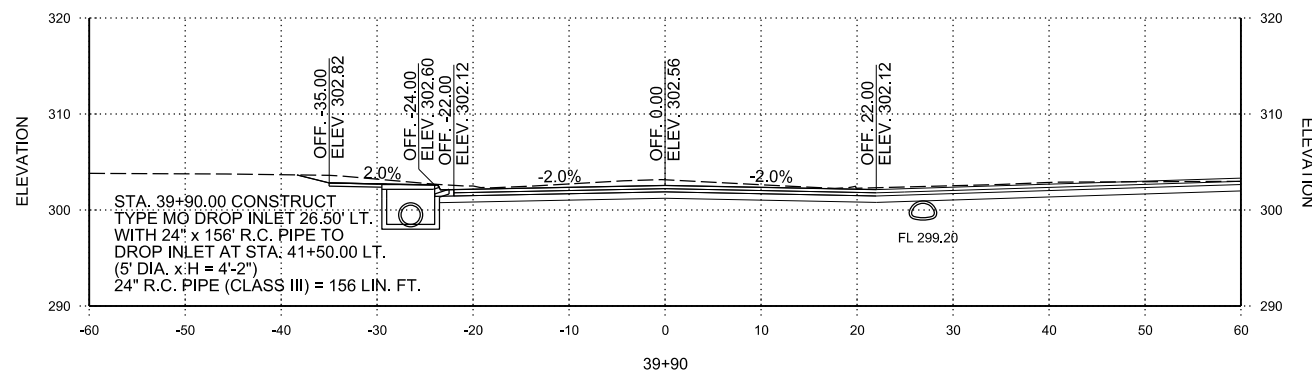
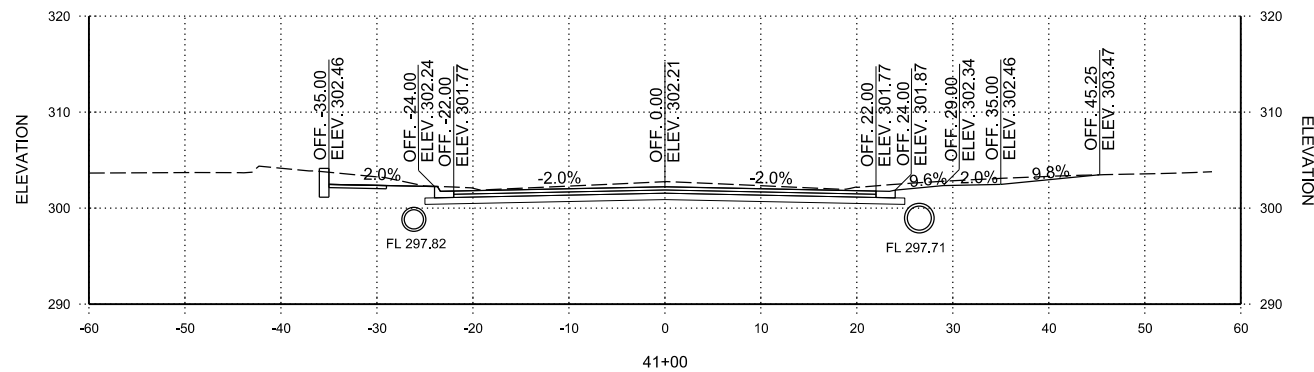
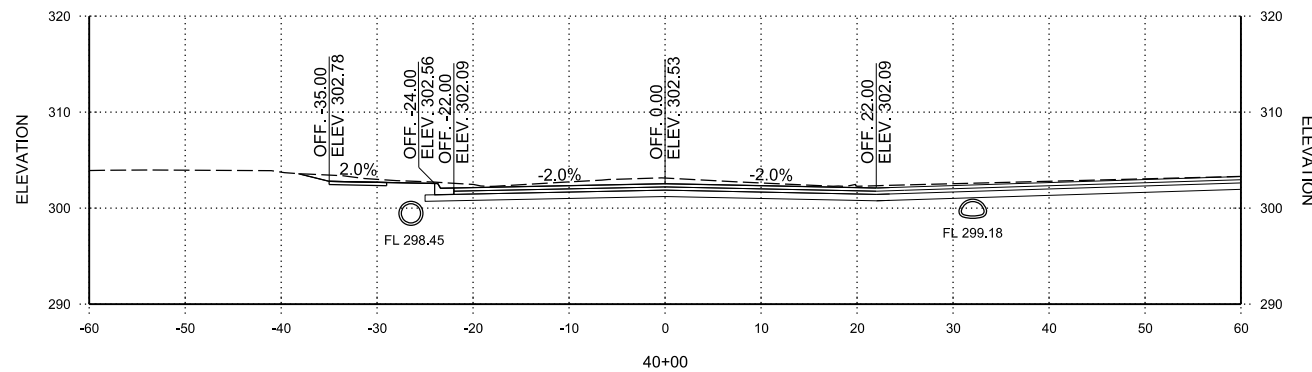
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DONAGHEY AVE.
 STA. 39+74 TO STA. 41+00

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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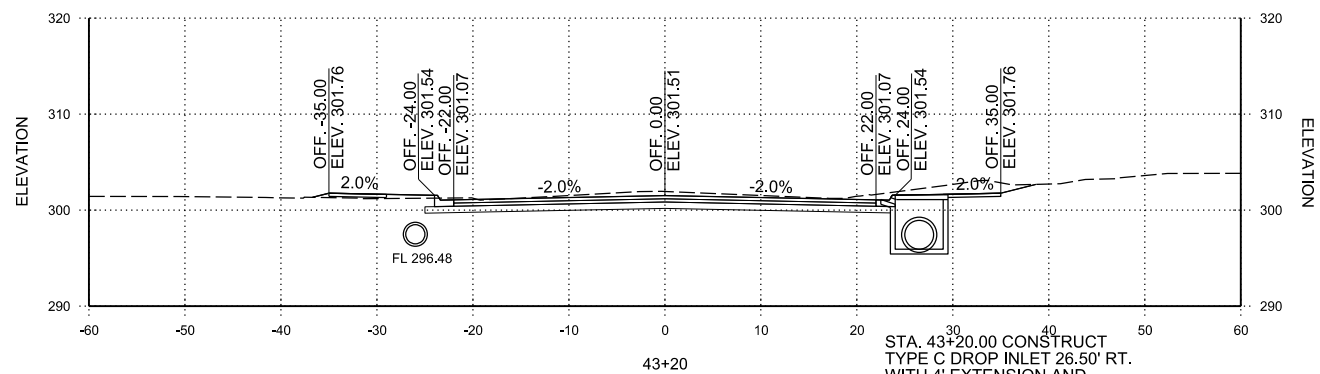
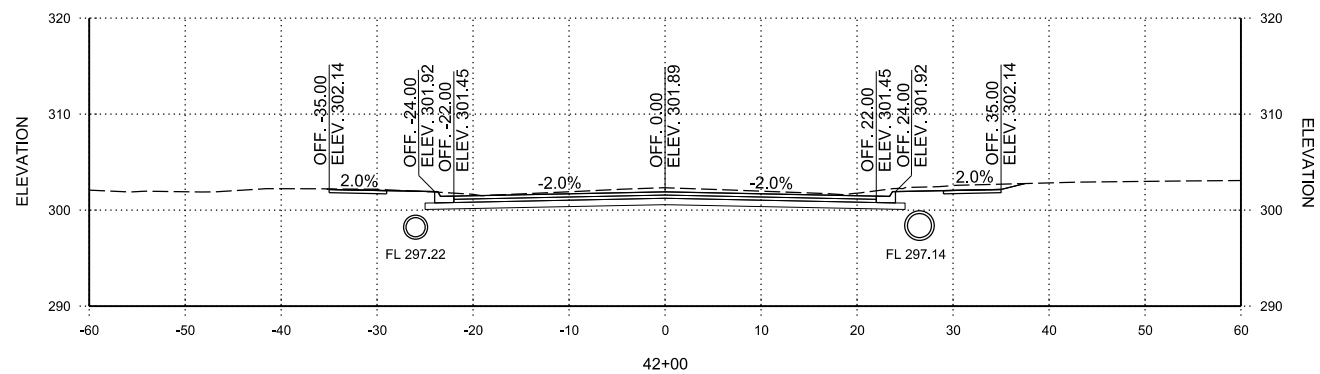
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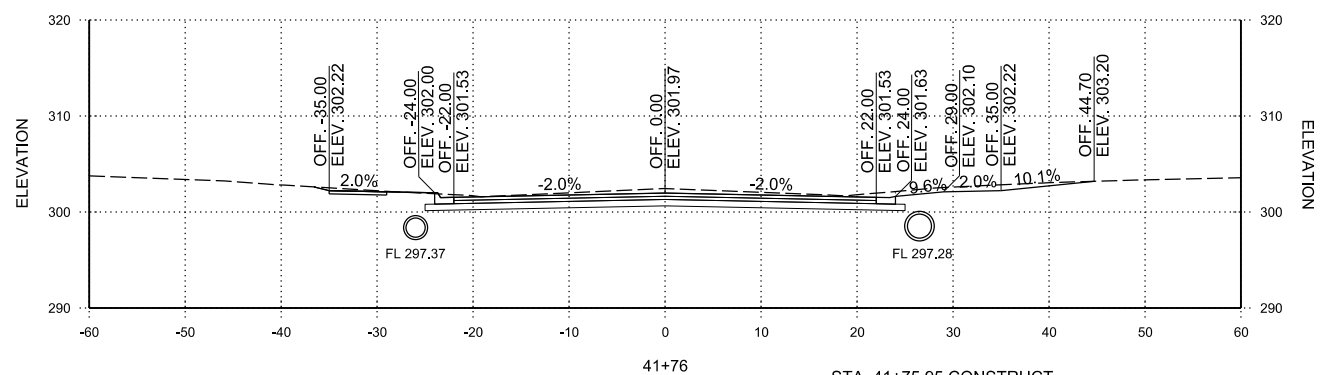
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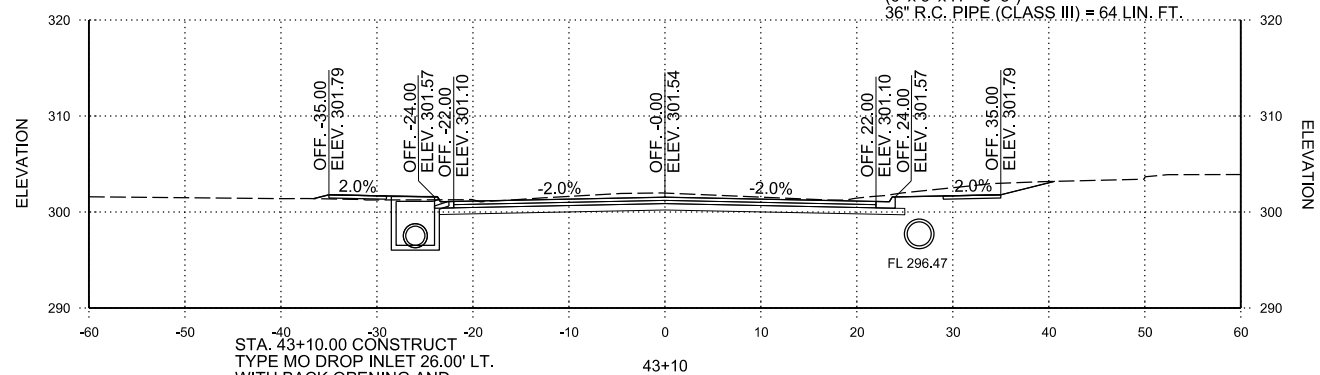
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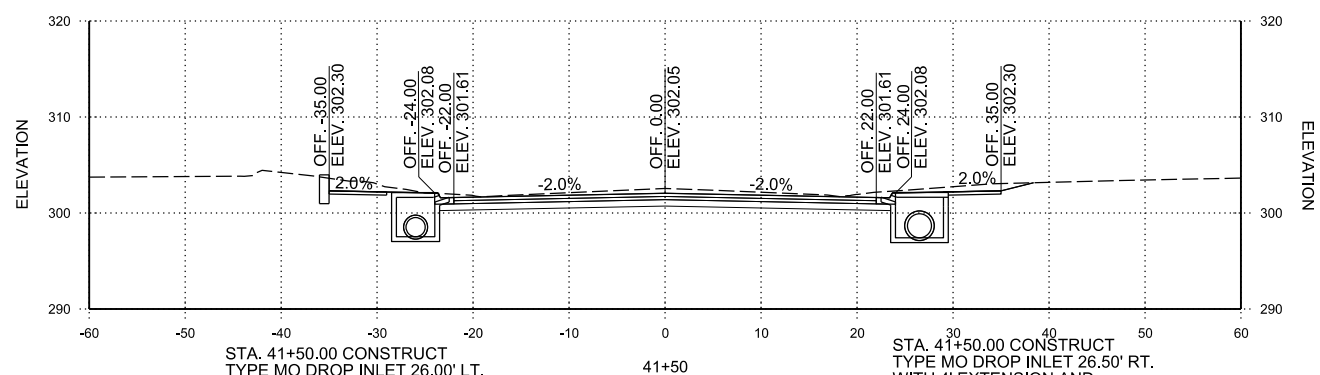
STA. 43+20.00 CONSTRUCT TYPE C DROP INLET 26.50' RT. WITH 4' EXTENSION AND 36" x 64' R.C. PIPE TO DROP INLET AT STA. 43+90.00 RT. (6' x 5' x H = 5'-8") 36" R.C. PIPE (CLASS III) = 64 LIN. FT.



STA. 41+75.95 CONSTRUCT RESIDENTIAL DRIVEWAY RT.

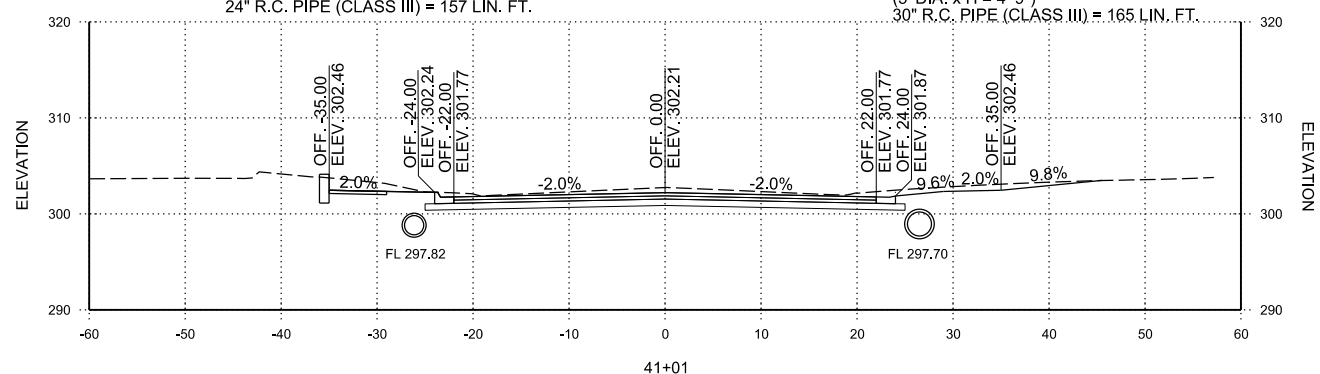
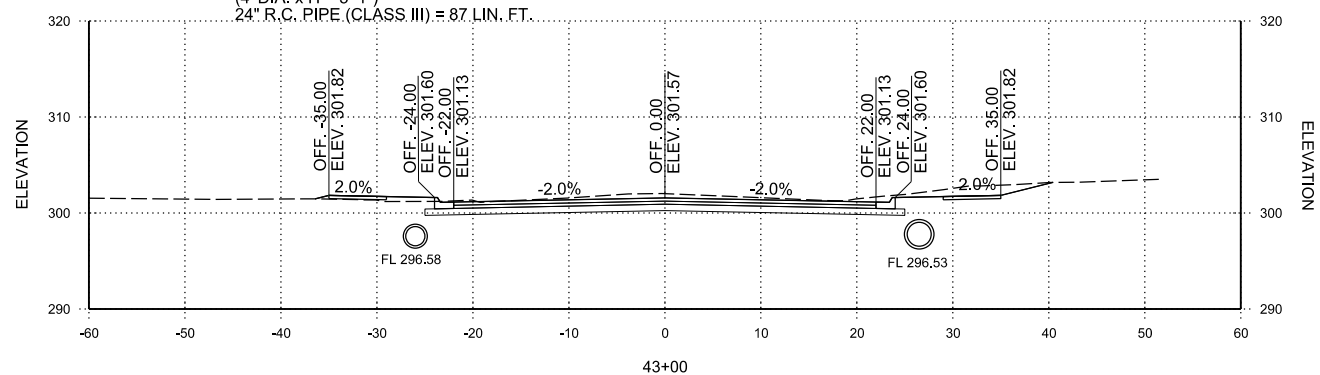


STA. 43+10.00 CONSTRUCT TYPE MO DROP INLET 26.00' LT. WITH BACK OPENING AND 24" x 87' R.C. PIPE TO DROP INLET AT STA. 44+00.00 LT. (4' DIA. x H = 5'-1") 24" R.C. PIPE (CLASS III) = 87 LIN. FT.

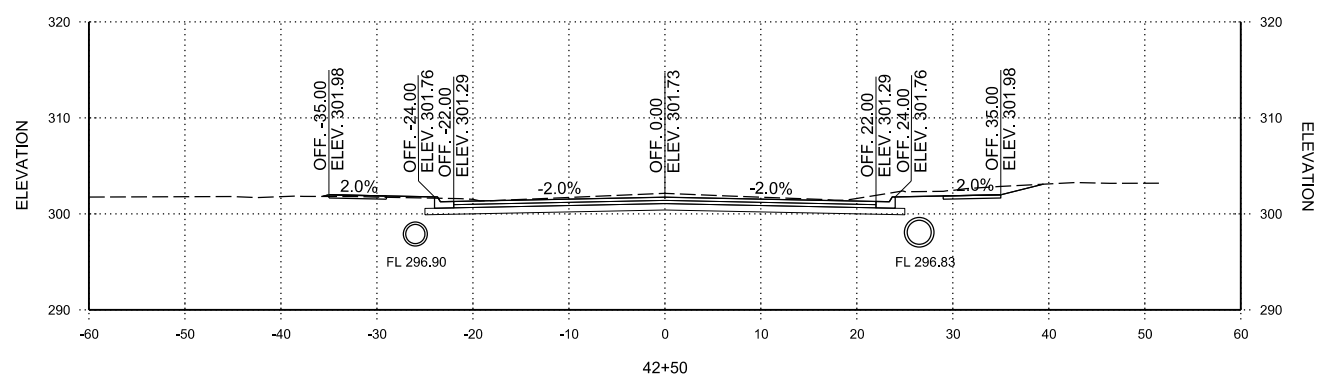


STA. 41+50.00 CONSTRUCT TYPE MO DROP INLET 26.00' LT. WITH 24" x 157' R.C. PIPE TO DROP INLET AT STA. 43+10.00 LT. (4' DIA. x H = 4'-7") 24" R.C. PIPE (CLASS III) = 157 LIN. FT.

STA. 41+50.00 CONSTRUCT TYPE MO DROP INLET 26.50' RT. WITH 4' EXTENSION AND 30" x 165' R.C. ARCH PIPE TO DROP INLET AT STA. 43+20.00 RT. (5' DIA. x H = 4'-9") 30" R.C. PIPE (CLASS III) = 165 LIN. FT.



STA. 41+00.94 CONSTRUCT RESIDENTIAL DRIVEWAY RT.



DONAGHEY AVE.
 STA. 41+01 TO STA. 43+20

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

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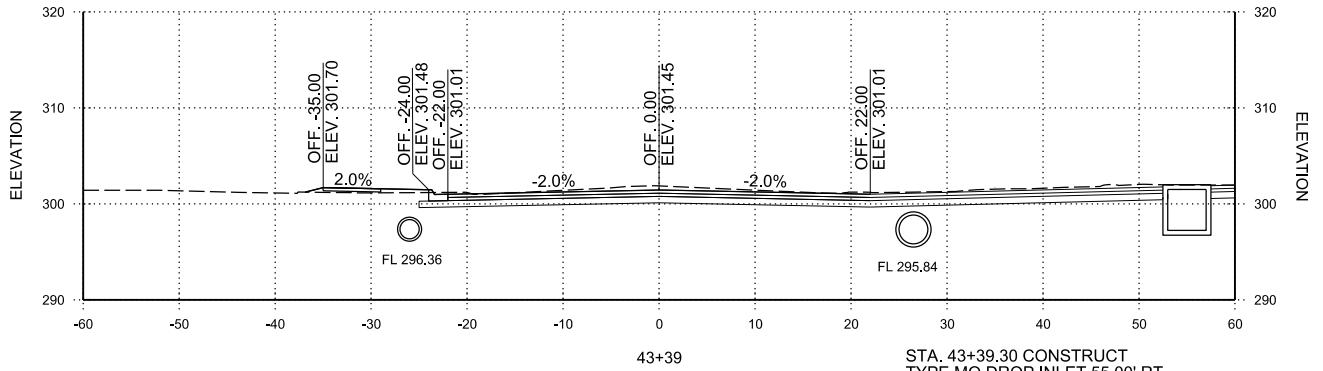
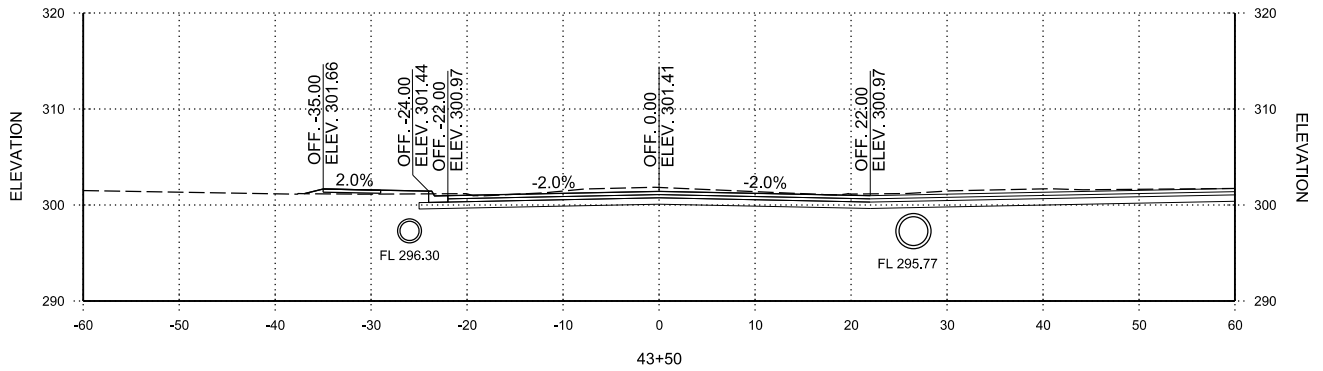
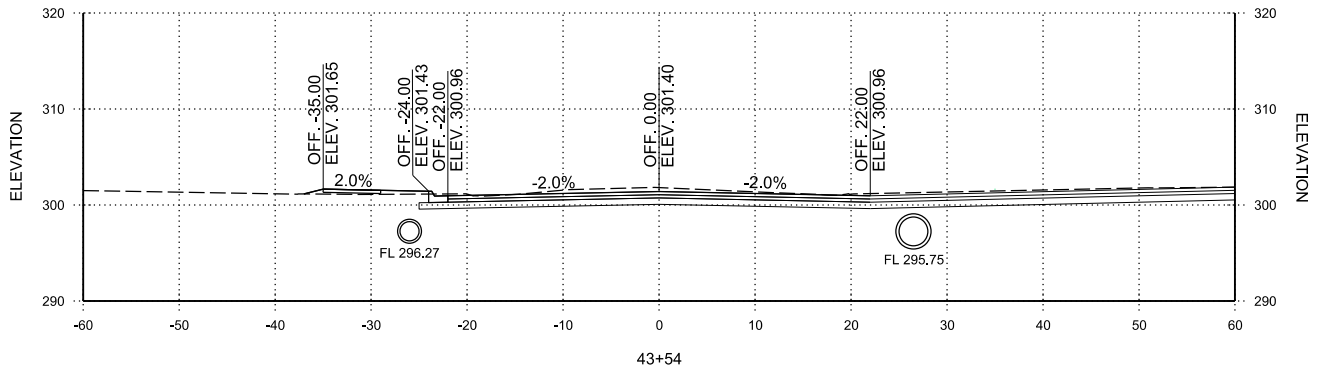
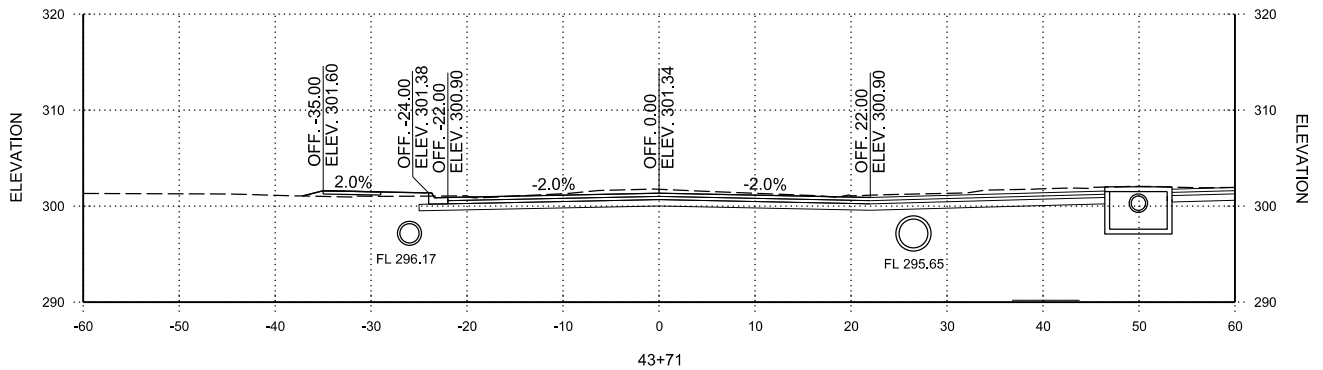
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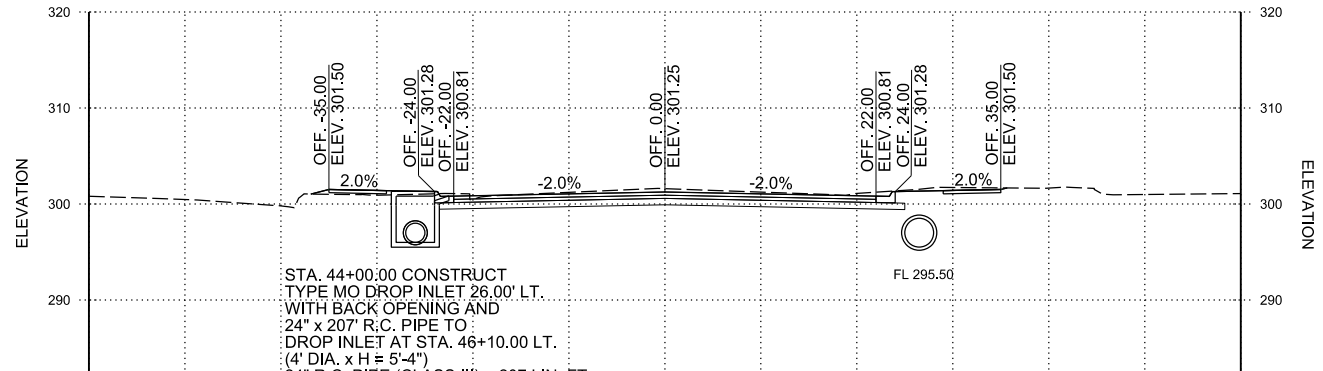
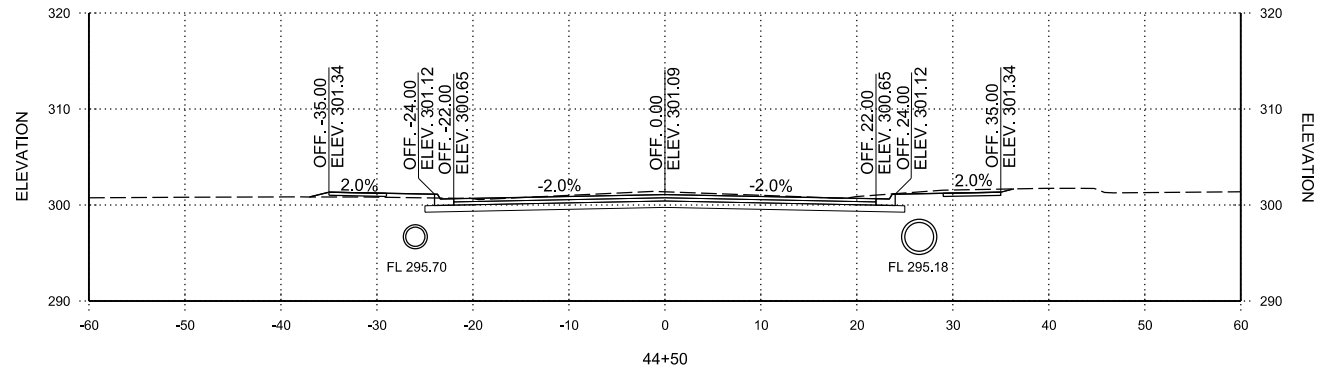


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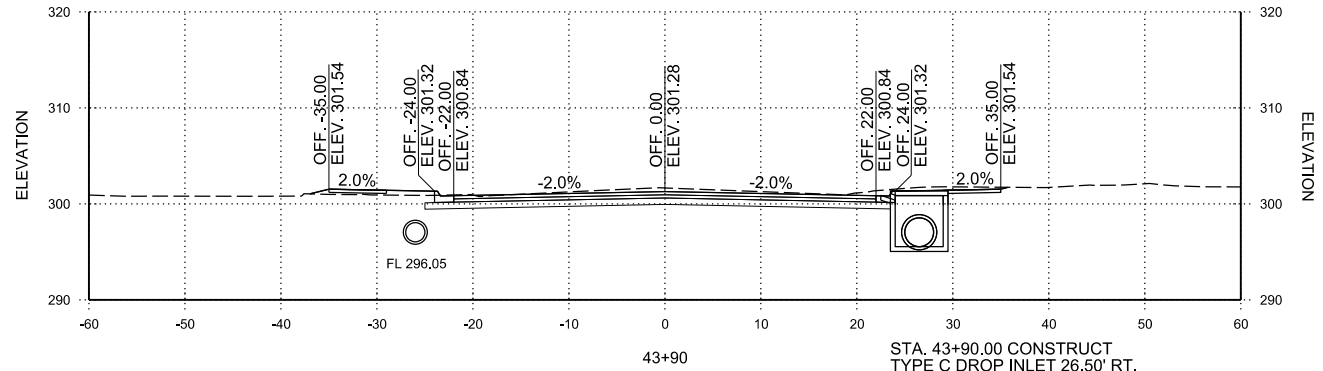
STA. 43+71.11 CONSTRUCT
 TYPE C DROP INLET 50.00' RT.
 WITH 4' EXTENSION AND
 24" x 26" R.C. PIPE TO
 DROP INLET AT STA. 43+90.00 RT.
 AND CONNECT TO EXISTING 18" R.C. PIPE
 (6' x 6" x H = 4'-4")
 24" R.C. PIPE (CLASS III) = 26 LIN. FT.



STA. 43+39.30 CONSTRUCT
 TYPE MO DROP INLET 55.00' RT.
 WITH 4' EXTENSION AND
 24" x 30" R.C. PIPE TO
 DROP INLET AT STA. 43+20.00 RT.
 AND CONNECT TO EXISTING 18" R.C. PIPE
 (4' DIA. x H = 4'-9")
 24" R.C. PIPE (CLASS III) = 30 LIN. FT.



STA. 44+00.00 CONSTRUCT
 TYPE MO DROP INLET 26.00' LT.
 WITH BACK OPENING AND
 24" x 20" R.C. PIPE TO
 DROP INLET AT STA. 46+10.00 LT.
 (4' DIA. x H = 5'-4")
 24" R.C. PIPE (CLASS III) = 207 LIN. FT.



STA. 43+90.00 CONSTRUCT
 TYPE C DROP INLET 26.50' RT.
 WITH 36" x 111" R.C. PIPE TO
 DROP INLET AT STA. 45+00.00 RT.
 (6' x 5' x H = 5'-10")
 36" R.C. PIPE (CLASS III) = 111 LIN. FT.

DONAGHEY AVE.
 STA. 43+39 TO STA. 44+50

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

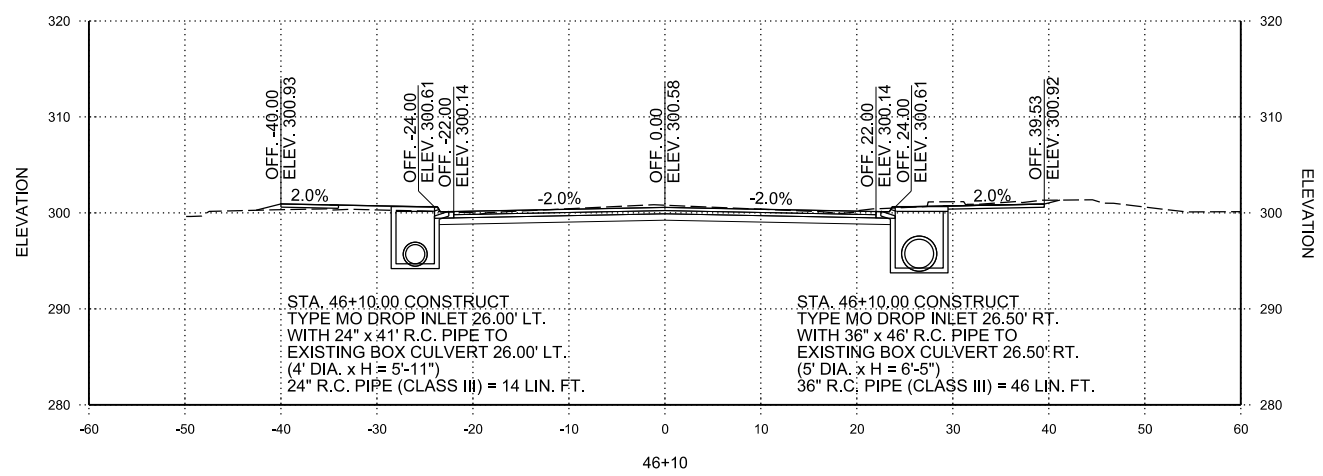
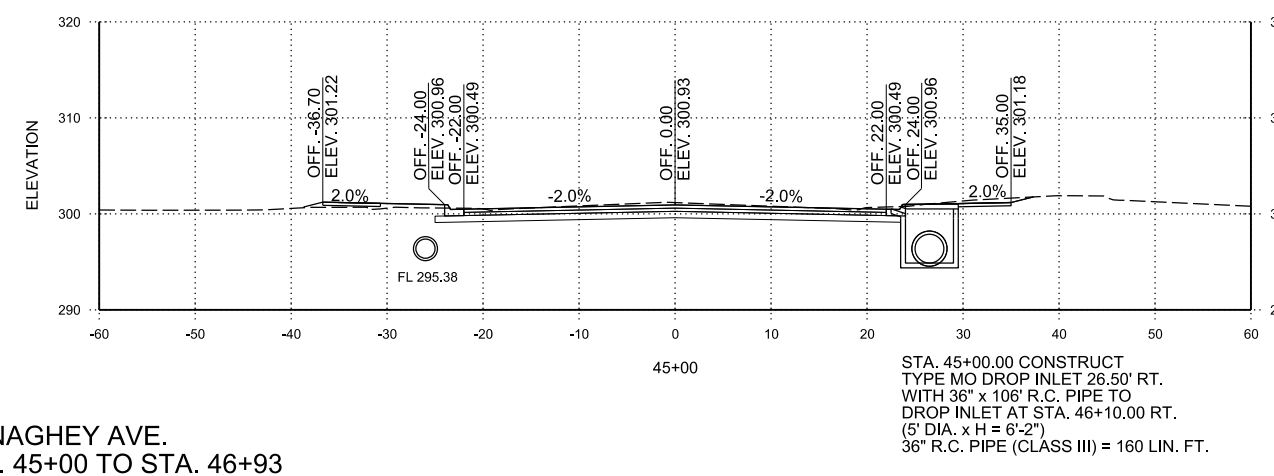
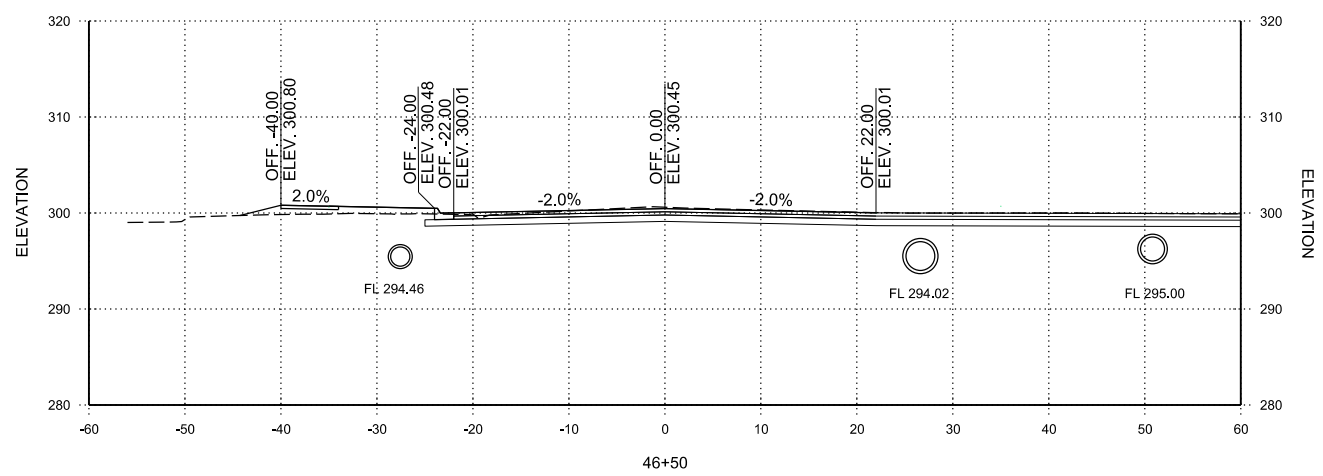
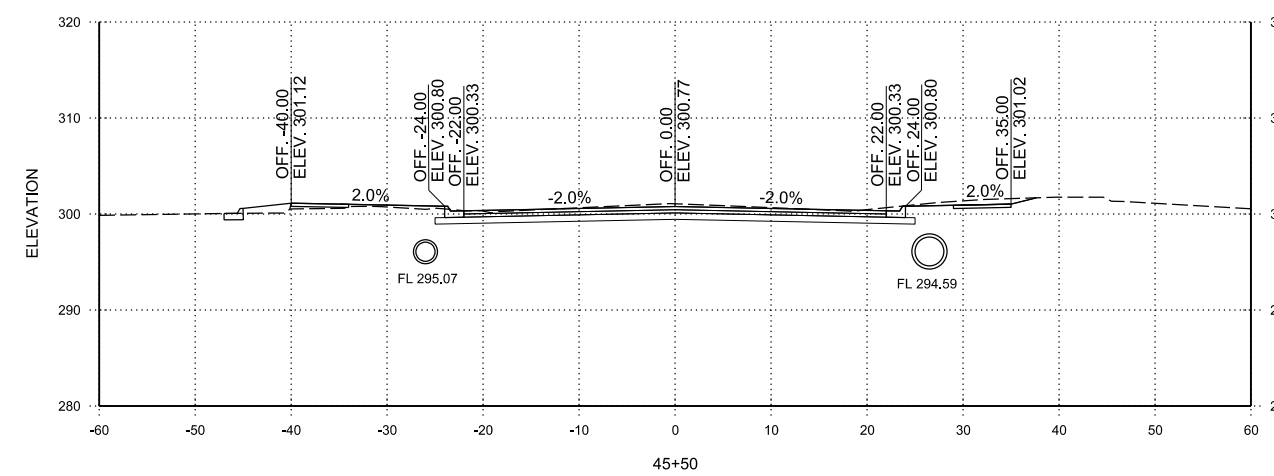
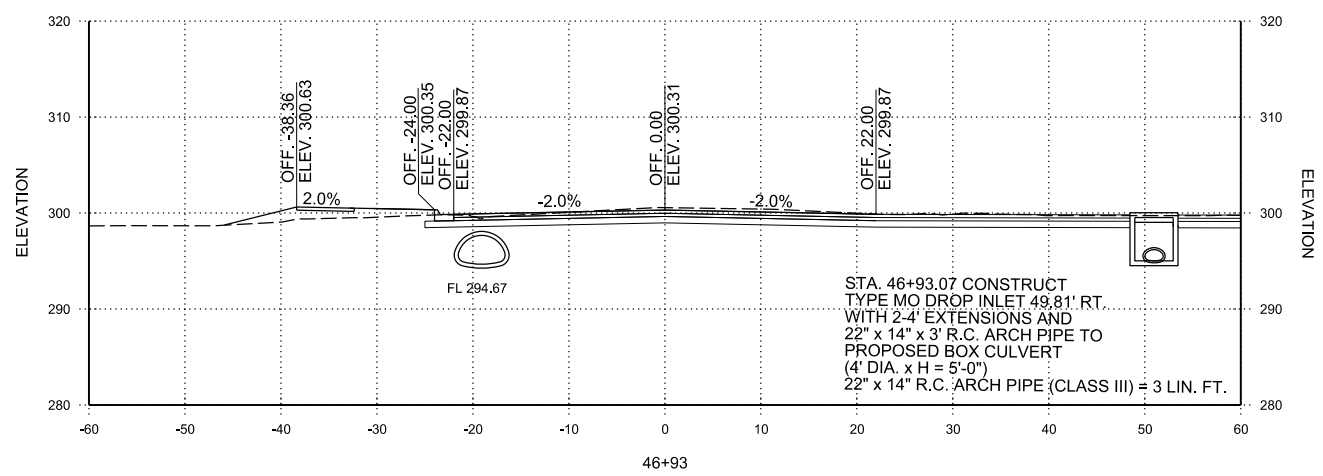
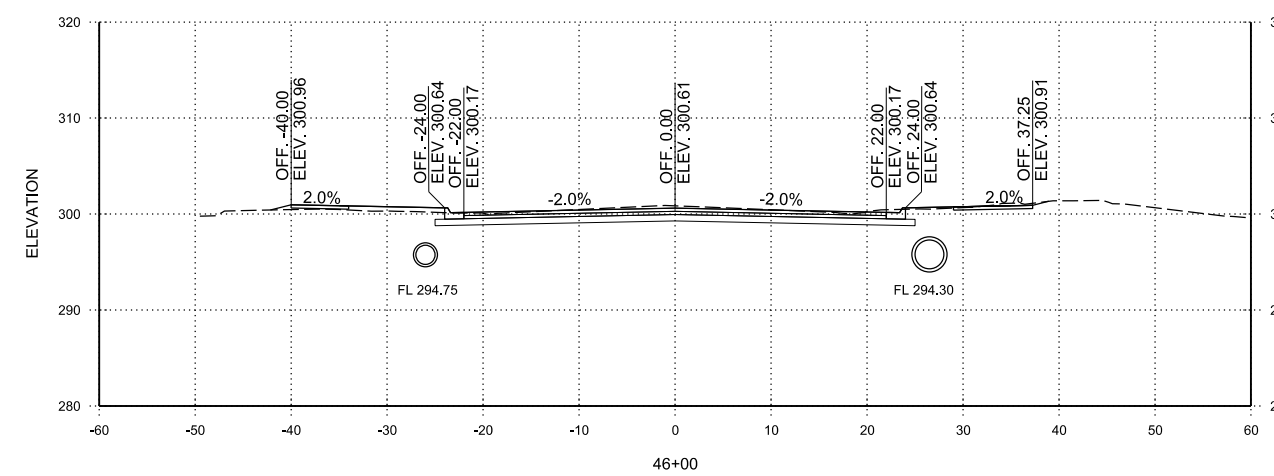
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SHEET NUMBER
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STA. 45+00.00 CONSTRUCT
 TYPE MO DROP INLET 26.50' RT.
 WITH 36" x 106' R.C. PIPE TO
 DROP INLET AT STA. 46+10.00 RT.
 (5' DIA. x H = 6'-2")
 36" R.C. PIPE (CLASS III) = 160 LIN. FT.

STA. 46+10.00 CONSTRUCT
 TYPE MO DROP INLET 26.00' LT.
 WITH 24" x 41' R.C. PIPE TO
 EXISTING BOX CULVERT 26.00' LT.
 (4' DIA. x H = 5'-11")
 24" R.C. PIPE (CLASS III) = 14 LIN. FT.

STA. 46+10.00 CONSTRUCT
 TYPE MO DROP INLET 26.50' RT.
 WITH 36" x 46' R.C. PIPE TO
 EXISTING BOX CULVERT 26.50' RT.
 (5' DIA. x H = 6'-5")
 36" R.C. PIPE (CLASS III) = 46 LIN. FT.

DONAGHEY AVE.
 STA. 45+00 TO STA. 46+93

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

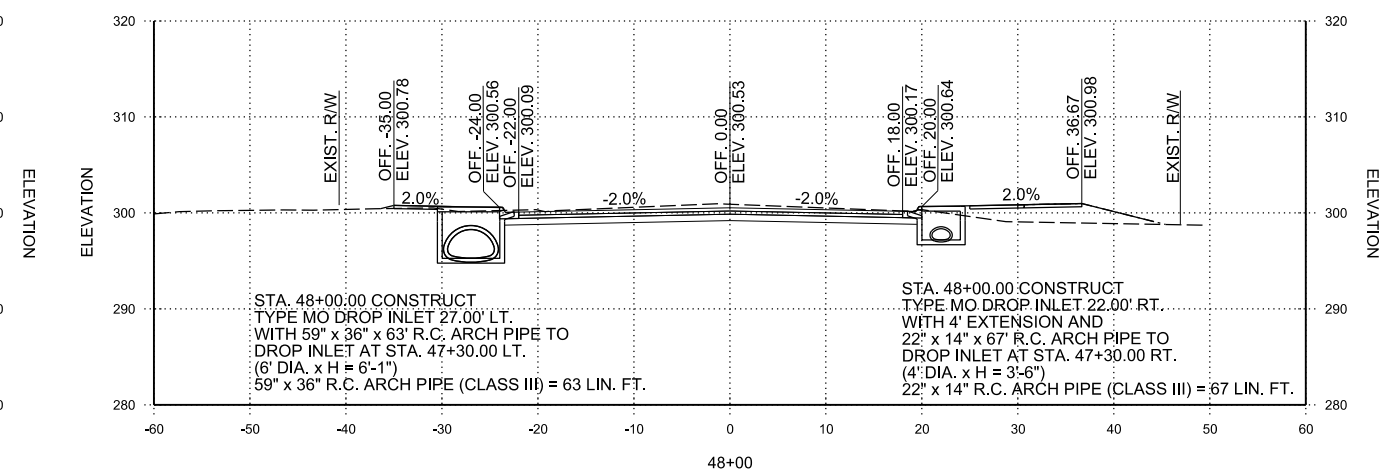
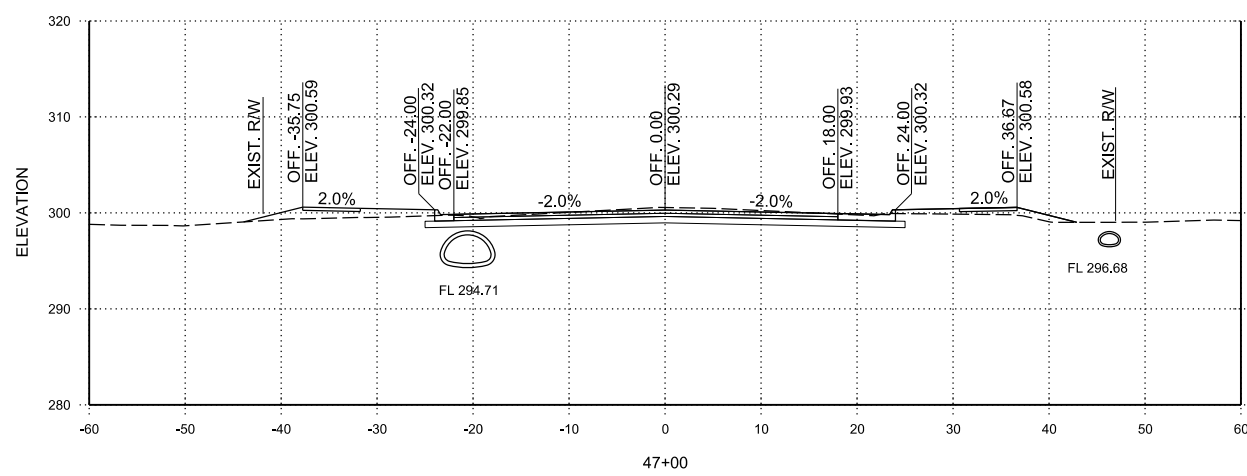
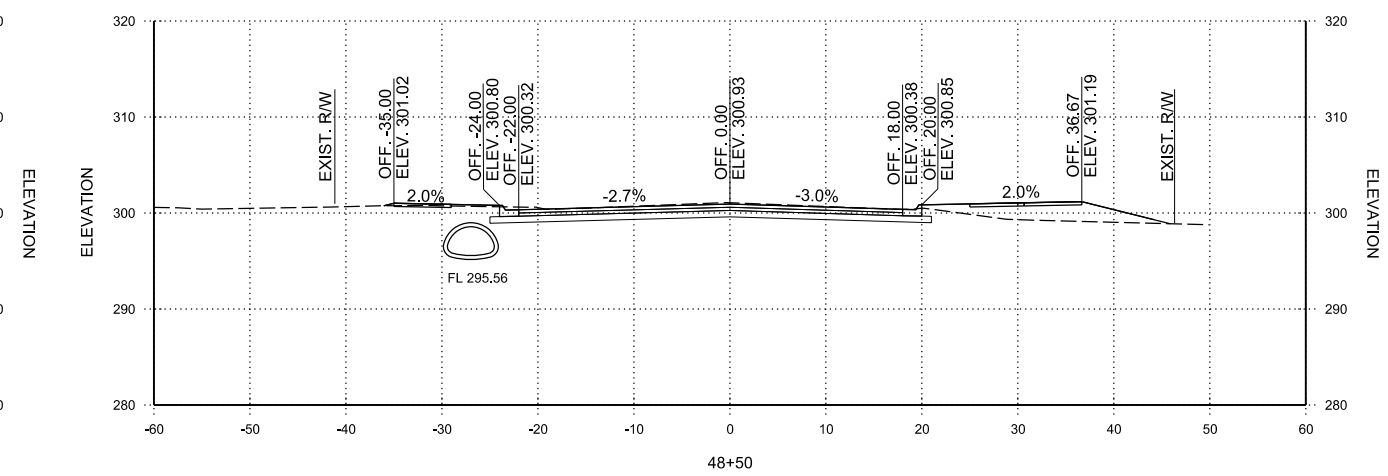
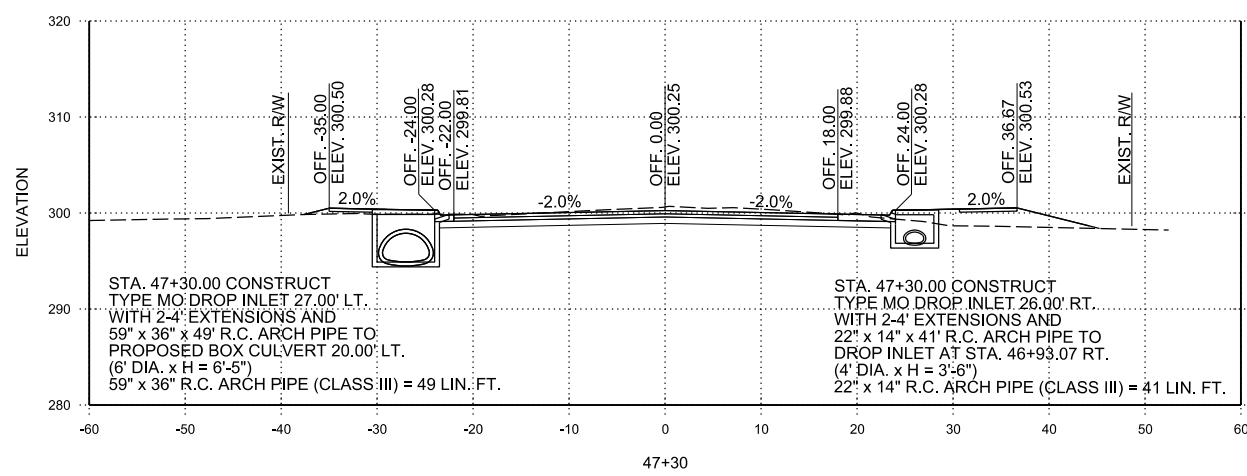
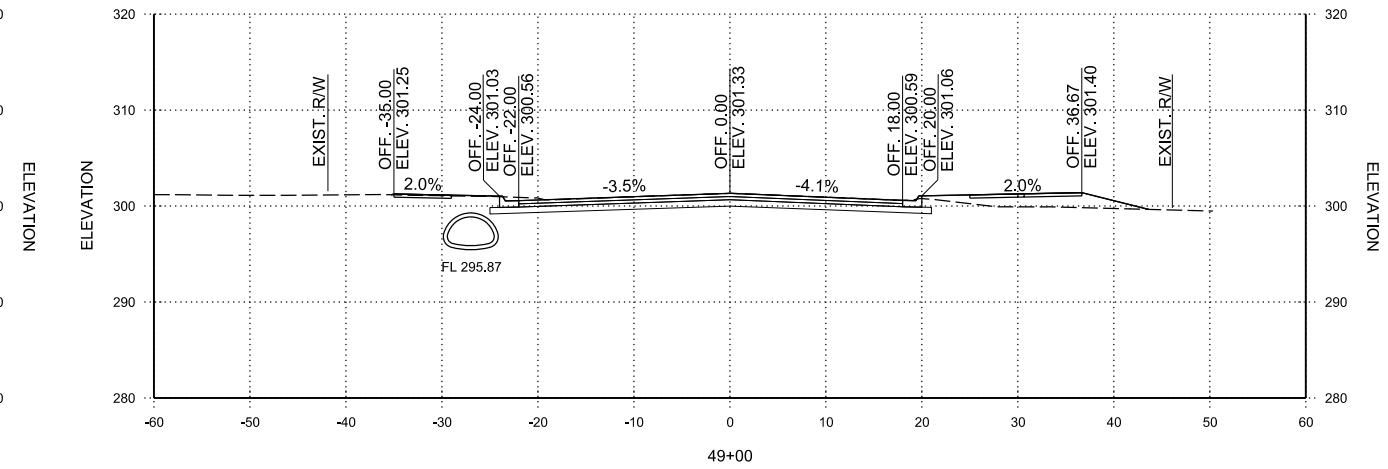
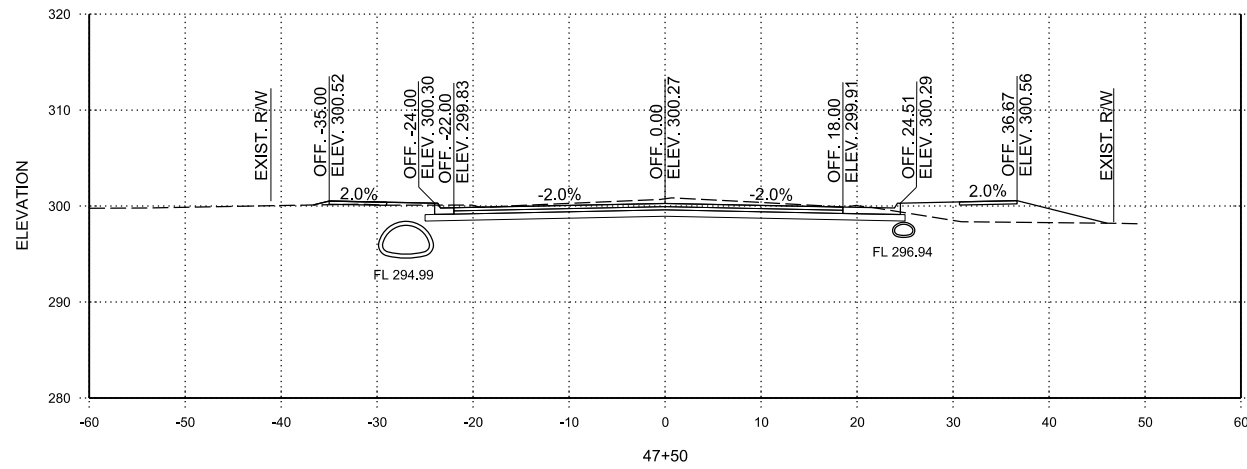
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DONAGHEY AVE.
 STA. 47+00 TO STA. 49+00

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
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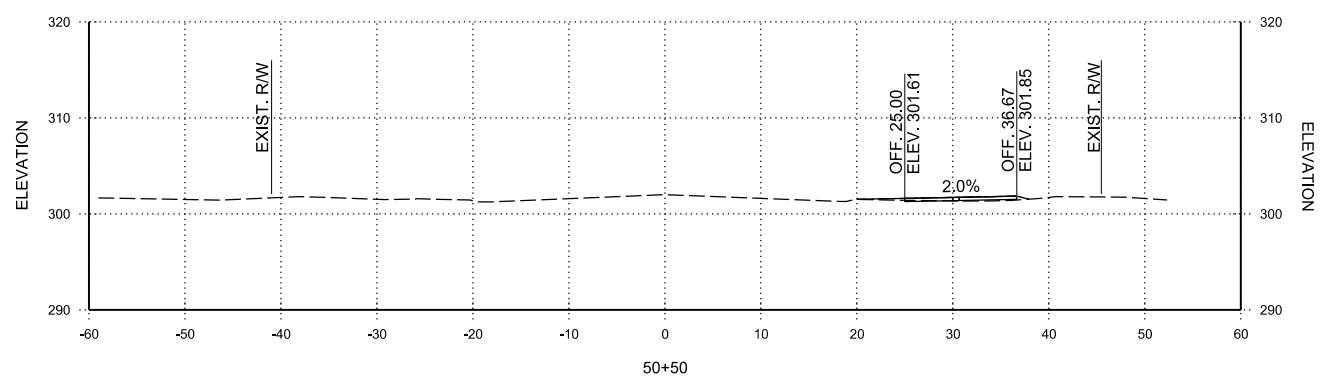
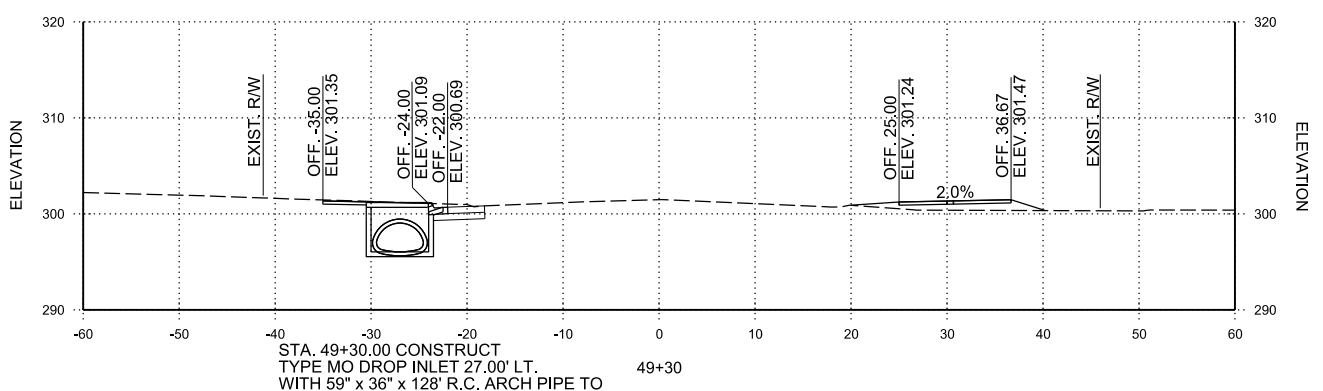
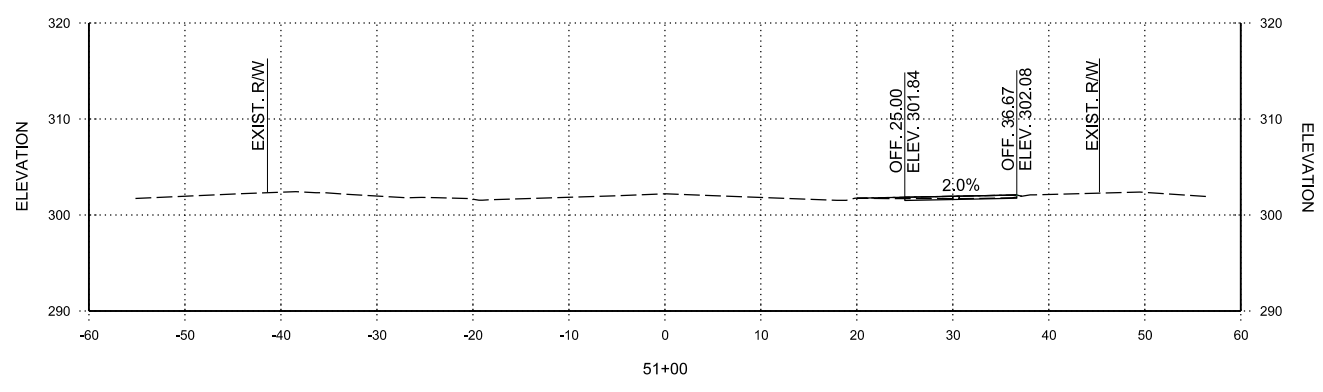
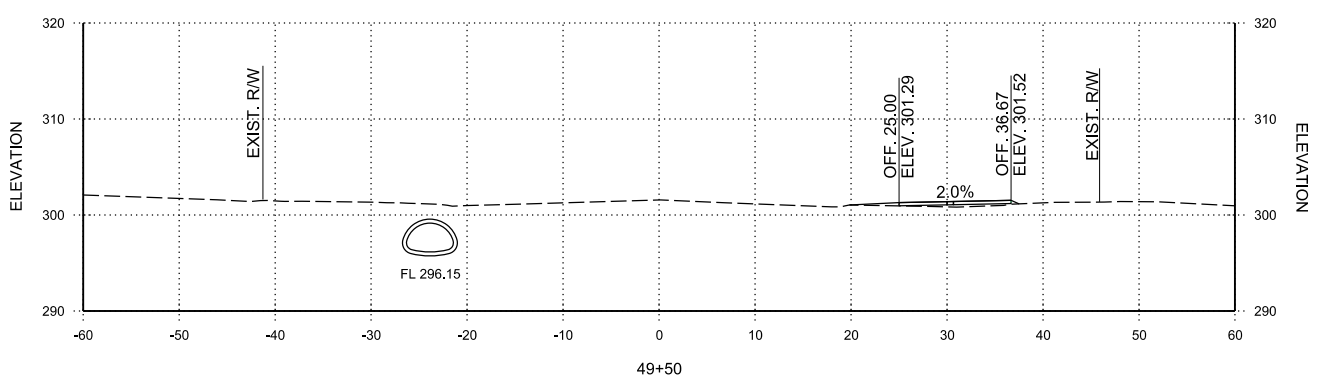
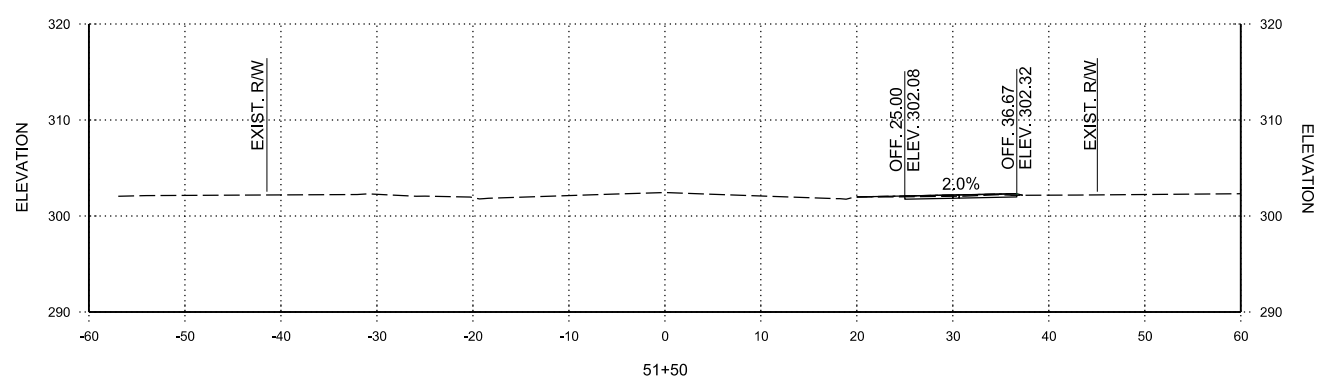
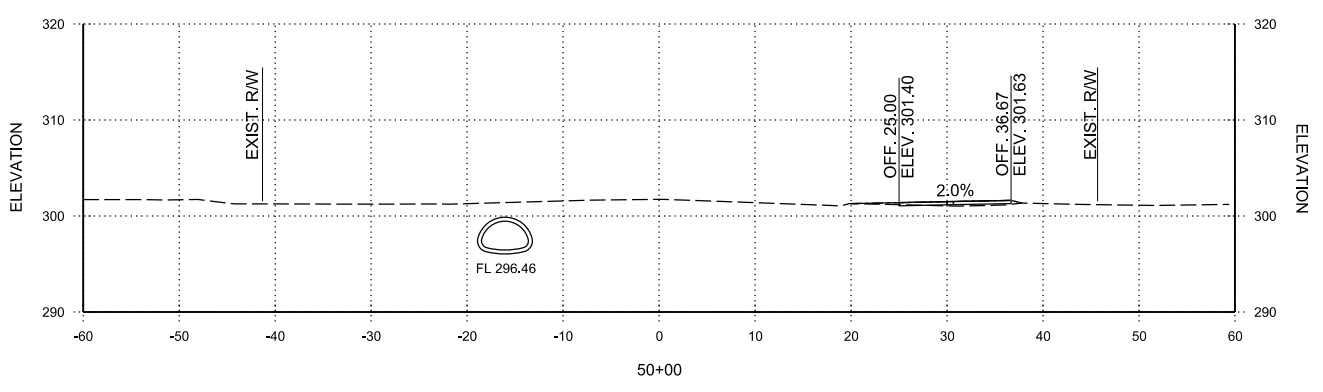
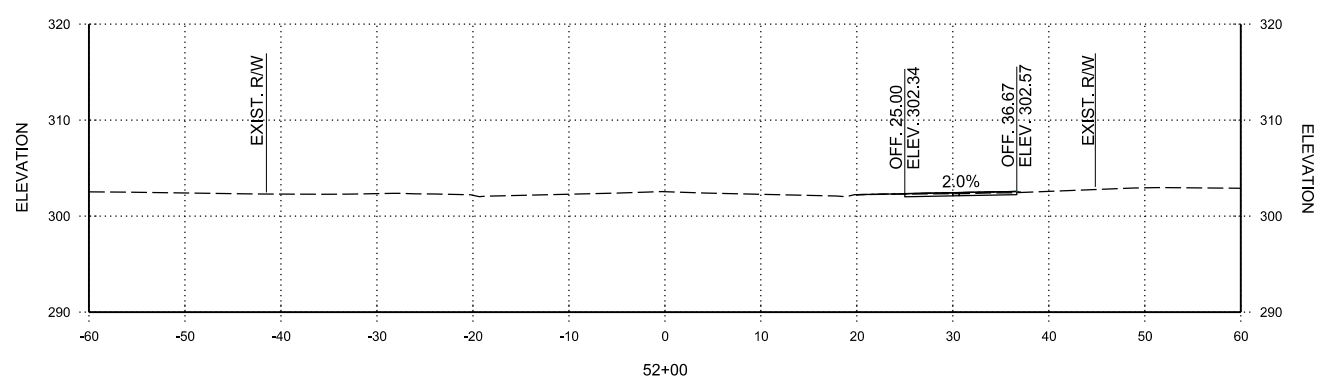
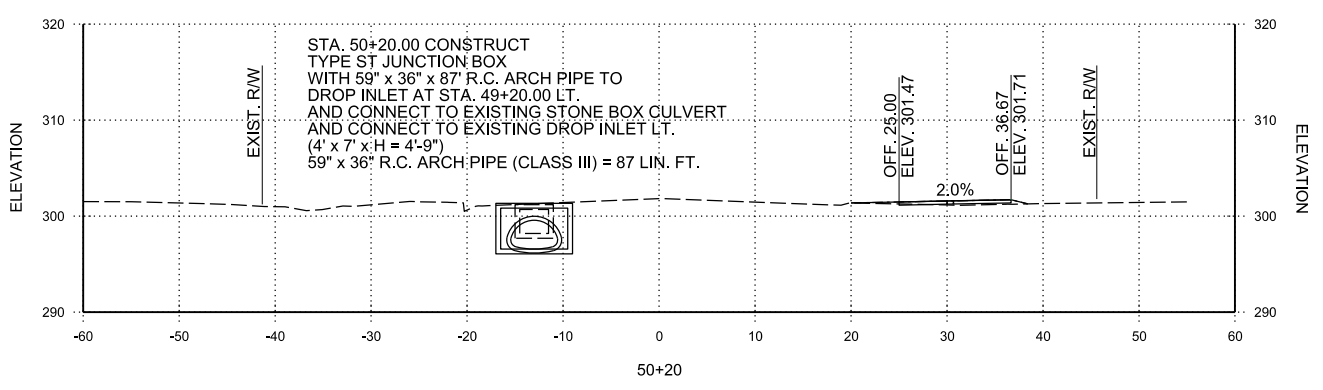
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**DONAGHEY AVE.
 STA. 49+30 TO STA. 52+00**

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
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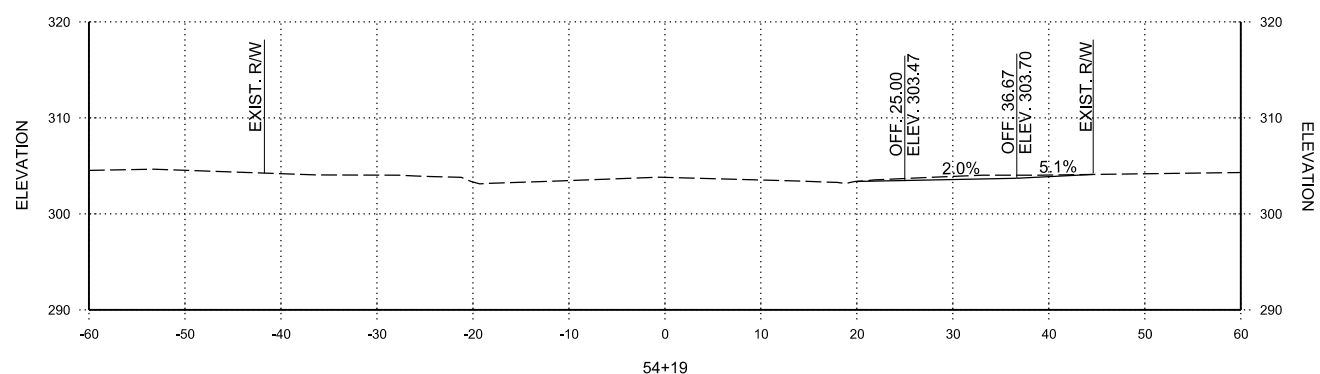
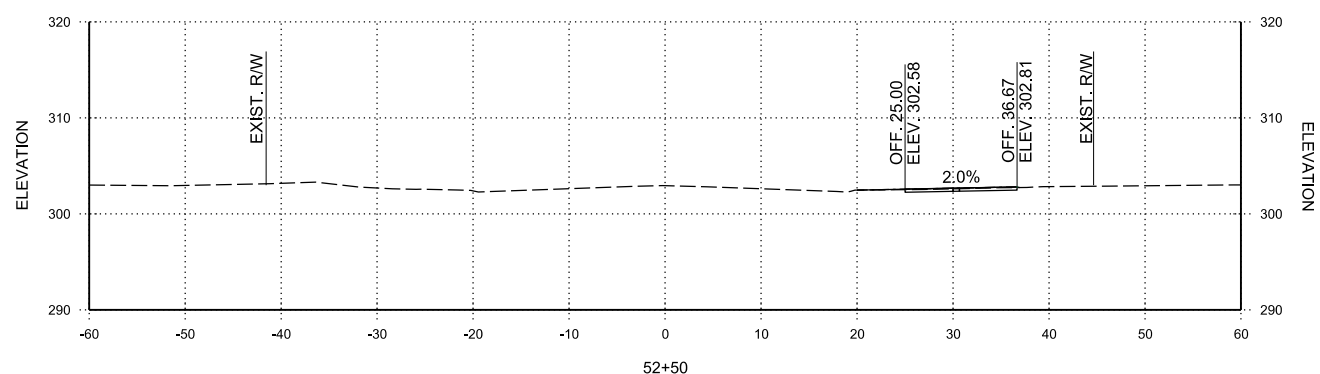
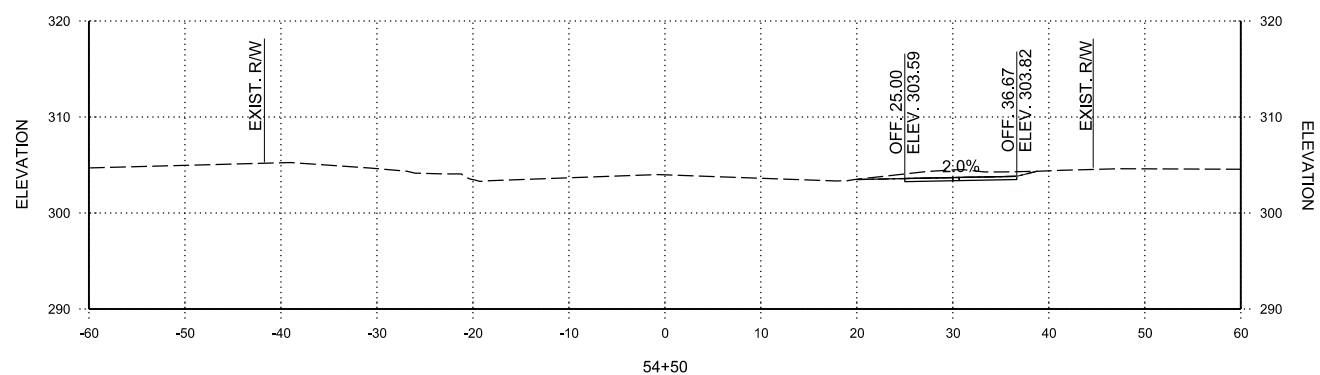
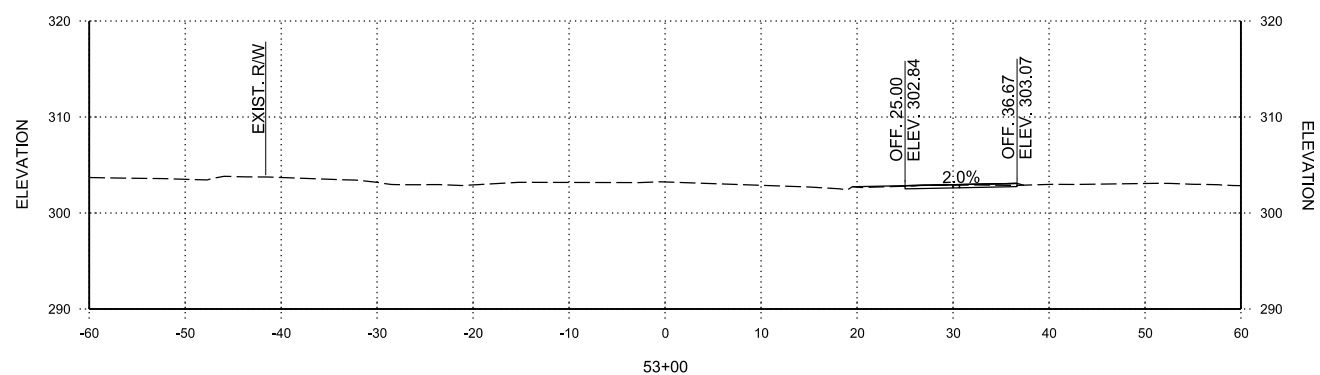
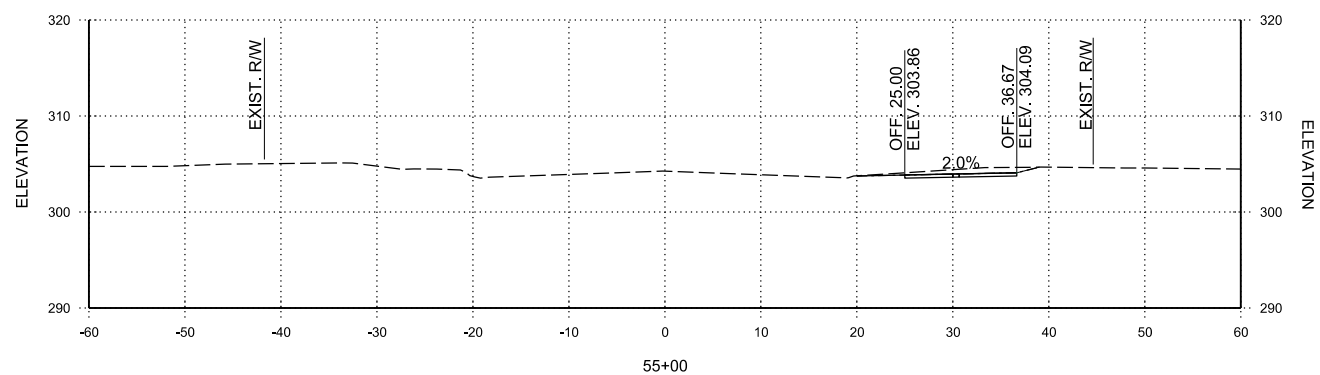
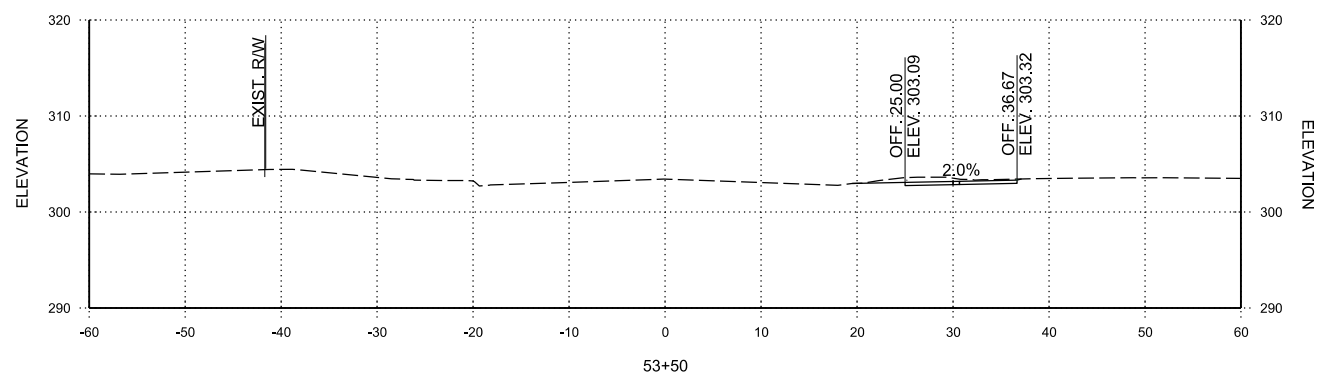
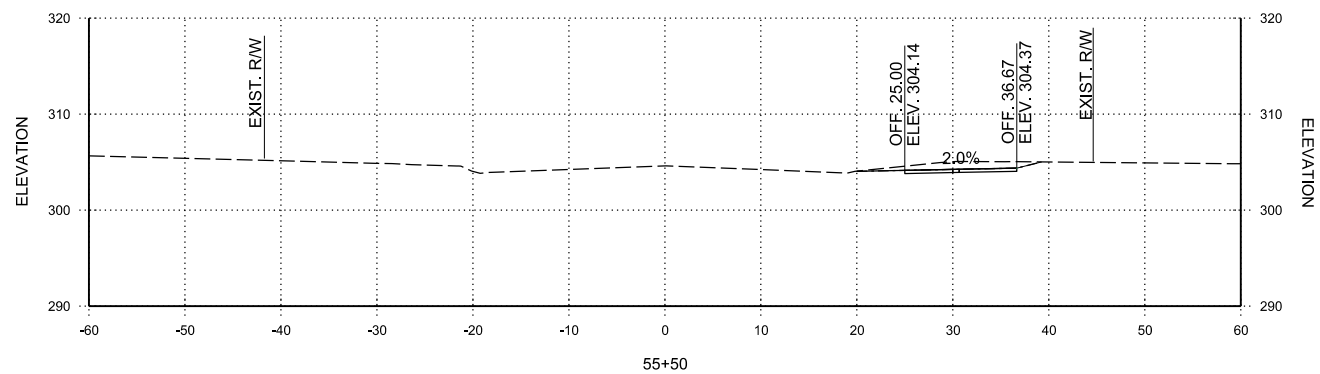
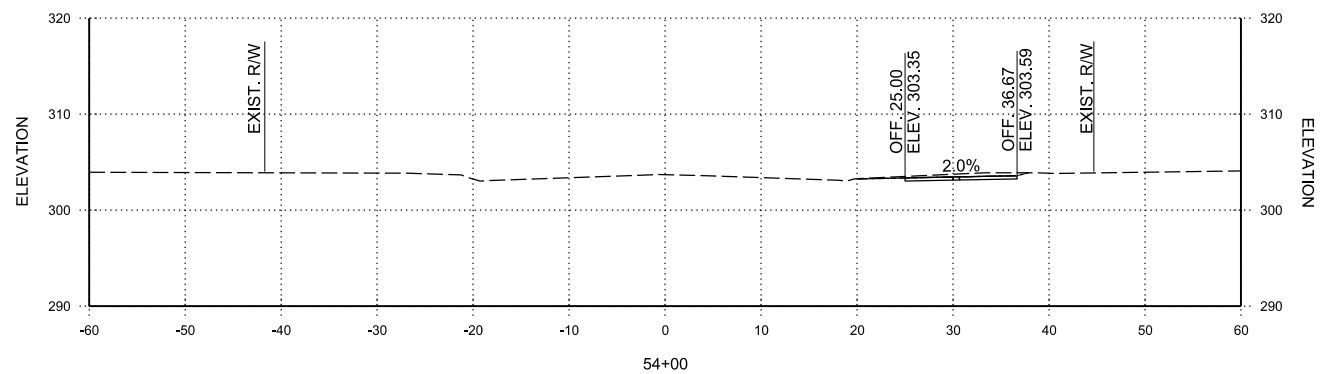
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DONAGHEY AVE.
 STA. 52+50 TO STA. 55+50

STA. 54+18.66 CONSTRUCT
 RESIDENTIAL DRIVEWAY RT.

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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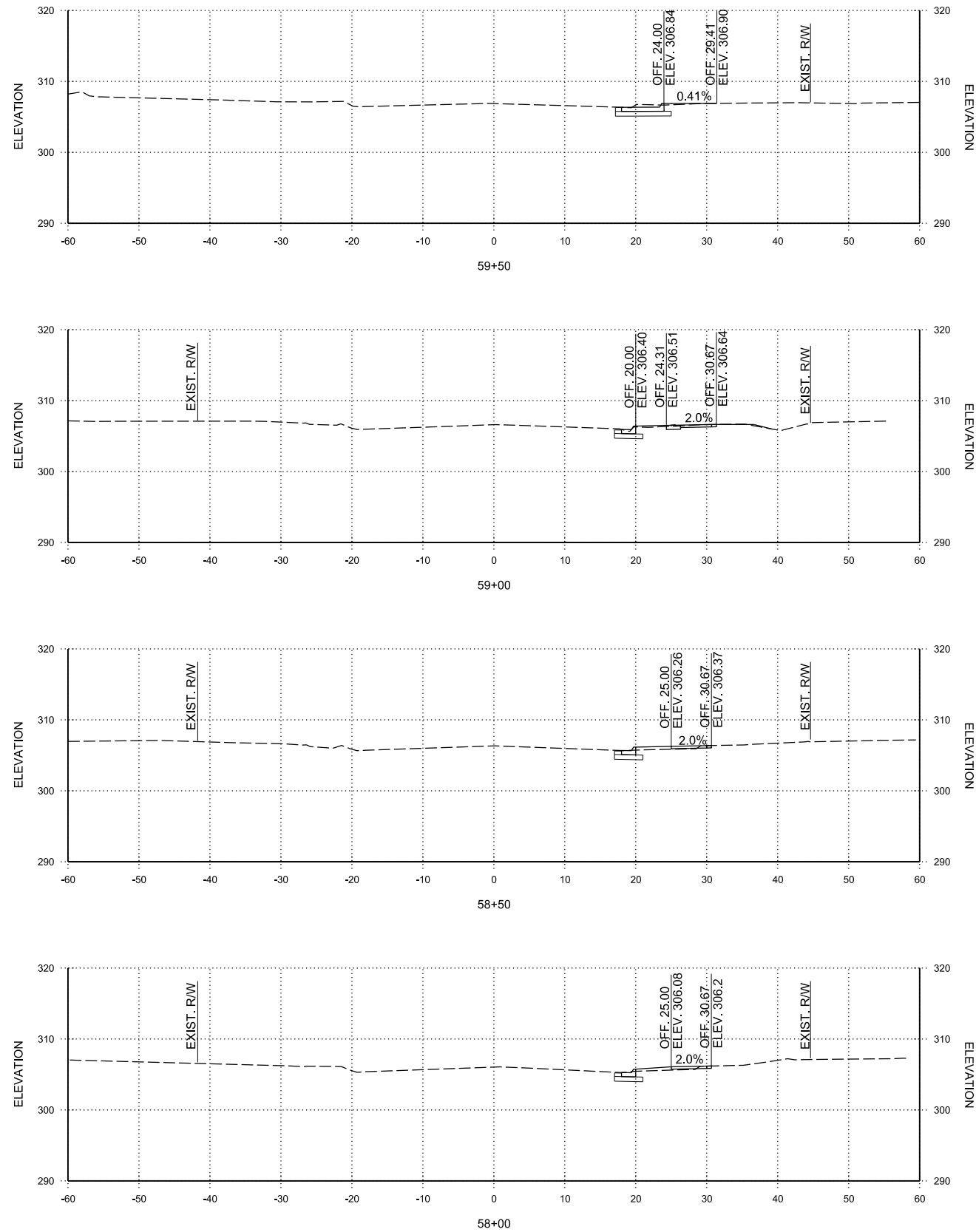
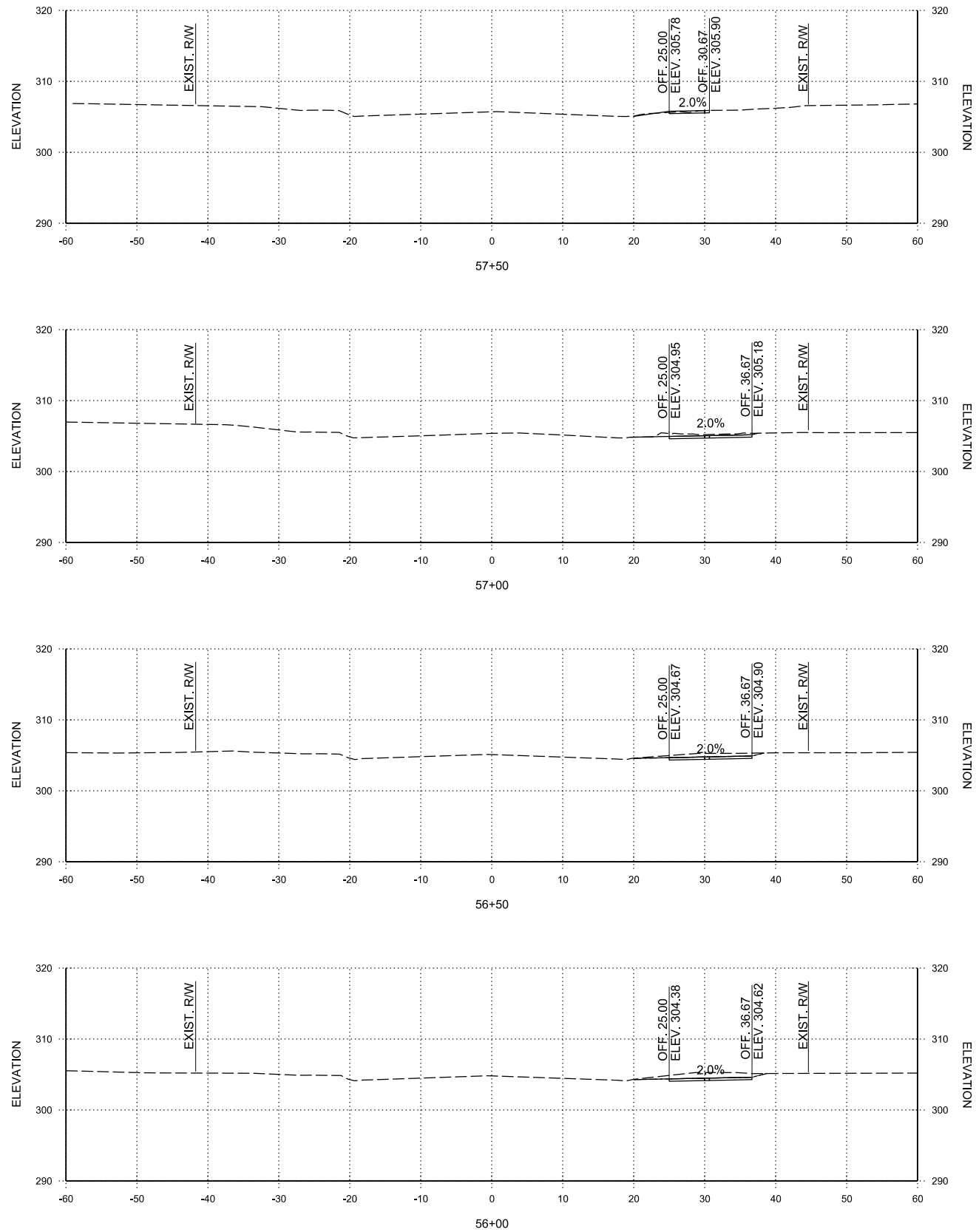
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SHEET NUMBER
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**DONAGHEY AVE.
 STA. 56+00 TO STA. 59+50**



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

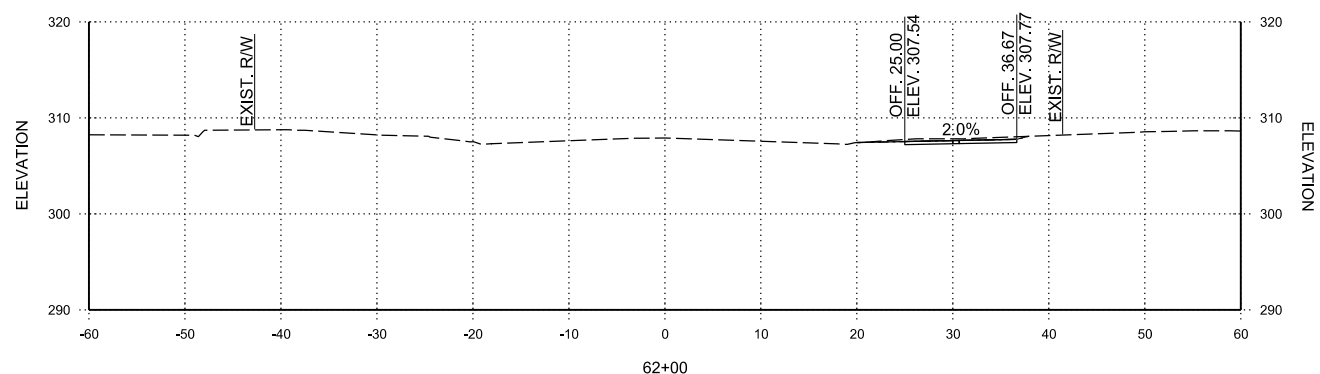
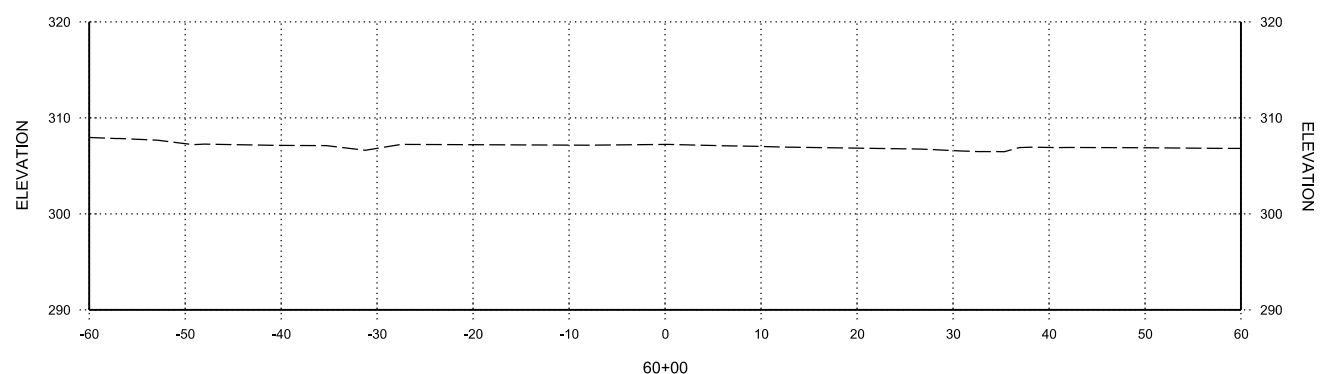
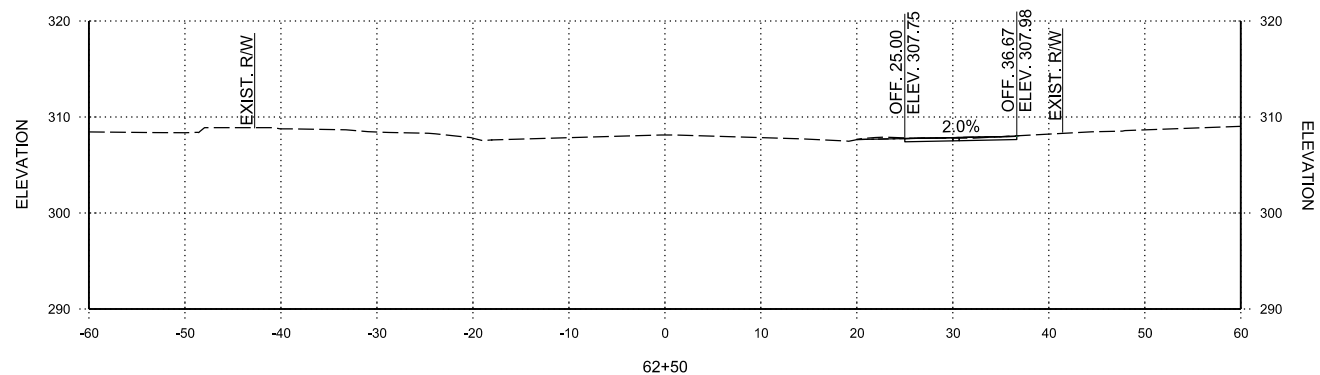
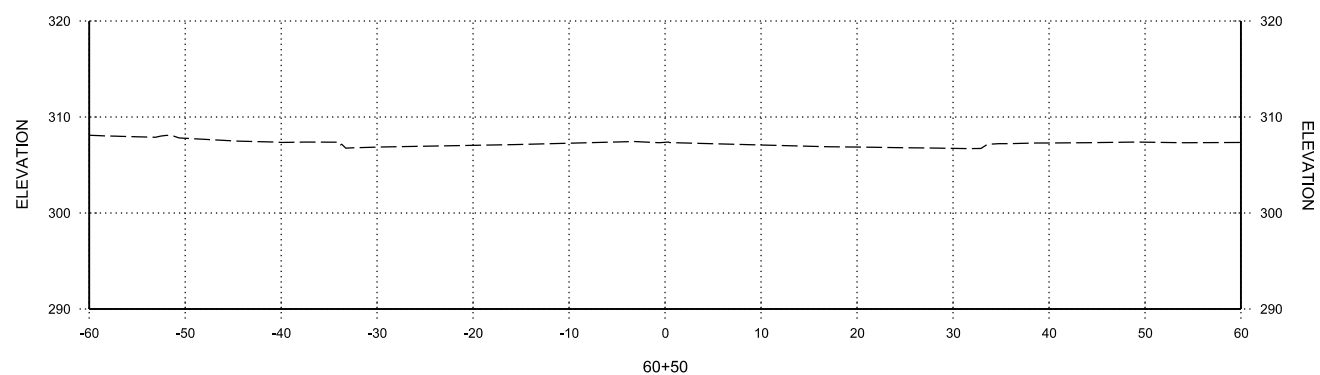
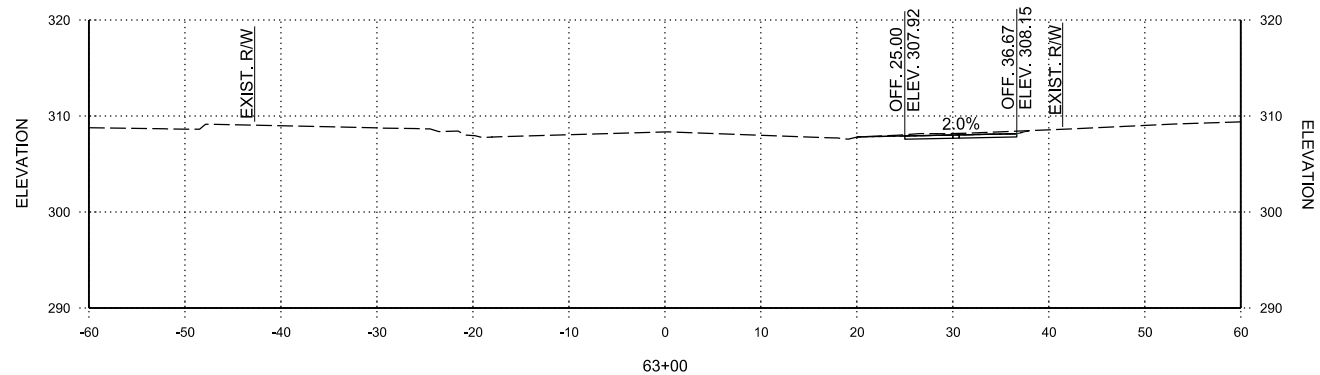
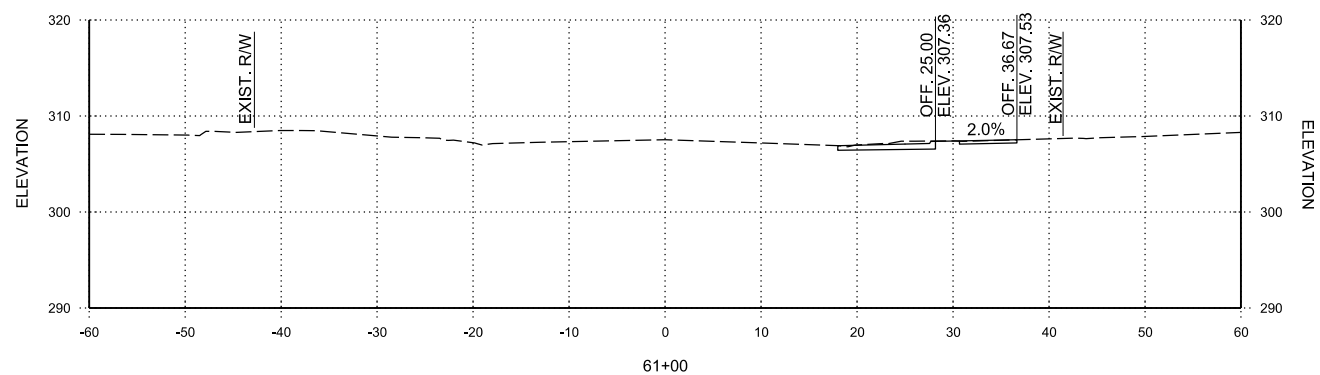
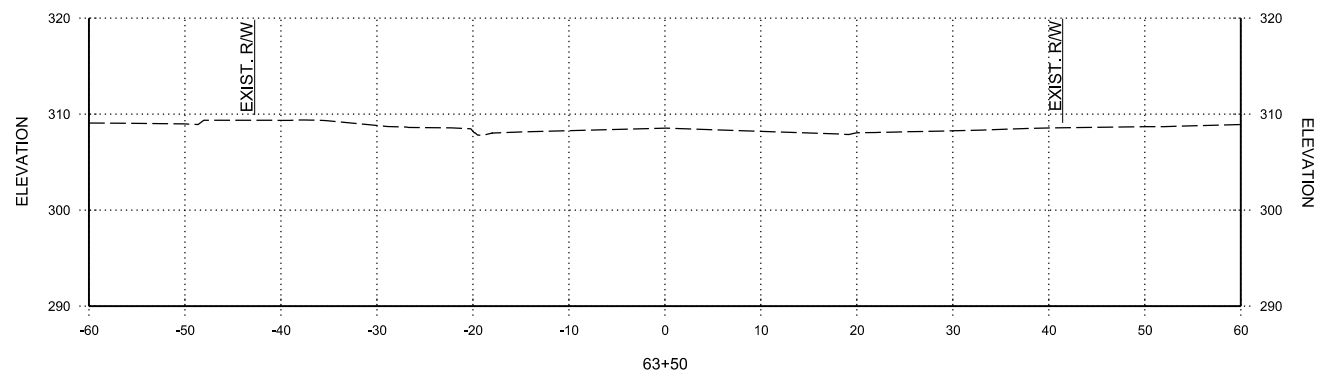
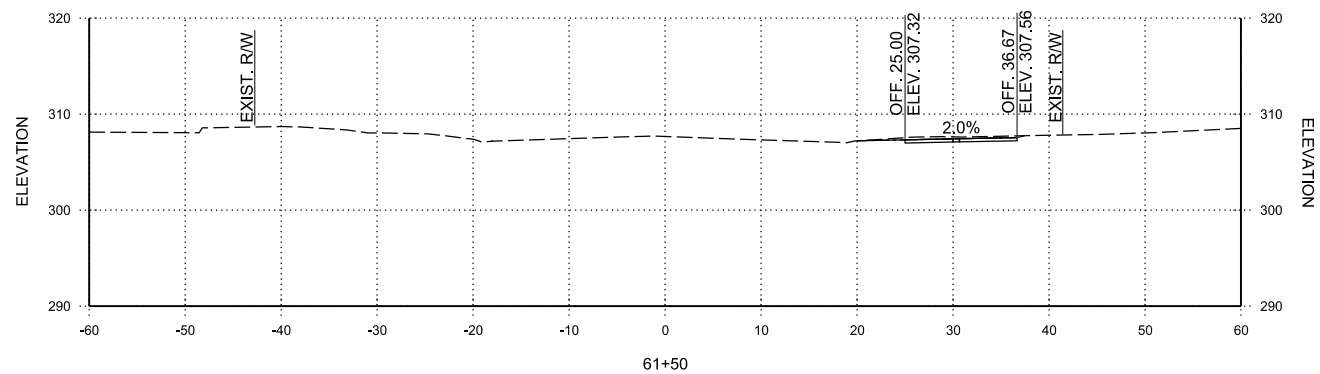
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 DATE: MAY 2020
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**DONAGHEY AVE.
 STA. 60+00 TO STA. 63+50**

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

**DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)**

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

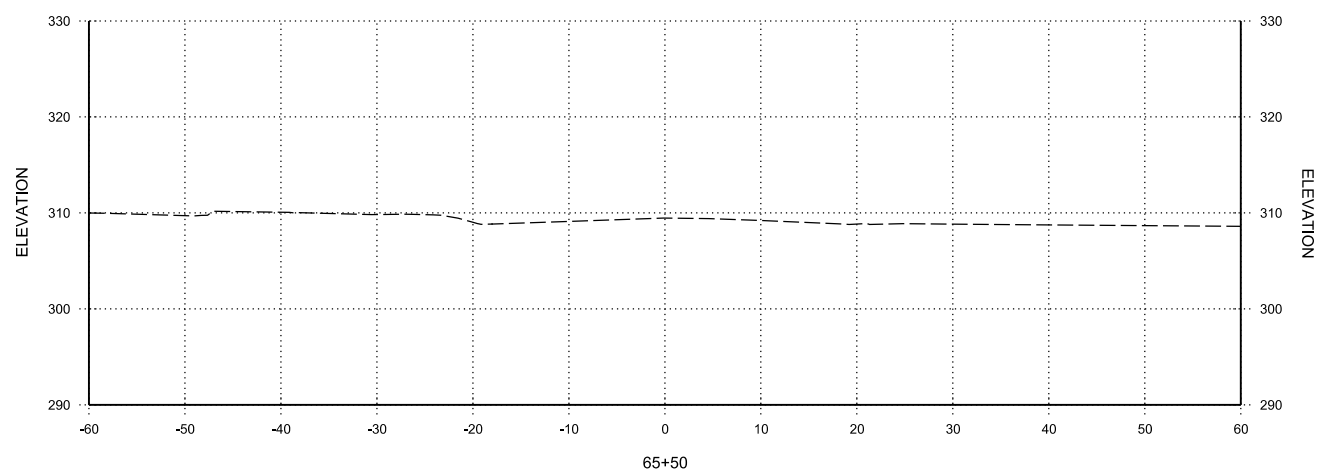
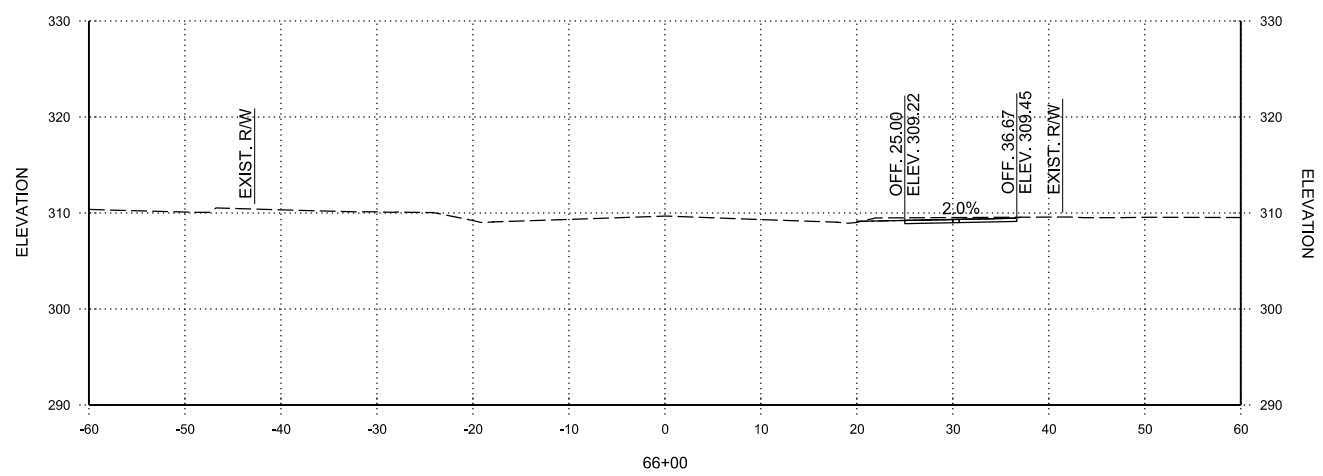
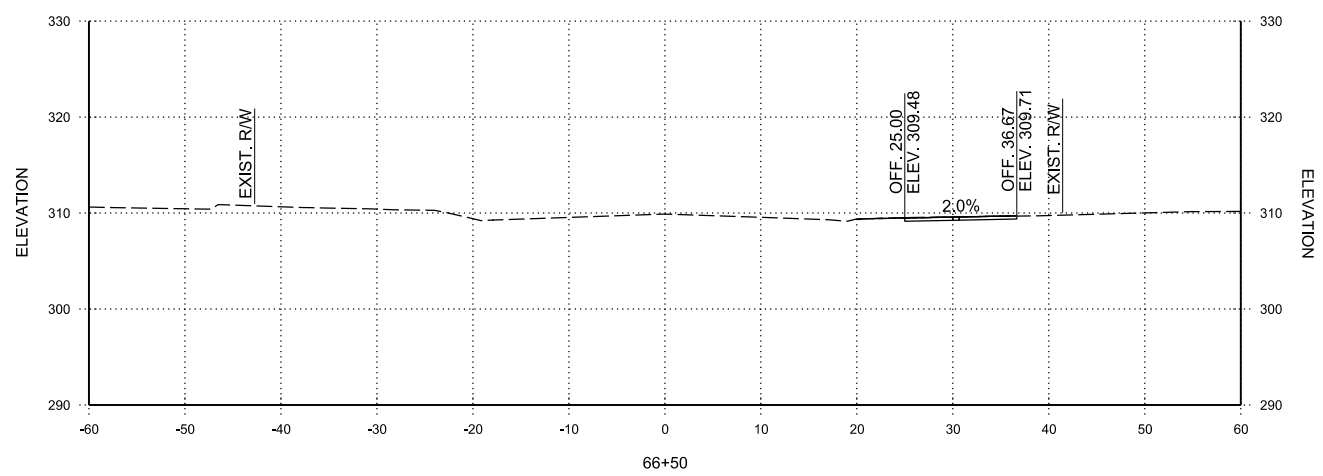
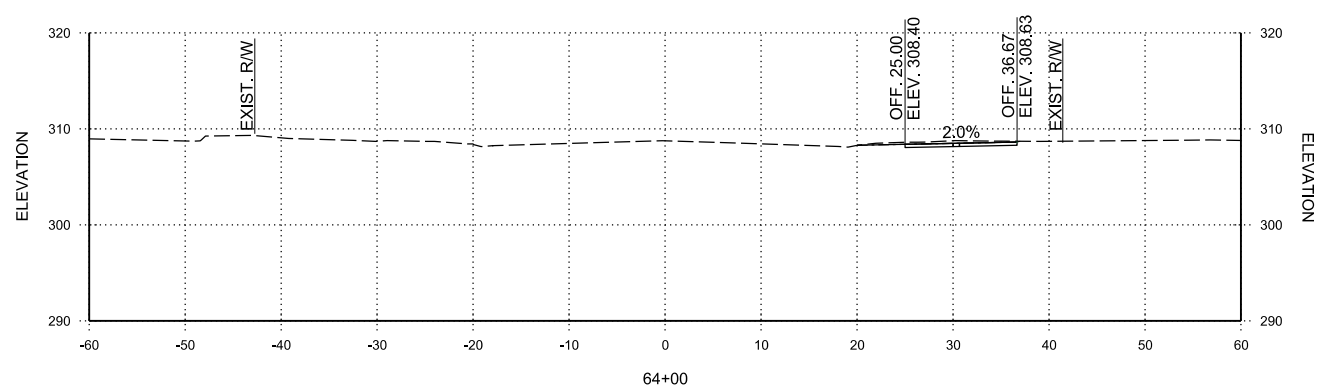
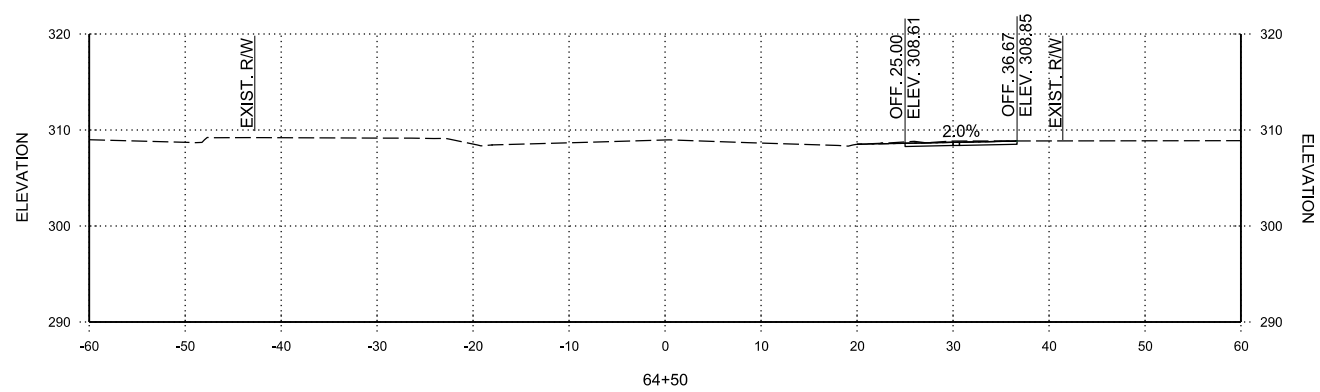
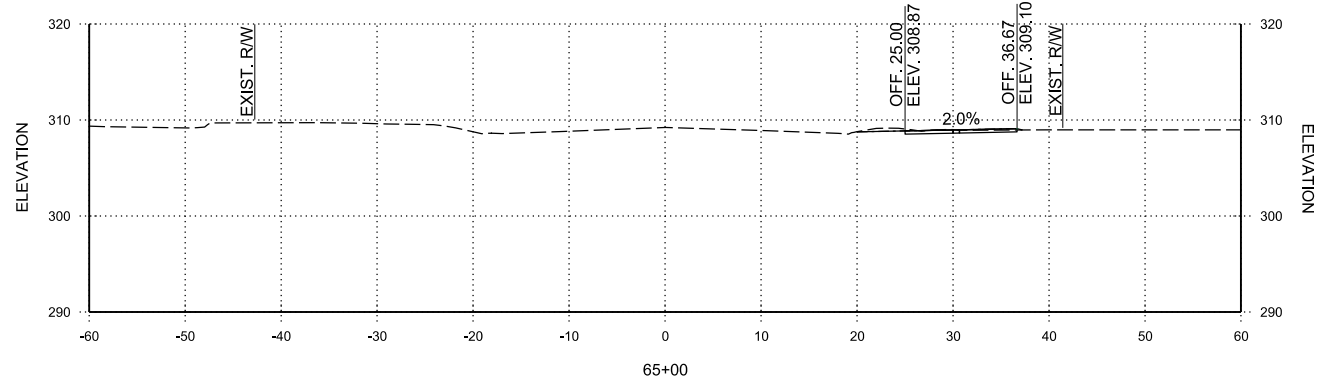
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**DONAGHEY AVE.
 STA. 64+00 TO STA. 66+50**



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

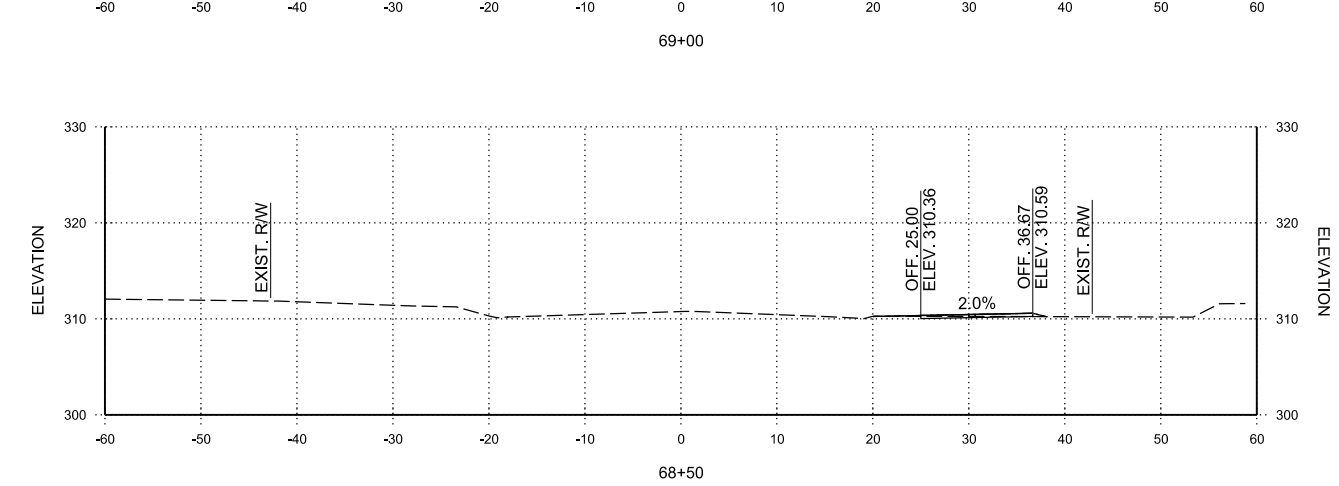
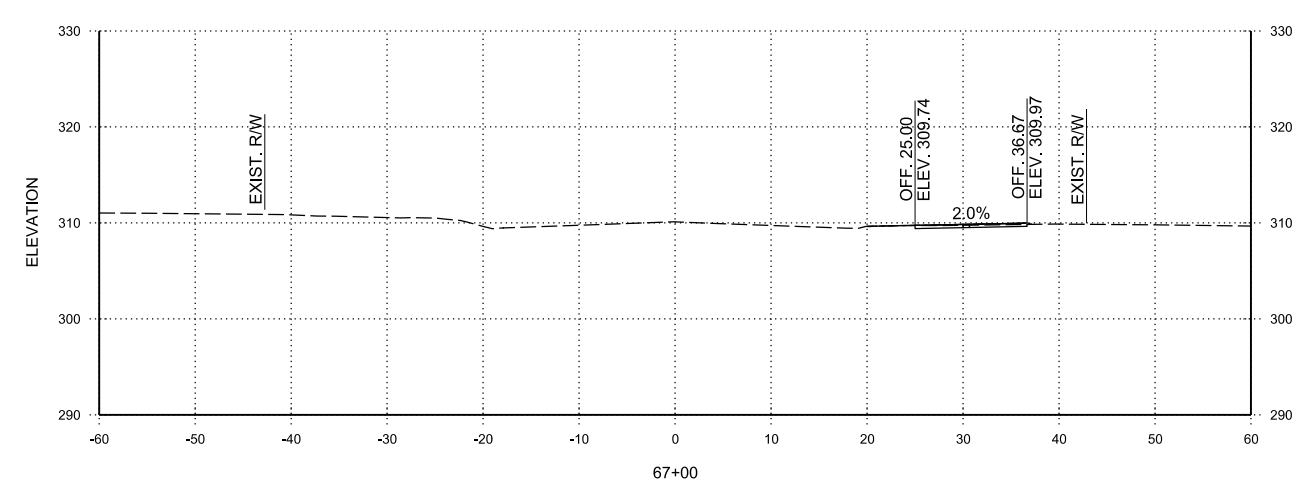
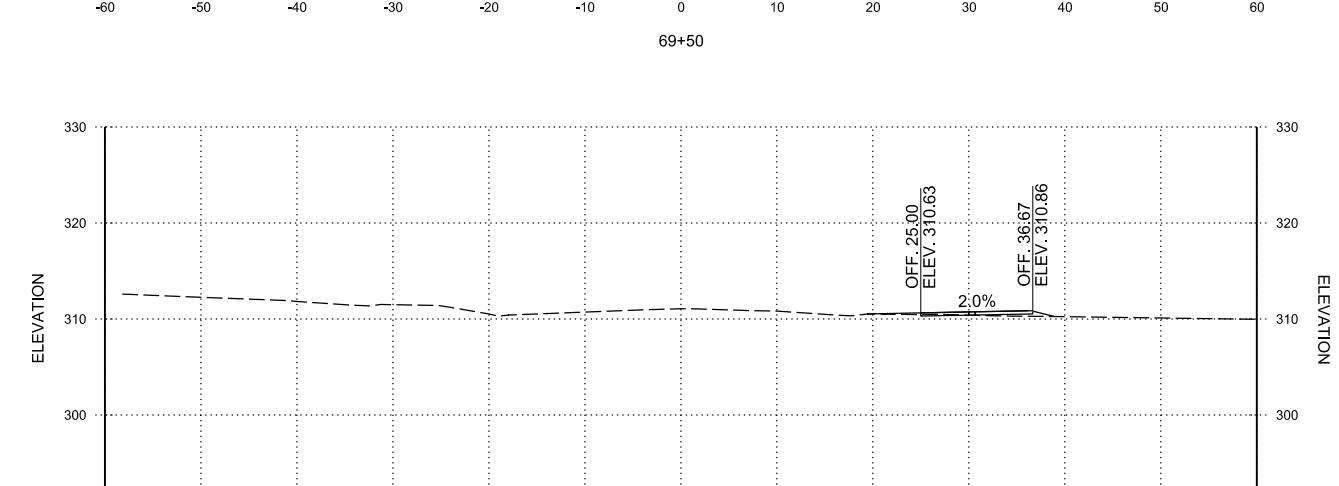
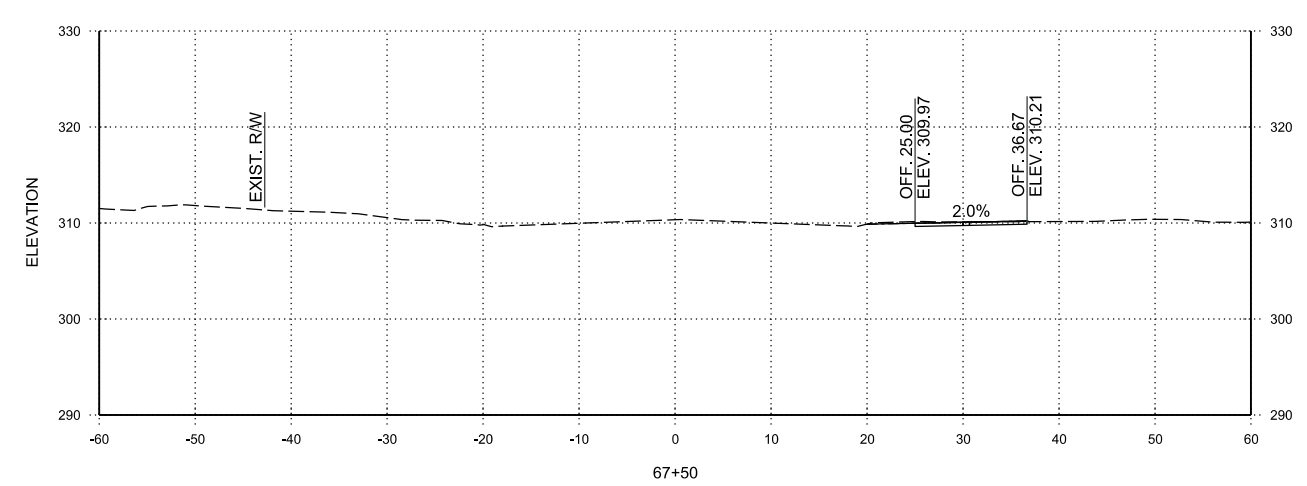
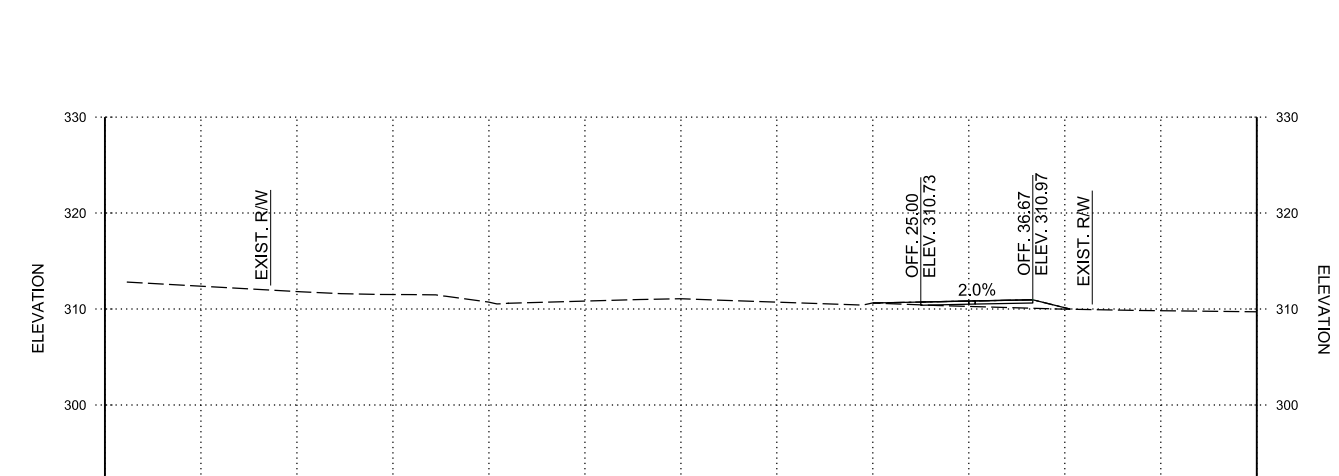
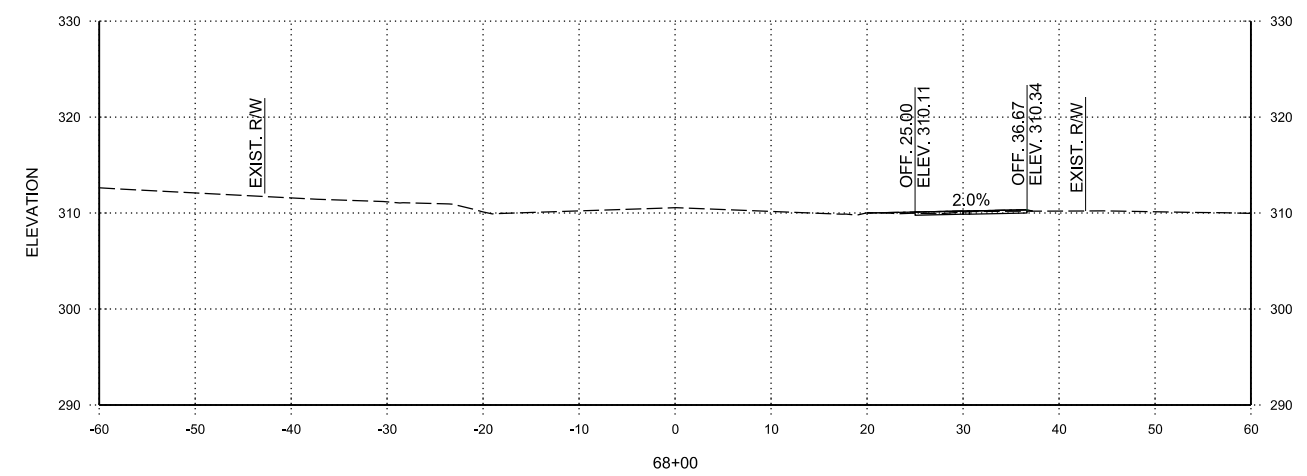
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**DONAGHEY AVE.
 STA. 67+00 TO STA. 69+50**

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

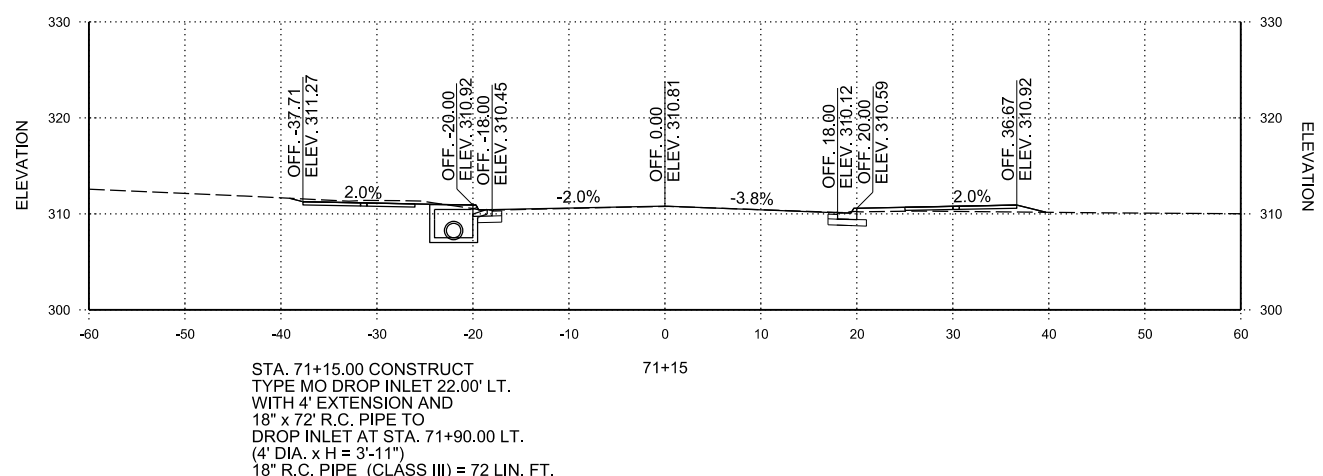
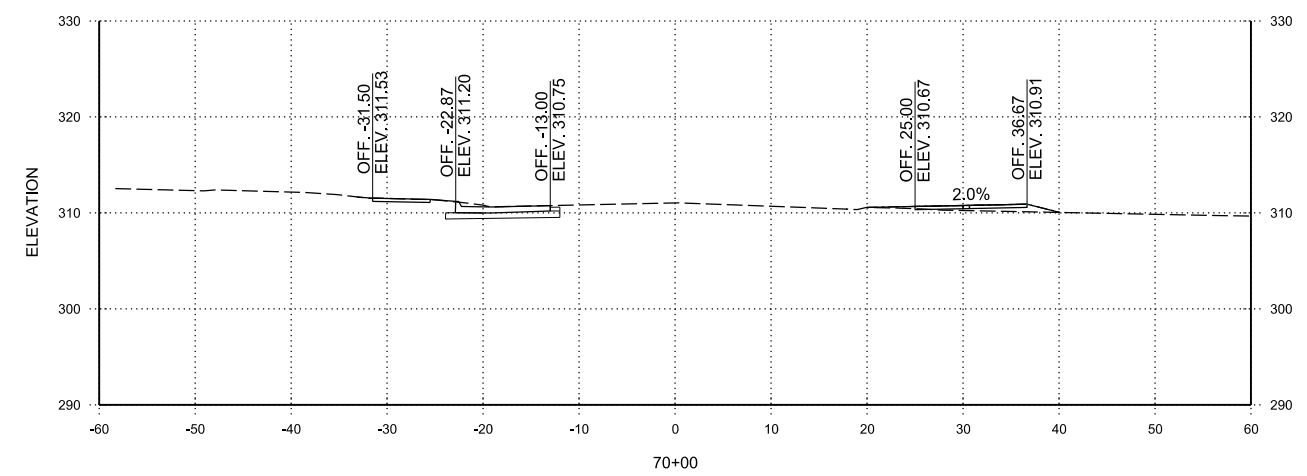
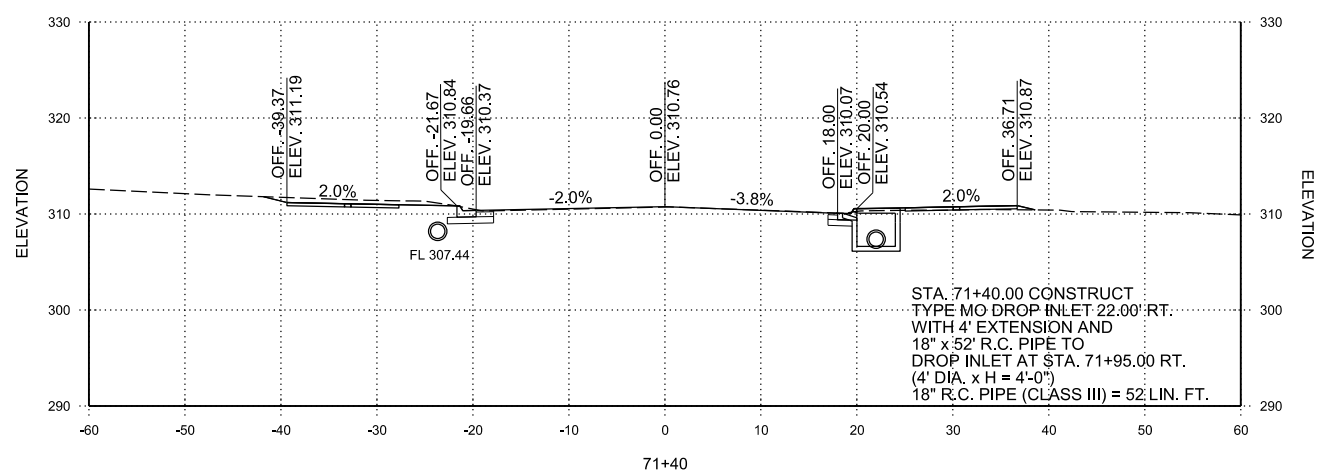
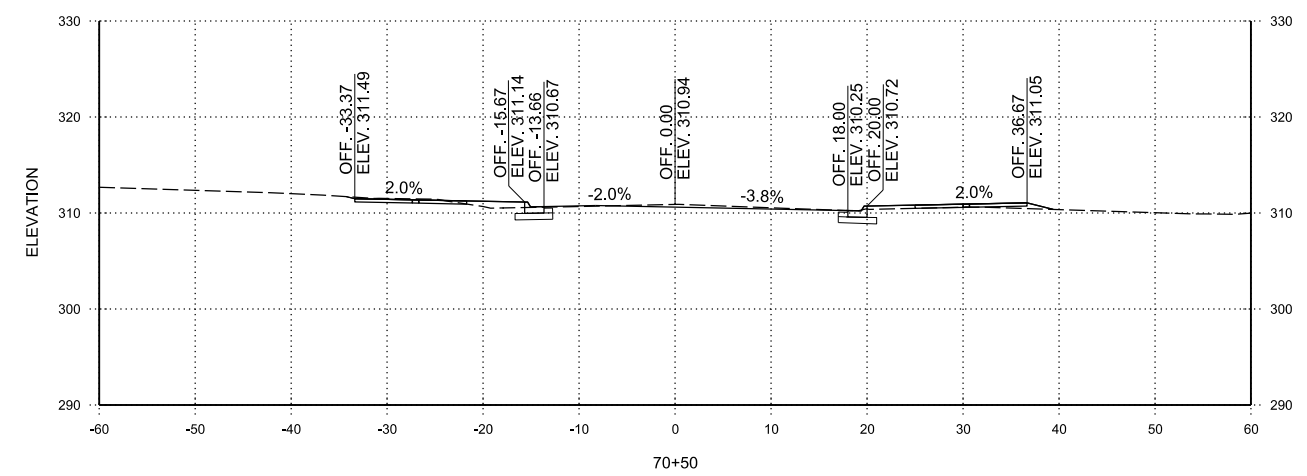
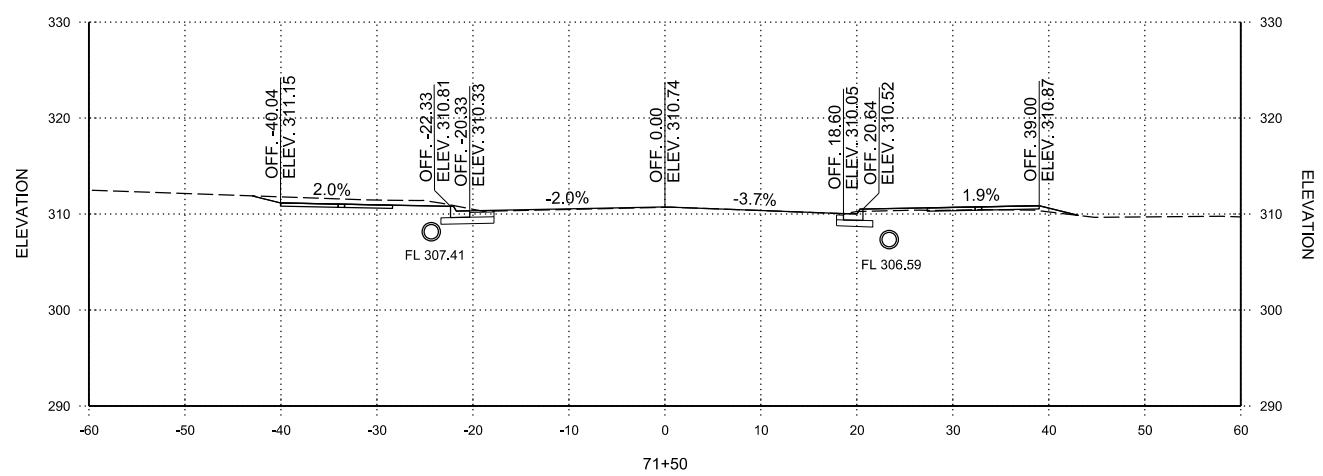
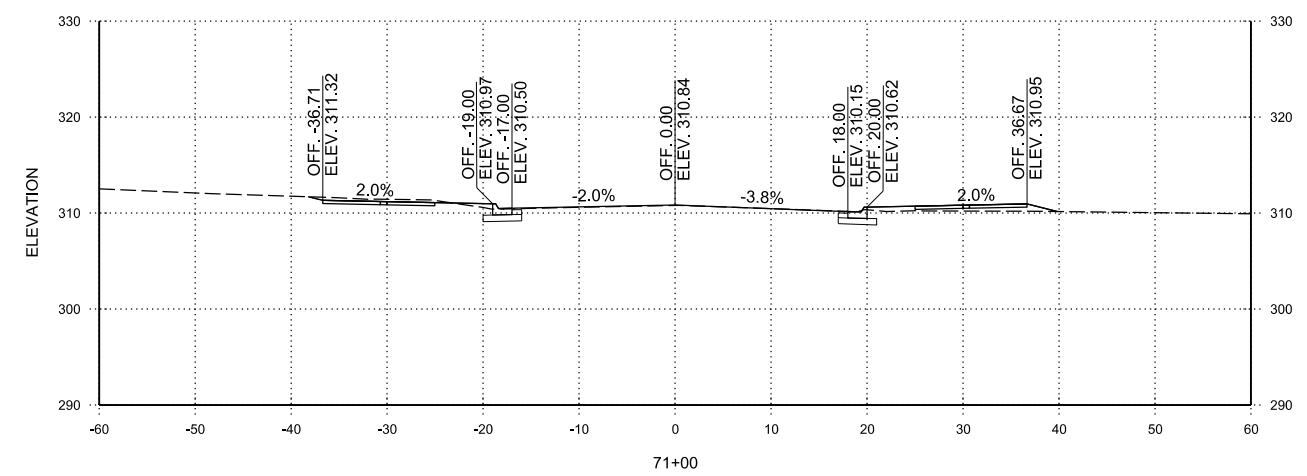
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 SHEET NUMBER **148**

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DONAGHEY AVE.
 STA. 70+00 TO STA. 71+50

STA. 71+15.00 CONSTRUCT
 TYPE MO DROP INLET 22.00' LT.
 WITH 4' EXTENSION AND
 18" x 72" R.C. PIPE TO
 DROP INLET AT STA. 71+90.00 LT.
 (4' DIA. x H = 3'-11")
 18" R.C. PIPE (CLASS III) = 72 LIN. FT.

STA. 71+40.00 CONSTRUCT
 TYPE MO DROP INLET 22.00' RT.
 WITH 4' EXTENSION AND
 18" x 52" R.C. PIPE TO
 DROP INLET AT STA. 71+95.00 RT.
 (4' DIA. x H = 4'-0")
 18" R.C. PIPE (CLASS III) = 52 LIN. FT.

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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

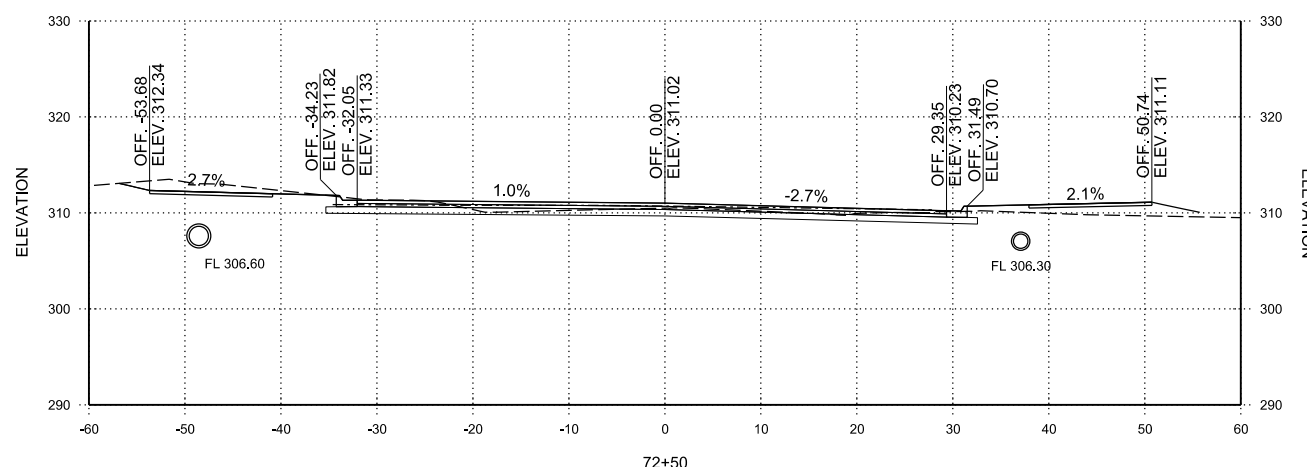
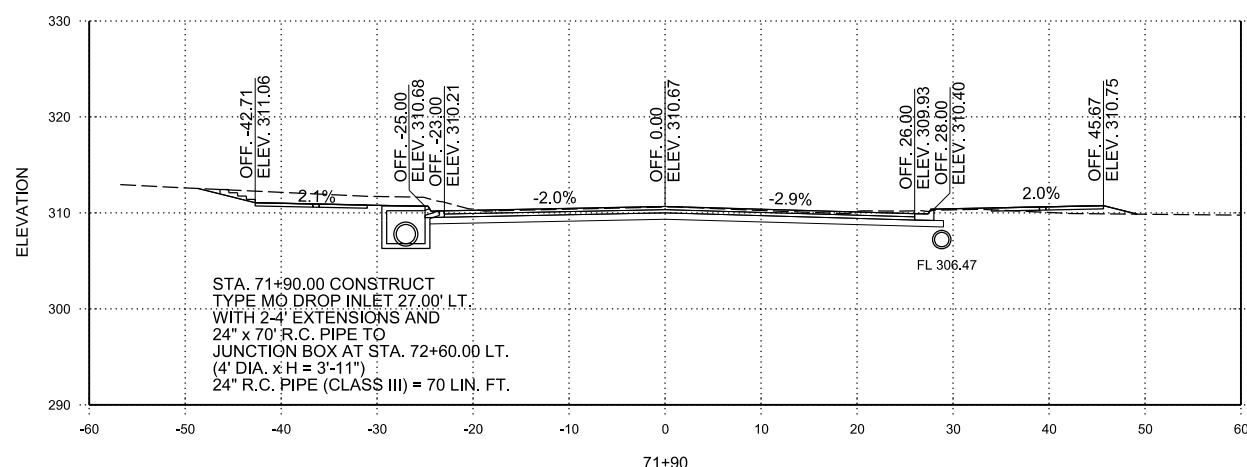
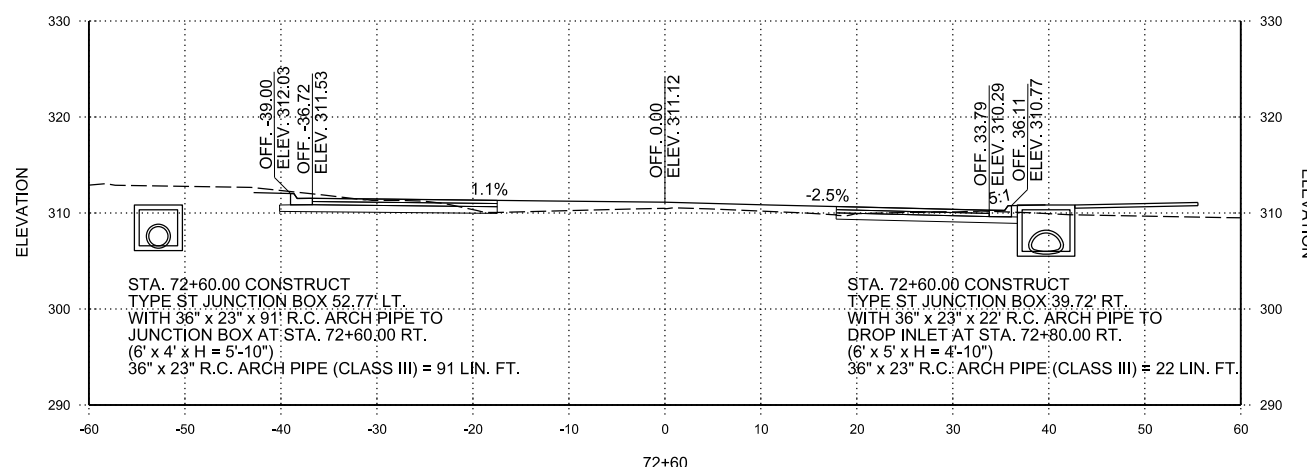
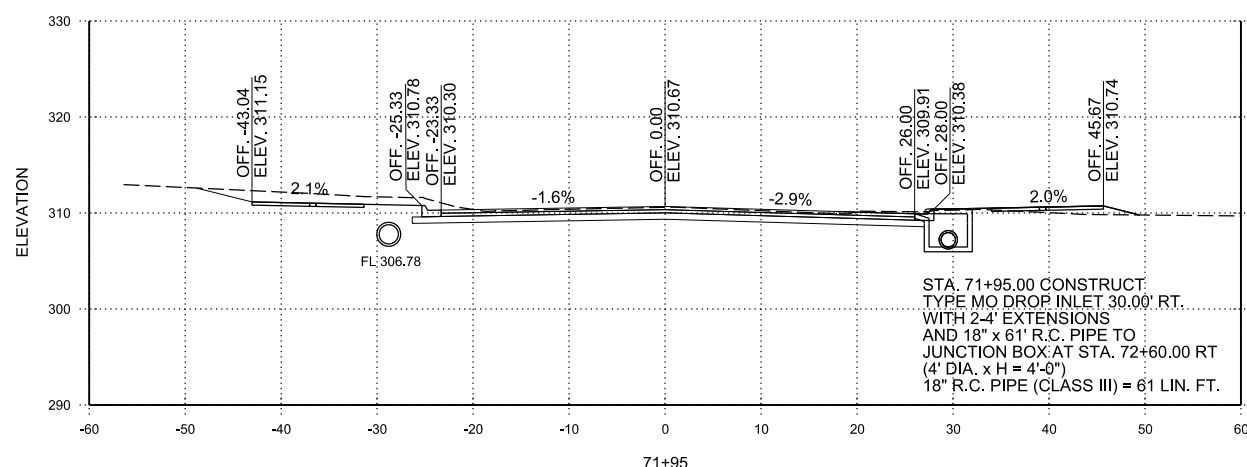
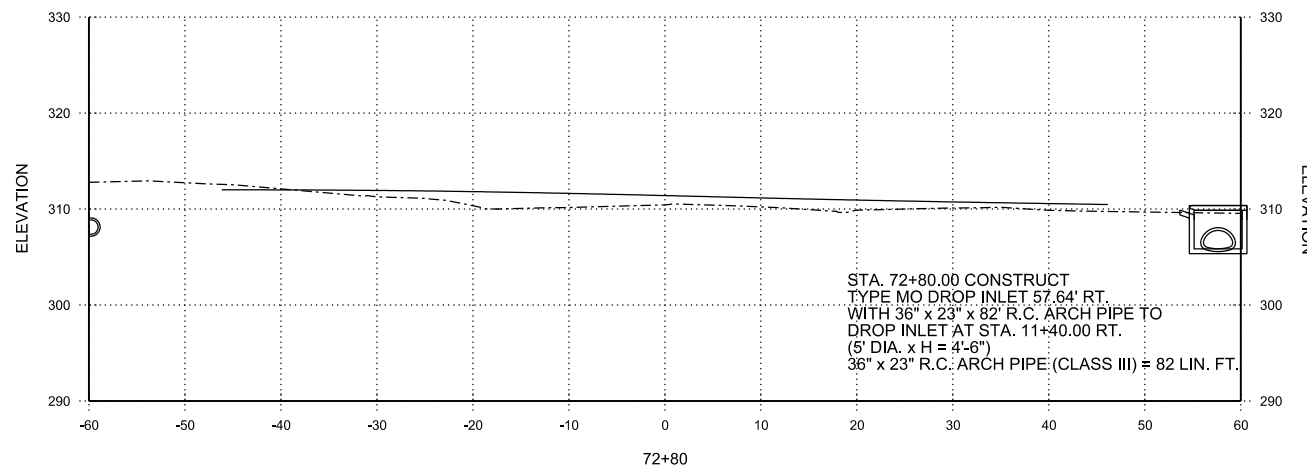
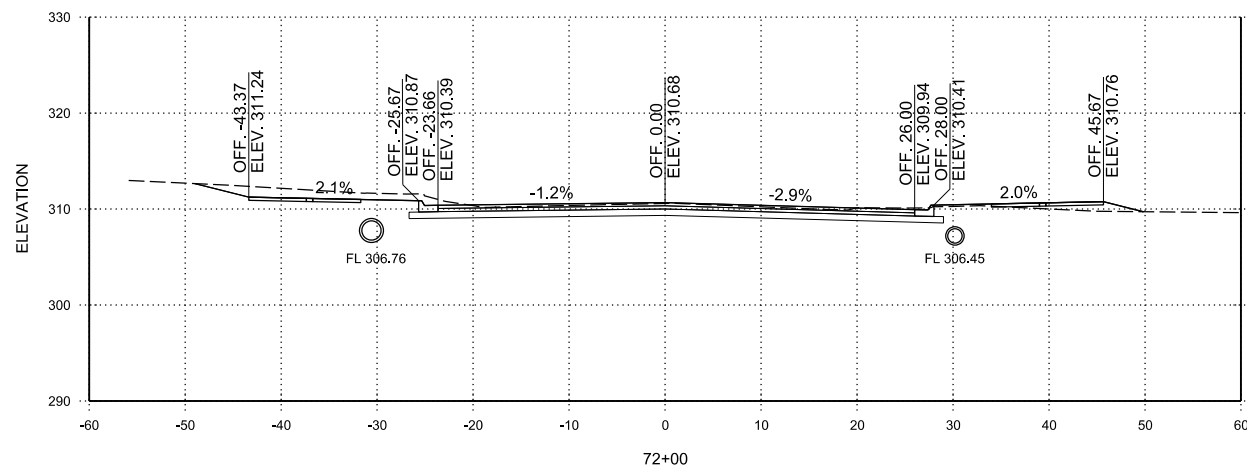
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DONAGHEY AVE.
 STA. 71+90 TO STA. 72+80

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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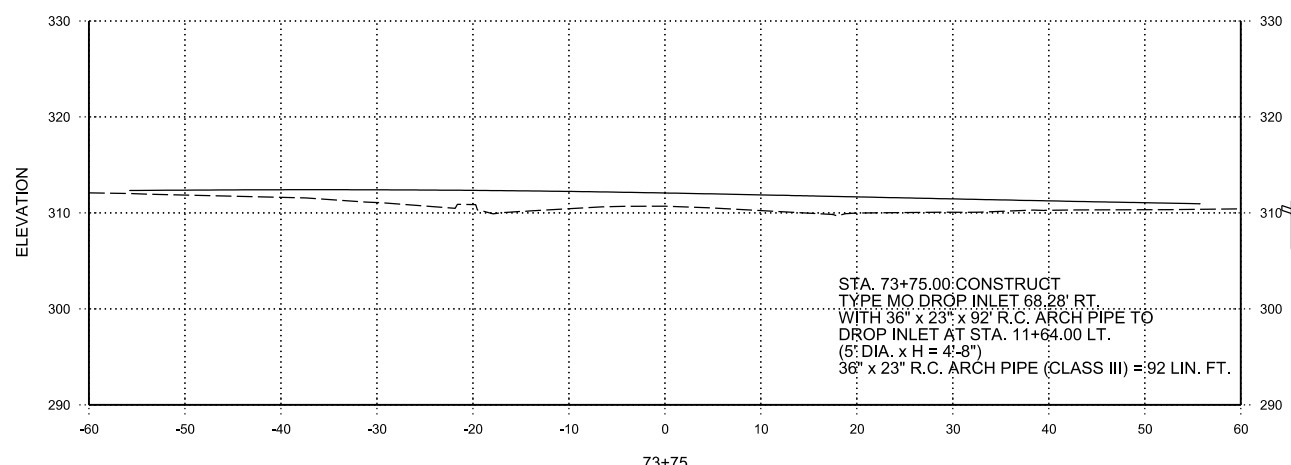
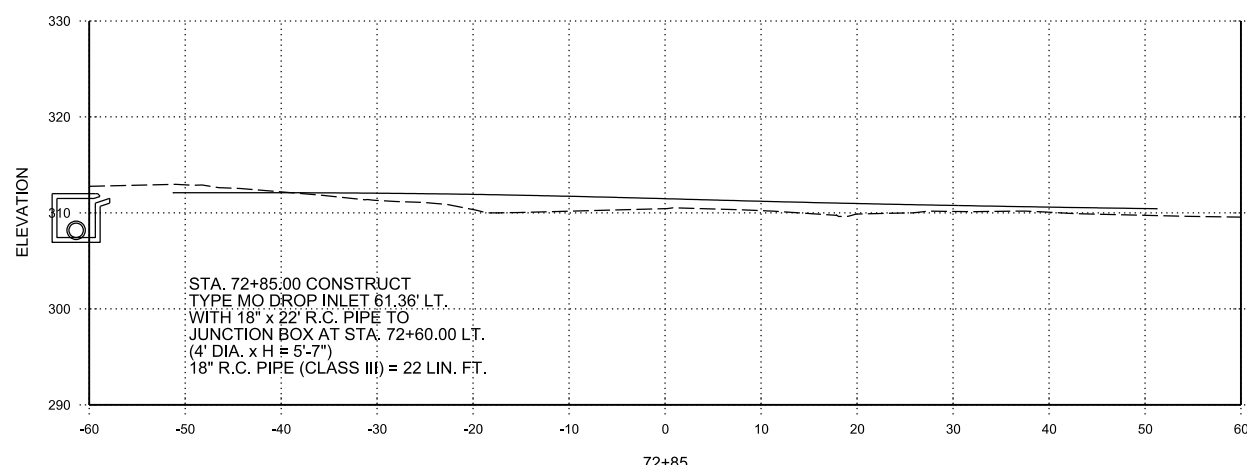
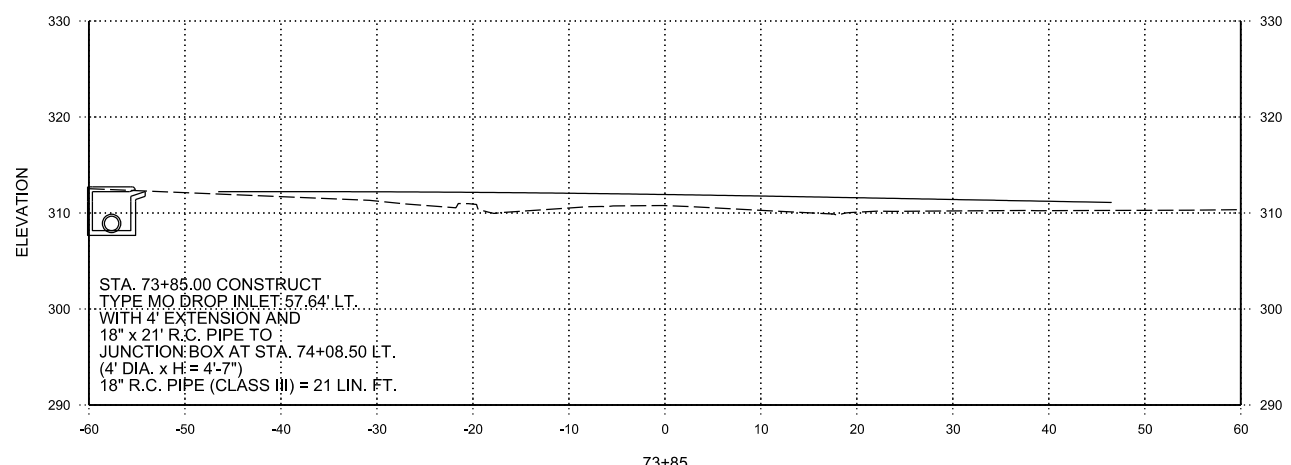
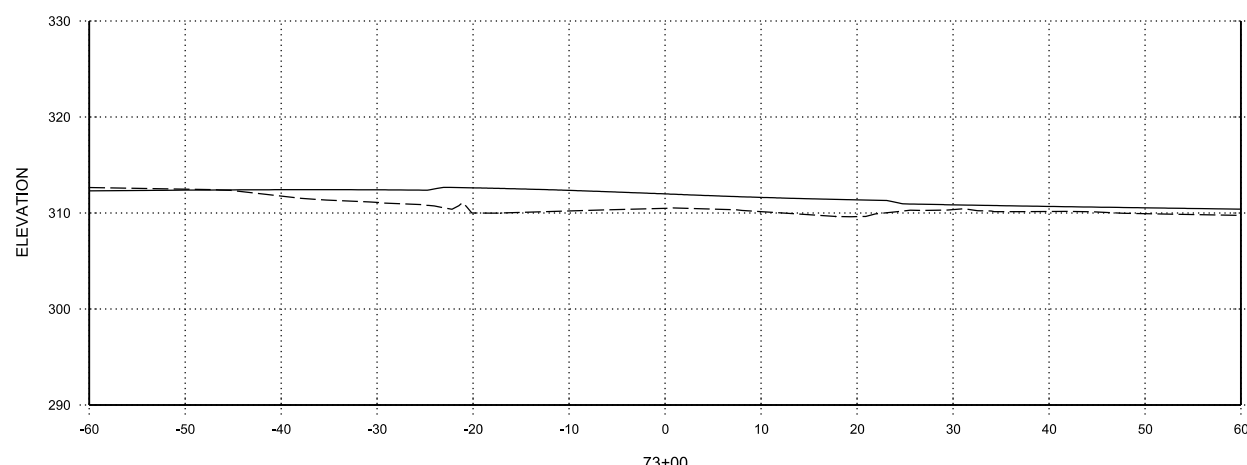
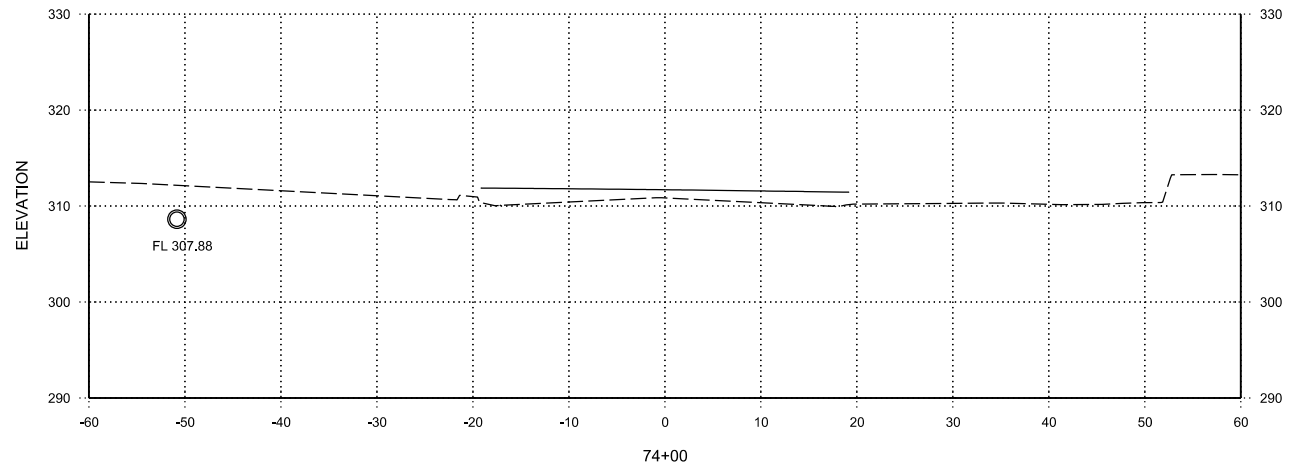
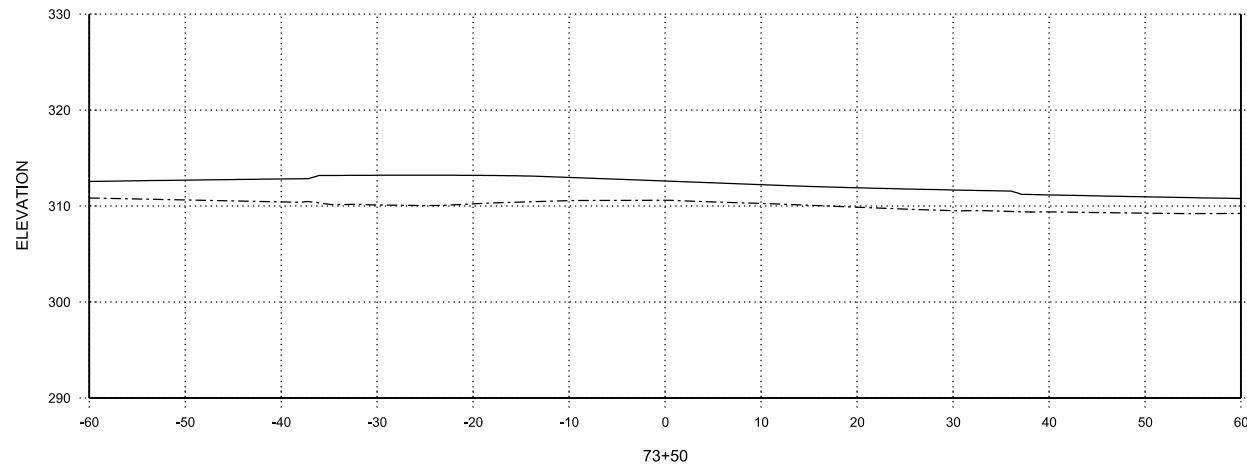
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**DONAGHEY AVE.
 STA. 72+85 TO STA. 74+00**

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

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SHEET NUMBER
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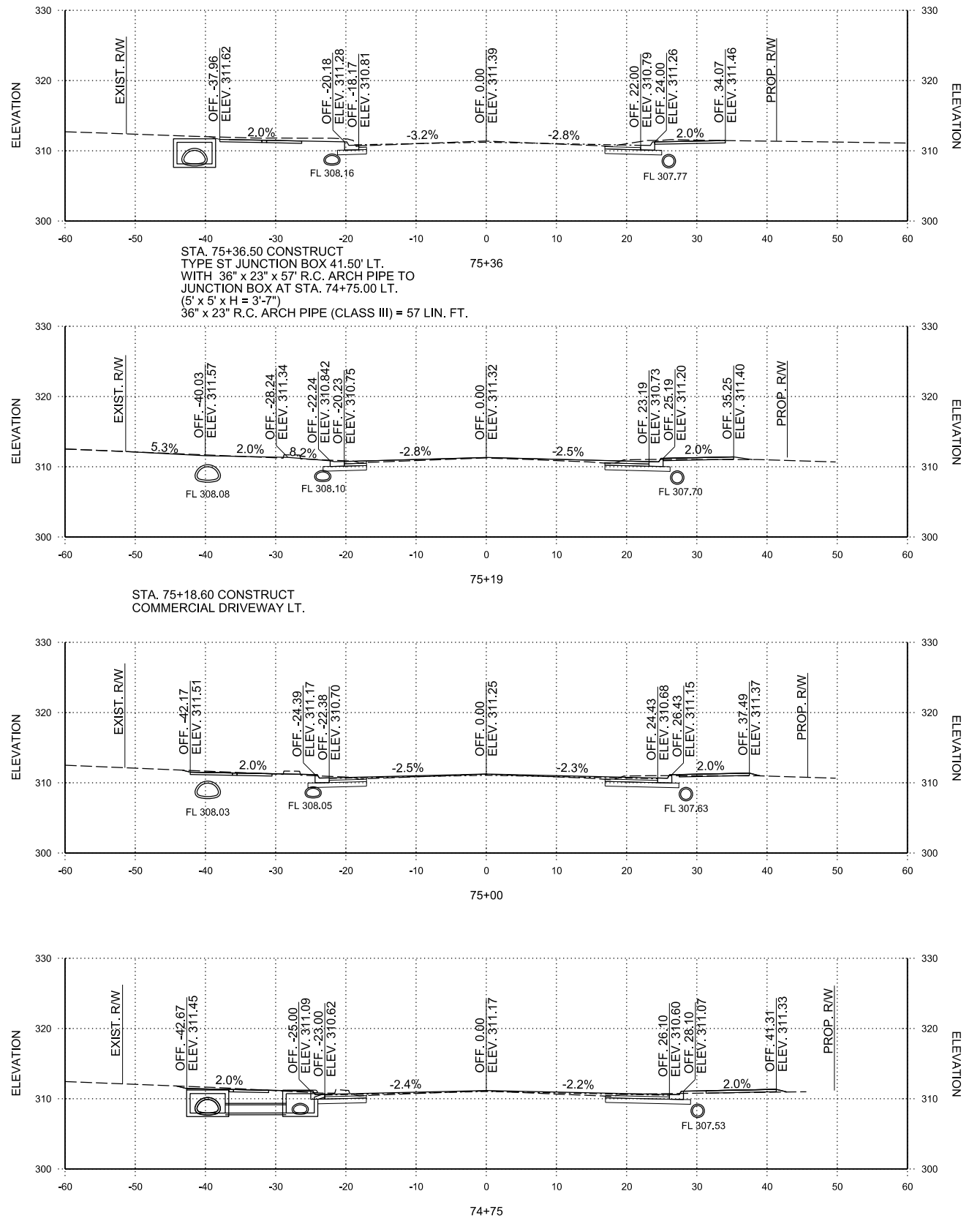
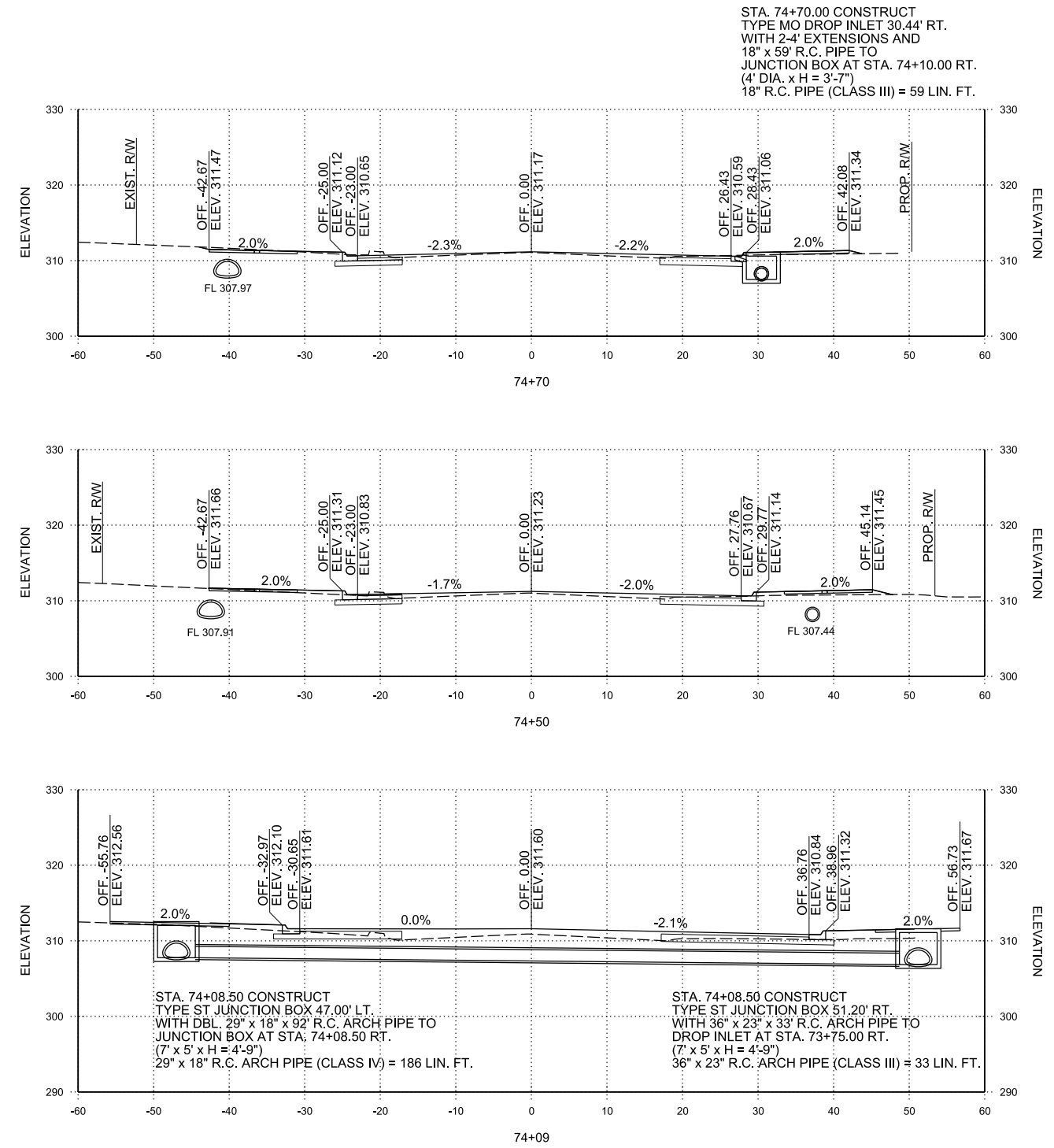
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**DONAGHEY AVE.
 STA. 74+09 TO STA. 75+36**



REV.	DATE	DESCRIPTION	BY

CITY OF CONWAY
 CONWAY, ARKANSAS

**DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)**

CROSS SECTIONS

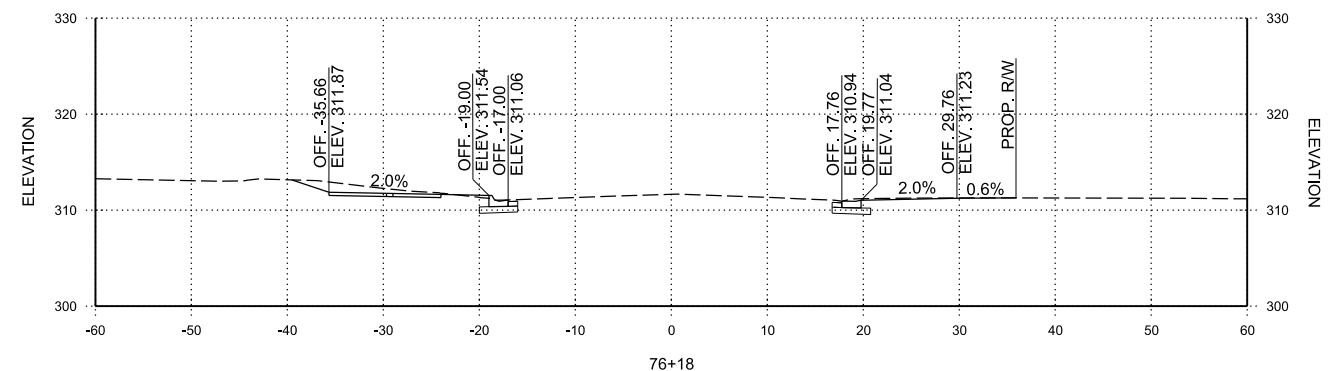
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 DESIGNED BY: DLT
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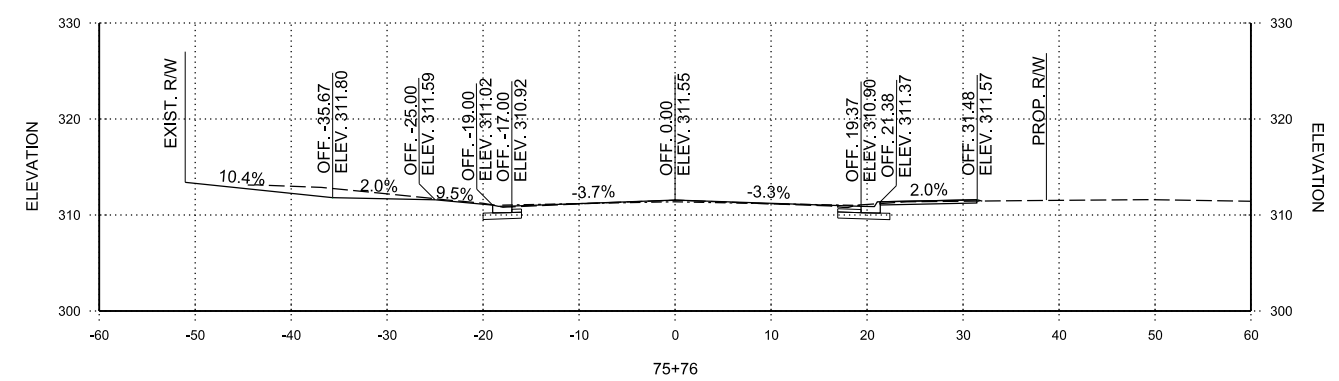
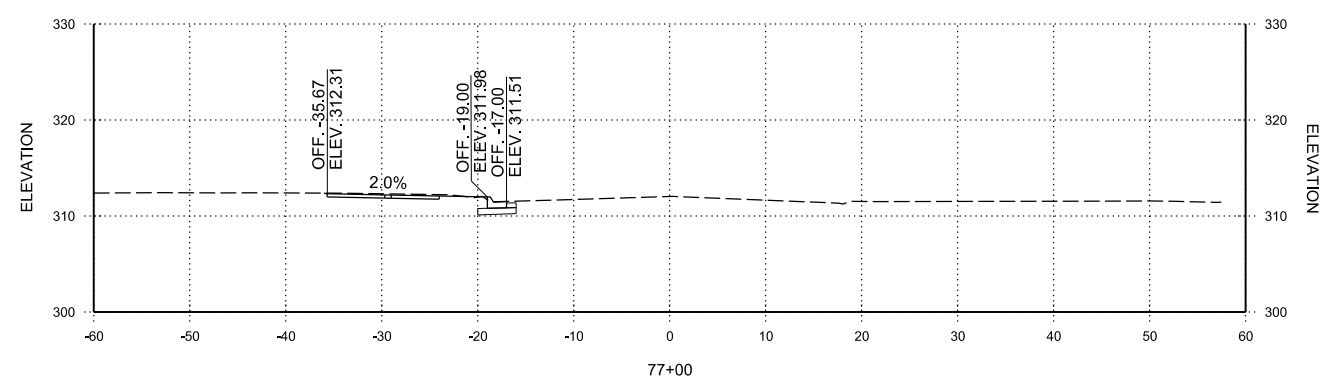
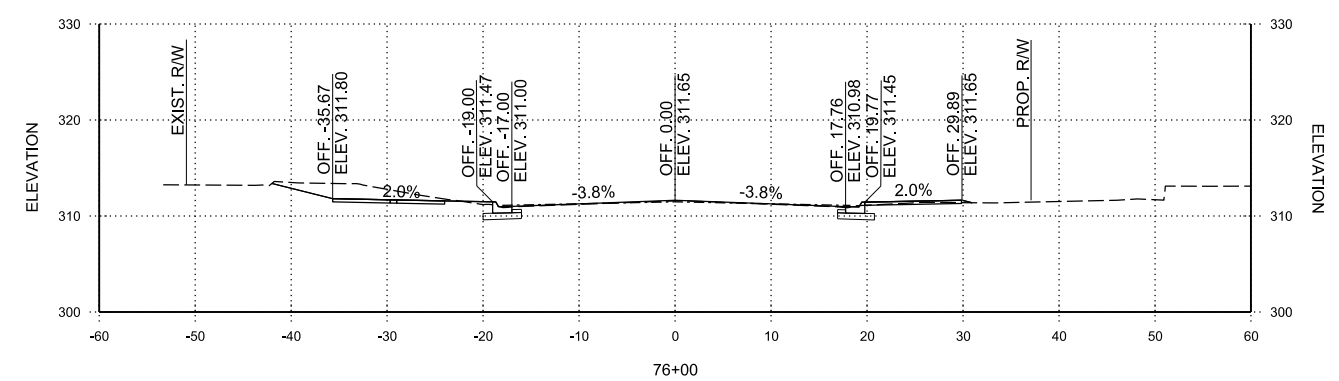
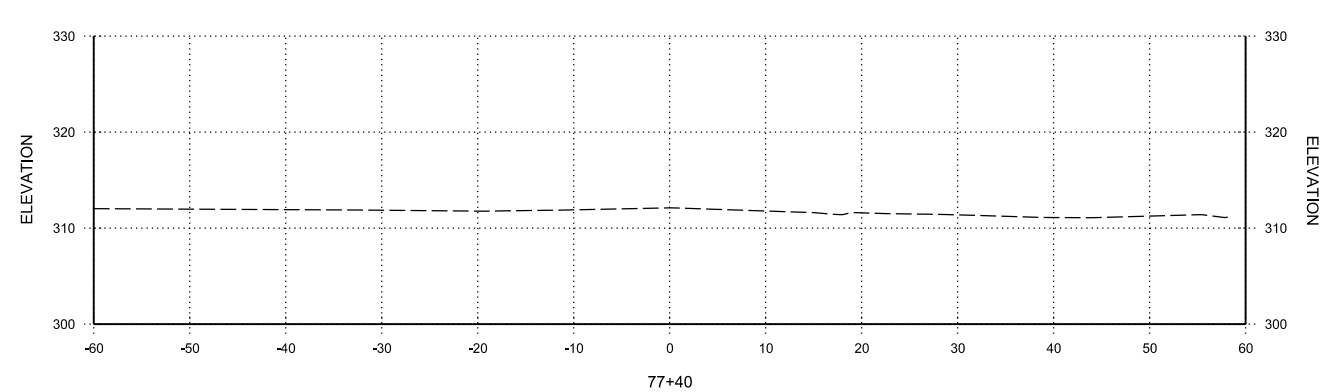
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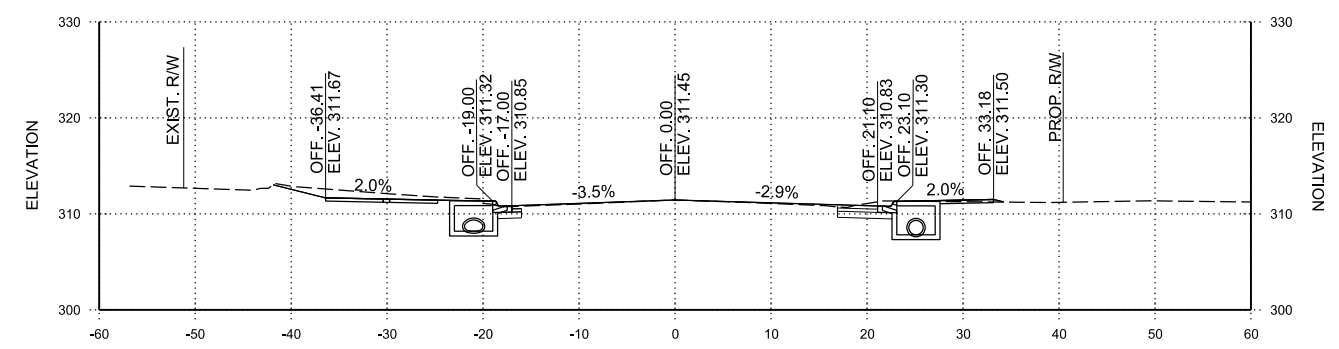
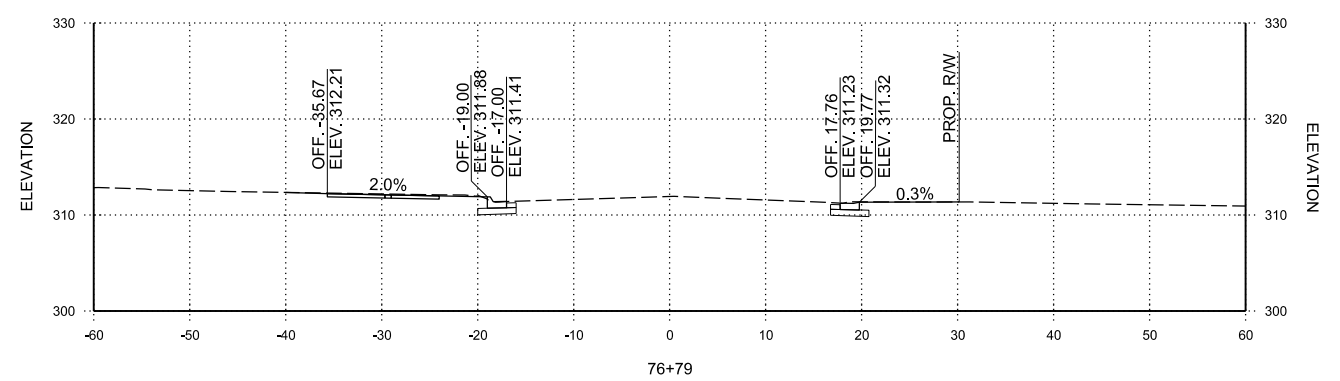
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STA. 76+17.59 CONSTRUCT ARDOT DRIVEWAY RT.

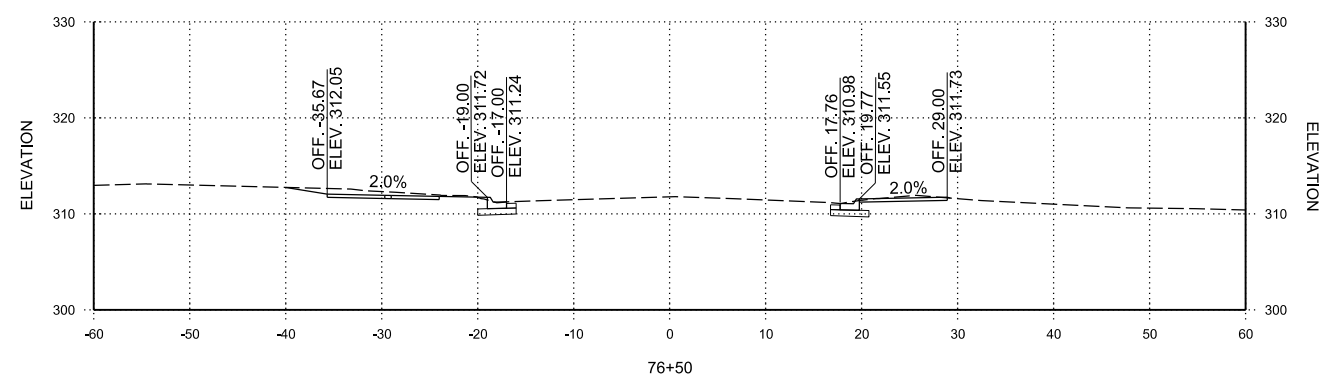


STA. 75+75.82 CONSTRUCT COMMERCIAL DRIVEWAY LT.



STA. 75+50.00 CONSTRUCT TYPE MO DROP INLET 21.00' LT. WITH 4' EXTENSION AND 22" x 14" x 71' R.C. ARCH PIPE TO DROP INLET AT STA. 74+75.00 LT. (4' DIA. x H = 3'-2") 22" x 14" R.C. ARCH PIPE (CLASS III) = 71 LIN. FT.

STA. 75+50.00 CONSTRUCT TYPE MO DROP INLET 25.10' RT. WITH 4' EXTENSION AND 18" x 76" R.C. PIPE TO DROP INLET AT STA. 74+70.00 RT. (4' DIA. x H = 3'-6") 18" R.C. PIPE (CLASS III) = 76 LIN. FT.



**DONAGHEY AVE.
 STA. 75+50 TO STA. 77+40**

REV.	DATE	DESCRIPTION	BY

CITY OF CONWAY
 CONWAY, ARKANSAS

**DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)**

CROSS SECTIONS

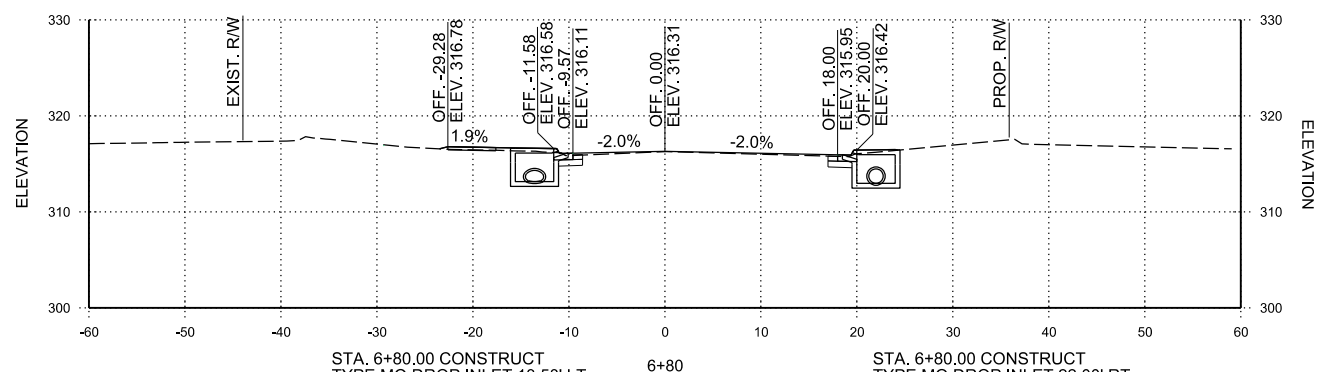
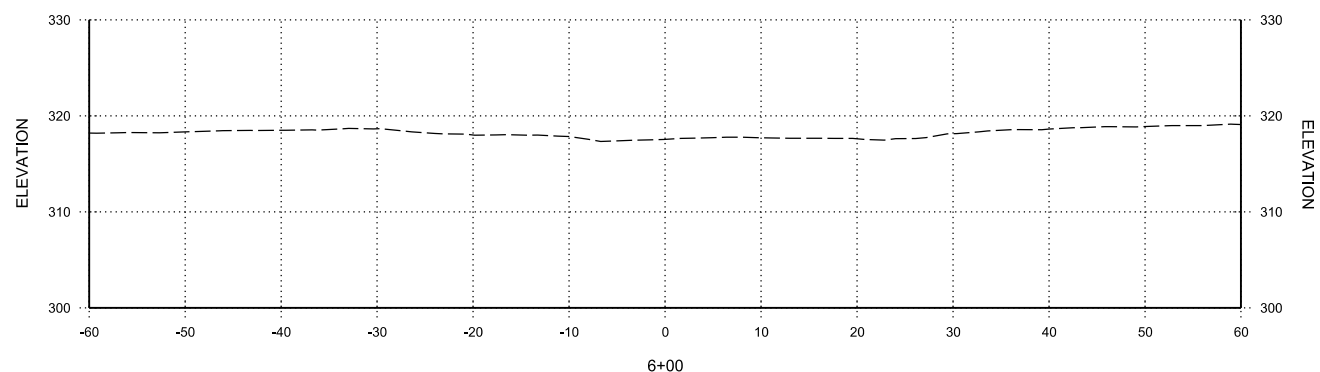
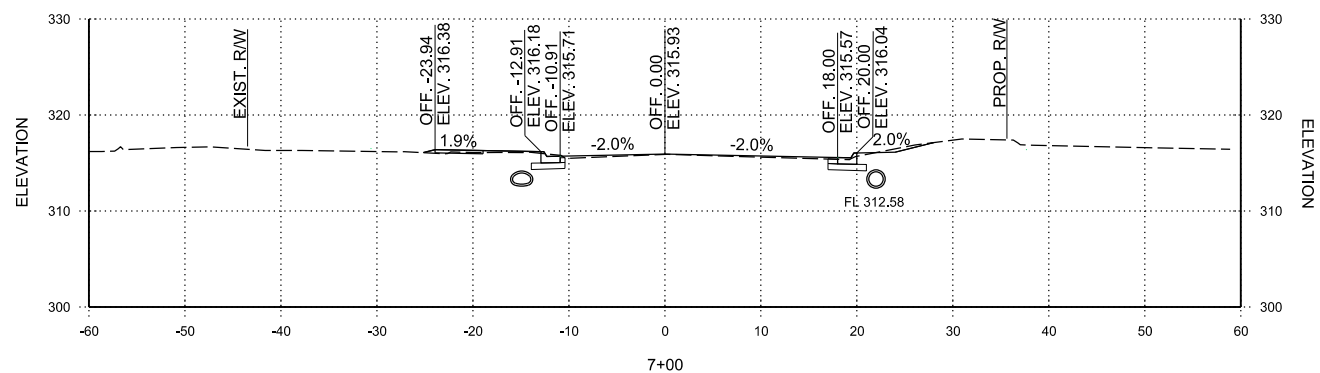
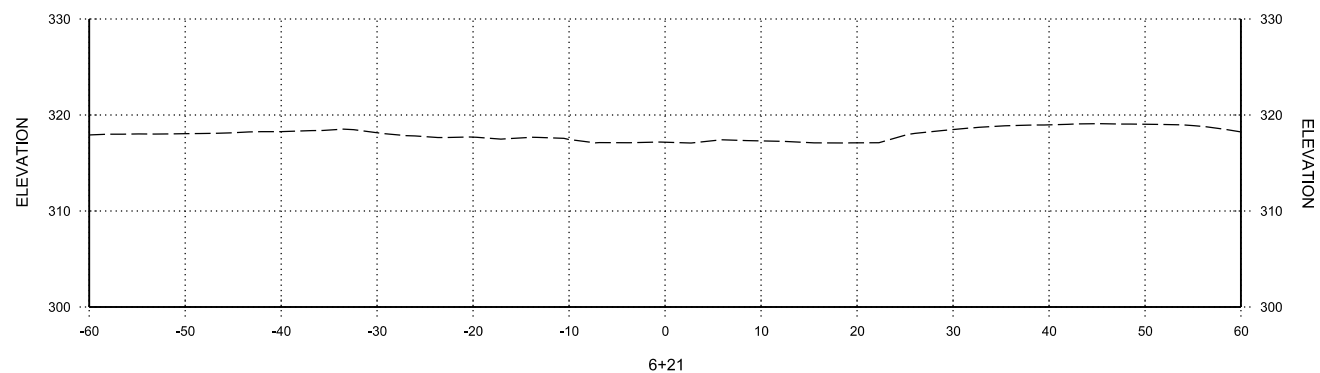
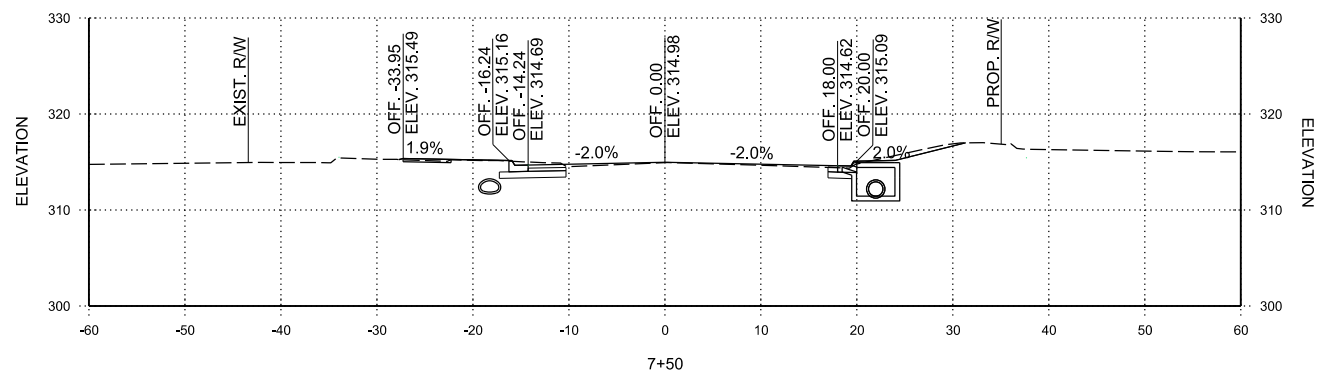
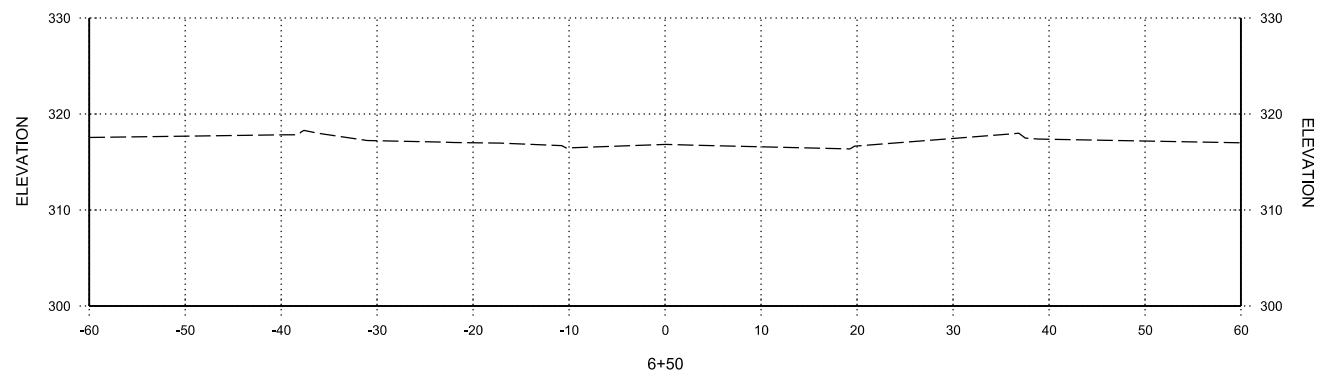
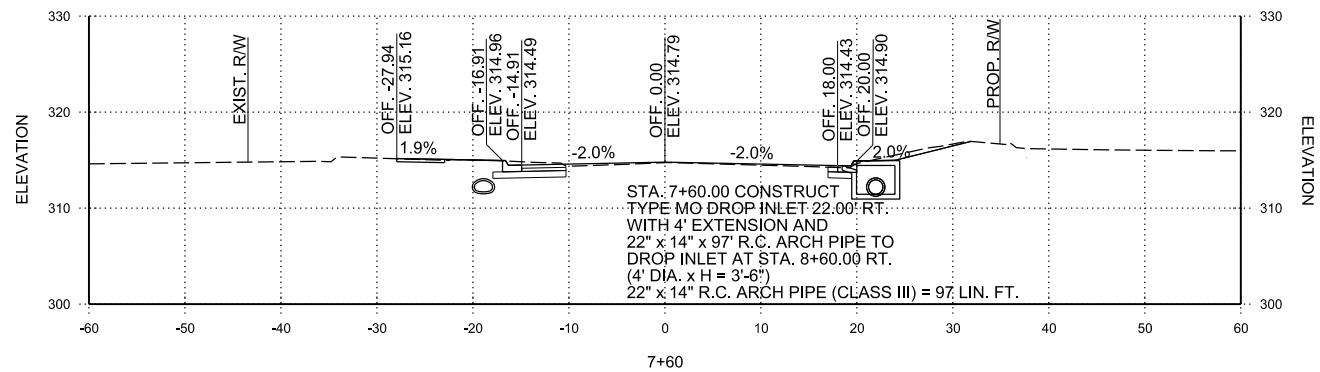
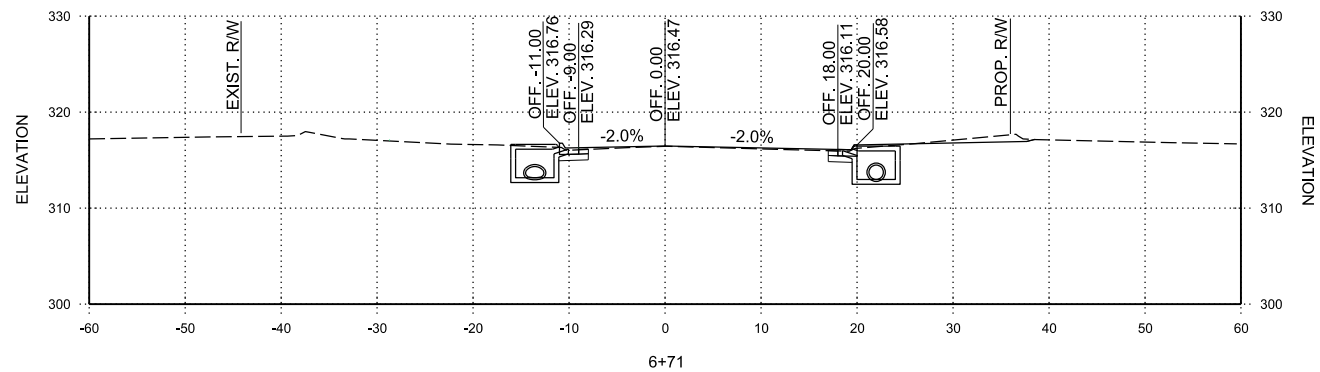
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**COLLEGE AVE. (WEST)
 STA. 6+00 TO STA. 7+60**

STA. 6+80.00 CONSTRUCT TYPE MO DROP INLET 13.58' LT. WITH 4' EXTENSION AND 22" x 14" x 114" R.C. ARCH PIPE TO DROP INLET AT STA. 7+97.00 LT. (4' DIA. x H = 3'-6") 22" x 14" R.C. ARCH PIPE (CLASS III) = 144 LIN. FT.

STA. 6+80.00 CONSTRUCT TYPE MO DROP INLET 22.00' RT. WITH 4' EXTENSION AND 22" x 14" x 77" R.C. ARCH PIPE TO DROP INLET AT STA. 7+60.00 RT. (4' DIA. x H = 3'-6") 22" x 14" R.C. ARCH PIPE (CLASS III) = 77 LIN. FT.

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
**DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)**

CROSS SECTIONS

JOB NO.: 18147010
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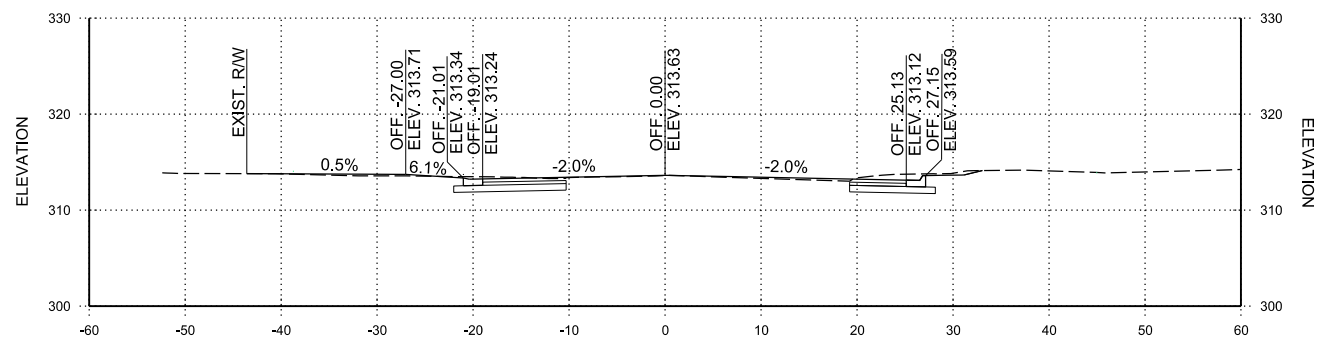
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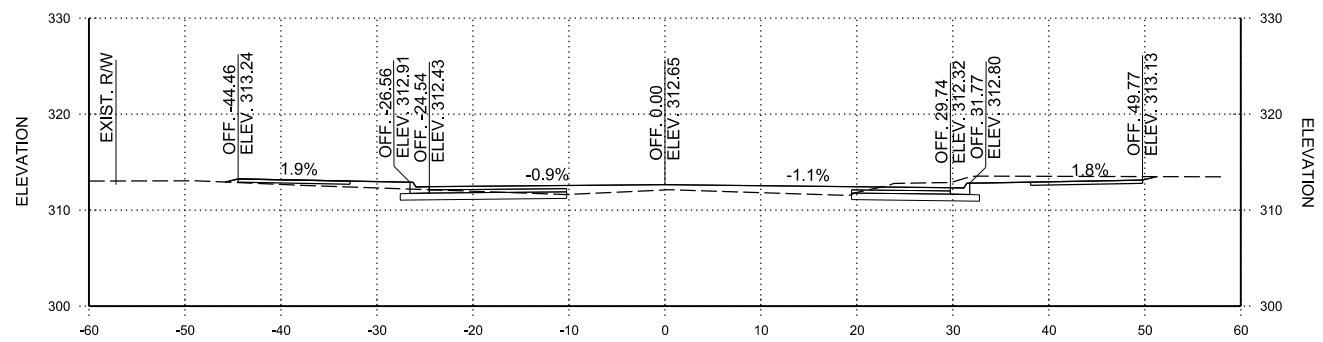
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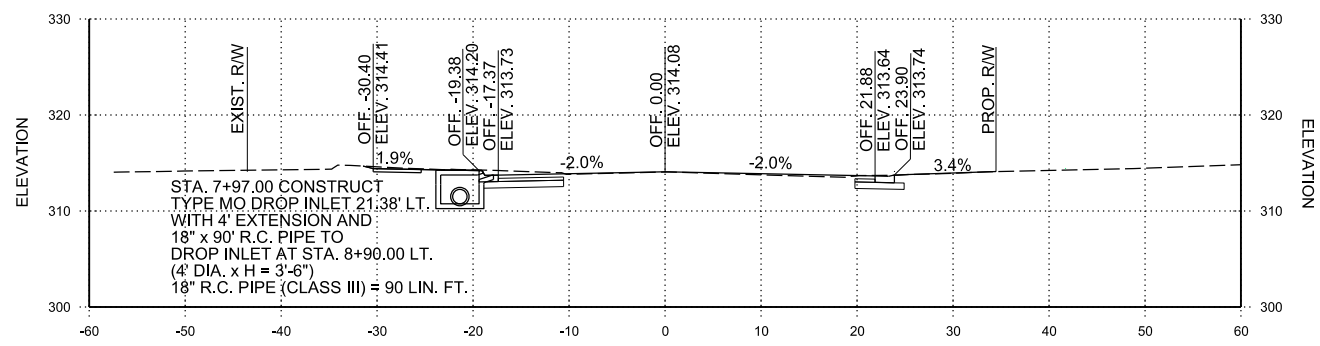
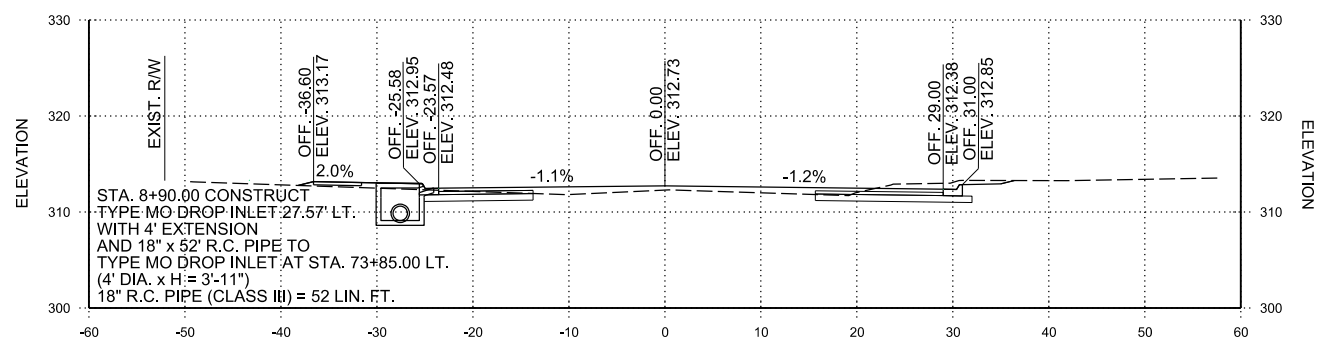
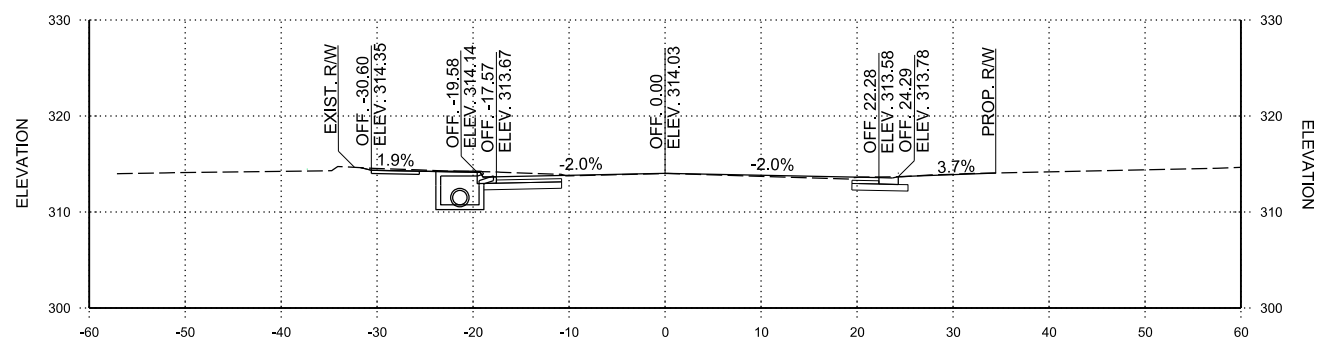
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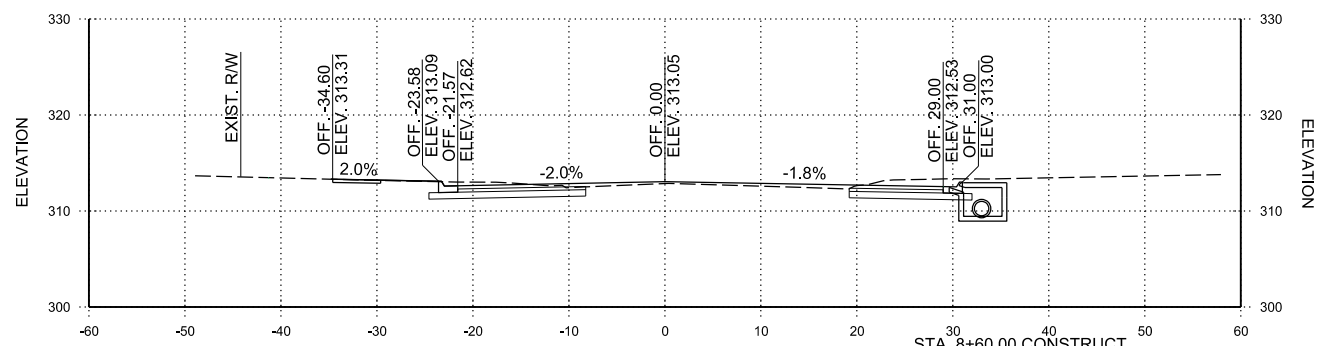
STA. 8+21.51 CONSTRUCT COMMERCIAL DRIVEWAY LT.



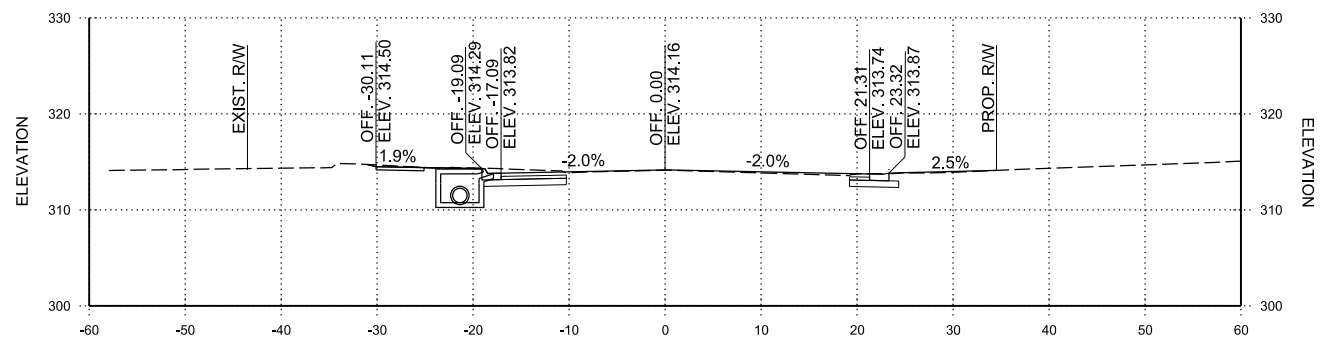
STA. 8+90.00 CONSTRUCT TYPE MO DROP INLET 27.57' LT. WITH 4' EXTENSION AND 18" x 52' R.C. PIPE TO TYPE MO DROP INLET AT STA. 73+85.00 LT. (4' DIA. x H= 3'-11") 18" R.C. PIPE (CLASS III) = 52 LIN. FT.



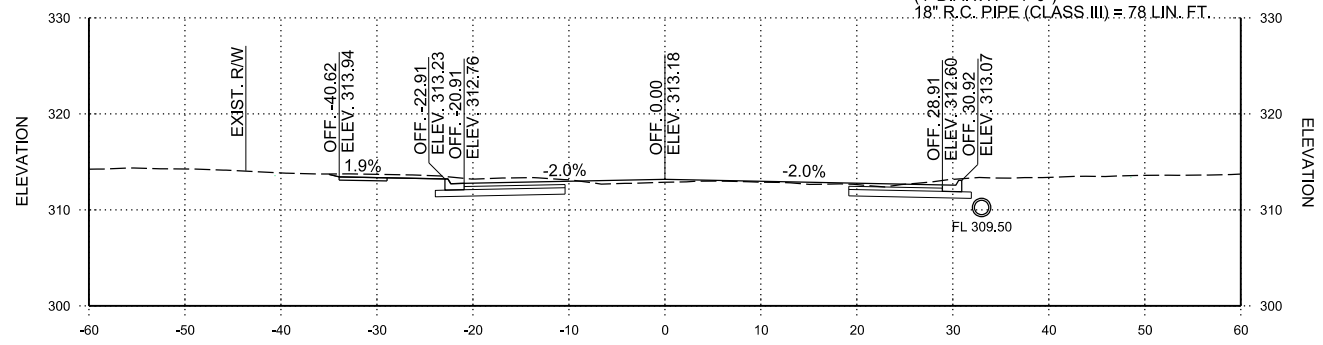
STA. 7+97.00 CONSTRUCT TYPE MO DROP INLET 21.38' LT. WITH 4' EXTENSION AND 18" x 90' R.C. PIPE TO DROP INLET AT STA. 8+90.00 LT. (4' DIA. x H = 3'-6") 18" R.C. PIPE (CLASS III) = 90 LIN. FT.



STA. 8+60.00 CONSTRUCT TYPE MO DROP INLET 33.00' RT. WITH 4' EXTENSION AND 18" x 78' R.C. PIPE TO DROP INLET AT STA. 72+85.00 LT. (4' DIA. x H = 4'-0") 18" R.C. PIPE (CLASS III) = 78 LIN. FT.



STA. 7+92.69 CONSTRUCT COMMERCIAL DRIVEWAY RT.



COLLEGE AVE. (WEST)
 STA. 7+93 TO STA. 9+00

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
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 DRAWN BY: HJB

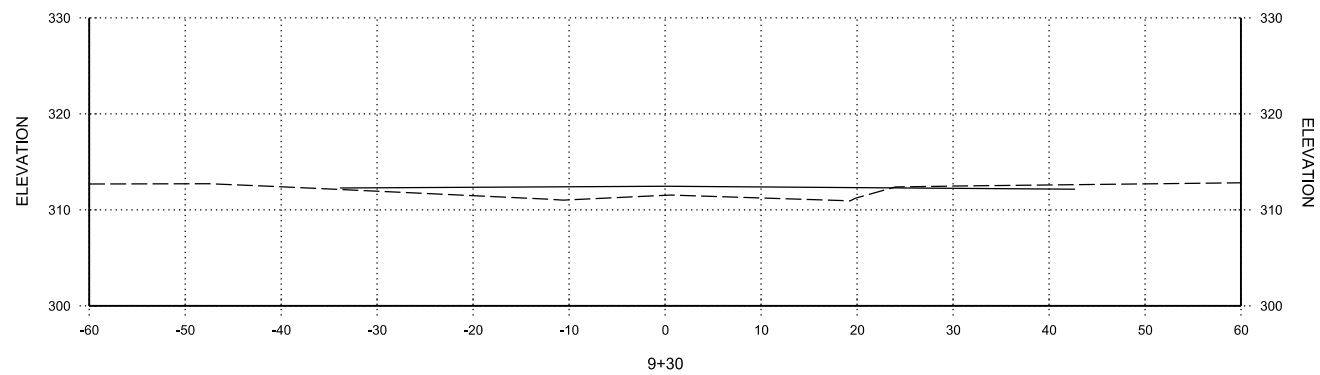
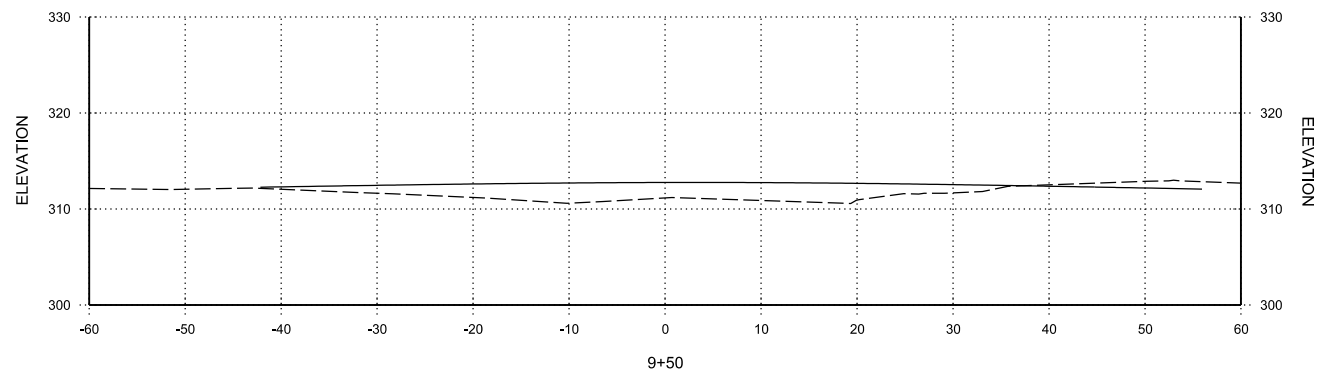
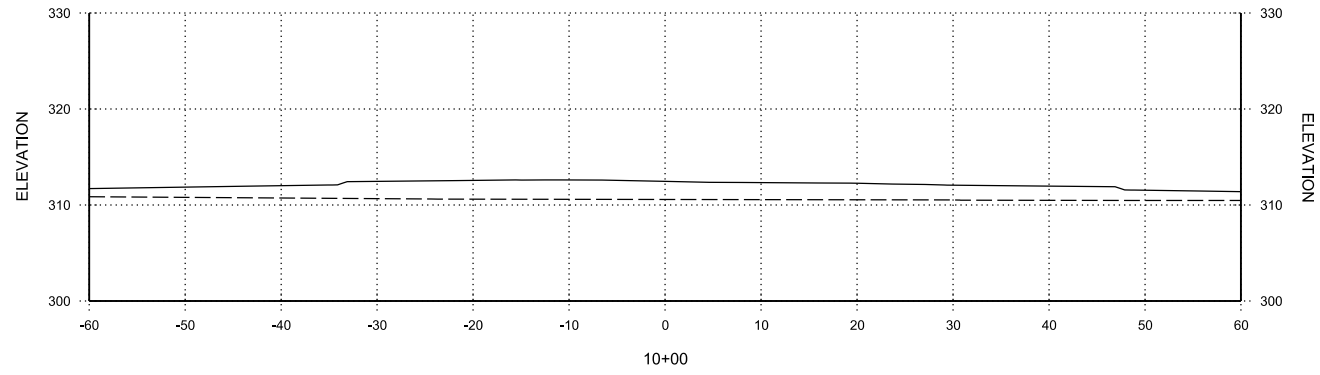
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COLLEGE AVE. (WEST)
 STA. 9+30 TO STA. 10+00

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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
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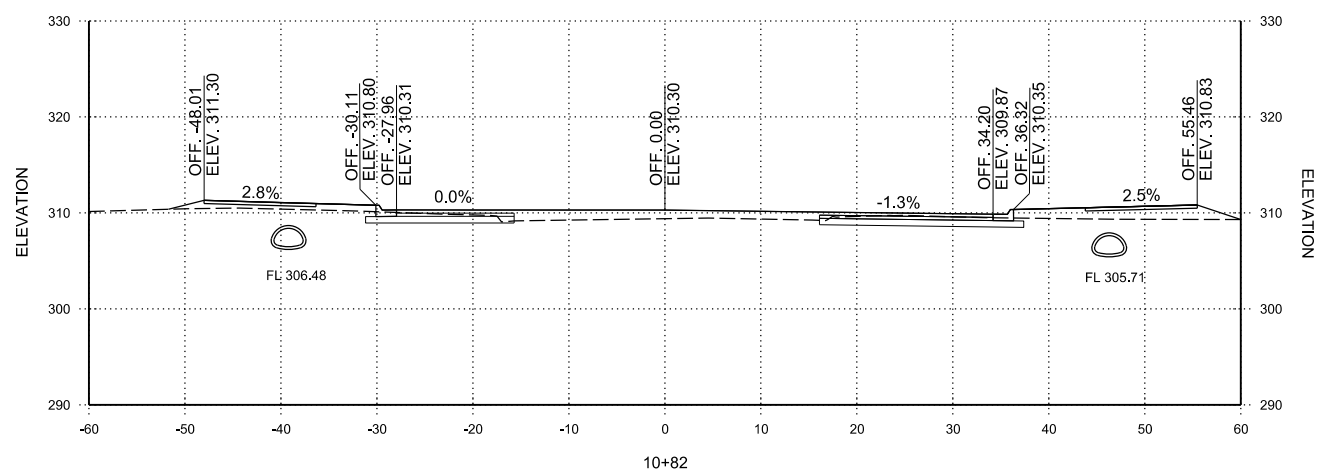
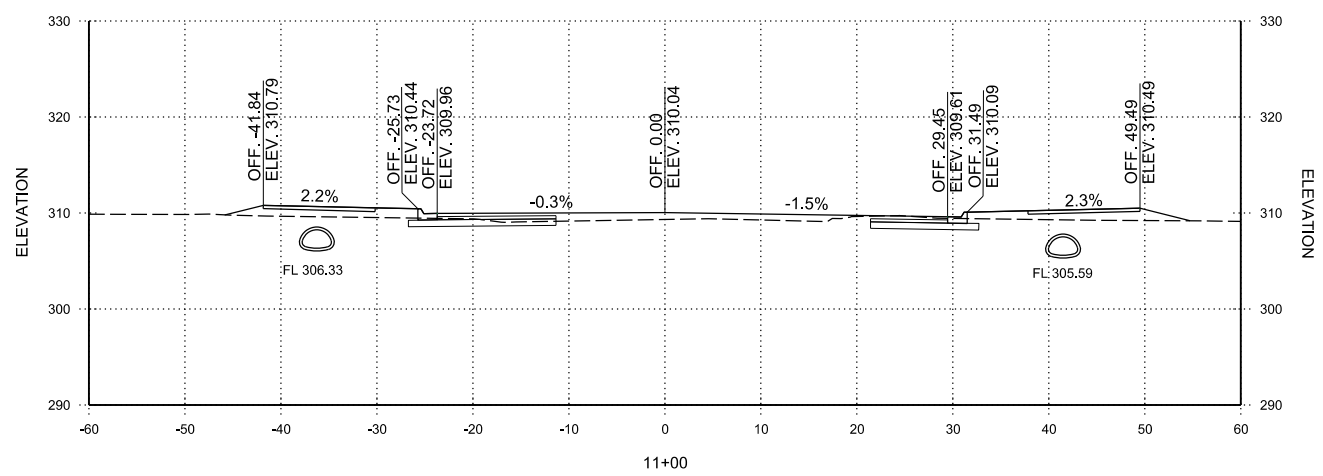
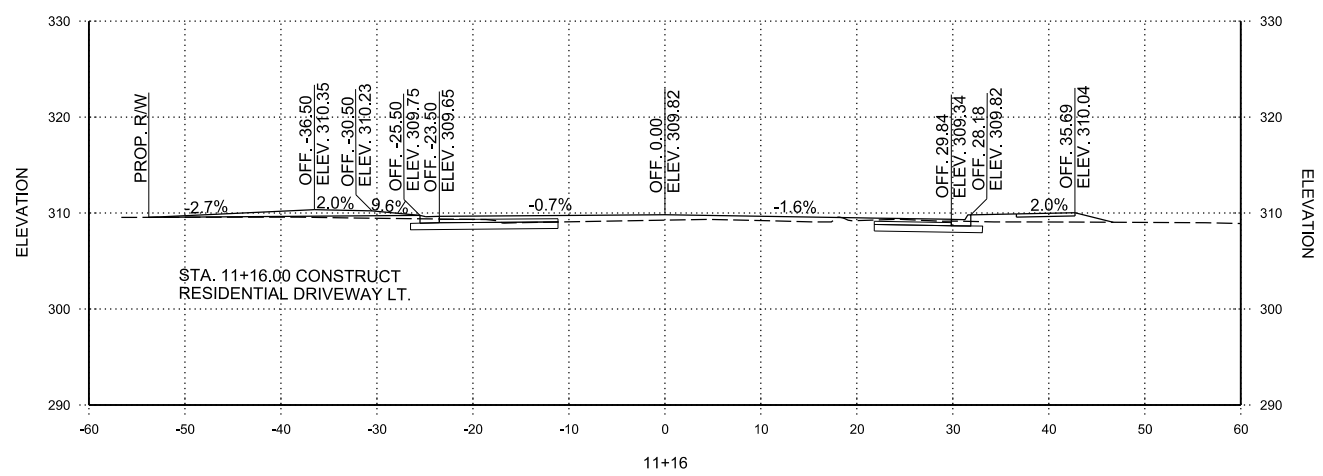
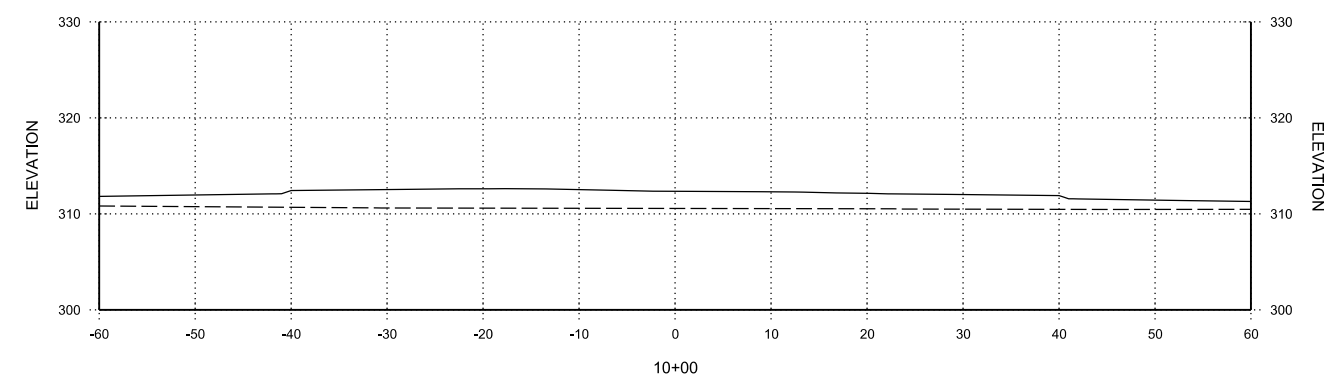
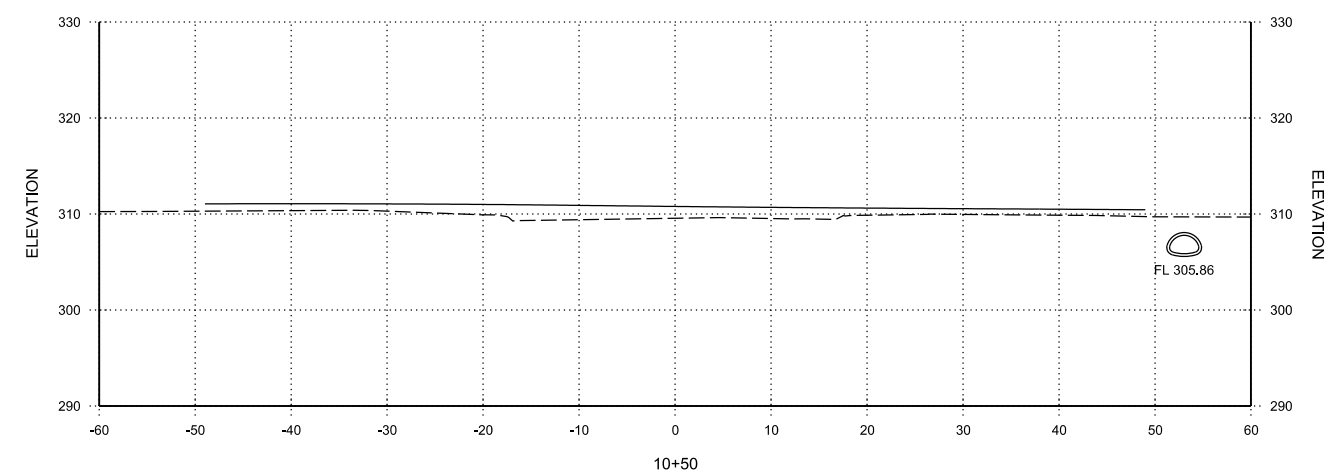
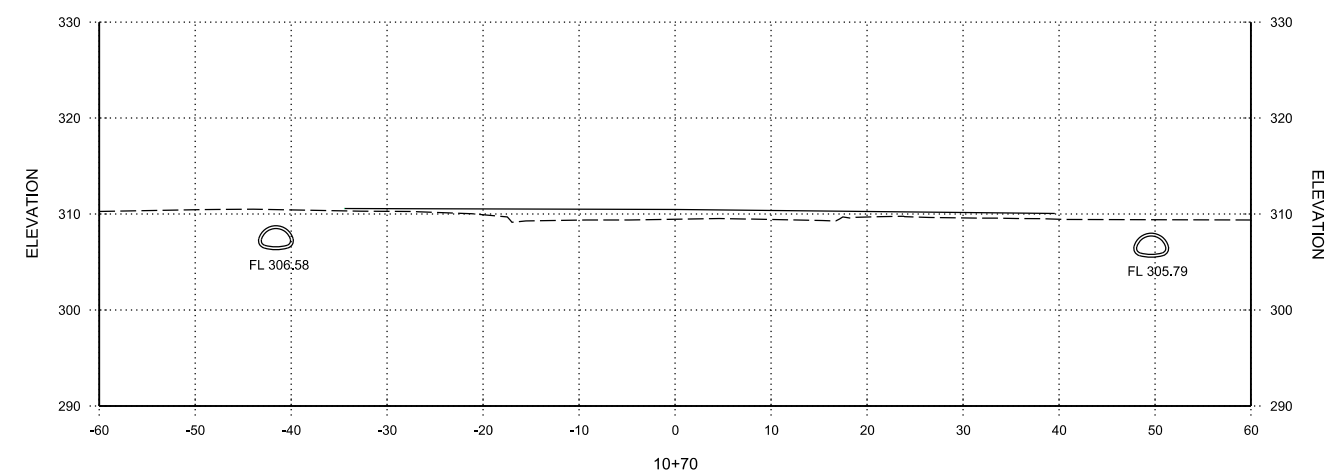
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CX-27

SHEET NUMBER
156



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COLLEGE AVE. (EAST)
 STA. 10+00 TO STA. 10+82

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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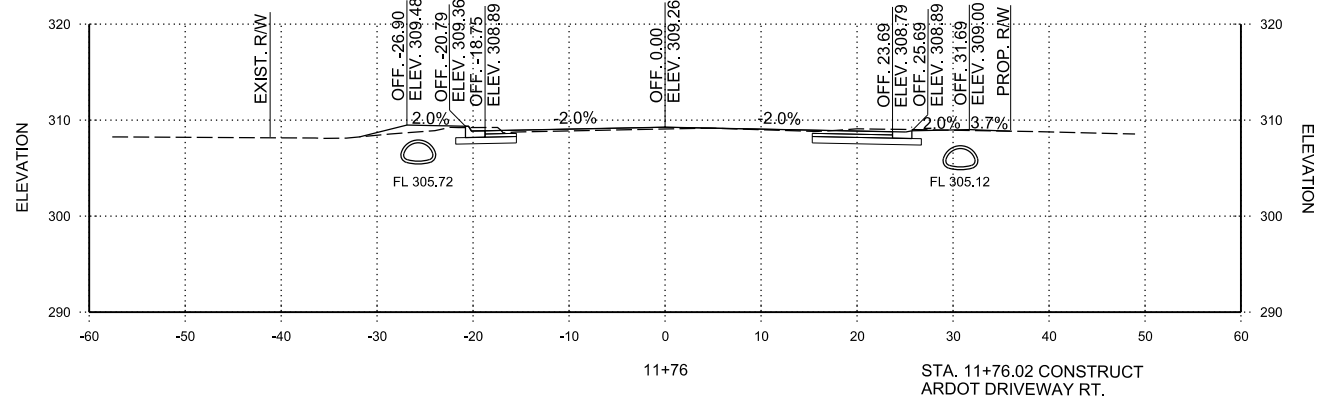
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SHEET NUMBER
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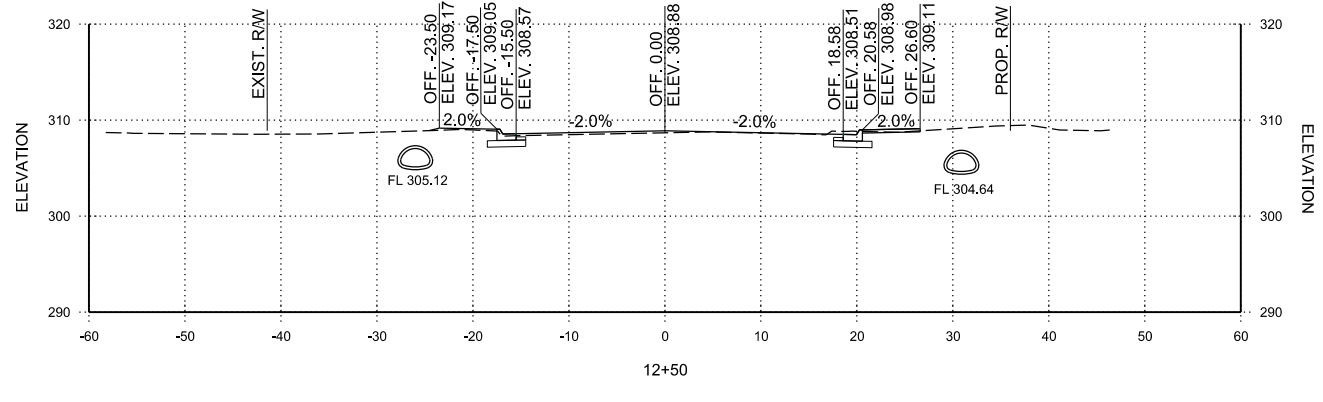
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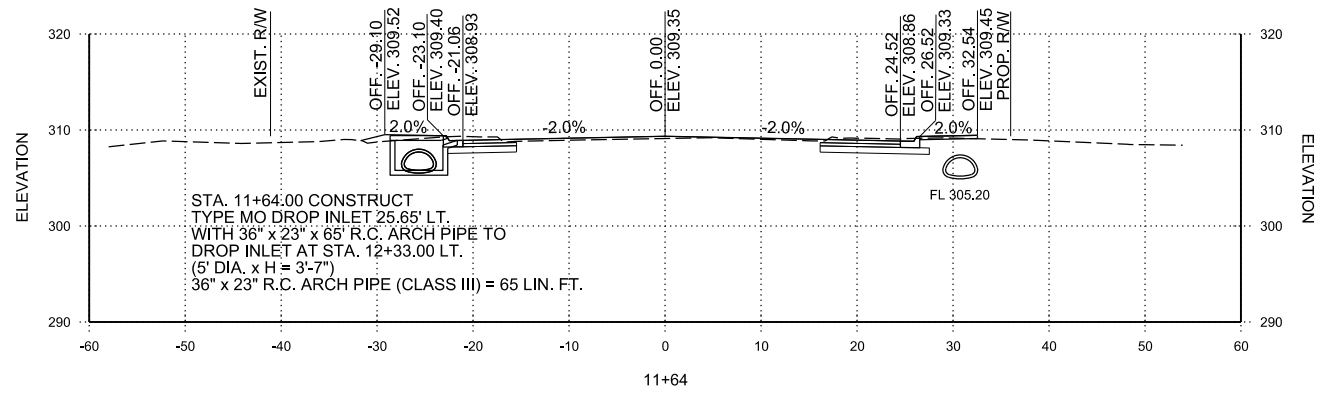
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STA. 11+76.02 CONSTRUCT
 ARDOT DRIVEWAY RT.

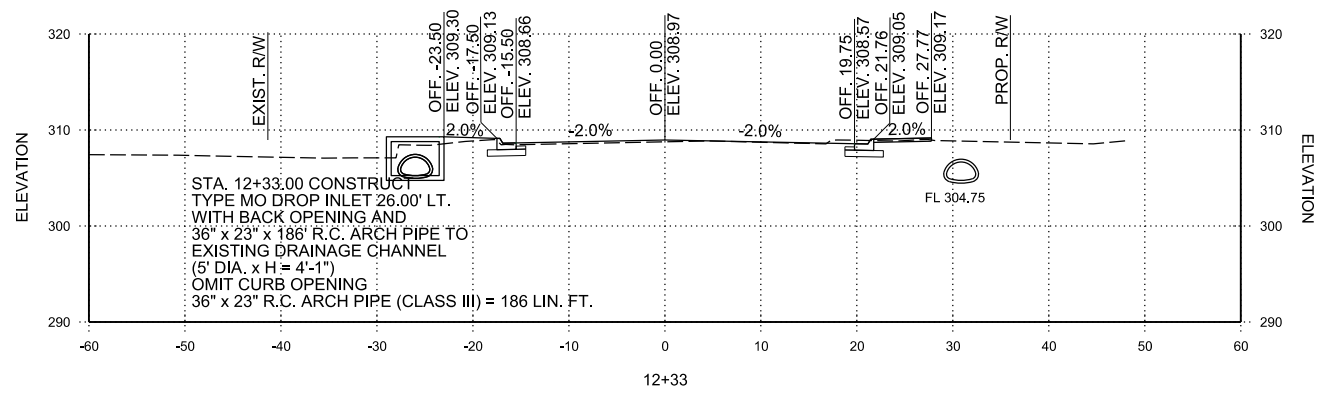


12+50



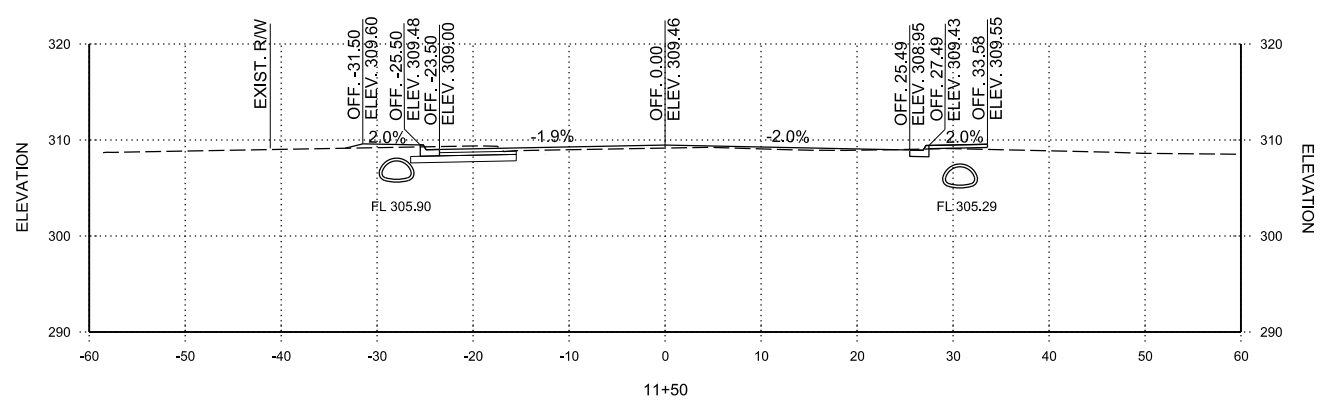
STA. 11+64.00 CONSTRUCT
 TYPE MO DROP INLET 25.65' LT.
 WITH 36" x 23" x 65' R.C. ARCH PIPE TO
 DROP INLET AT STA. 12+33.00 LT.
 (5' DIA. x H = 3'-7")
 36" x 23" R.C. ARCH PIPE (CLASS III) = 65 LIN. FT.

11+64

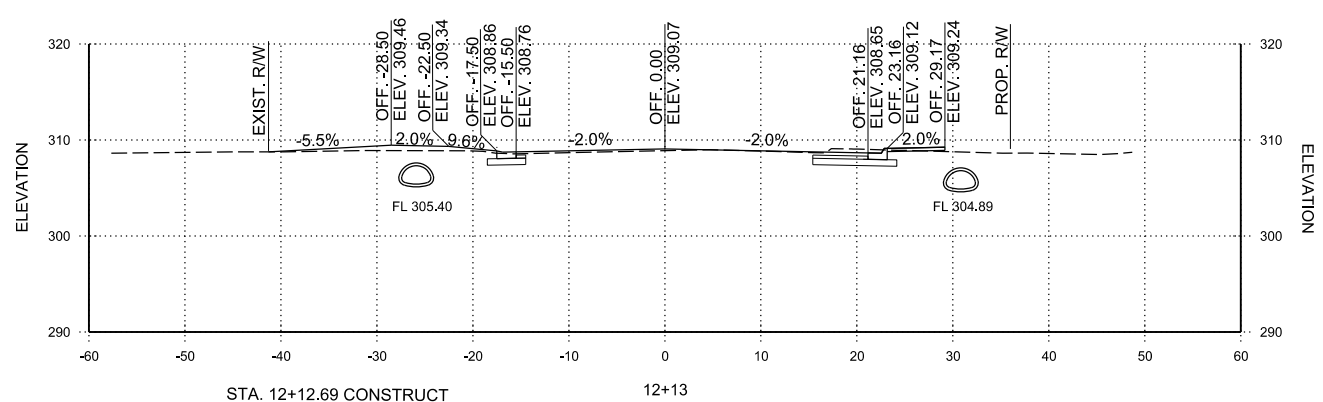


STA. 12+33.00 CONSTRUCT
 TYPE MO DROP INLET 26.00' LT.
 WITH BACK OPENING AND
 36" x 23" x 186' R.C. ARCH PIPE TO
 EXISTING DRAINAGE CHANNEL
 (5' DIA. x H = 4'-1")
 OMIT CURB OPENING
 36" x 23" R.C. ARCH PIPE (CLASS III) = 186 LIN. FT.

12+33

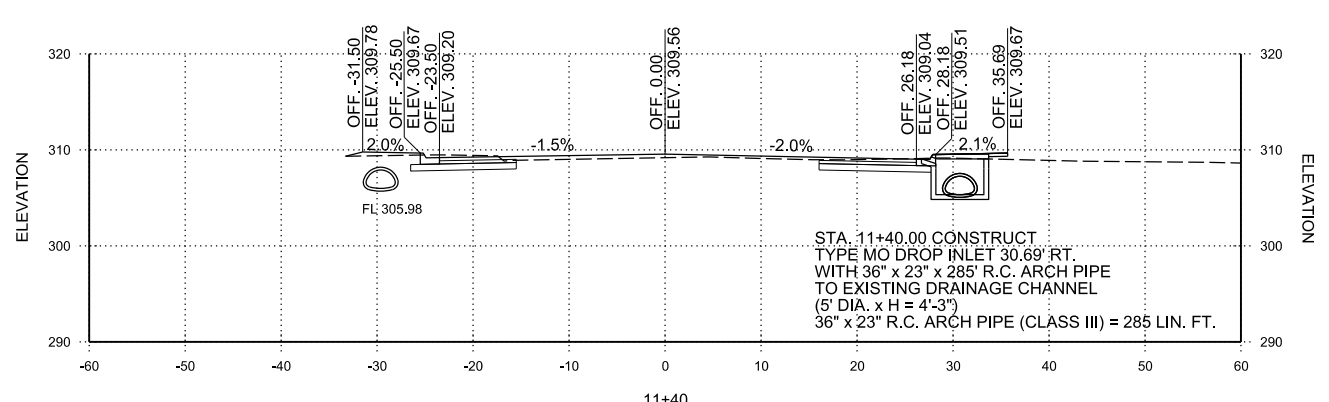


11+50



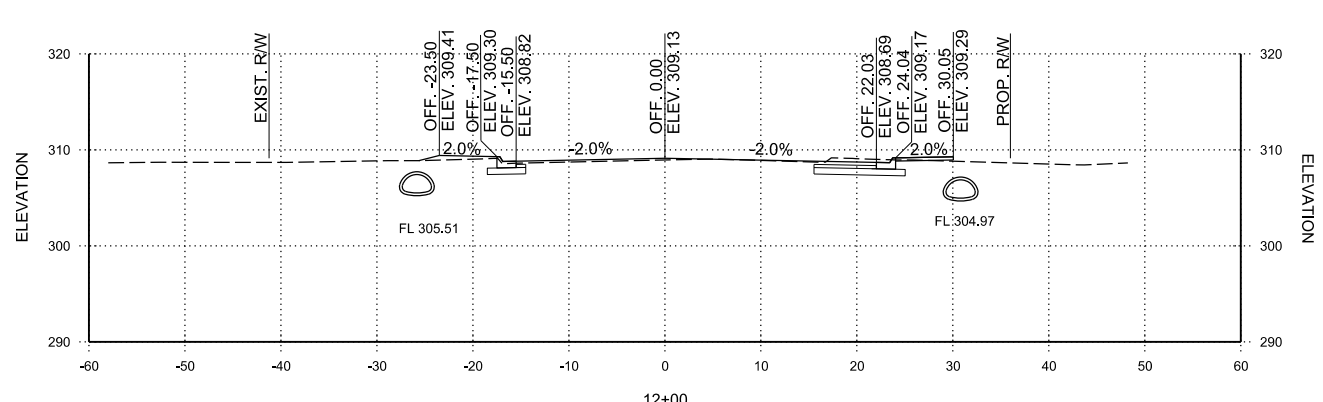
STA. 12+12.69 CONSTRUCT
 RESIDENTIAL DRIVEWAY LT.

12+13



STA. 11+40.00 CONSTRUCT
 TYPE MO DROP INLET 30.69' RT.
 WITH 36" x 23" x 285' R.C. ARCH PIPE
 TO EXISTING DRAINAGE CHANNEL
 (5' DIA. x H = 4'-3")
 36" x 23" R.C. ARCH PIPE (CLASS III) = 285 LIN. FT.

11+40



12+00

COLLEGE AVE. (EAST)
 STA. 11+40 TO STA. 12+50

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS SECTIONS

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

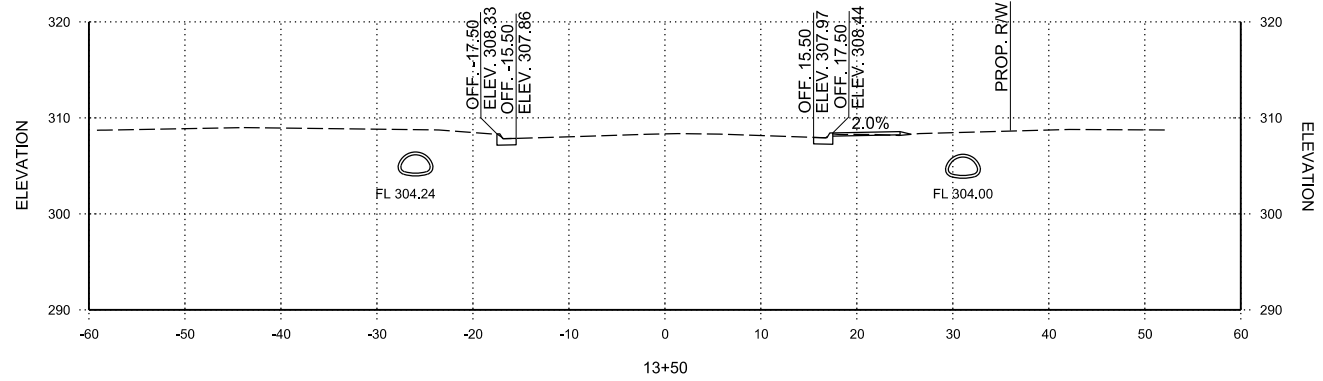
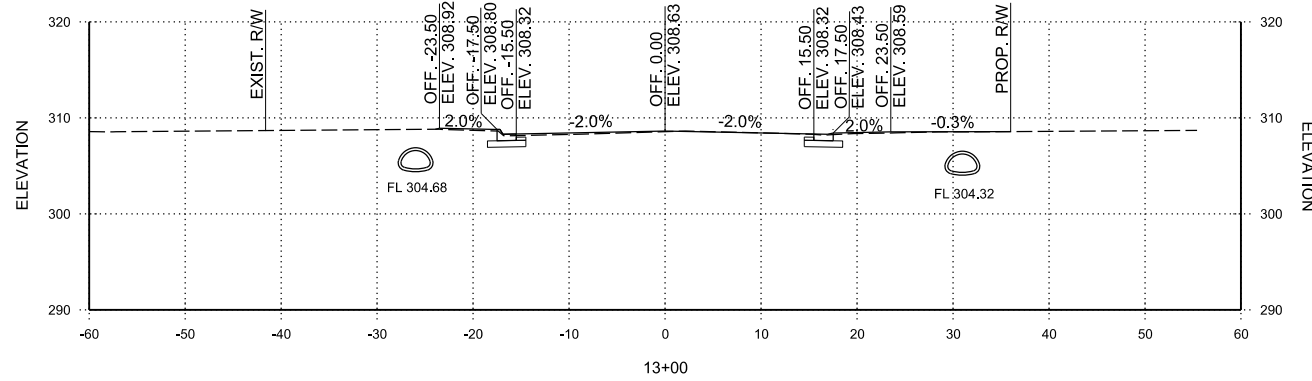
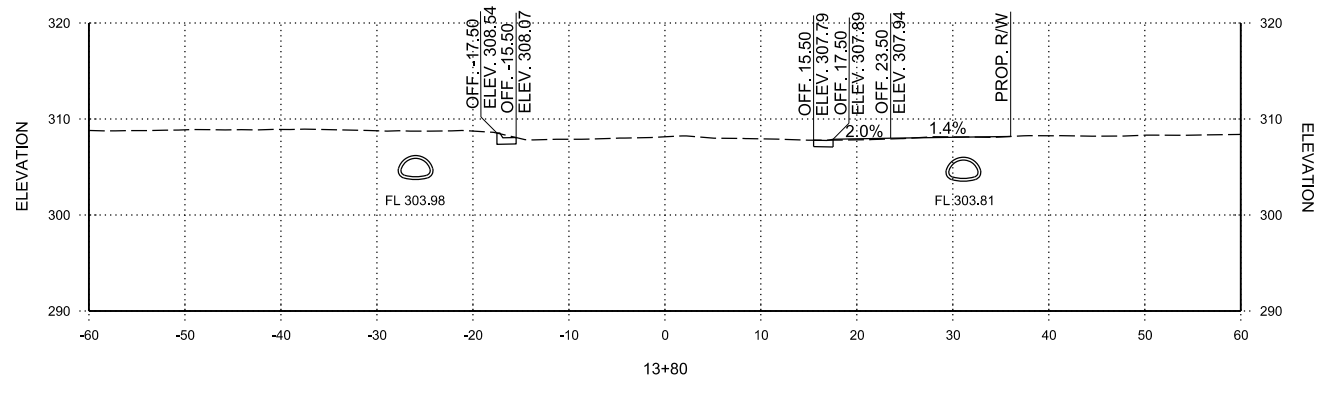
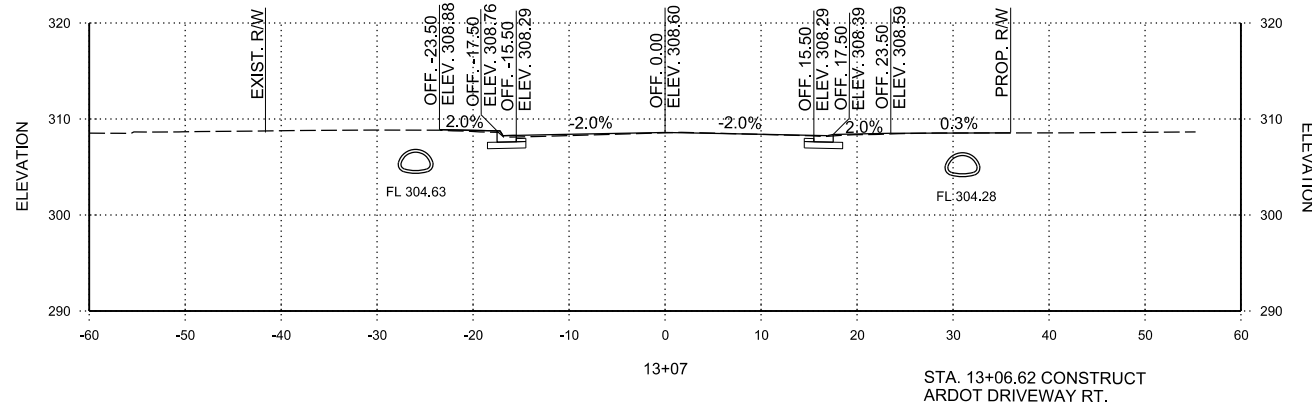
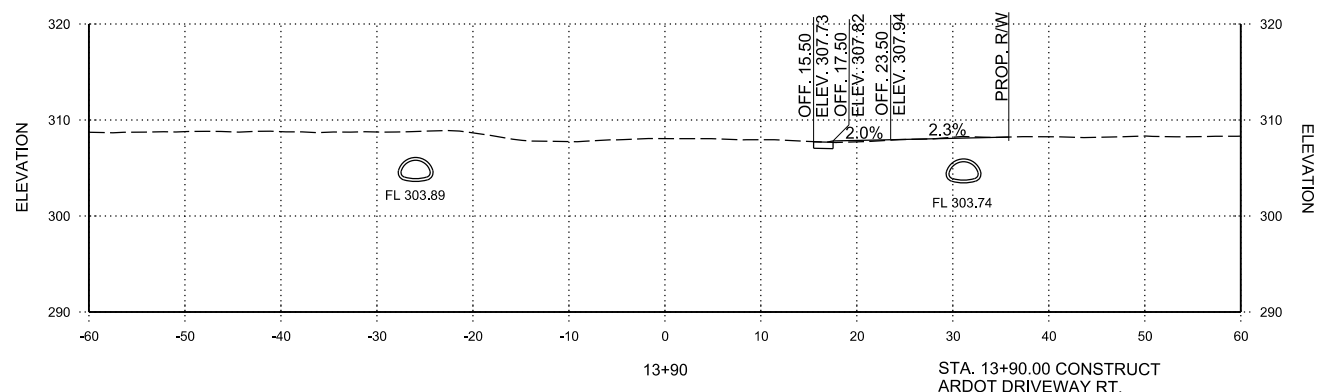
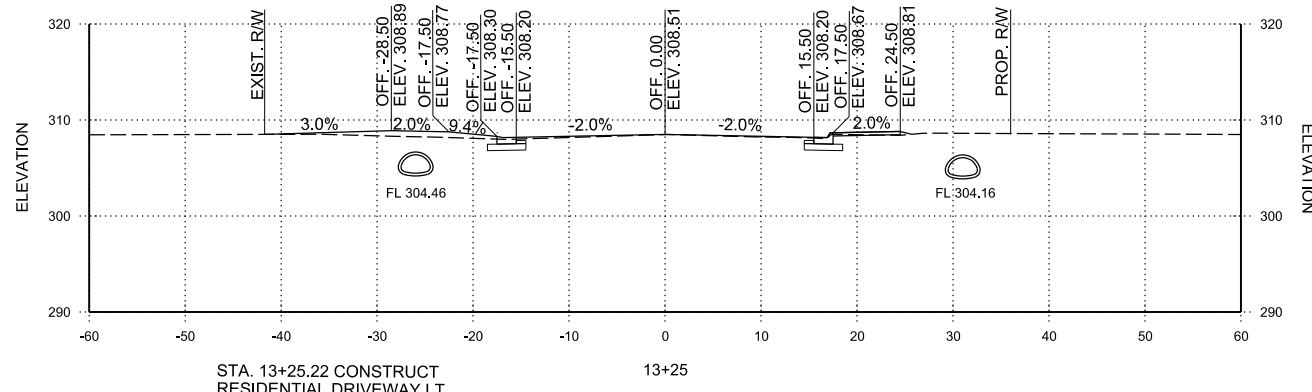
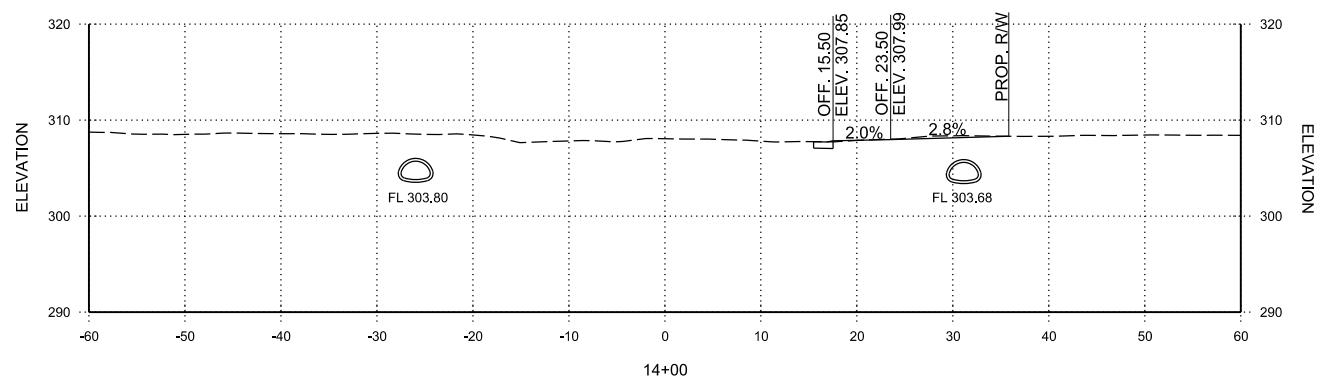
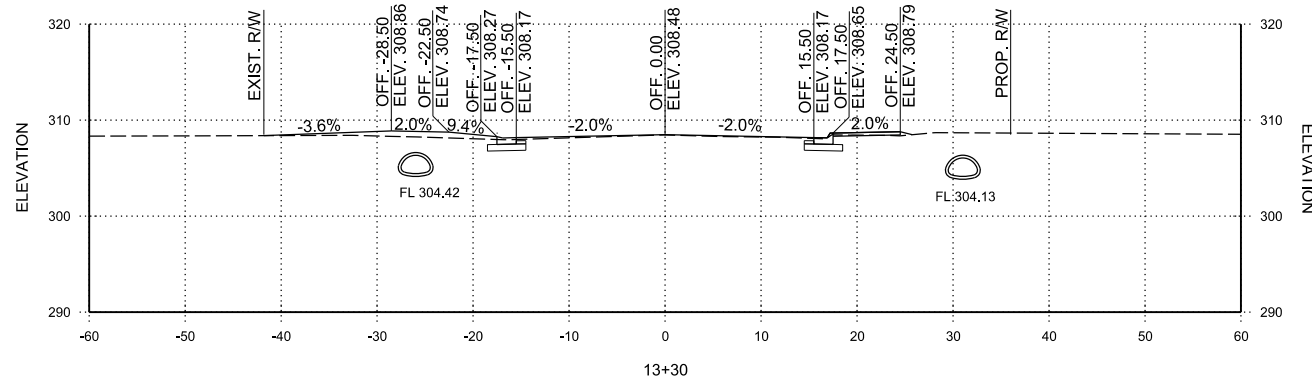
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CX-29

SHEET NUMBER
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COLLEGE AVE. (EAST)
 STA. 13+00 TO STA. 14+00



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

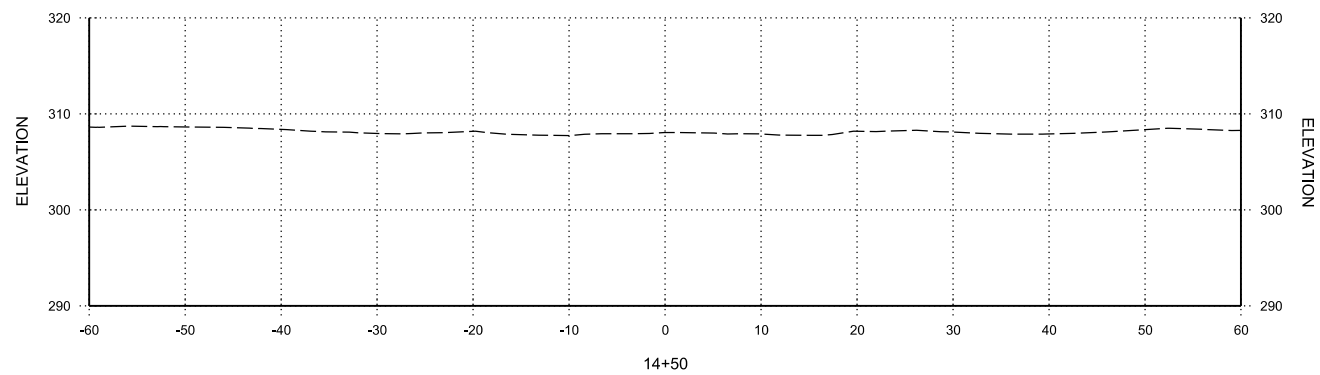
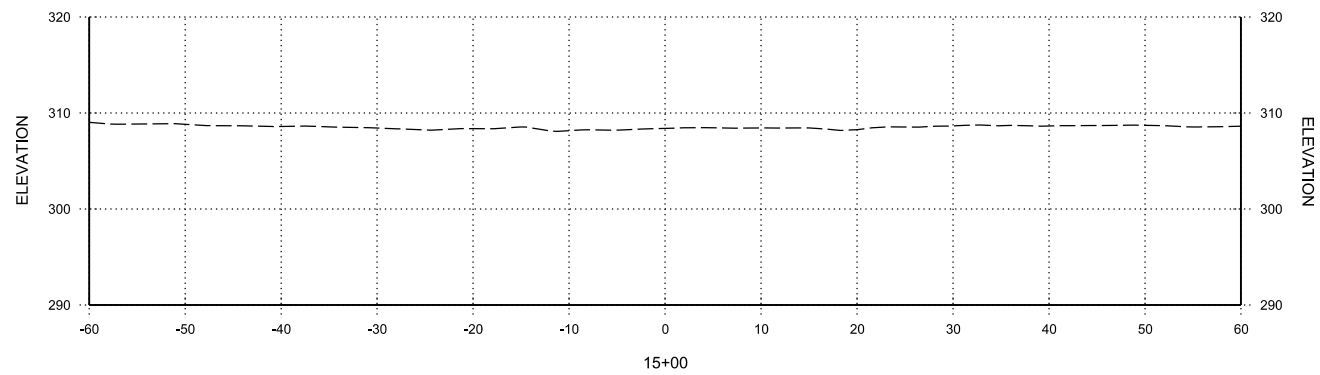
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JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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CX-30
 SHEET NUMBER
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COLLEGE AVE. (EAST)
 STA. 14+50 TO STA. 15+00



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REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS

DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

CROSS
 SECTIONS

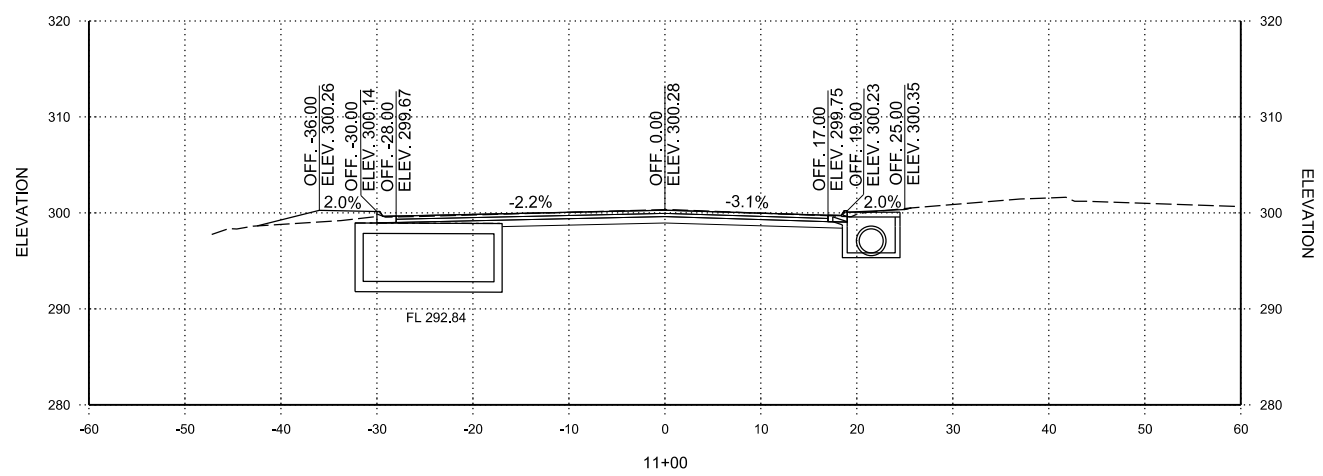
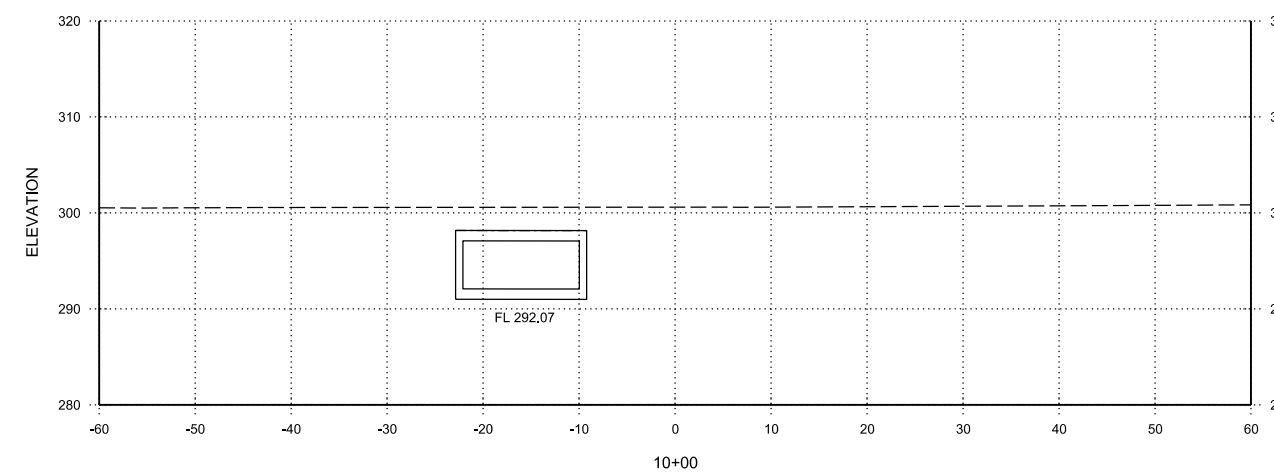
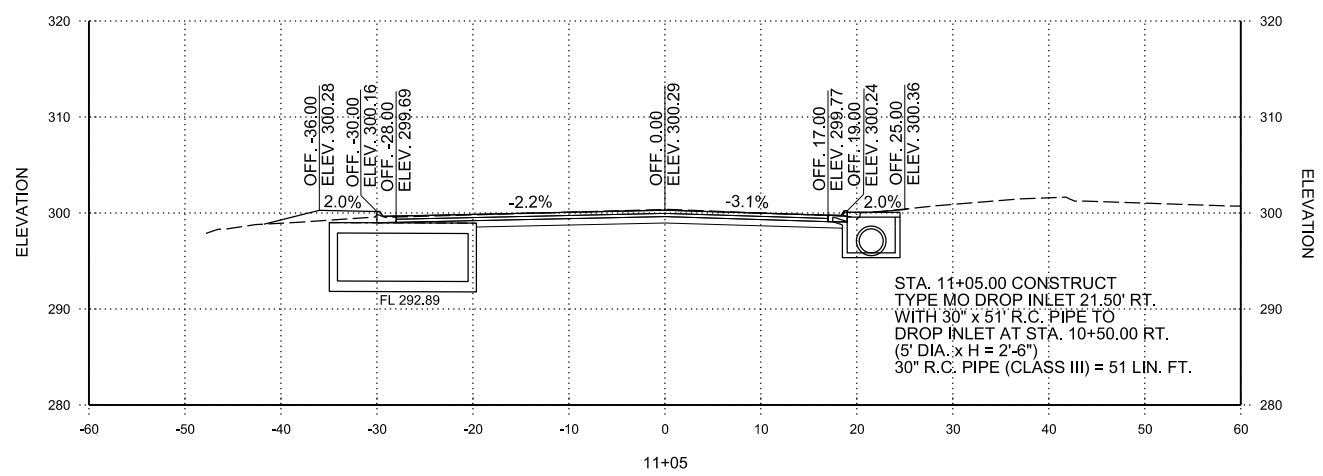
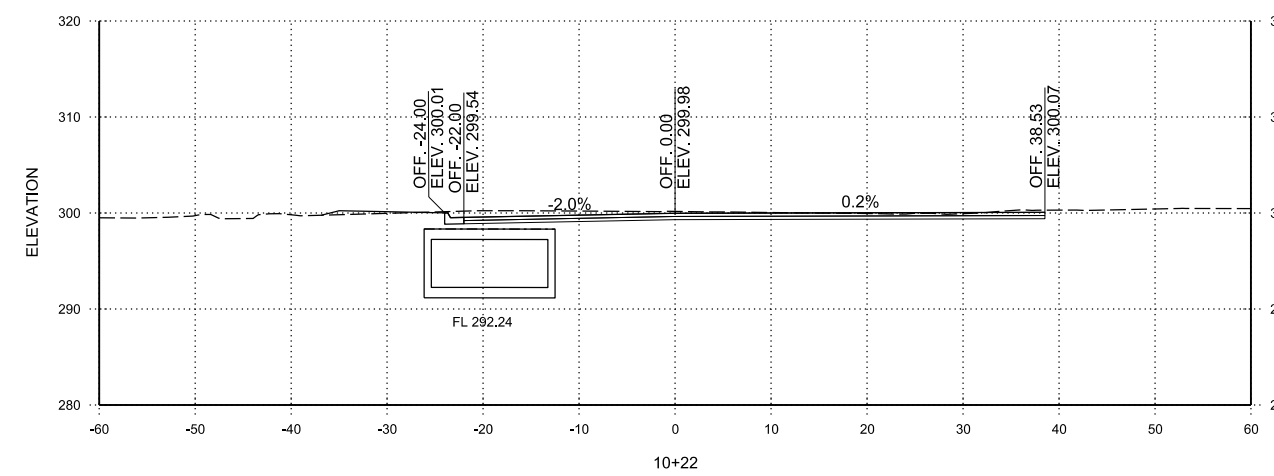
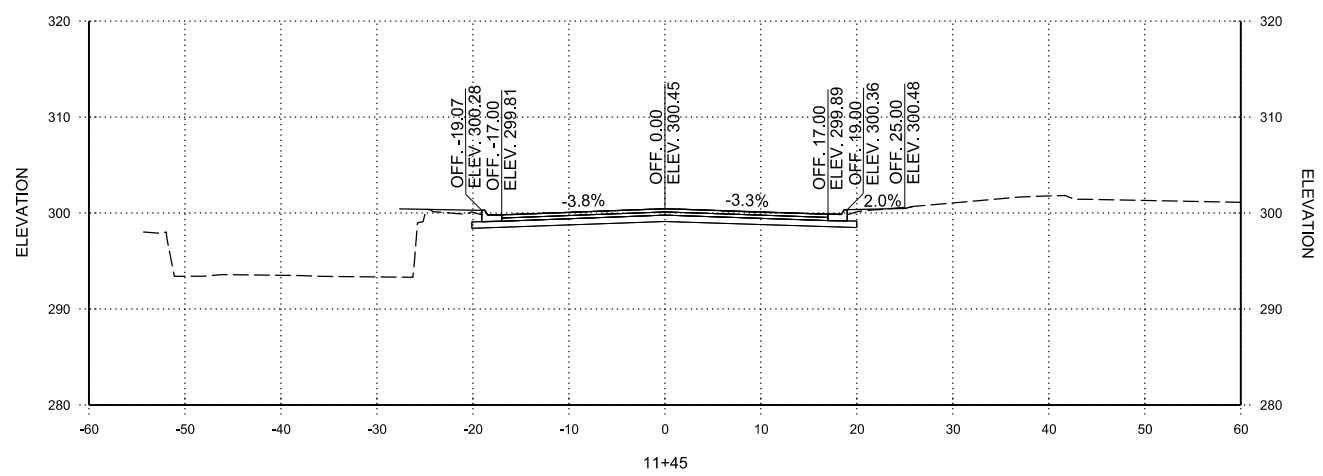
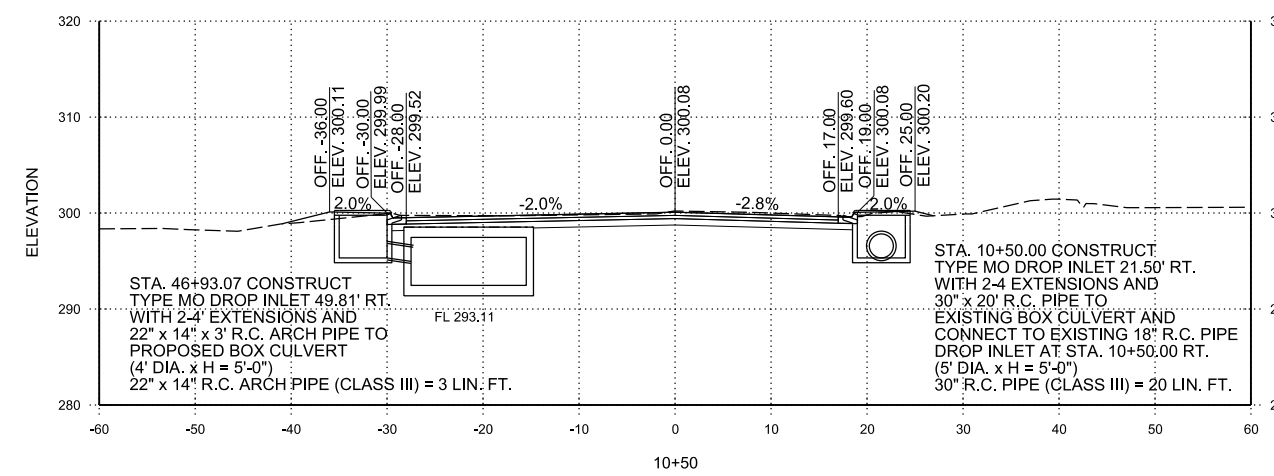
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 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB

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ROBINS ST.
 STA. 10+00 TO STA. 11+45

REV.	DATE	DESCRIPTION	BY



CITY OF CONWAY
 CONWAY, ARKANSAS
 DONAGHEY AVE. IMPROVEMENTS
 PHASE 1
 (DAVE WARD DR. TO ADA AVE.)

JOB NO.: 18147010
 DATE: MAY 2020
 DESIGNED BY: DLT
 DRAWN BY: HJB


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CX-32
 SHEET NUMBER
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GENERAL CONSTRUCTION REQUIREMENTS:

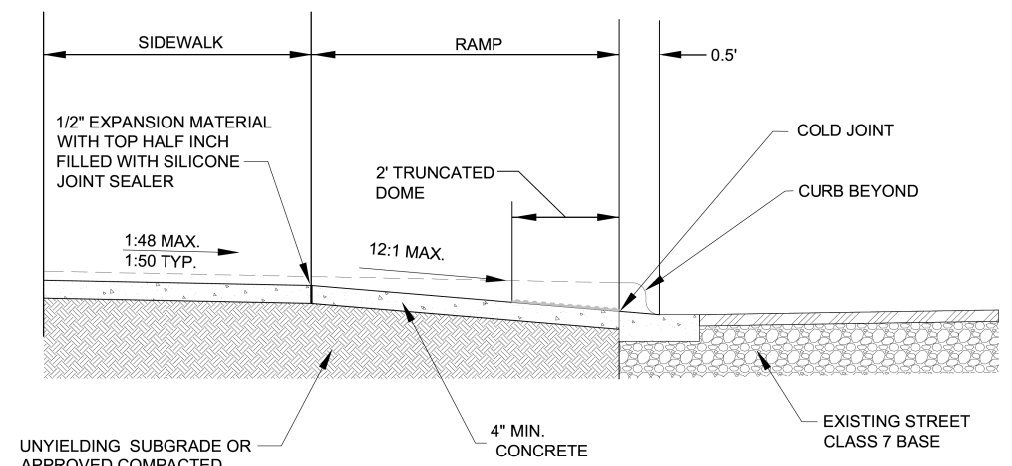
1. (ALL CONSTRUCTION WITHIN THE PUBLIC ROW OR PUBLIC EASEMENTS SHALL CONFORM TO ARDOT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION UNLESS AMENDED HEREIN.)
2. THE CONWAY TRANSPORTATION DEPARTMENT SHALL BE NOTIFIED TWENTY-FOUR (24) HOURS PRIOR TO PLACEMENT OF ANY FILL MATERIAL, INSTALLATION OF STORM DRAINAGE PIPE OR DRAINAGE STRUCTURES, CONCRETE CURB & GUTTER, PLACEMENT OF CRUSHED STONE OR ASPHALT. THE SUB-GRADE SHALL BE APPROVED BY THE CONWAY TRANSPORTATION DEPARTMENT PRIOR TO PLACEMENT OF CURB & GUTTER OR CRUSHED STONE.
3. EARTHWORK EQUIPMENT SHALL INCLUDE AN APPROPRIATE SIZED VIBRATORY SHEEPS FOOT COMPACTOR, (WATER TRUCK) AND MOTOR PATROL.
4. THE STREETS SHALL BE SHAPED AND GRADED IN ACCORDANCE WITH THE APPROVED STREET PLANS. THE SUB GRADE SHALL BE COMPACTED TO 95% MODIFIED PROCTOR DENSITY (AS SHOWN ON THE CITY OF CONWAY TYPICAL SECTIONS.) SOFT, YIELDING SECTIONS OF SUB GRADE SHALL BE REMOVED AND REPLACED IN SIX (6) INCH MAXIMUM LIFT THICKNESSES WITH EACH LIFT COMPACTED WITH A SHEEPS FOOT ROLLER, COMPACTION WITH TRACK EQUIPMENT OR OTHER EQUIPMENT NOT SPECIFICALLY DESIGNED FOR EARTHWORK COMPACTION IS NOT SUITABLE. TO 95% MODIFIED PROCTOR DENSITY. FILL MATERIAL SHALL BE APPROVED BY THE CONWAY TRANSPORTATION DEPARTMENT PRIOR TO USE IN STREET FILLS, NO TOP SOIL OR ORGANIC MATERIAL SHALL BE INCLUDED IN THE FILL MATERIAL. THE MOISTURE CONTENT OF THE FILL MATERIAL SHALL BE PLUS 3% OR MINUS 1% OF OPTIMUM. ALL EARTHWORK, INCLUDING THE SUB GRADE (AS SHOWN ON THE CITY OF CONWAY TYPICAL SECTIONS) SHALL CONFORM TO THE REQUIREMENTS OF SECTIONS 210 AND 212 OF THE ARKANSAS DEPARTMENT OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION). PRIOR TO PLACEMENT OF THE CRUSHED STONE BASE COURSE, THE SUB GRADE MUST FIELD DEMONSTRATE THAT IT IS FIRM AND UNYIELDING TO THE PASSAGE OF EQUIPMENT OVER THE SUB GRADE. THE THE SUB GRADE SHALL BE APPROVED BY THE CONWAY TRANSPORTATION DEPARTMENT BEFORE CURB AND GUTTER OR CRUSHED STONE IS PLACED.
5. THE (ASPHALT STREET) BASE SHALL CONSIST OF CRUSHED STONE BASE COURSE CONFORMING TO THE REQUIREMENTS OF CLASS 7 AGGREGATE BASE COURSE AS DESCRIBED IN SECTION 303 OF THE ARKANSAS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION). (THE BASE COURSE GRADATION SHALL CONFORM TO THIS SPECIFICATION AFTER IT HAS BEEN PLACED AND COMPACTED. SAMPLES OF THE IN-PLACE MATERIAL MAY BE OBTAINED AND TESTED BY THE OWNER TO ASSURE COMPLIANCE. MATERIAL NOT CONFORMING TO THE SPECIFICATIONS SHALL BE REMOVED AND REPLACED. IN ADDITION, THE CRUSHED STONE BASE COURSE MATERIAL SHALL HAVE A MINIMUM CBR ,CALIFORNIA BEARING RATIO, OF 75 AS DESCRIBED IN THE PROJECT SPECIFICATIONS.)
6. THE (ASPHALT STREET) SURFACE COURSE SHALL CONSIST OF ASPHALTIC CONCRETE HOT MIX CONFORMING TO THE REQUIREMENTS OF SECTION 407 OF THE ARKANSAS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION) OR THE ACHM SURFACE COURSE SPECIFICATION FOUND HEREIN.
7. (CONCRETE STREET) BASE COURSE OR SUB GRADE IS TO BE COMPACTED IN KEEPING WITH ARKANSAS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION), SECTION 302. CONCRETE PAVEMENT IS TO BE CONSTRUCTED ACCORDING TO SECTION 501 OF THE ARKANSAS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION). CONCRETE IS TO BE 3,000 LBS. PSI COMPRESSED STRENGTH PLACED ON A THOROUGHLY COMPACTED AND GRADED SUB GRADE. CONCRETE IS TO BE POURED IN SEPARATE LANES WITH MAXIMUM WIDTH OF 15 FEET. TRANSVERSE JOINTS SHALL BE OF TWO (2) INCHES WIDE FELT STRIP 1/16TH TO 1/4 INCH THICK AT 15-FOOT INTERVALS FOR CONTRACTION JOINTS. THE POURED SURFACE GRADE SHALL HAVE A MINIMUM THICKNESS OF SIX, 6, INCHES OF CONCRETE. REINFORCING TIE BARS, WHERE REQUIRED, SHALL BE IN GENERAL CONFORMANCE WITH THE PORTLAND CEMENT ASSOCIATION RECOMMENDATIONS.
8. EXPANSION JOINTS SHALL BE PLACED ON EACH SIDE OF DRAINAGE STRUCTURES, AT THE ENDS OF THE RADIUS AT INTERSECTIONS AND CUL-DE-SACS AND AT MAXIMUM ON HUNDRED, 100, FOOT SPACING THROUGHOUT THE LENGTH OF THE CURB AND GUTTER. EXPANSION JOINTS, 1/2" PREMOLDED MATERIAL, SHALL BE PROVIDED IN THE SIDEWALK WHERE ABUTTING DRIVEWAYS, CONCRETE CURB AND GUTTER OR OTHER RIGID ITEMS AND AT ONE HUNDRED, 100, FOOT MAXIMUM SPACING THROUGHOUT THE LENGTH OF THE SIDEWALK. MATERIAL AND CONSTRUCTION SHALL CONFORM TO THE REQUIREMENTS OF SECTION 634 OF THE ARKANSAS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION).
9. ALL STORM DRAINAGE PIPE (INCLUDING FLARED END SECTIONS AND HEADWALLS) SHALL BE RCP CLASS III UNLESS SPECIFICALLY APPROVED OTHERWISE BY THE CITY ENGINEER.
10. ALL MUD AND SOIL (INCLUDING LOOSE GRAVEL) SHALL BE REMOVED FROM THE CRUSHED STONE BASE AND CONCRETE CURB AND GUTTER PRIOR TO SET UP OF THE CRUSHED STONE COURSES (AND THE PLACEMENT OF ASPHALT).
11. STORM DRAINAGE PIPES, DITCHES AND DRAINAGE STRUCTURES MUST BE FREE OF SEDIMENTS, TRASH, DEBRIS AND PONDING WATER PRIOR TO FINAL APPROVAL OF THE STREETS.
12. THE CITY ENGINEER SHALL BE GIVEN TWENTY-FOUR, 24, HOURS NOTICE PRIOR TO PLACEMENT OF STORM DRAINAGE PIPE (INCLUDING DRAINAGE STRUCTURES, FORMING, AND REINFORCING STEEL), SUB GRADE PREPARATION, CURB AND GUTTER CONSTRUCTION, CRUSHED STONE BASE COURSE PLACEMENT OR STREET PAVEMENT. NO WORK SHALL PROCEED UNTIL APPROVAL HAS BEEN GIVEN BY THE CITY ENGINEER.

	CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165	GENERAL INFORMATION	DATE: FEBRUARY 2017	SHEET:
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				G-1
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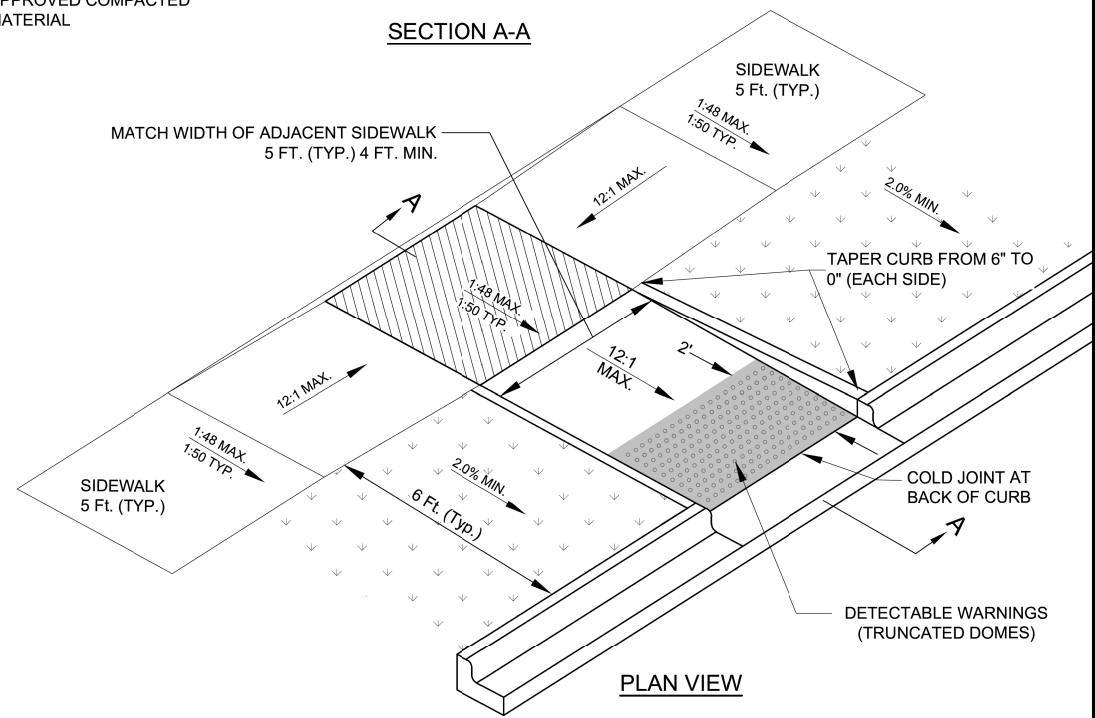
GENERAL CONSTRUCTION REQUIREMENTS CONTINUED:

13. (CONSTRUCTION SHALL NOT COMMENCE ON THIS PROJECT UNTIL A STORMWATER POLLUTION PREVENTION PLAN HAS BEEN PROPERLY IMPLEMENTED AND THE REQUIREMENTS OF ADEQ HAVE BEEN MET.)
14. (THE CONTRACTOR SHALL PROVIDE APPROPRIATE ADVANCED WARNING DEVICES, BARRICADES, BARRELS AND OTHER MEASURES AS NEEDED TO PROPERLY CONTROL AND ADVISE TRAFFIC OF CONSTRUCTION EQUIPMENT THAT MAY BE ON OR ADJACENT TO THE PUBLIC STREETS.)
15. (THE CONTRACTOR SHALL KEEP THE ADJACENT PUBLIC STREETS CLEAN AND FREE OF SEDIMENT, GRAVEL AND OTHER DEBRIS.)
16. (THE CONTRACTOR SHALL REPAIR ANY DAMAGE TO THE PUBLIC STREETS AND INFRASTRUCTURE THAT THE CONSTRUCTION ACTIVITY OR HAULING OF MATERIAL MAY CAUSE.)

	CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165	GENERAL INFORMATION	DATE: FEBRUARY 2017	SHEET:
		GENERAL NOTES	REVISED	
				G-2
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SECTION A-A



PLAN VIEW

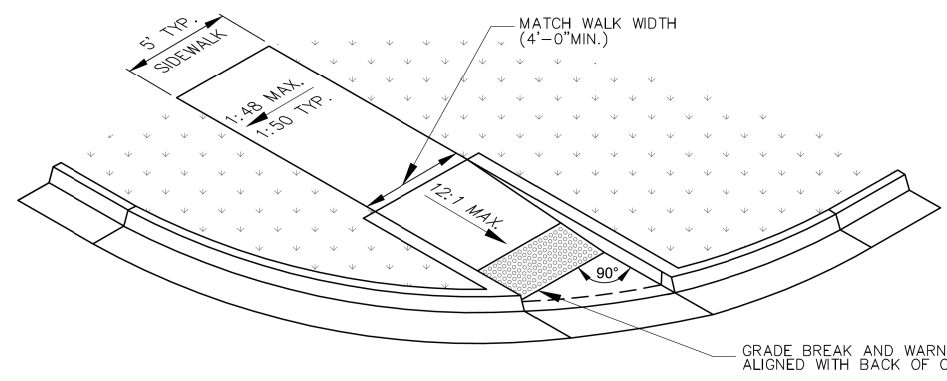
NOTES:

1. ALL MATERIALS AND CONSTRUCTION SHALL COMPLY WITH SECTION 633 OF THE ARKANSAS HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.
2. FULL DEPTH EXPANSION JOINTS (FOUR INCHES) SHALL BE PROVIDED AT THE EDGE OF THE SIDEWALK AND RAMP.
3. ALL SIDEWALKS AND DRIVEWAY APPROACHES SHALL BE CONSTRUCTED WITH A BROOM FINISH.
4. ALL SIDEWALKS AND CURB CUTS FOR HANDICAP RAMPS REQUIRE AN INSPECTION PRIOR TO CONCRETE PLACEMENT.

TYPE 3A ACCESS RAMP

N.T.S.

	CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165		TITLE: ACCESS RAMP DETAILS DESCRIPTION: STANDARD ACCESS RAMP (TYPE 3A)	DATE: FEBRUARY 2017 REVISED:	SHEET: AR-3
	DRAWN BY: NTR CHECKED BY: BJV FILE NAME: AR-3 TYPE 3A HANDI RAMP.dwg				



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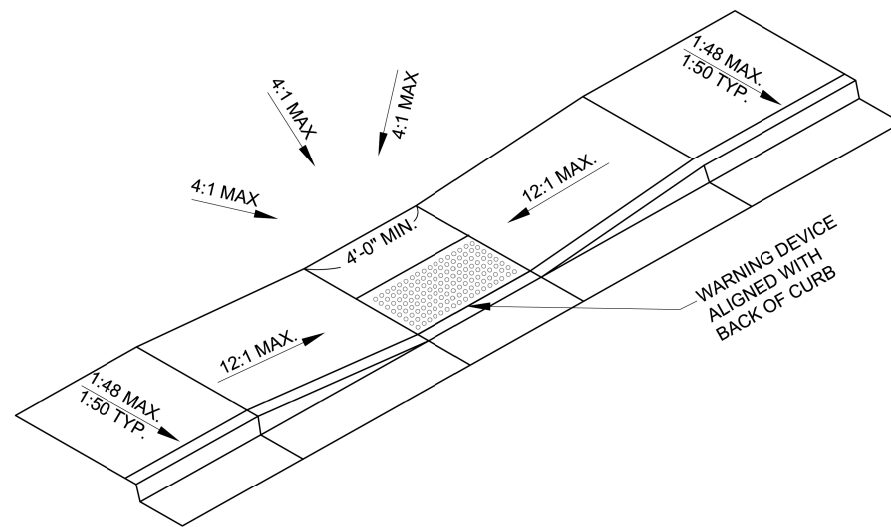
1. ALL MATERIALS AND CONSTRUCTION SHALL COMPLY WITH SECTION 633 OF THE ARKANSAS HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.
2. FULL DEPTH EXPANSION JOINTS (FOUR INCHES) SHALL BE PROVIDED AT THE EDGE OF THE SIDEWALK AND RAMP.
3. ALL SIDEWALKS AND DRIVEWAY APPROACHES SHALL BE CONSTRUCTED WITH A BROOM FINISH.
4. ALL SIDEWALKS AND CURB CUTS FOR HANDICAP RAMPS REQUIRE AN INSPECTION PRIOR TO CONCRETE PLACEMENT.

TYPE 3B ACCESS RAMP

N.T.S.

	CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165		TITLE: ACCESS RAMP DETAILS DESCRIPTION: STANDARD ACCESS RAMP (TYPE 3B)	DATE: FEBRUARY 2017 REVISED:	SHEET: AR-4
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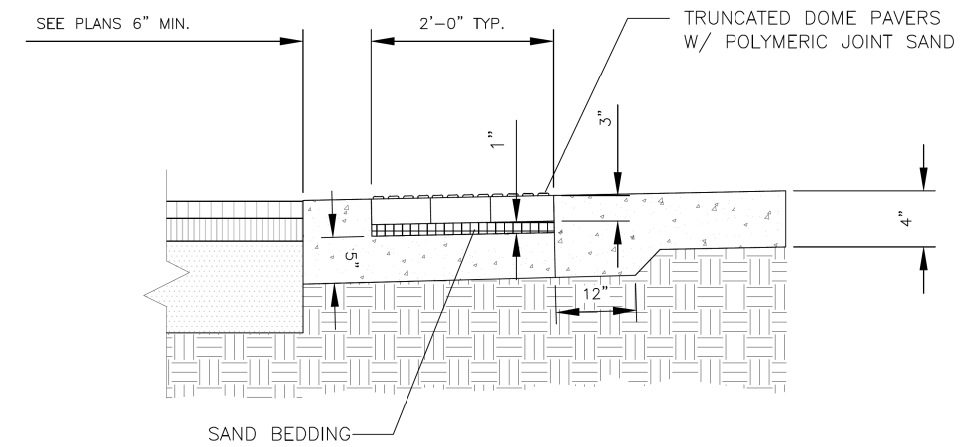
NOTES:

1. ALL MATERIALS AND CONSTRUCTION SHALL COMPLY WITH SECTION 633 OF THE ARKANSAS HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.
2. FULL DEPTH EXPANSION JOINTS (FOUR INCHES) SHALL BE PROVIDED AT THE EDGE OF THE SIDEWALK AND RAMP.
3. ALL SIDEWALKS AND DRIVEWAY APPROACHES SHALL BE CONSTRUCTED WITH A BROOM FINISH.
4. **ALL SIDEWALKS AND CURB CUTS FOR HANDICAP RAMPS REQUIRE AN INSPECTION PRIOR TO CONCRETE PLACEMENT.**

TYPE 5 ACCESS RAMP

N.T.S.

	CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165	TITLE: ACCESS RAMP DETAILS DESCRIPTION: STANDARD ACCESS RAMP (TYPE 5)	DATE: FEBRUARY 2017 REVISED:	SHEET:
	DRAWN BY: NTR CHECKED BY: BPV FILE NAME: AR-6 TYPE 5 HANDI RAMP.dwg	AR-6		



TRUNCATED DOME PAVERS

NOTES:

1. PAVERS TO BE SURROUNDED WITH 6" MIN. CONCRETE BAND FOR EDGE RESTRAINT.
2. JOINT SAND SHALL BE POLYMERIC.
3. TRUNCATED DOME PAVERS TO BE PINE HALL 4"x8" TRUNCATED ADA RED 360 OR APPROVED EQUAL.
4. SAND BEDDING FOR PAVERS TO BE MAX. 1" TO MIN. 1/2" THICK MASONRY SAND.

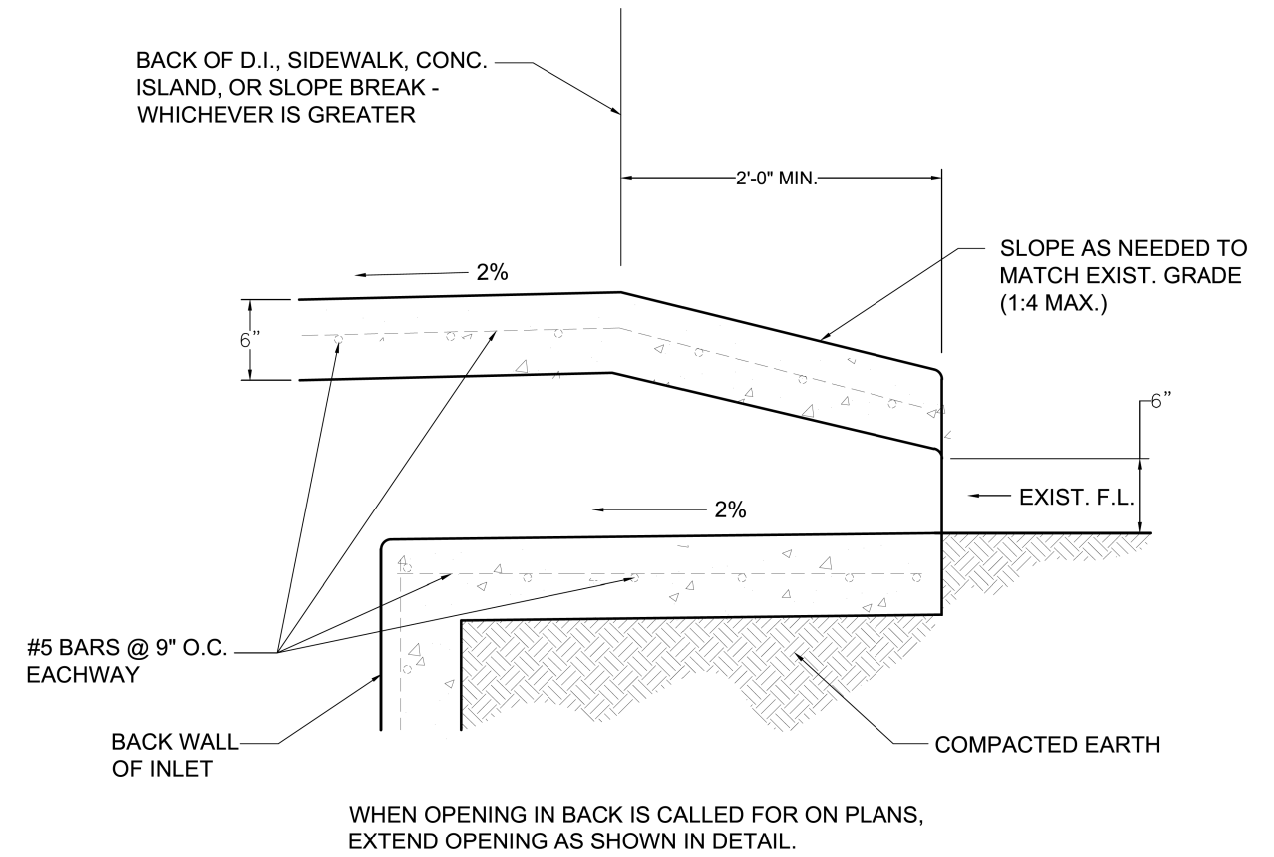
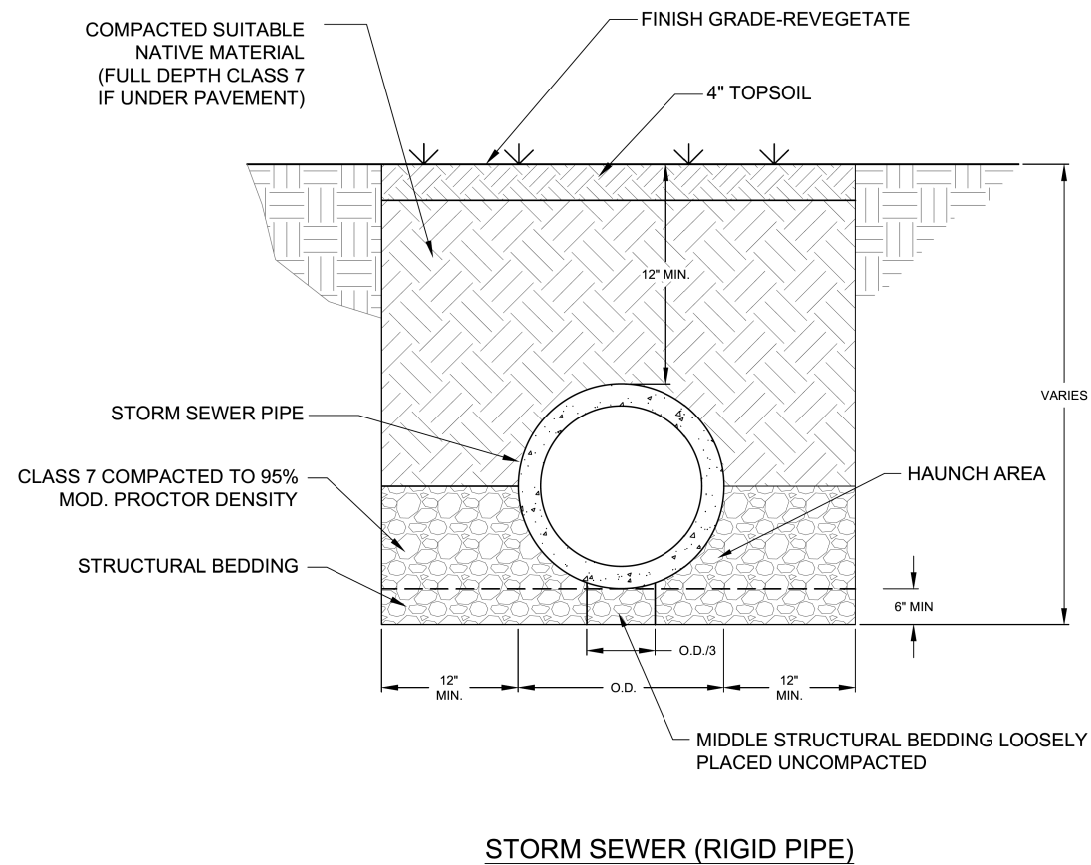
	CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165	TITLE: STREET DETAILS DESCRIPTION: TRUNCATED DOME PAVERS	DATE: FEBRUARY 2017 REVISED:	SHEET:
	DRAWN BY: NTR CHECKED BY: BPV FILE NAME: AR-7 TRUNCATED DOME PAVERS.dwg	AR-7		

NOTES:

1. ALL TRENCH EXCAVATION SHALL BE IN ACCORDANCE WITH OSHA REQUIREMENTS.
2. ALL WORK SHALL COMPLY WITH SECTION 606 OF THE AHTD STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.
3. ALL PIPE SHALL BE PROTECTED DURING CONSTRUCTION BY A COVER SUFFICIENT TO PREVENT DAMAGE FROM PASSAGE OF EQUIPMENT.
4. THE MINIMUM TRENCH WIDTH SHALL BE THE OUTSIDE DIAMETER OF THE PIPE PLUS 24 INCHES. THE MAXIMUM ALLOWABLE TRENCH WIDTH SHALL BE THE MINIMUM WIDTH PRACTICABLE FOR WORKING CONDITIONS.
5. MULTIPLE PIPE CULVERTS SHALL BE INSTALLED WITH A MINIMUM CLEARANCE OF 24 INCHES BETWEEN STRINGS OF PIPE.
6. MATERIAL IN THE HAUNCH AND OUTER STRUCTURAL BEDDING SHALL BE COMPACTED TO 95% OF THE MODIFIED PROCTOR DENSITY.
7. FULL DEPTH CLASS 7 BACKFILL COMPACTED TO 95% MODIFIED PROCTOR DENSITY REQUIRED UNDER ALL PAVEMENT.

CONSTRUCTION SEQUENCE:

1. PLACE STRUCTURAL BEDDING MATERIAL TO GRADE. DO NOT COMPACT.
2. INSTALL PIPE TO GRADE.
3. COMPACT STRUCTURAL BEDDING OUTSIDE THE MIDDLE THIRD OF THE PIPE.
4. PLACE AND COMPACT THE HAUNCH AREA UP TO THE MIDDLE OF THE PIPE.
5. COMPLETE BACKFILL ACCORDING TO SPECIFICATIONS LISTED ABOVE.



BACK OPENING

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CITY OF CONWAY
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 100 EAST ROBINS
 CONWAY, ARKANSAS 72032
 501-450-6165

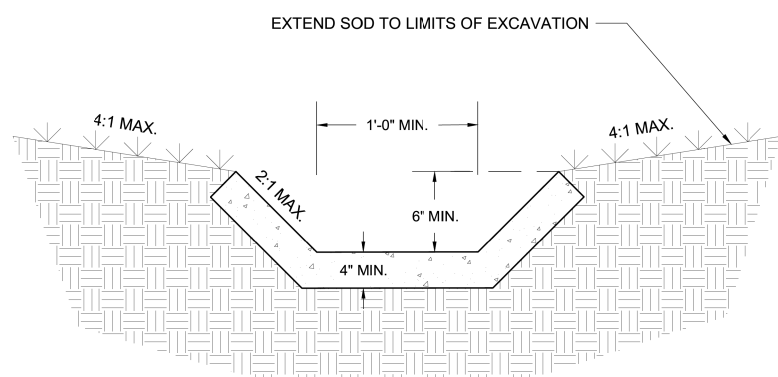
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DESCRIPTION:	STORM SEWER (RIGID PIPE)	REVISED	

DRAWN BY: NTR	CHECKED BY: BFV	FILE NAME: D-4 STORM SEWER (RIGID PIPE).dwg	D-4


CITY OF CONWAY
 TRANSPORTATION DEPARTMENT
 100 EAST ROBINS
 CONWAY, ARKANSAS 72032
 501-450-6165

TITLE:	DRAINAGE DETAILS	DATE: FEBRUARY 2017	SHEET:
DESCRIPTION:	BACK OPENING	REVISED	

DRAWN BY: NTR	CHECKED BY: BFV	FILE NAME: D-5 BACK OPENING.dwg	D-5



*SWALE ONLY TO BE USED WHEN COLLECTING A WATERSHED 5 ACRES OR LESS

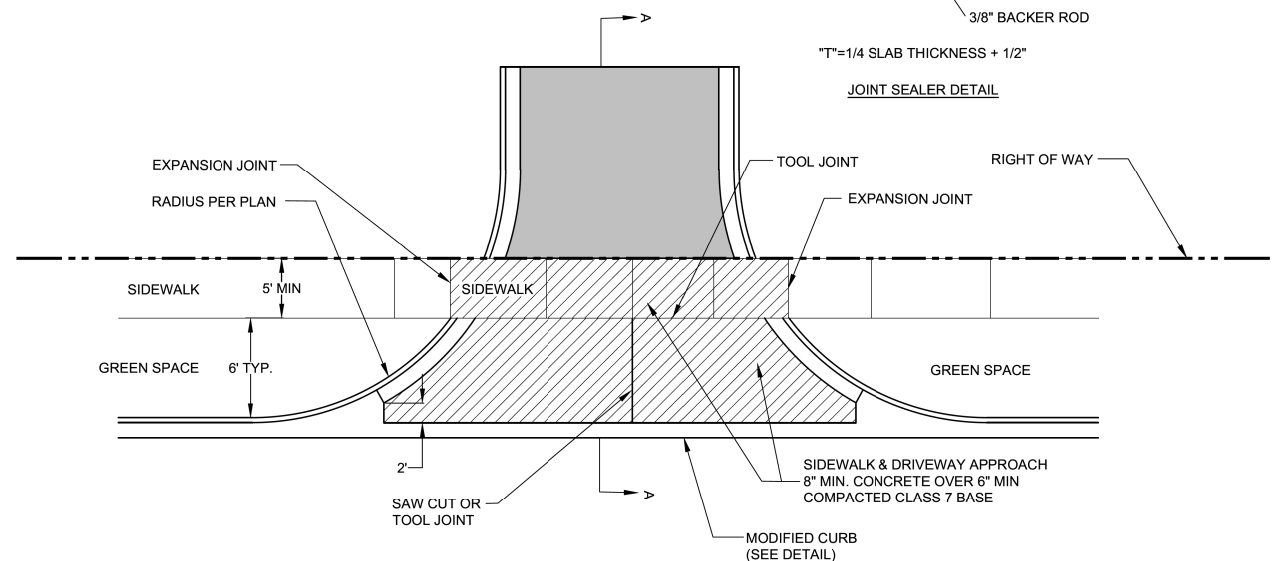
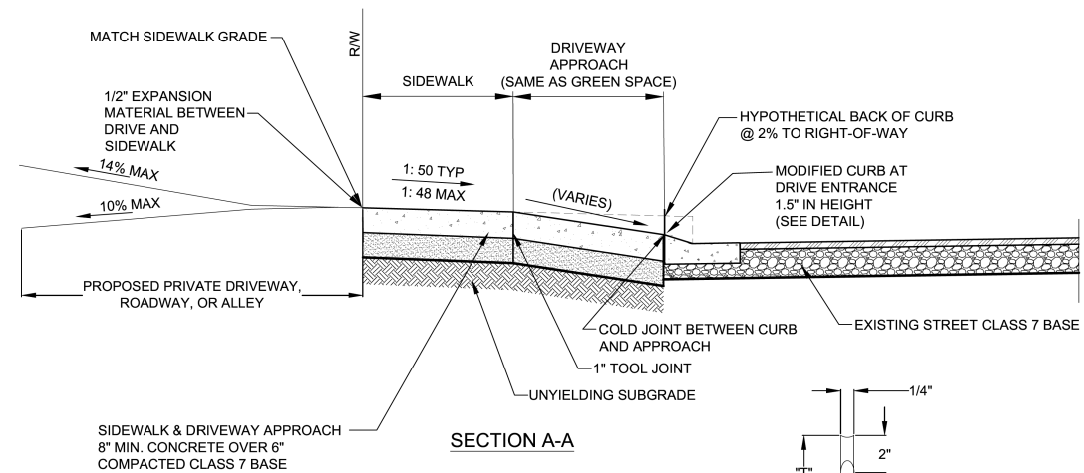
CONCRETE SWALE SECTION*
N.T.S.

NOTES:

1. ALL MATERIALS & CONSTRUCTION SHALL COMPLY WITH SECTION 605 OF THE ARKANSAS HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, 2014 EDITION.
2. THE FULL WIDTH OF EACH SECTION SHALL BE POURED MONOLITHICALLY.
3. SOLID SOD ALONG DITCH PAVING TO BE PLACED WITHIN 14 DAYS OF DITCH PAVING CONSTRUCTION.
4. 1/2" WIDE TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE DITCH PAVING AT MAX. 45' INTERVALS. THE SPACE SHALL BE FILLED WITH APPROVED JOINT FILLER COMPLYING WITH AASHTO M213.

 CITY OF CONWAY
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501-450-6165

TITLE:	DRAINAGE DETAILS	DATE:	MAY 2017	SHEET:	
DESCRIPTION:	CONCRETE SWALE PAVING	REVISED:			
DRAWN BY:	NTR	CHECKED BY:	BFV	FILE NAME:	D-7 CONCRETE SWALE.dwg
					D-7



NOTES:

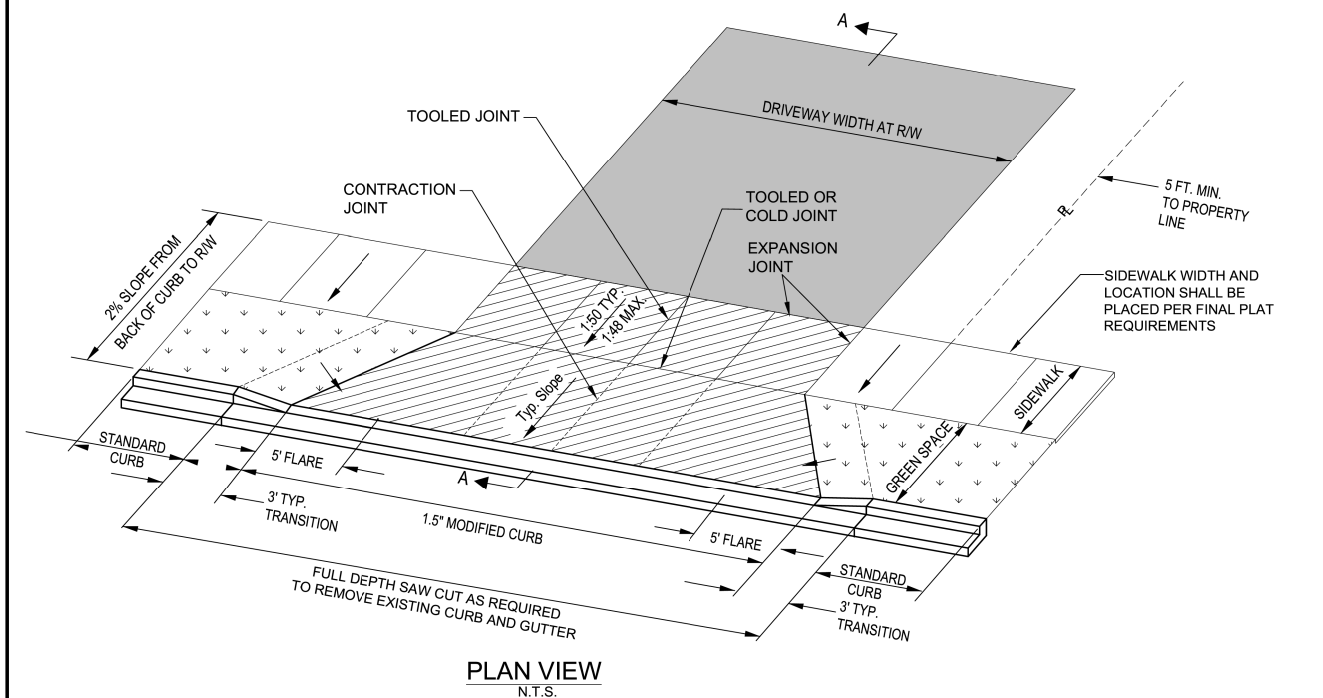
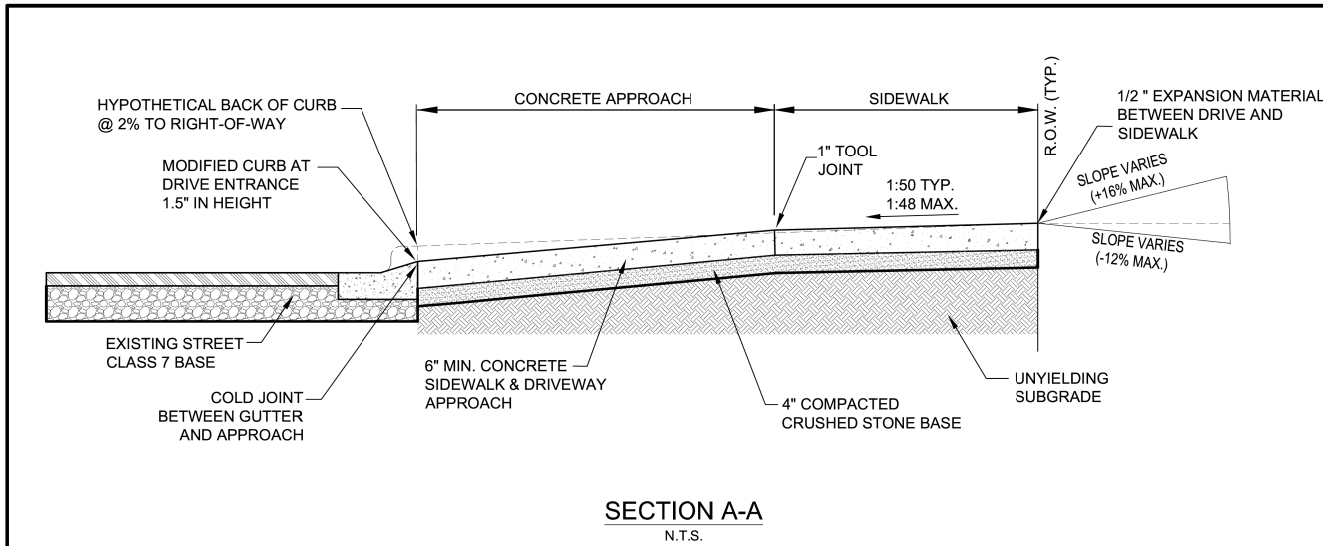
1. FULL DEPTH EXPANSION JOINTS (FOUR INCHES) SHALL BE PROVIDED AT THE EDGE OF THE SIDEWALK OPPOSITE THE STREET.
2. CONCRETE TO BE SAW-CUT OR PLACE A TOOL JOINT AT THE CENTER OF DRIVE AND SEALED. IF POSSIBLE CONTRACTOR SHALL TRY TO ALIGN THE JOINT AT THE CENTER OF THE DRIVE WITH THE A JOINT IN THE ADJACENT SIDEWALK.
3. ALL WORK SHALL COMPLY WITH SECTION 505 OF THE ARKANSAS HIGHWAY AND TRANSPORTATION STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION, 2014 EDITION
4. *ALL SIDEWALKS AND CURB CUTS FOR DRIVEWAY APPROACHES REQUIRE AN INSPECTION PRIOR TO CONCRETE PLACEMENT.*

COMMERCIAL DRIVEWAY DETAIL
N.T.S.

 CITY OF CONWAY
TRANSPORTATION DEPARTMENT
100 EAST ROBINS
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501-450-6165

TITLE:	DRIVEWAY DETAILS	DATE:	FEBRUARY 2017	SHEET:	
DESCRIPTION:	COMMERCIAL DRIVEWAY	REVISED:			
DRAWN BY:	NTR	CHECKED BY:	BFV	FILE NAME:	DW-1 DRIVEWAY (COMMERCIAL).dwg
					DW-1

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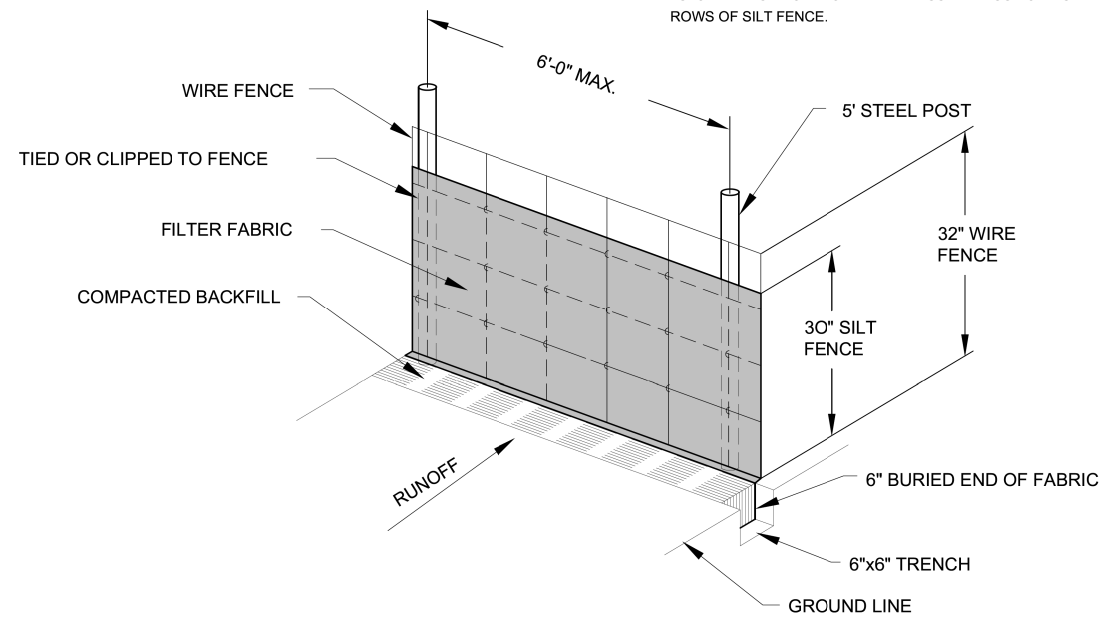


- NOTES:
1. FULL DEPTH EXPANSION JOINTS (FOUR INCHES) SHALL BE PROVIDED AT THE EDGE OF THE SIDEWALK OPPOSITE THE STREET.
 2. CONCRETE TO BE SAW-CUT OR PLACE A TOOL JOINT AT THE CENTER OF DRIVE AND SEALED. IF POSSIBLE CONTRACTOR SHALL TRY TO ALIGN THE JOINT AT THE CENTER OF THE DRIVE WITH THE A JOINT IN THE ADJACENT SIDEWALK.
 3. ALL WORK SHALL COMPLY WITH SECTION 505 OF THE ARKANSAS HIGHWAY AND TRANSPORTATION STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION, 2014 EDITION

CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165	TITLE: DRIVEWAY DETAILS	DATE: FEBRUARY 2017	SHEET:
	DESCRIPTION: STANDARD RESIDENTIAL DRIVEWAY (CURB & GUTTER)	REVISED	DW-2
	DRAWN BY: NTR	CHECKED BY: BFV	FILE NAME: DW-2 DRIVEWAY (RESIDENTIAL).dwg

Slope-Percent	Maximum Slope Length (ft) Above Fence	
	Standard (18" High) Silt Fence	Reinforced (30" High) Silt Fence
2 (or less)	150	250
5	100	250
10	50	150
15	35	100
20	25	70
25	20	55
30	15	45
35	15	40
40	15	35
45	10	30
50	10	25

- NOTES:
1. THE SLOPE LENGTH SHOWN IS THE DISTANCE FROM THE FENCE TO THE DRAINAGE DIVIDE OR THE NEAREST UPSLOPE CHANNEL.
 2. SLOPE LENGTH CANNOT BE ADDRESSED BY USE OF MULTIPLE ROWS OF SILT FENCE.

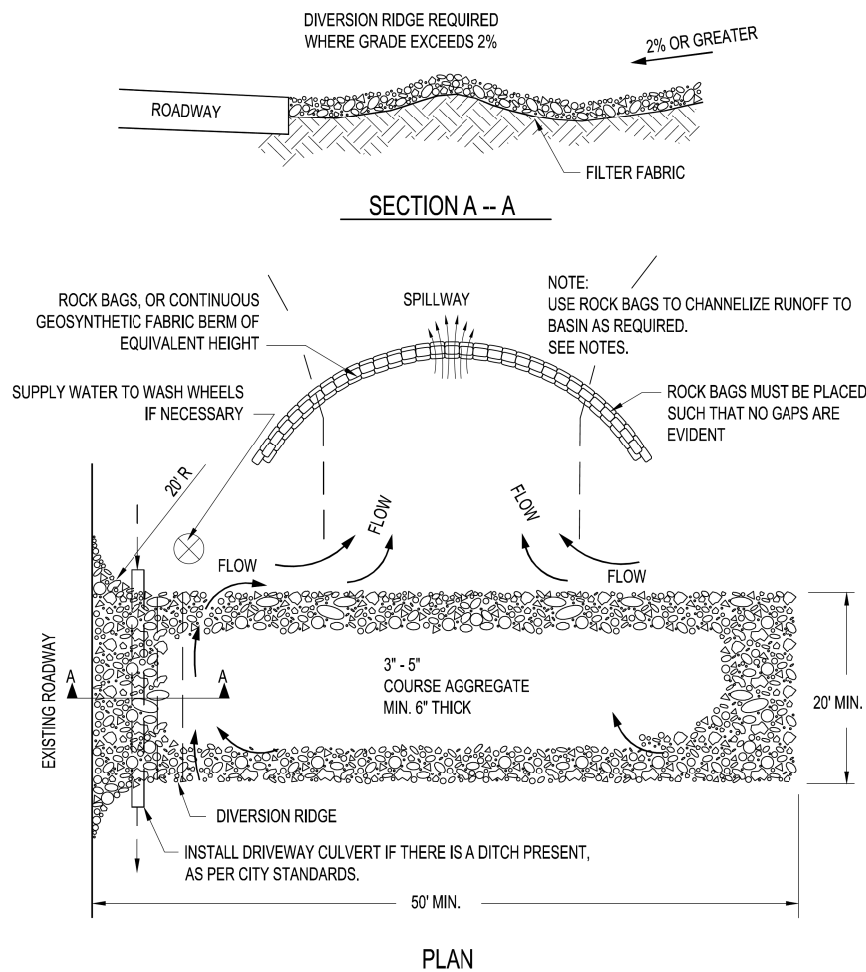


- SILT FENCE NOTES**
1. POSTS WHICH SUPPORT THE SILT FENCE SHALL BE INSTALLED ON A SLIGHT ANGLE TOWARD THE ANTICIPATED RUNOFF SOURCE. POST MUST BE EMBEDDED A MINIMUM OF ONE FOOT.
 2. THE TOE OF THE SILT FENCE SHALL BE TRENCHED IN WITH A SPADE OR MECHANICAL TRENCHER, SO THAT THE DOWNSLOPE FACE OF THE TRENCH IS FLAT AND PERPENDICULAR TO THE LINE OF FLOW. WHERE FENCE CANNOT BE TRENCHED IN (e.g. PAVEMENT), WEIGHT FABRIC FLAP WITH ROCK ON UPHILL SIDE TO PREVENT FLOW FROM SEEPING UNDER FENCE.
 3. THE TRENCH MUST BE A MINIMUM OF 6 INCHES DEEP AND 6 INCHES WIDE TO ALLOW FOR THE SILT FENCE FABRIC TO BE LAID IN THE GROUND AND BACKFILLED WITH COMPACTED MATERIAL.
 4. SILT FENCE SHOULD BE SECURELY FASTENED TO EACH SUPPORT POST OR TO WOVEN WIRE, WHICH IN TURN IS ATTACHED TO THE FENCE POST. THERE SHALL BE A 3 FOOT OVERLAP, SECURELY FASTENED WHERE ENDS OF FABRIC MEET.
 5. REPAIR OR REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
 6. SILT FENCE SHALL BE REMOVED WHEN THE SITE IS COMPLETELY STABILIZED SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE.
 7. ACCUMULATED SILT SHALL BE REMOVED WHEN IT REACHES A DEPTH OF HALF THE HEIGHT OF THE FENCE. THE SILT SHALL BE DISPOSED OF AT AN APPROVED SITE AND IN SUCH A MANNER AS TO NOT CONTRIBUTE TO ADDITIONAL SILTATION.

WIRE REINFORCED SILT FENCE
N.T.S.

CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165	TITLE: EROSION CONTROL DETAILS	DATE: FEBRUARY 2017	SHEET:
	DESCRIPTION: WIRE REINFORCED SILT FENCE	REVISED	EC-1
	DRAWN BY: NTR	CHECKED BY: BFV	FILE NAME: EC-1 WIRE REINFORCED SILT FENCE.dwg

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NOTE: THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION THAT WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAYS. THIS MAY REQUIRE TOP DRESSING, REPAIR AND/OR CLEANOUT OF ANY MEASURES USED TO TRAP SEDIMENT.

STABILIZED CONSTRUCTION EXIT

N.T.S.

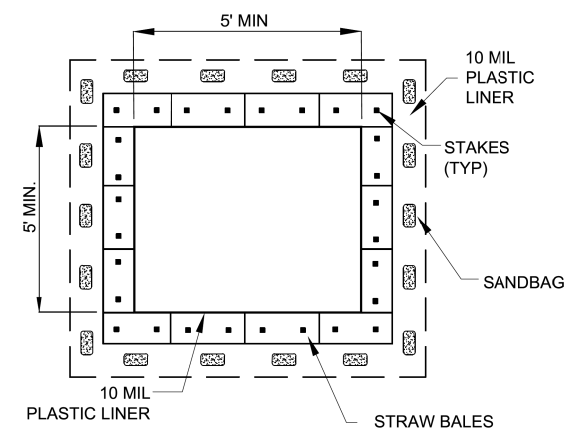
NOTES

1. STONE SIZE: 3"-5" OPEN GRADED ROCK.
2. LENGTH: AS EFFECTIVE BUT NOT LESS THAN 50'.
3. THICKNESS: NOT LESS THAN 8".
4. WIDTH: NOT LESS THAN FULL WIDTH OF ALL POINTS OF INGRESS/EGRESS.
5. WASHING - WHEN NECESSARY, WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC ROADWAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH CRUSHED STONE WHICH DRAINS INTO AN APPROVED TRAP OR SEDIMENT BASIN. ALL SEDIMENT SHALL BE PREVENTED FROM ENTERING ANY STORM DRAIN, DITCH OR WATERCOURSE USING APPROVED METHODS.
6. MAINTENANCE: THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC ROADWAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE CONDITIONS DEMAND, AND REPAIR AND CLEAN OUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENTS SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC ROADWAY MUST BE REMOVED IMMEDIATELY.
7. DRAINAGE: ENTRANCE MUST BE PROPERLY GRADED OR INCORPORATE A DRAINAGE SWALE TO PREVENT RUNOFF FROM LEAVING THE CONSTRUCTION SITE.

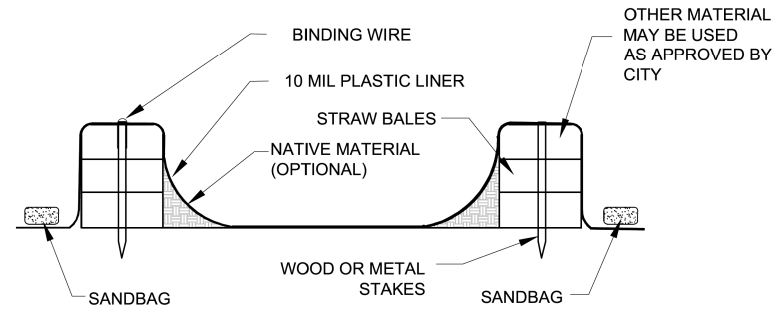
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	DESCRIPTION: STABILIZED CONSTRUCTION EXIT	REVISED		
			EC-3	
	DRAWN BY: NTR	CHECKED BY: BPV	FILE NAME: EC-3 CONSTRUCTION EXIT.dwg	

WASHOUT NOTES

1. NO WASHING OUT OF CONCRETE TRUCKS OR WASHING OF SWEEPINGS FROM EXPOSED AGGREGATE CONCRETE INTO STORM DRAINS, OPEN DITCHES, STREETS, OR STREAMS IS ALLOWED.
2. EXCESS CONCRETE IS NOT ALLOWED TO BE DUMPED ON-SITE, EXCEPT IN DESIGNATED TEMPORARY CONCRETE WASHOUT PIT AREAS.
3. ON-SITE TEMPORARY CONCRETE WASHOUT AREAS WILL BE LOCATED AT LEAST 50 FEET FROM STORM DRAINS, OPEN DITCHES, OR WATER BODIES AS DETERMINED IN THE FIELD.
4. TEMPORARY CONCRETE WASHOUT FACILITIES WILL BE CONSTRUCTED AND MAINTAINED IN SUFFICIENT QUANTITY AND SIZE TO CONTAIN ALL LIQUID AND CONCRETE WASTE GENERATED BY WASHOUT OPERATIONS.
5. WASHOUT FACILITIES WILL BE CLEANED OUT OR REPLACED ONCE THE WASHOUT IS 75% FULL.
6. PLASTIC LINING MATERIAL WILL BE MINIMUM OF 10 MIL POLYETHYLENE SHEETING AND WILL BE FREE OF HOLES, TEARS, OR OTHER DEFECTS.
7. WHEN WASHOUT FACILITIES ARE NO LONGER REQUIRED FOR WORK, THE HARDENED CONCRETE WILL BE REMOVED AND DISPOSED OF OFFSITE. MATERIALS USED TO CONSTRUCT TEMPORARY CONCRETE WASHOUT FACILITIES WILL BE REMOVED FROM THE SITE AND DISPOSED OF.



PLAN



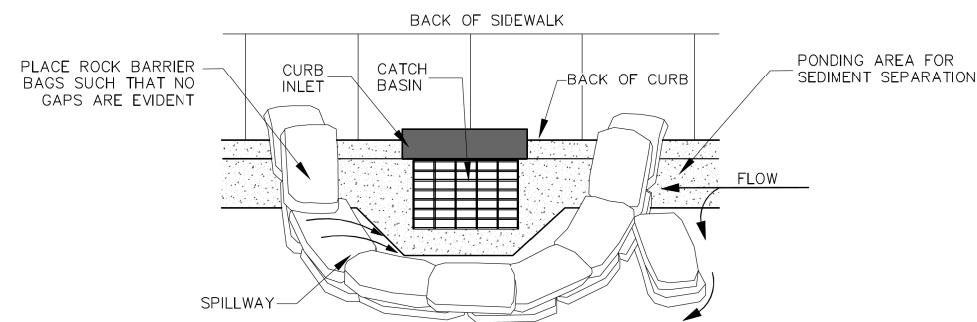
SECTION

CONCRETE WASHOUT

N.T.S.

<p>CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165</p>	TITLE: EROSION CONTROL DETAILS	DATE: FEBRUARY 2017	SHEET:	
	DESCRIPTION: CONCRETE WASHOUT	REVISED		
			EC-4	
	DRAWN BY: NTR	CHECKED BY: BPV	FILE NAME: EC-4 CONCRETE WASHOUT.dwg	

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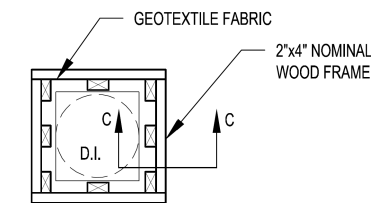
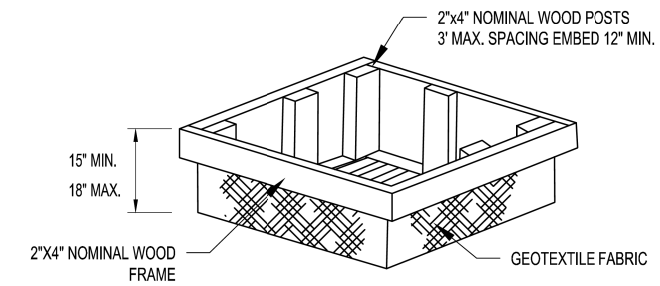


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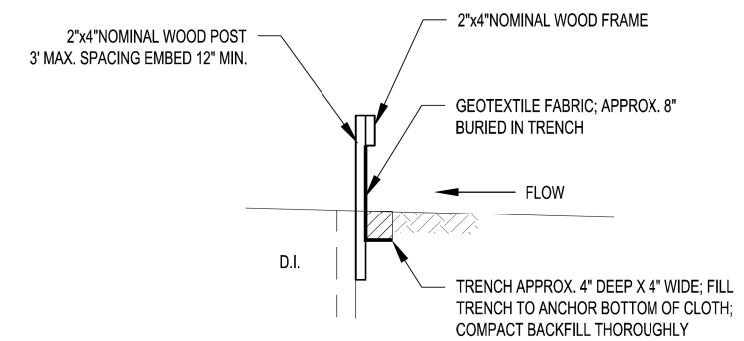
1. BAGS OF WOVEN GEOTEXTILE FABRIC, FILLED WITH GRAVEL MUST BE LAYERED SUCH THAT NO GAPS ARE EVIDENT.
2. LEAVE ONE SANDBAG GAP IN THE TOP ROW ON THE SIDE AWAY FROM FLOW, TO PROVIDE A SPILLWAY; OR IN THE CENTER IF PONDING IS NEEDED ON BOTH SIDES.
3. INSPECT BARRIERS AND REMOVE SEDIMENT AFTER EACH STORM EVENT, SEDIMENT AND GRAVEL MUST BE REMOVED FROM THE TRAVELED WAY IMMEDIATELY

CURB INLET PROTECTION

N.T.S.



PLAN



SECTION C-C

SILT FENCE AT DROP INLET

N.T.S.

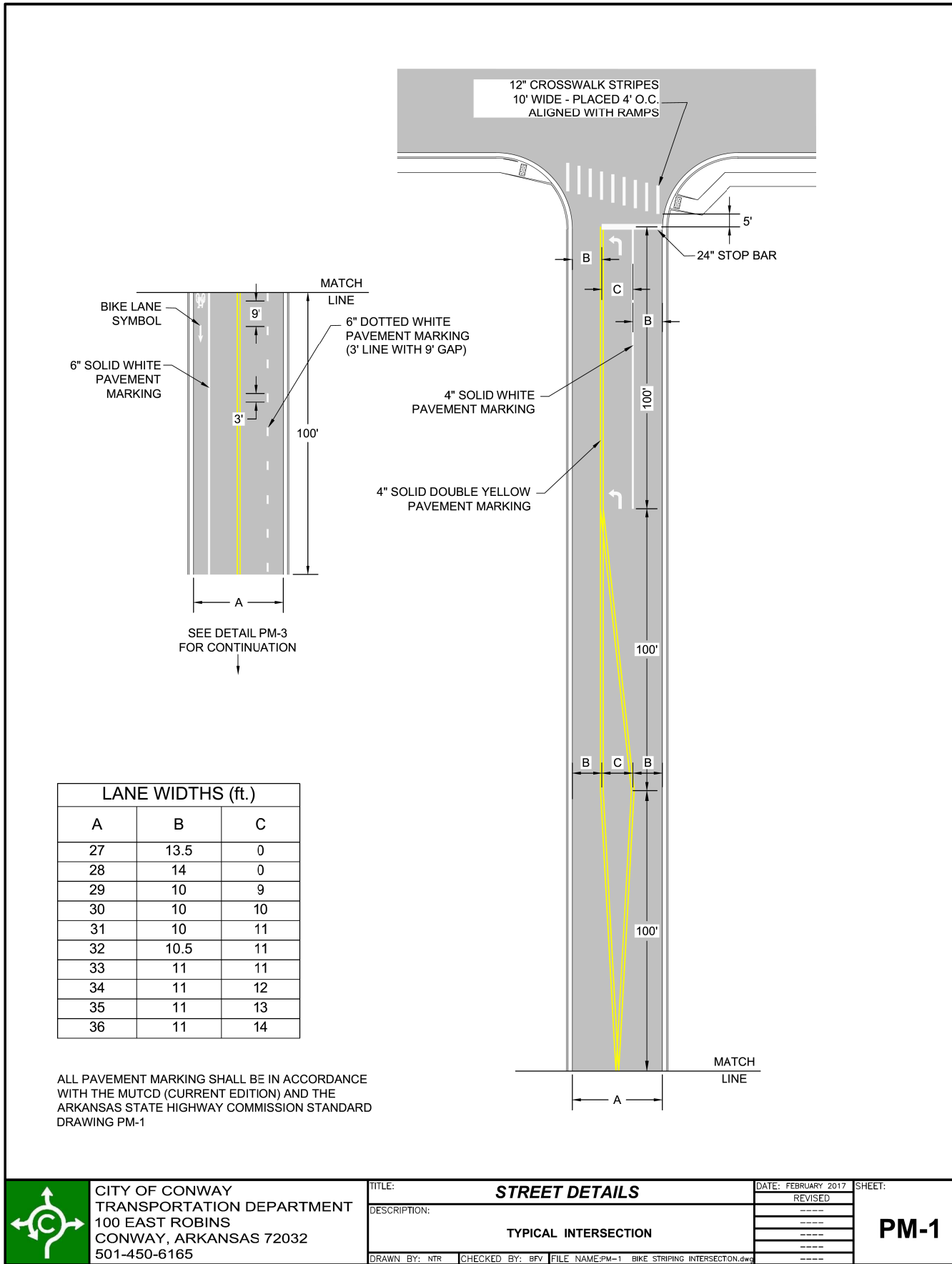
CITY OF CONWAY
TRANSPORTATION DEPARTMENT
100 EAST ROBINS
CONWAY, ARKANSAS 72032
501-450-6165

TITLE:	EROSION CONTROL DETAILS	DATE: FEBRUARY 2017	SHEET:	EC-5
DESCRIPTION:	CURB INLET PROTECTION	REVISED		
DRAWN BY:	NTR	CHECKED BY:	BPV	
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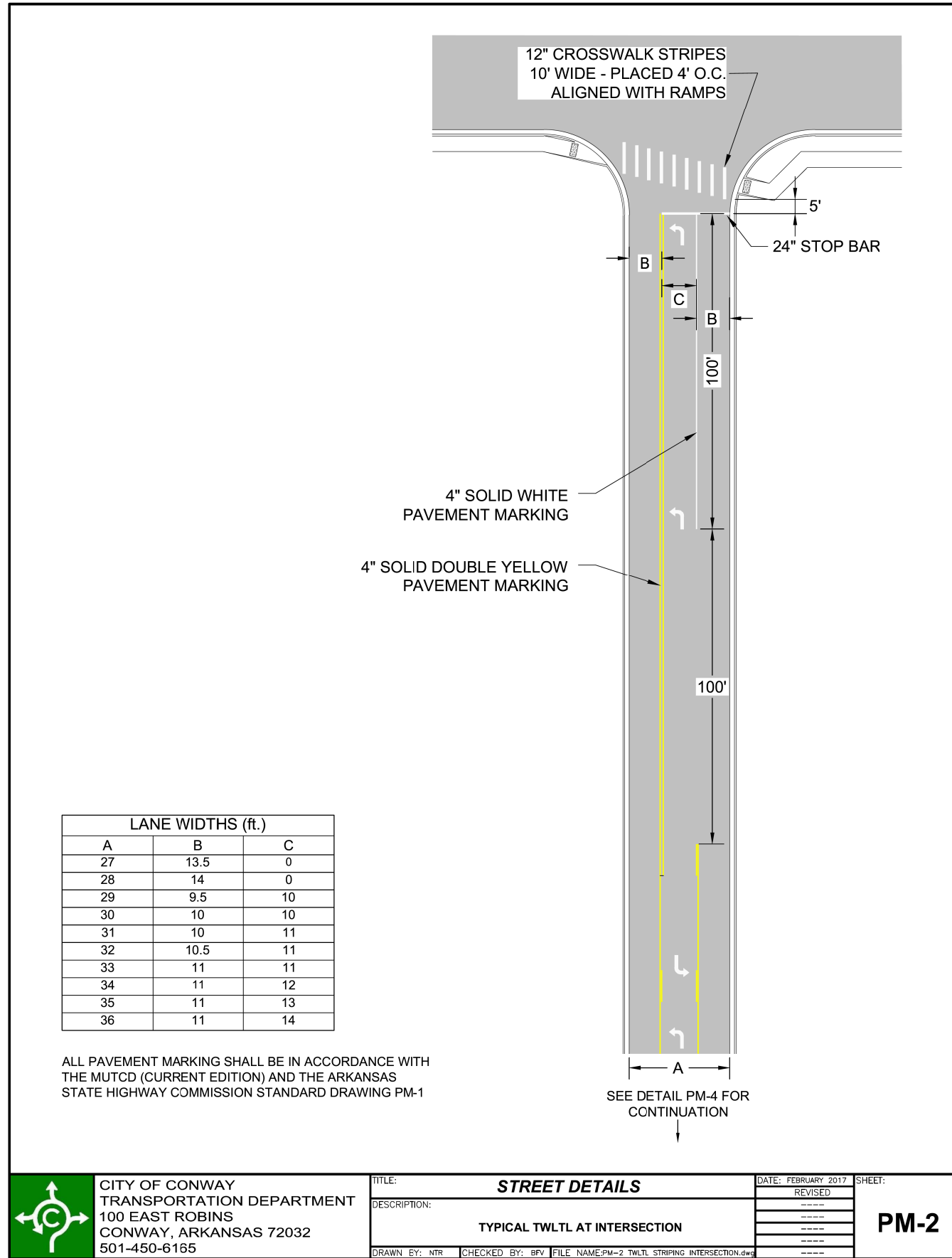
CITY OF CONWAY
TRANSPORTATION DEPARTMENT
100 EAST ROBINS
CONWAY, ARKANSAS 72032
501-450-6165

TITLE:	EROSION CONTROL DETAILS	DATE: FEBRUARY 2017	SHEET:	EC-6
DESCRIPTION:	SILT FENCE AT DROP INLET	REVISED		
DRAWN BY:	NTR	CHECKED BY:	BPV	
FILE NAME:	EC-6 DROP INLET SILT FENCE.dwg			

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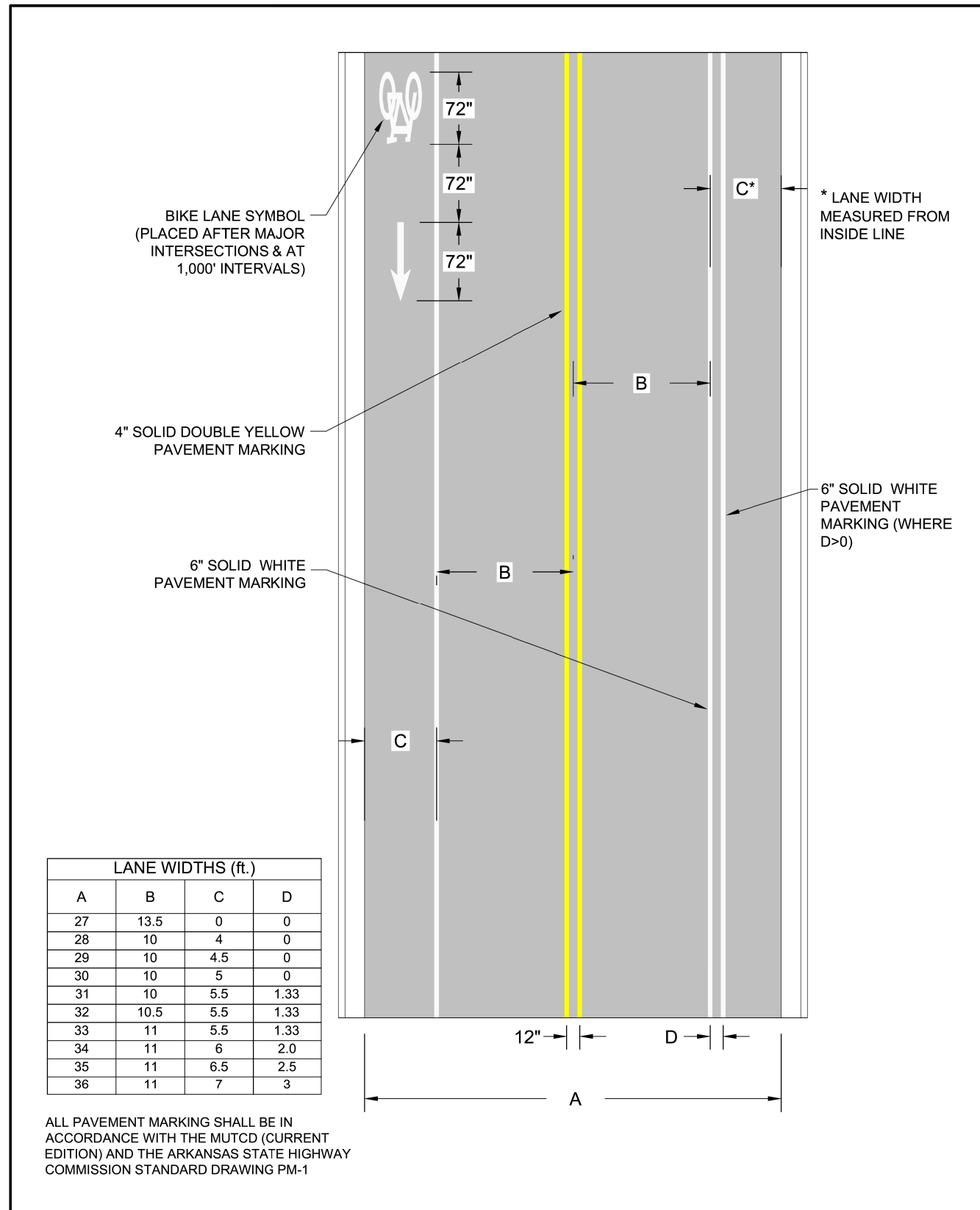


CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165	TITLE:	STREET DETAILS	DATE: FEBRUARY 2017	SHEET:
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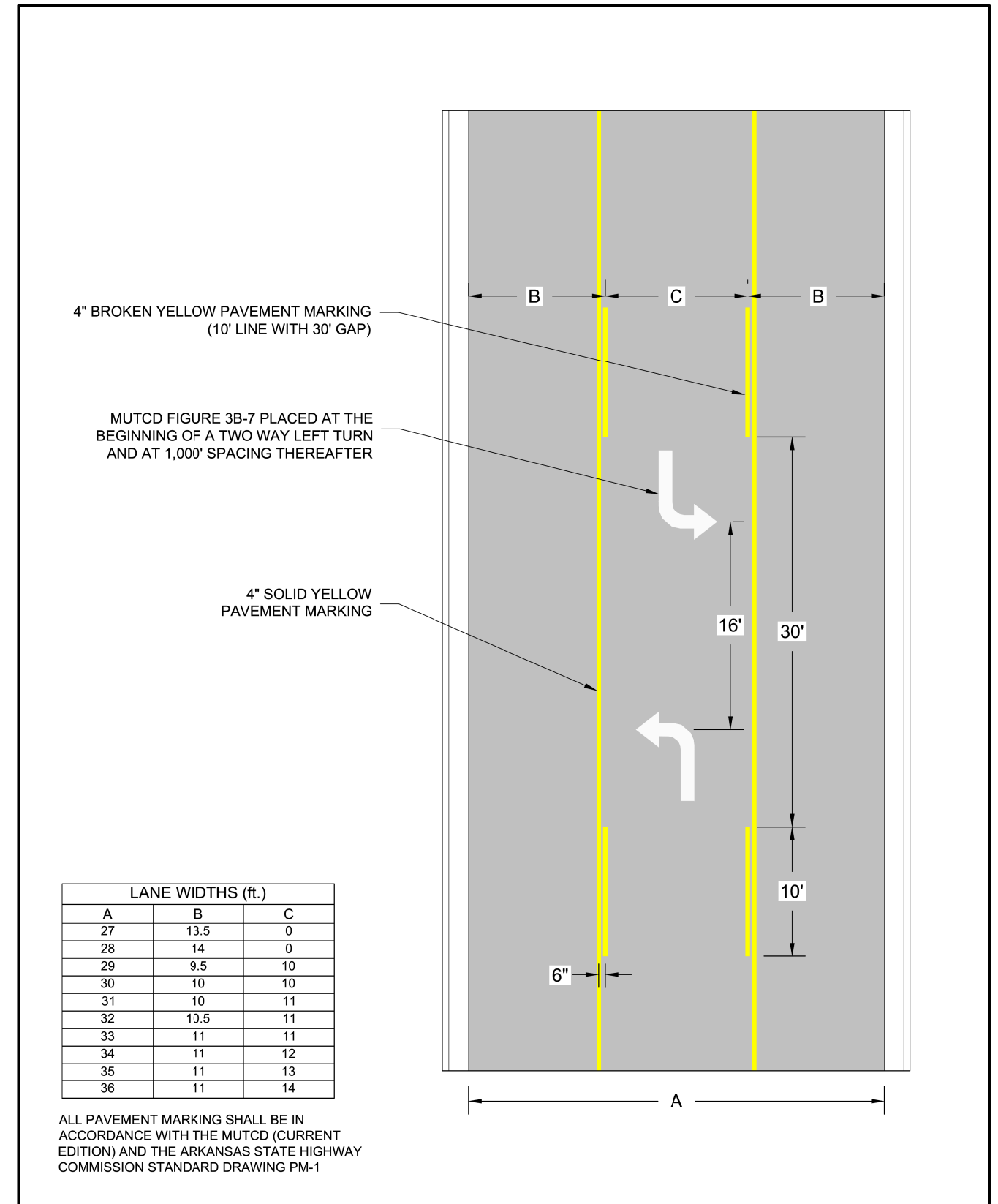


CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165	TITLE:	STREET DETAILS	DATE: FEBRUARY 2017	SHEET:
	DESCRIPTION:	TYPICAL TWLTL AT INTERSECTION	REVISED	PM-2
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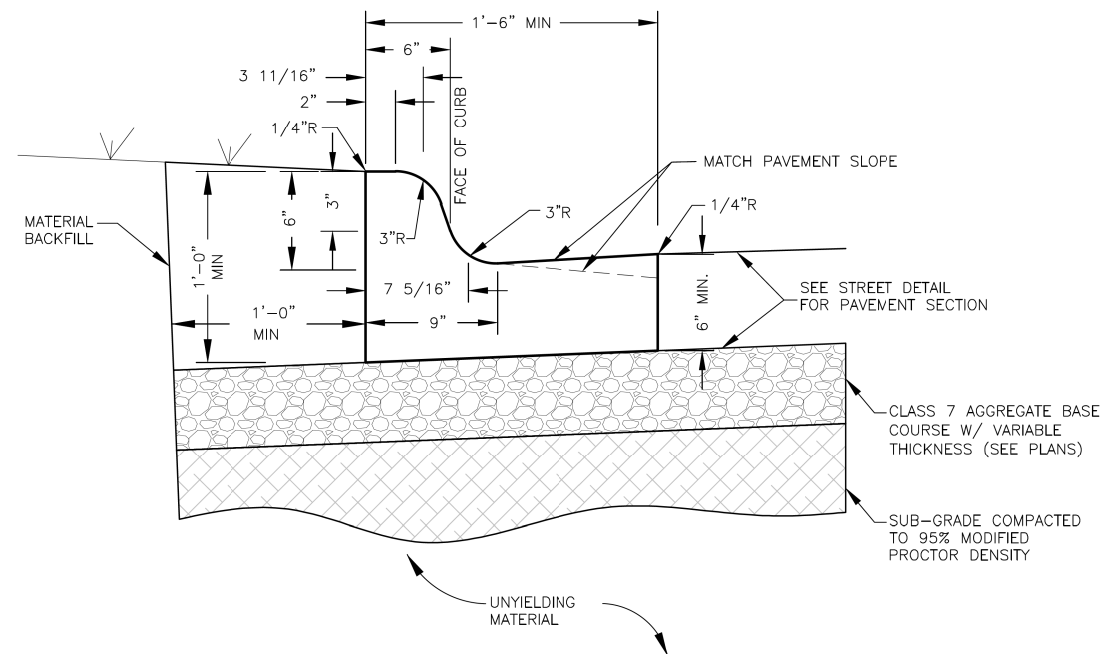
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CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165	TITLE: STREET DETAILS	DATE: FEBRUARY 2017	SHEET:
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DRAWN BY: NTR		CHECKED BY: BFV	FILE NAME: PM-3 BIKE LANE STRIPING DETAIL.dwg



CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165	TITLE: STREET DETAILS	DATE: FEBRUARY 2017	SHEET:
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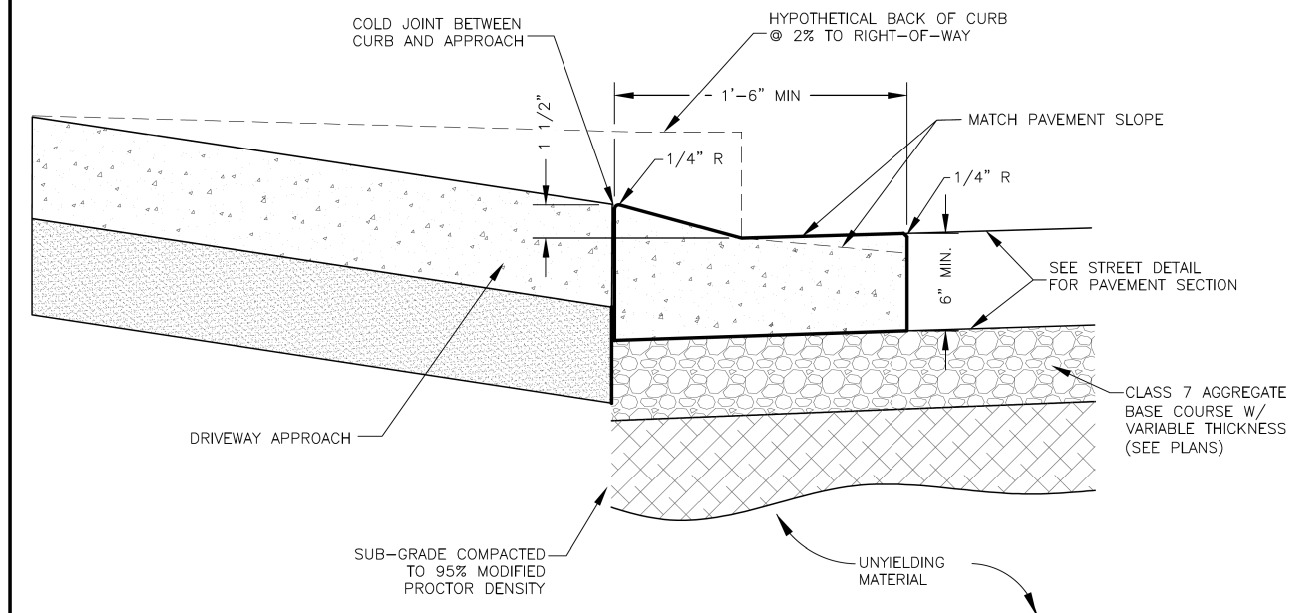


CURB & GUTTER NOTES:

1. CONTRACTION JOINTS SHALL BE 1/8" TO 3/8" X 1 1/2" AT 15 FT. INTERVALS. ALL CONTRACTION JOINTS AND COLD JOINTS SHALL BE FILLED WITH JOINT SEALANT TO FINISHED GRADE.
2. EXPANSION MATERIAL SHALL BE REQUIRED AT ALL STATIONARY STRUCTURES AND ENDS OF CURB RETURNS. THE EXPANSION MATERIAL SHALL BE 1/2" ASPHALT IMPREGNATED FIBERBOARD CONFORMING TO AASHTO M-213. EXPANSION JOINT MATERIAL SHALL BE LEFT 1/2" LOWER THAN GRADE OR TRIMMED 1/2" LOWER THAN GRADE.
3. EXPANSION JOINTS SHALL BE FILLED WITH JOINT SEALANT SHAPED TO THE CROSS SECTION OF THE CURB AND CONSTRUCTED AT RIGHT ANGLES WITH THE CURB LINE.
4. CONCRETE JOINT SEALANT SHALL COMPLY WITH SECTION 501.02 (h) OF THE AHTD STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION, 2014 EDITION.
5. FINISH SHALL BE MEDIUM BROOM FINISH.
6. ALL WORK SHALL COMPLY WITH SECTION 634 OF THE AHTD STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION, 2014 EDITION.

 CITY OF CONWAY
TRANSPORTATION DEPARTMENT
100 EAST ROBINS
CONWAY, ARKANSAS 72032
501-450-6165

TITLE:	STREET DETAILS	DATE: FEBRUARY 2017	SHEET:
DESCRIPTION:	TYPE 'A' CURB & GUTTER	REVISED	ST-1
DRAWN BY: NTR	CHECKED BY: BFV	FILE NAME: ST-1 CURB & GUTTER.dwg	



CURB & GUTTER NOTES:

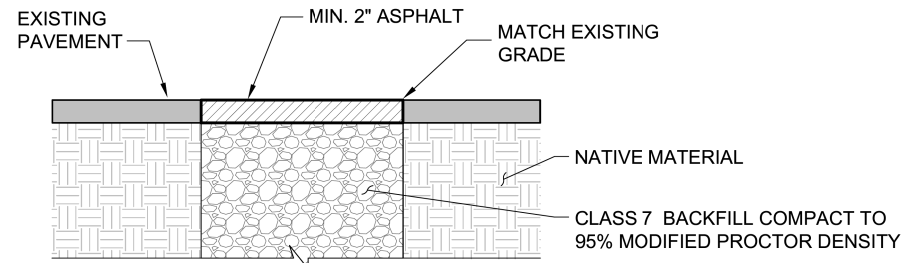
1. CONTRACTION JOINTS SHALL BE 1/8" TO 3/8" X 1 1/2" AT 15 FT. INTERVALS. ALL CONTRACTION JOINTS AND COLD JOINTS SHALL BE FILLED WITH JOINT SEALANT TO FINISHED GRADE.
2. EXPANSION MATERIAL SHALL BE REQUIRED AT ALL STATIONARY STRUCTURES AND ENDS OF CURB RETURNS. THE EXPANSION MATERIAL SHALL BE 1/2" ASPHALT IMPREGNATED FIBERBOARD CONFORMING TO AASHTO M-213. EXPANSION JOINT MATERIAL SHALL BE LEFT 1/2" LOWER THAN GRADE OR TRIMMED 1/2" LOWER THAN GRADE.
3. EXPANSION JOINTS SHALL BE FILLED WITH JOINT SEALANT SHAPED TO THE CROSS SECTION OF THE CURB AND CONSTRUCTED AT RIGHT ANGLES WITH THE CURB LINE.
4. CONCRETE JOINT SEALANT SHALL COMPLY WITH SECTION 501.02 (h) OF THE AHTD STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION, 2014 EDITION.
5. FINISH SHALL BE MEDIUM BROOM FINISH.
6. ALL WORK SHALL COMPLY WITH SECTION 634 OF THE AHTD STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION, 2014 EDITION.

 CITY OF CONWAY
TRANSPORTATION DEPARTMENT
100 EAST ROBINS
CONWAY, ARKANSAS 72032
501-450-6165

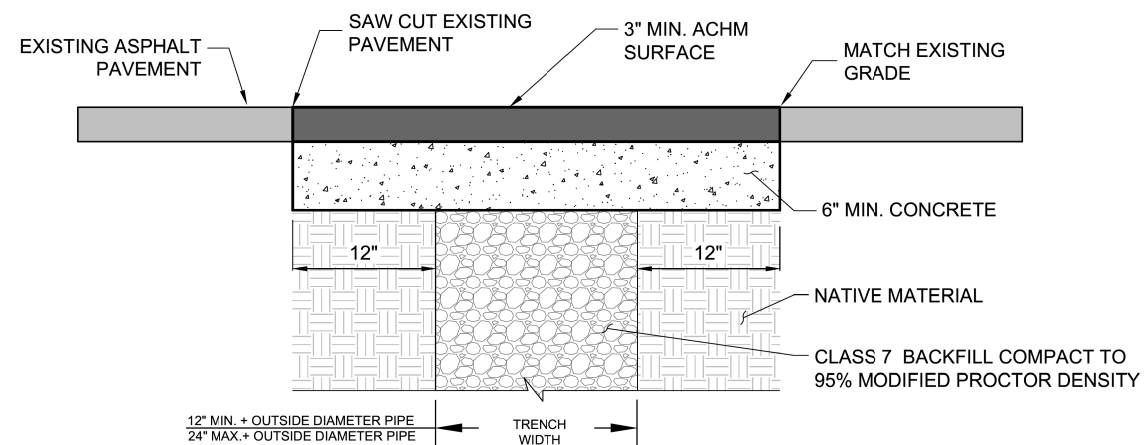
TITLE:	STREET DETAILS	DATE: FEBRUARY 2017	SHEET:
DESCRIPTION:	MODIFIED CURB & GUTTER	REVISED	ST-2
DRAWN BY: NTR	CHECKED BY: BFV	FILE NAME: ST-2 MODIFIED CURB & GUTTER.dwg	

NOTES:

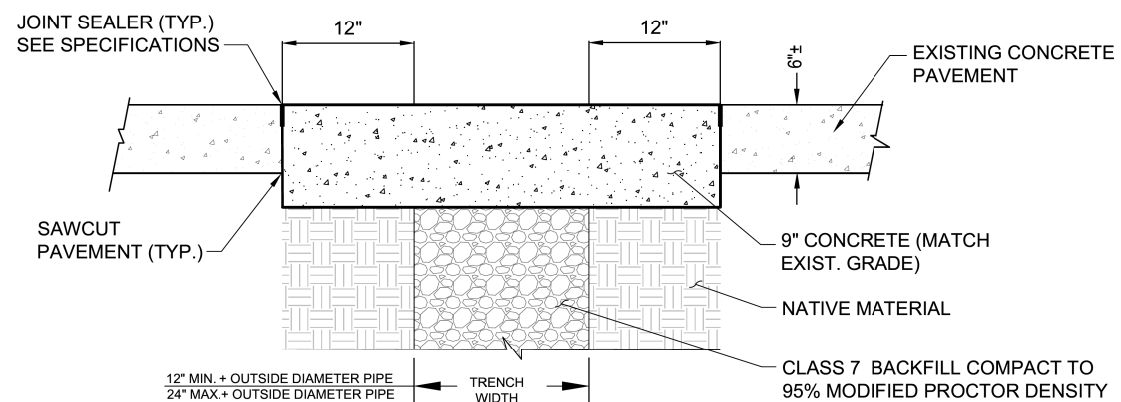
1. CONTRACTOR TO CONSTRUCT ALL TRENCH EXCAVATION IN ACCORDANCE WITH ALL OSHA REGULATIONS (29 CFR CH. XVII, SUBPART B).
2. ALL WORK MUST CONFORM TO SECTIONS 303 & 615 OF THE ARKANSAS HIGHWAY TRANSPORTATION DEPARTMENT STANDARD SPECIFICATION FOR HIGHWAY CONSTRUCTION, 2014 EDITION.
3. ASPHALT FOR TEMPORARY REPAIRS MAY BE HOT MIX OR COLD MIX.



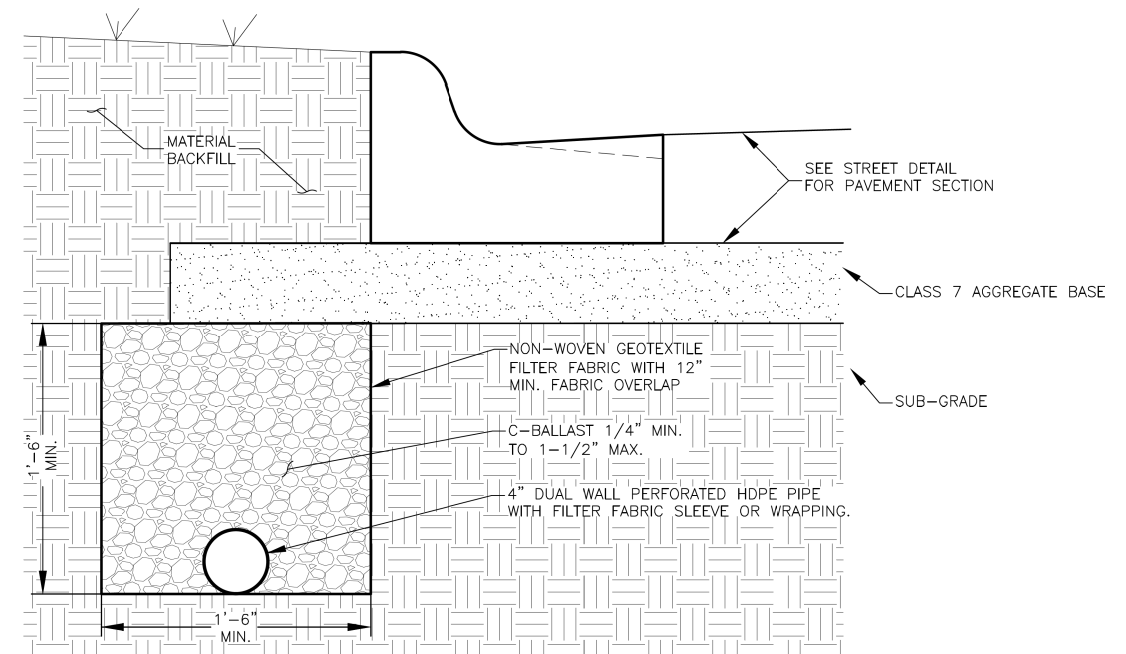
TEMPORARY PAVEMENT REPAIR



ASPHALT PAVEMENT REPAIR



CONCRETE PAVEMENT REPAIR



UNDERDRAIN NOTES:

1. FINAL LOCATION OF UNDERDRAIN TO BE DETERMINED BY ENGINEER DURING CONSTRUCTION
2. RUNNING SLOPE TO MATCH ROADWAY PROFILE
3. DRAIN SHOULD TERMINATE AT DRAINAGE STRUCTURE



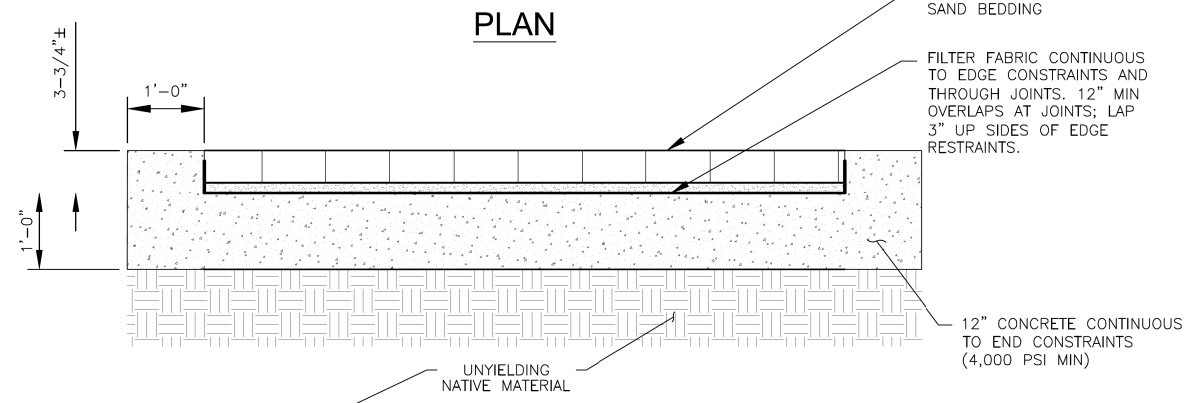
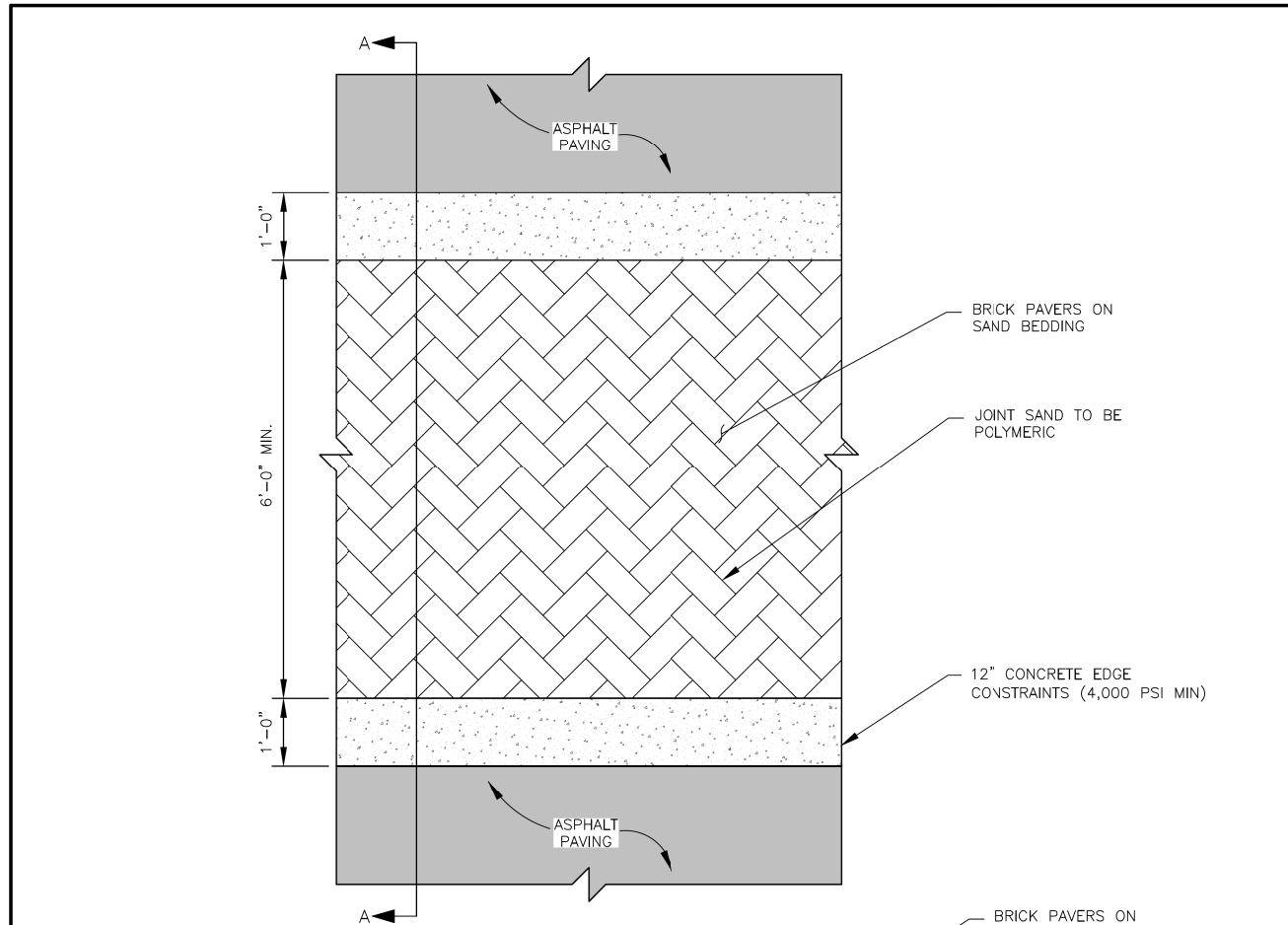
CITY OF CONWAY
 TRANSPORTATION DEPARTMENT
 100 EAST ROBINS
 CONWAY, ARKANSAS 72032
 501-450-6165

TITLE:	STREET DETAILS	DATE: FEBRUARY 2017	SHEET:
DESCRIPTION:	PAVEMENT REPAIR	REVISED	
DRAWN BY: NTR	CHECKED BY: BFV	FILE NAME: ST-3 PAVEMENT REPAIR.dwg	ST-3



CITY OF CONWAY
 TRANSPORTATION DEPARTMENT
 100 EAST ROBINS
 CONWAY, ARKANSAS 72032
 501-450-6165

TITLE:	STREET DETAILS	DATE: FEBRUARY 2017	SHEET:
DESCRIPTION:	UNDERDRAIN	REVISED	
DRAWN BY: NTR	CHECKED BY: BFV	FILE NAME: ST-4 CURB UNDERDRAIN.dwg	ST-4



SECTION A-A

NOTES:

1. PAVERS TO BE SURROUNDED WITH 1'-0" CONCRETE BAND FOR EDGE RESTRAINT.
2. JOINT SAND SHALL BE POLYMERIC.
3. PAVER SHALL BE PINE HALL ENGLISH EDGE HEAVY DUTY RED (4"x8"x2-3/4") OR APPROVED EQUAL.
4. SAND BEDDING FOR PAVERS TO BE MAX. 1" TO MIN. 1/2" THICK MASONRY SAND.

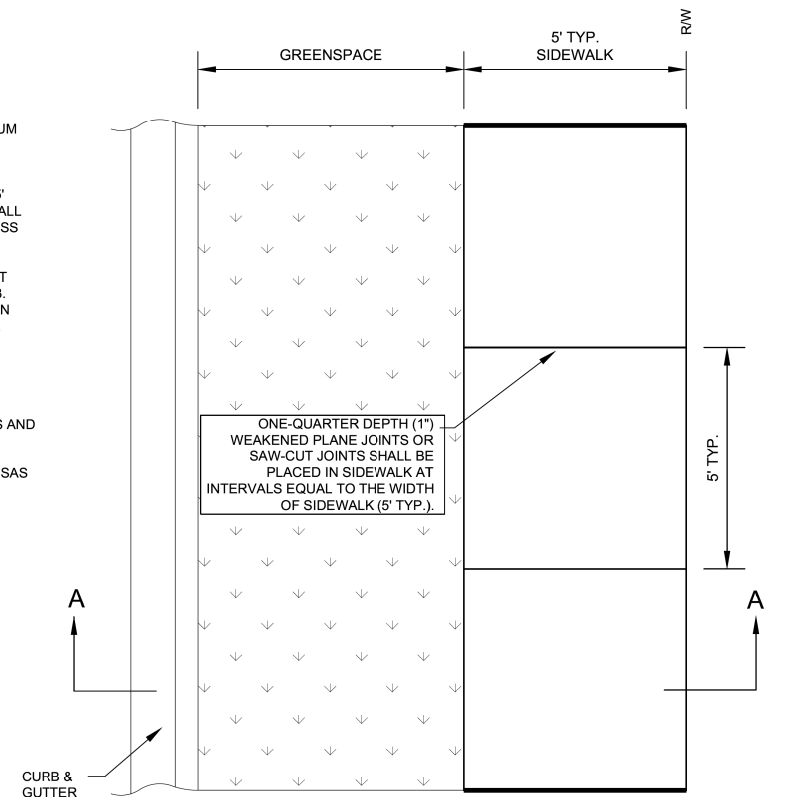
BRICK PAVER CROSSWALKS

<p>CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165</p>	TITLE: STREET DETAILS	DATE: FEBRUARY 2017	SHEET:
	DESCRIPTION: BRICK PAVER CROSSWALKS	REVISED	
	DRAWN BY: NTR	CHECKED BY: BFV	FILE NAME: ST-5 BRICK CROSSWALK.dwg
			ST-5

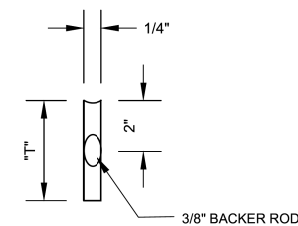
SIDEWALK CONSTRUCTION NOTES:

1. EXPANSION MATERIAL SHALL BE REQUIRED AT 50 FT. MAXIMUM SPACING.
2. SIDEWALK SHALL HAVE TOOLED OR SAW-CUT TRANSVERSE JOINTS AT INTERVALS EQUAL TO THE WIDTH OF SIDEWALK (5' TYP.). THESE WEAKENED PLANE (CONTRACTION) JOINTS SHALL BE CONSTRUCTED TO 1/4 DEPTH OF THE SIDEWALK THICKNESS AND SHALL BE 1/8" TO 3/8" WIDE.
3. EXPANSION JOINT MATERIAL SHALL BE PREFORMED ASPHALT IMPREGNATED FIBERBOARD CONFORMING TO AASHTO M-213. EXPANSION JOINT MATERIAL SHALL BE LEFT 1/2" LOWER THAN GRADE OR TRIMMED 1/2" LOWER, AND FILLED WITH SILICONE SEALER TO FINISHED GRADE.
4. ALL COLD JOINTS AND SAW-CUT JOINTS SHALL BE FILLED TO FINISHED GRADE WITH JOINT SEALANT.
5. ALL SIDEWALKS SHALL HAVE ONE-HALF INCH ROLLED EDGES AND A BROOMED FINISH.
6. ALL WORK SHALL COMPLY WITH SECTION 633 OF THE ARKANSAS HIGHWAY AND TRANSPORTATION DEPARTMENT STANDARD SPECIFICATIONS.

ALL SIDEWALKS REQUIRE INSPECTION BEFORE AND AFTER CONCRETE PLACEMENT. SIDEWALK WILL BE CHECKED FOR ADA REQUIREMENTS & WORKMANSHIP.

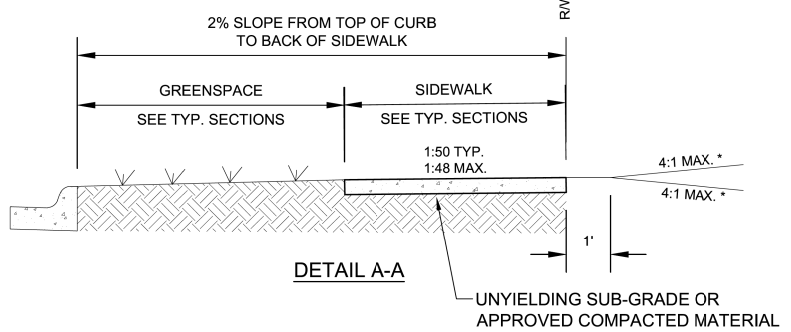


PLAN



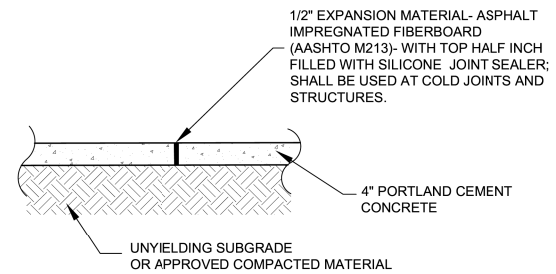
"T"=1/4 SLAB THICKNESS + 1/2"

JOINT SEALANT DETAIL



DETAIL A-A

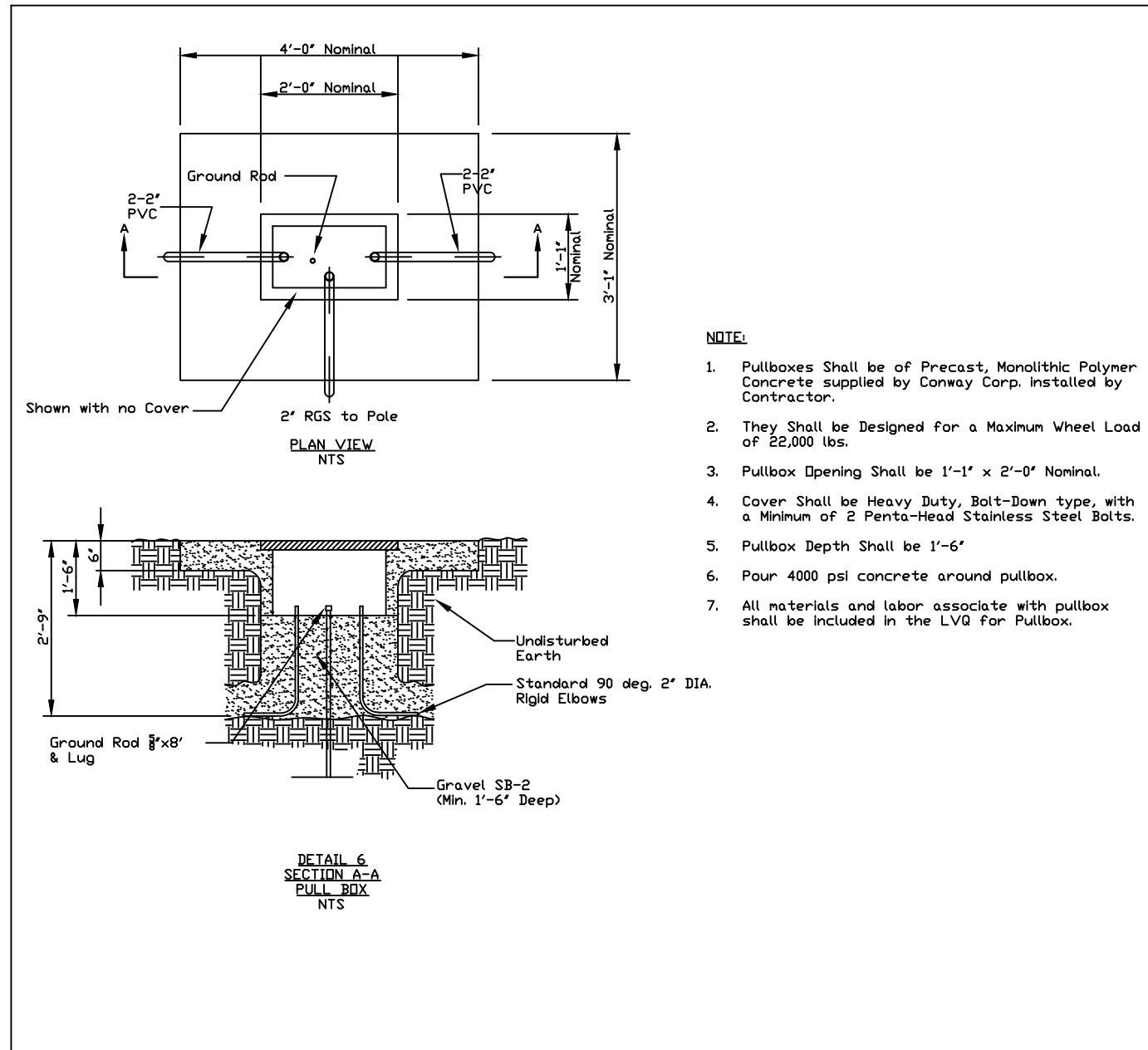
* SEE "DRIVEWAY DETAILS" FOR SIDEWALK SECTION THROUGH DRIVEWAY
EXCAVATION PERMIT REQUIRED TO EXCEED 4:1 MAX SLOPE



ELEVATION

<p>CITY OF CONWAY TRANSPORTATION DEPARTMENT 100 EAST ROBINS CONWAY, ARKANSAS 72032 501-450-6165</p>	TITLE: SIDEWALK DETAILS	DATE: FEBRUARY 2017	SHEET:
	DESCRIPTION: SIDEWALK WITH GREENSPACE	REVISED	
	DRAWN BY: NTR	CHECKED BY: BFV	FILE NAME: SW-1 SIDEWALK WITH GREENSPACE.dwg
			SW-1

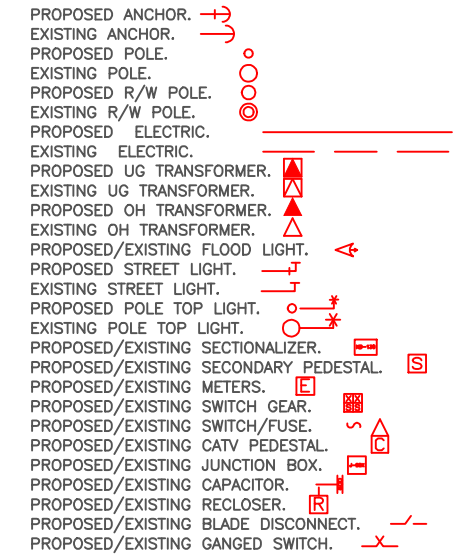
LIGHTING DETAILS



NOTE:

1. Pullboxes Shall be of Precast, Monolithic Polymer Concrete supplied by Conway Corp. installed by Contractor.
2. They Shall be Designed for a Maximum Wheel Load of 22,000 lbs.
3. Pullbox Opening Shall be 1'-1" x 2'-0" Nominal.
4. Cover Shall be Heavy Duty, Bolt-Down type, with a Minimum of 2 Penta-Head Stainless Steel Bolts.
5. Pullbox Depth Shall be 1'-6"
6. Pour 4000 psi concrete around pullbox.
7. All materials and labor associate with pullbox shall be included in the LVQ for Pullbox.

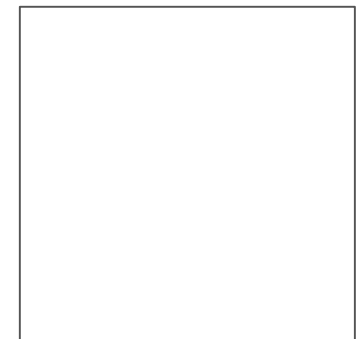
CONWAY CORPORATION ELECTRIC LEGEND.



CATV/INTERNET/VOIP PHONE PRE-WIRE SPECS:
 CATV/Internet pre-wire cable is 75 ohm size RG-6 (Commscope #F660BV or equal), copper clad steel conductor, with foam or solid dielectric, 100% aluminum foil bonded to dielectric, 60% aluminum braid, with a vinyl jacket. One homerun per outlet to designated CATV equipment location. No splices or splitters or loop through systems are allowed. Contractor must provide a CATV entrance route into structure. CATV pre-wire homerun exterior stub-outs should be in one conduit per designated location. The stub-out should be 5' from finished grade and 2' either side of the furthest electric module opposite from the telephone stub-out location. The CATV Department will furnish CATV boxes to be mounted on the exterior wall if required. A pre-wire inspection must be approved before the installation of insulation or wall coverings to receive a final inspection from the City of Conway. Call 1-501-548-3050 for an inspection or if you have any questions.
 VOIP Phone pre-wire cable is Category 5, 24 AWG, 4 pair, VOIP Phone pre-wire cable may be homerun or daisy-chained. Call 1-501-548-3050 if you have any questions.

Care in Performance of Duties:
 Each employee shall use reasonable care in the performance of his duties and act in such a manner as to assure at all times maximum safety to himself, his fellow employees and the public.
 a) Before beginning a job, the employee shall satisfy himself that he can perform the task without injury. If he is in doubt as to his ability to perform the work, he shall call this to the attention of his foreman.
 b) Before starting a job, each employee shall thoroughly understand the work to be done, his part in the work and the safety rules, which apply.

1"=2000'



CONWAY, FAULKNER COUNTY, ARKANSAS



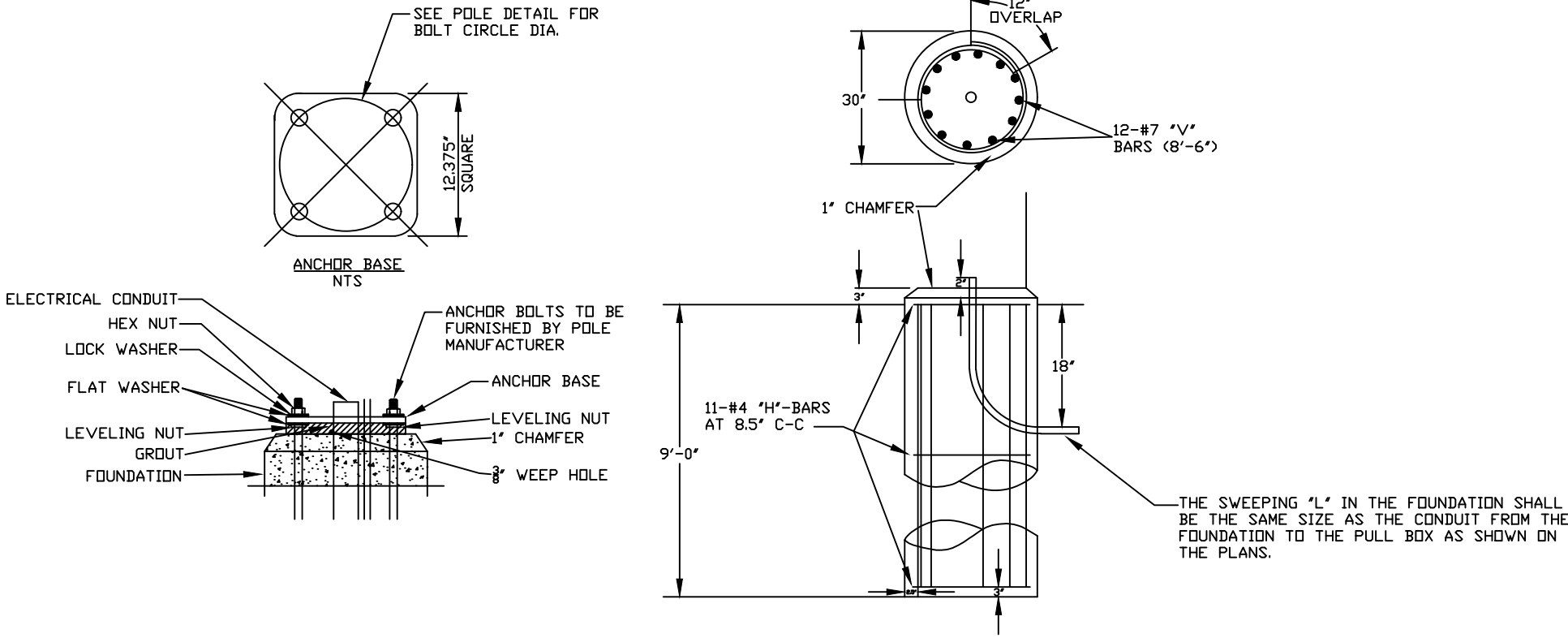
Conway Corporation Operators of the City-owned Electric, Electronic & Water Systems		
LIGHTING DETAILS		
SCALE:	ELEC & ECC	DWG. DATE:
NO SCALE:	JOB #: 17-089-1	4-21-2020
VERT.:	DRAWN BY: DWG	REVISED DATE:
REF.: RASTER	PAGE: 2 of 2	00-00-00

LIGHTING DETAILS

CONWAY CORPORATION ELECTRIC LEGEND.

- PROPOSED ANCHOR.
- EXISTING ANCHOR.
- PROPOSED POLE.
- EXISTING POLE.
- PROPOSED R/W POLE.
- EXISTING R/W POLE.
- PROPOSED ELECTRIC.
- EXISTING ELECTRIC.
- PROPOSED UG TRANSFORMER.
- EXISTING UG TRANSFORMER.
- PROPOSED OH TRANSFORMER.
- EXISTING OH TRANSFORMER.
- PROPOSED/EXISTING FLOOD LIGHT.
- PROPOSED STREET LIGHT.
- EXISTING STREET LIGHT.
- PROPOSED POLE TOP LIGHT.
- EXISTING POLE TOP LIGHT.
- PROPOSED/EXISTING SECTIONALIZER.
- PROPOSED/EXISTING SECONDARY PEDESTAL.
- PROPOSED/EXISTING METERS.
- PROPOSED/EXISTING SWITCH/GEAR.
- PROPOSED/EXISTING CATV PEDESTAL.
- PROPOSED/EXISTING JUNCTION BOX.
- PROPOSED/EXISTING CAPACITOR.
- PROPOSED/EXISTING RECLOSER.
- PROPOSED/EXISTING BLADE DISCONNECT.
- PROPOSED/EXISTING GANGED SWITCH.

90 MPH WIND ZONE DESIGN

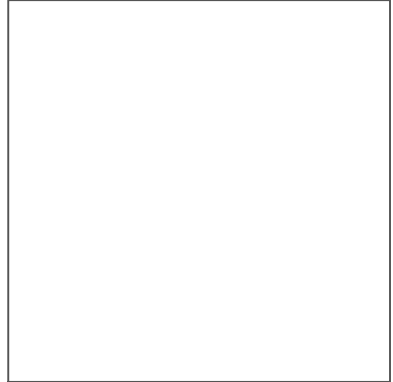


**DETAIL 7
POLE BASE/FOUNDATION**
Anchor Bolts shall be furnished by pole manufacturer and shall include as a minimum, one leveling nut, two flat washers, one lock washer, and one Hex. Nut. Perimeter of anchor base shall be grouted with a 3/8" weep hole. All concrete shall be Class "S" or greater. All reinforcing steel shall be Grade 40 minimum.

THE SWEEPING "L" IN THE FOUNDATION SHALL BE THE SAME SIZE AS THE CONDUIT FROM THE FOUNDATION TO THE PULL BOX AS SHOWN ON THE PLANS.

CATV/INTERNET/VOIP PHONE PRE-WIRE SPECS:
CATV/Internet pre-wire cable is 75 ohm size RG-6 (Commscope #F660BV or equal), copper clad steel conductor, with foam or solid dielectric, 100% aluminum foil bonded to dielectric, 60% aluminum braid, with a vinyl jacket. One homerun per outlet to designated CATV equipment location. No splices or splitters or loop through systems are allowed. Contractor must provide a CATV entrance route into structure. CATV pre-wire homerun exterior stub-outs should be in one conduit per designated location. The stub-out should be 5' from finished grade and 2' either side of the furthest electric module opposite from the telephone stub-out location. The CATV Department will furnish CATV boxes to be mounted on the exterior wall if required. A pre-wire inspection must be approved before the installation of insulation or wall coverings to receive a final inspection from the City of Conway. Call 1-501-548-3050 for an inspection or if you have any questions.
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1"=2000'



CONWAY, FAULKNER COUNTY, ARKANSAS



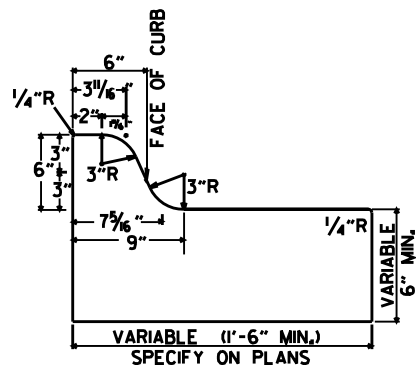
Conway Corporation
Operators of the City-owned Electric, Electronic & Water Systems

LIGHTING DETAILS

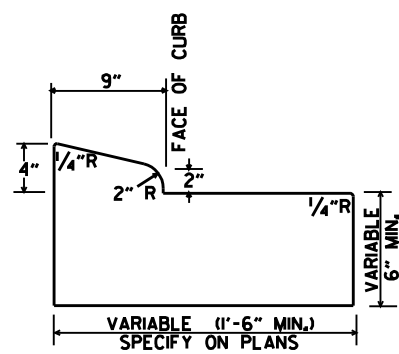
SCALE:	ELEC & ECC	DWG. DATE:
NO SCALE:	JOB #: 17-089-1	4-14-2020
VERT.:	DRAWN BY: DWG	REVISED DATE:
REF.: RASTER	PAGE: 1 of 2	00-00-00

176

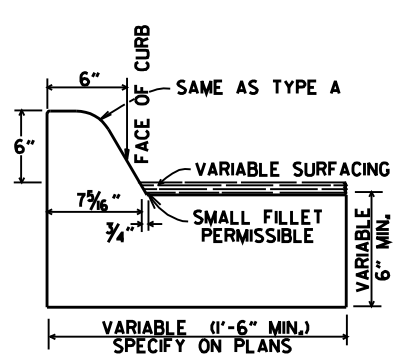
N:\Land Projects\Donaghy_Am_Widening_Dove_Ward_to_Ads_17-089\dwg\Details_17-089.dwg



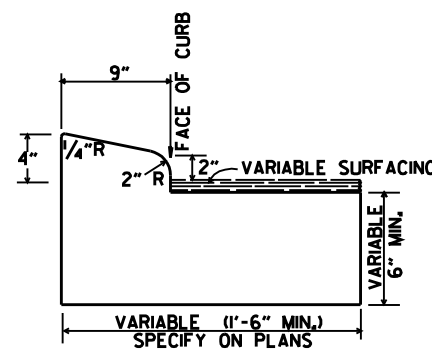
TYPE A



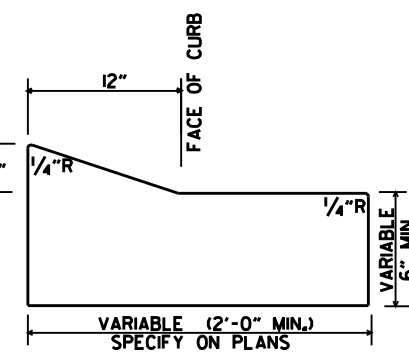
TYPE B-1



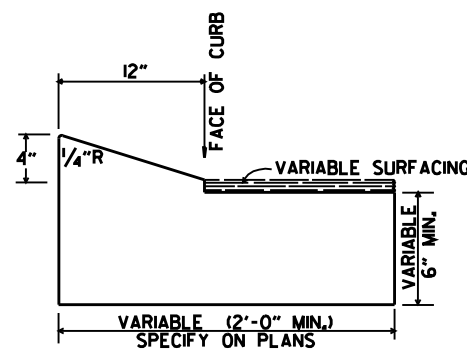
TYPE C



TYPE B-2

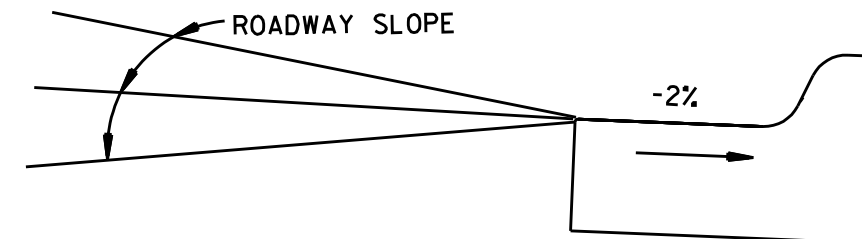


TYPE E-1

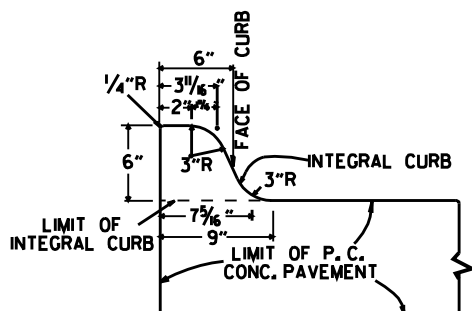


TYPE E-2

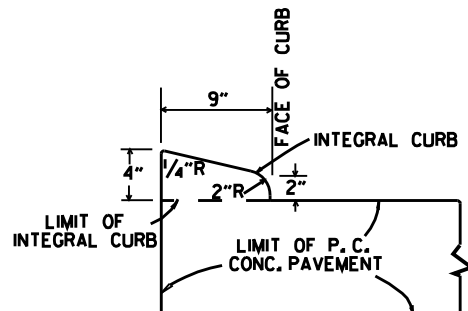
CONCRETE COMBINATION CURB AND GUTTER



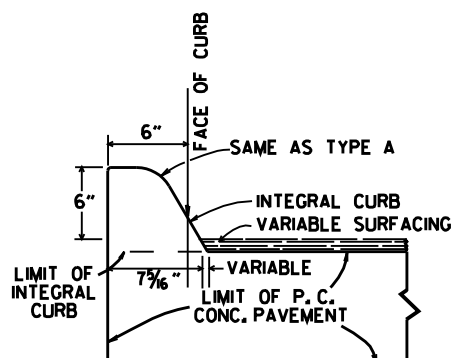
DETAIL OF GUTTER SLOPE
GUTTER SHALL BE CONSTRUCTED ON 2% SLOPE AWAY FROM ROADWAY, REGARDLESS OF ROADWAY SLOPE.



TYPE A

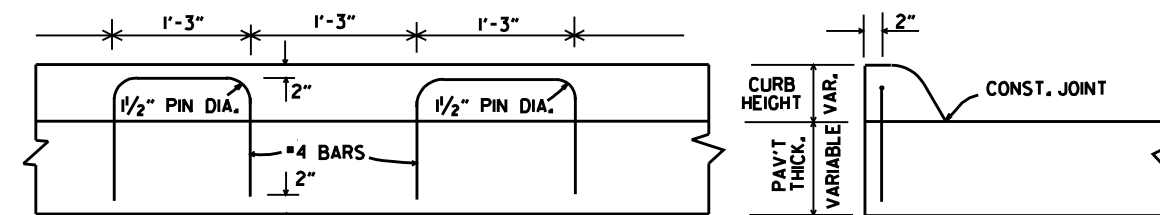


TYPE B



TYPE C

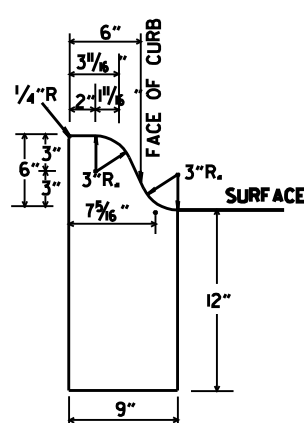
INTEGRAL CURB



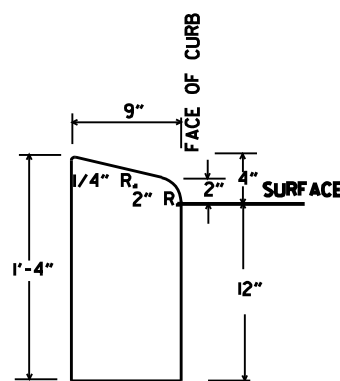
LONGITUDINAL SECTION

ELEVATION

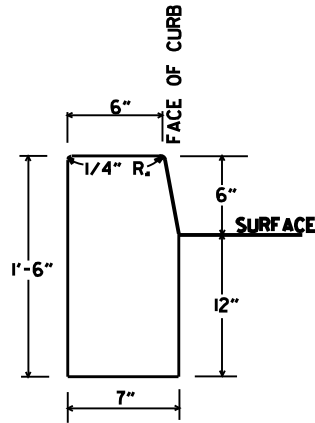
ALTERNATE CONSTRUCTION METHOD FOR INTEGRAL CURB



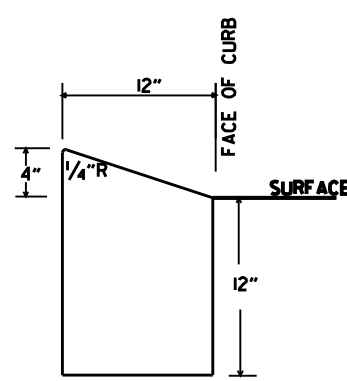
TYPE A



TYPE B

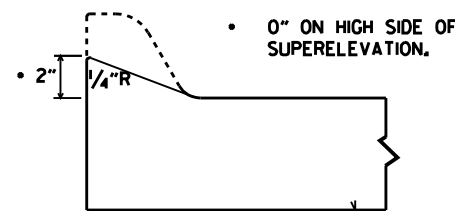


TYPE D



TYPE E

CONCRETE CURB



NOTE: USE MODIFIED CURB AS SPECIFIED ON STD. DR-1. COMPENSATION FOR MODIFIED CURB WILL BE CONSIDERED INCLUDED IN THE PRICE BID FOR THE TYPE OF CURB OR CURB AND GUTTER SPECIFIED.

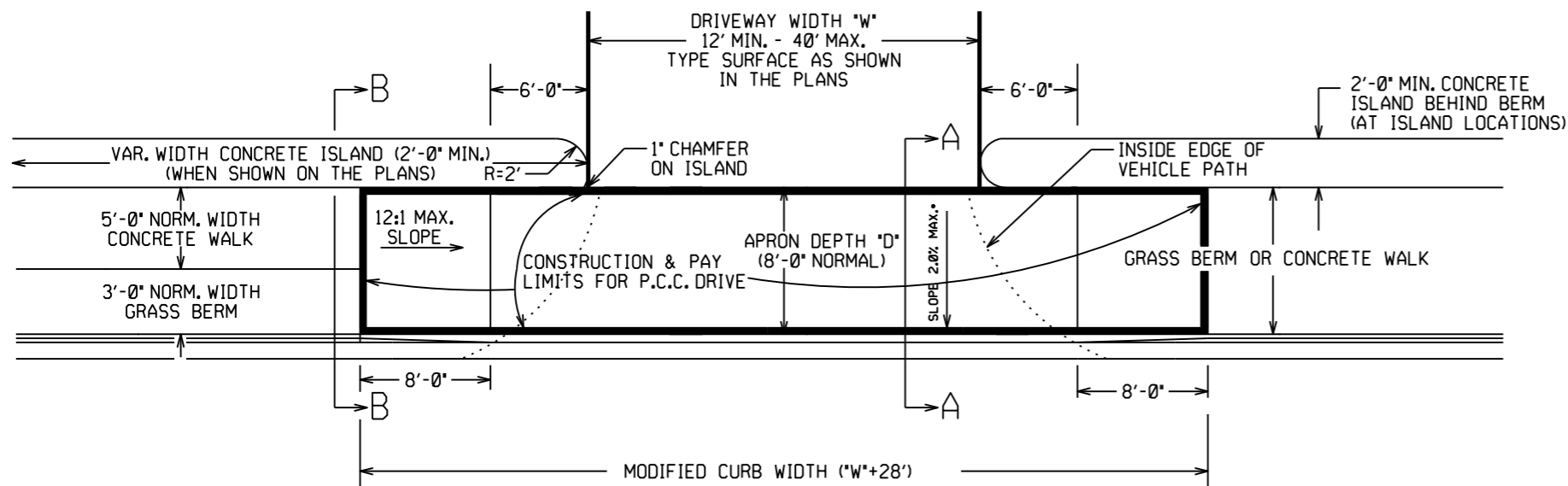
DETAILS OF MODIFIED CURB

DATE	REVISION	DATE FILMED
11-29-07	REVISED GUTTER SLOPE & MODIFIED CURB DETAILS	
11-10-05	ADDED DETAILS OF TYPE E CURBS	
11-16-01	REVISED CONCRETE CURB TYPE B	
11-18-98	REVISED MODIFIED CURB	
6-2-94	ADDED NOTE TO SPECIAL MODIFIED CURB	
8-5-93	CORRECTED GUTTER SLOPE	8-5-93
10-1-92	ADDED DETAILS OF GUTTER SLOPE	10-1-92
5-24-90	ADDED DETAILS OF MODIFIED CURB	5-24-90
11-30-89	VARIABLE DEPTH TYPE A & B 1	11-30-89
7-15-88	REVISED MODIFIED CURB	630-7-15-88
11-1-73	REVISED MODIFIED CURB	500-11-1-73
10-2-72	REVISED AND REDRAWN	512-10-2-72

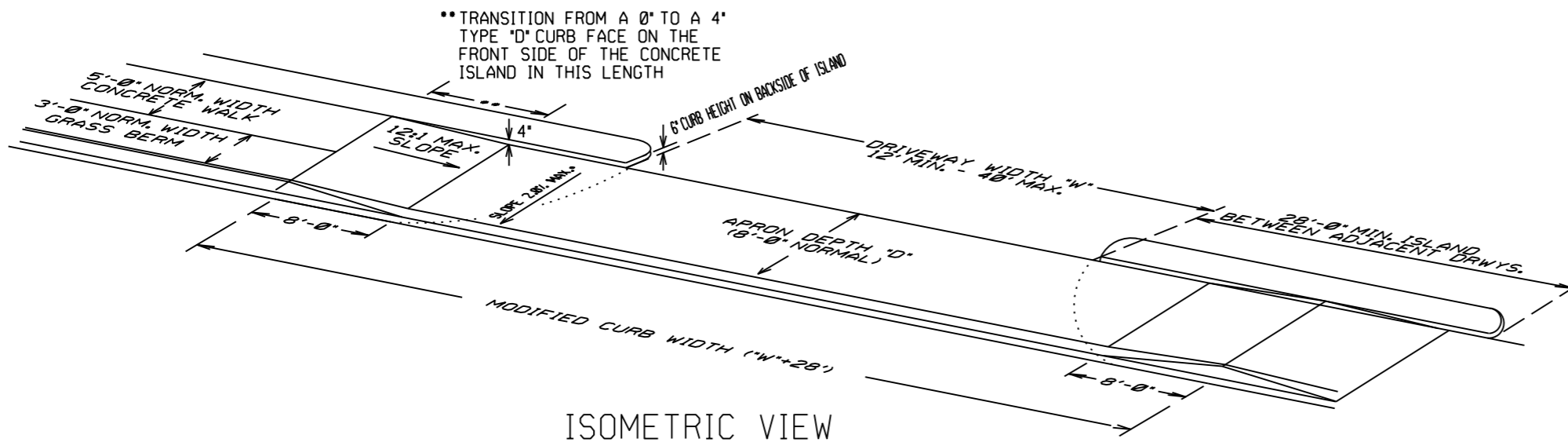
ARKANSAS STATE HIGHWAY COMMISSION

CURBING DETAILS

STANDARD DRAWING CG-1

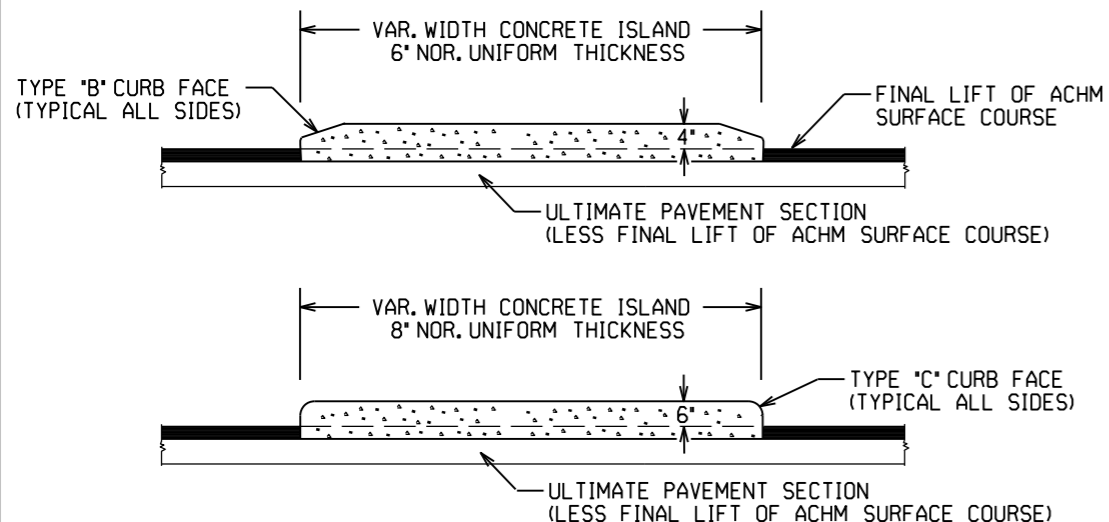


PLAN VIEW

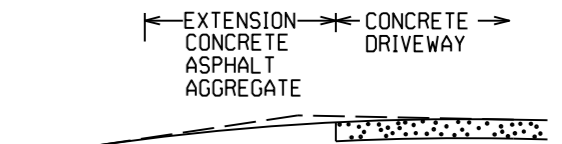


ISOMETRIC VIEW

REFER TO PLANS FOR TYPE OF CURB FACE TO BE USED. NO DIRECT PAYMENT WILL BE MADE FOR THE CURB FACES SHOWN ON THE ISLAND DETAILS. PAYMENT FOR THE CURB FACE WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE ITEM "CONCRETE ISLAND".



CURBED ISLANDS FOR CHANNELIZATION

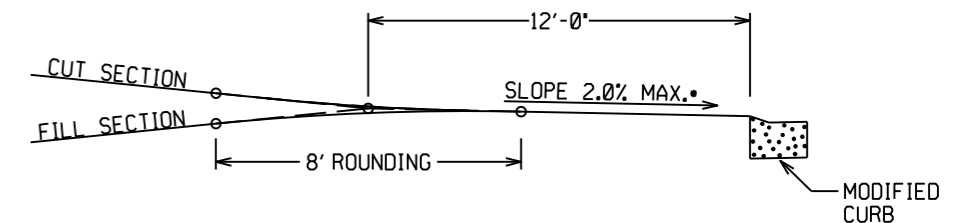


EXTENSION TYPICAL SECTIONS

- 1: CONCRETE - 6" P.C. CONCRETE DRIVEWAY
- 2: ASPHALT - 2" ACHM SURFACE COURSE (1/2")
4" ACHM BINDER COURSE (1") OR
4" ACHM BASE COURSE (1-1/2")
- 3: ASPHALT - 2" ACHM SURFACE COURSE (1/2")
7" AGGREGATE BASE COURSE
- 4: AGGREGATE - 6" AGGREGATE BASE COURSE

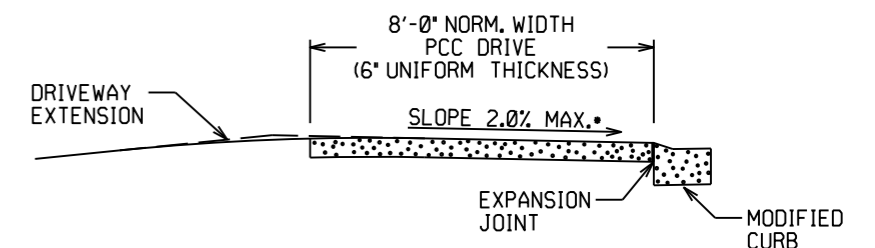
THE TYPE OF EXTENSION SHALL BE AS SHOWN IN THE PLANS. THE CONTRACTOR MAY, WITH THE APPROVAL OF THE ENGINEER, SUBSTITUTE A LOWER NUMBERED TYPE OF EXTENSION IN LIEU OF THE TYPE SPECIFIED IN THE PLANS, BUT AT NO ADDITIONAL COST TO THE DEPARTMENT.

DRIVEWAY EXTENSION DETAILS

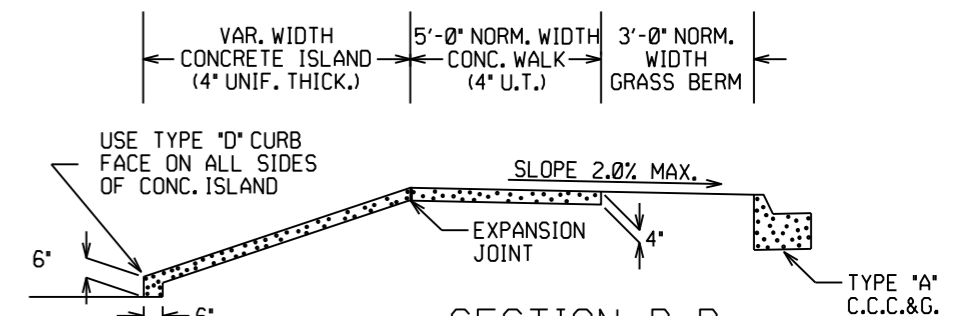


DRIVEWAY VERTICAL ALIGNMENT DETAILS

NOTE: DRIVEWAYS MAY NOT BE SLOPED AWAY FROM THE ROADWAY UNLESS APPROVED BY THE ENGINEER.

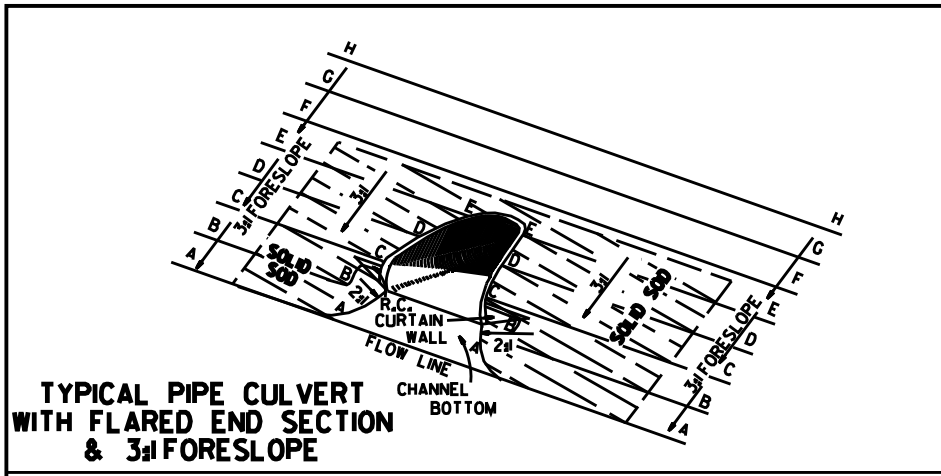


SECTION A-A

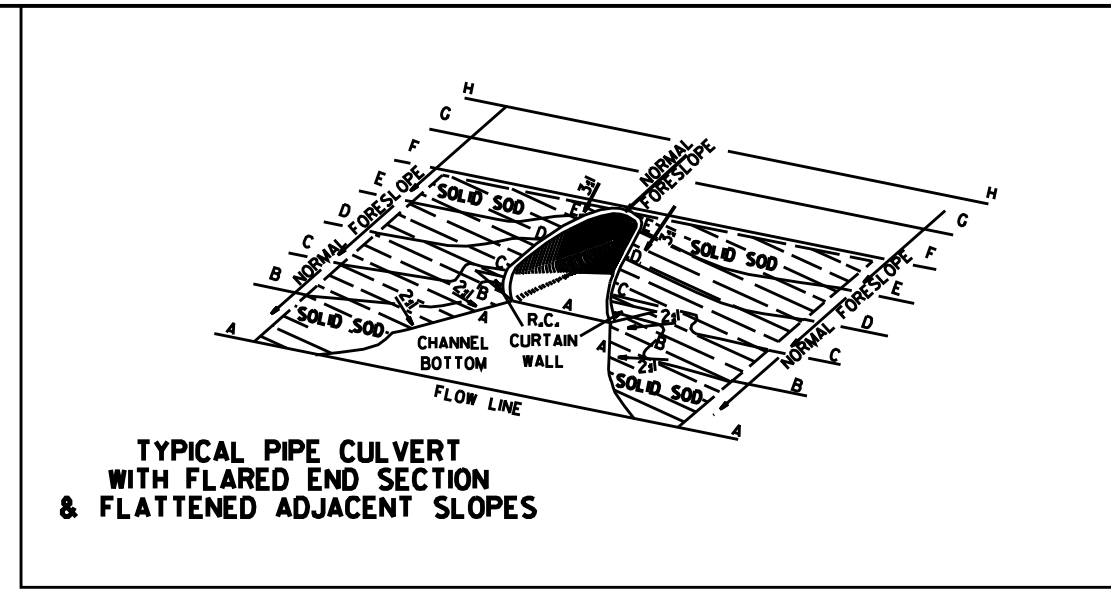


SECTION B-B
CURBED ISLAND BEHIND WALK

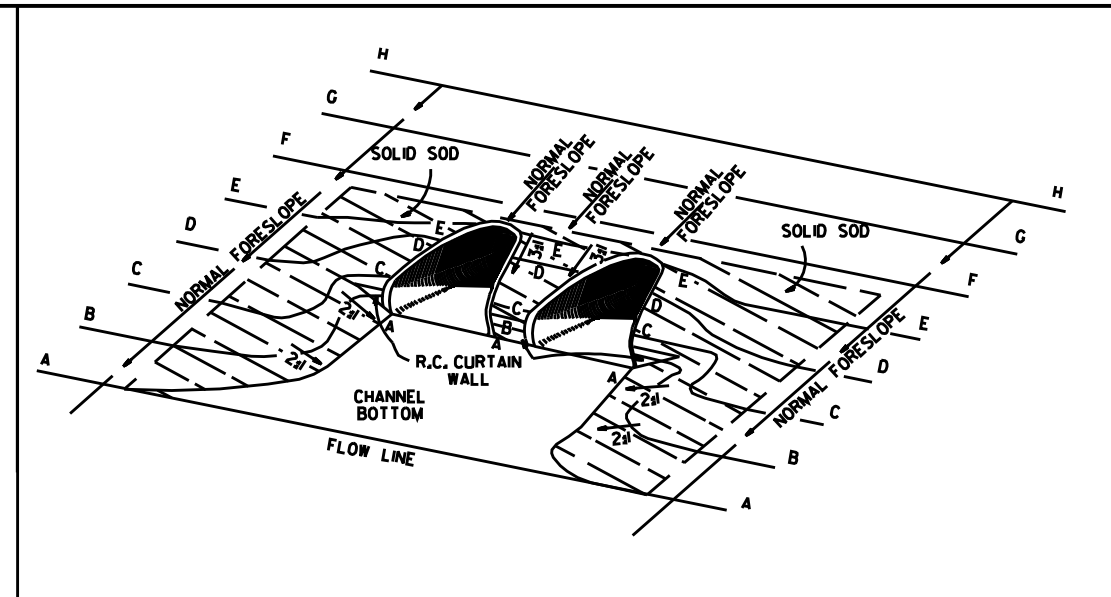
DATE	REV	DATE FILMED	DESCRIPTION
11-07-19			REVISED WALK DETAILS
2-27-14			REVISED PLAN & ISOMETRIC VIEW
11-29-07			ADDED CHANNELIZATION ISLAND WITH TYPE C CURB FACE & REVISED DRIVEWAY SLOPE NOTE & VERTICAL ALIGNMENT DETAIL
11-10-05			REV. APRON SLOPE & DEPTH OF AGG. BASE.
8-22-02			ADDED ISLAND DETAILS & NOTES
3-30-00			REV. MOD. CURB WIDTH & TRANS. NOTE
11-19-98			REVISED NOTES
11-18-98			REDRAWN AND REISSUED



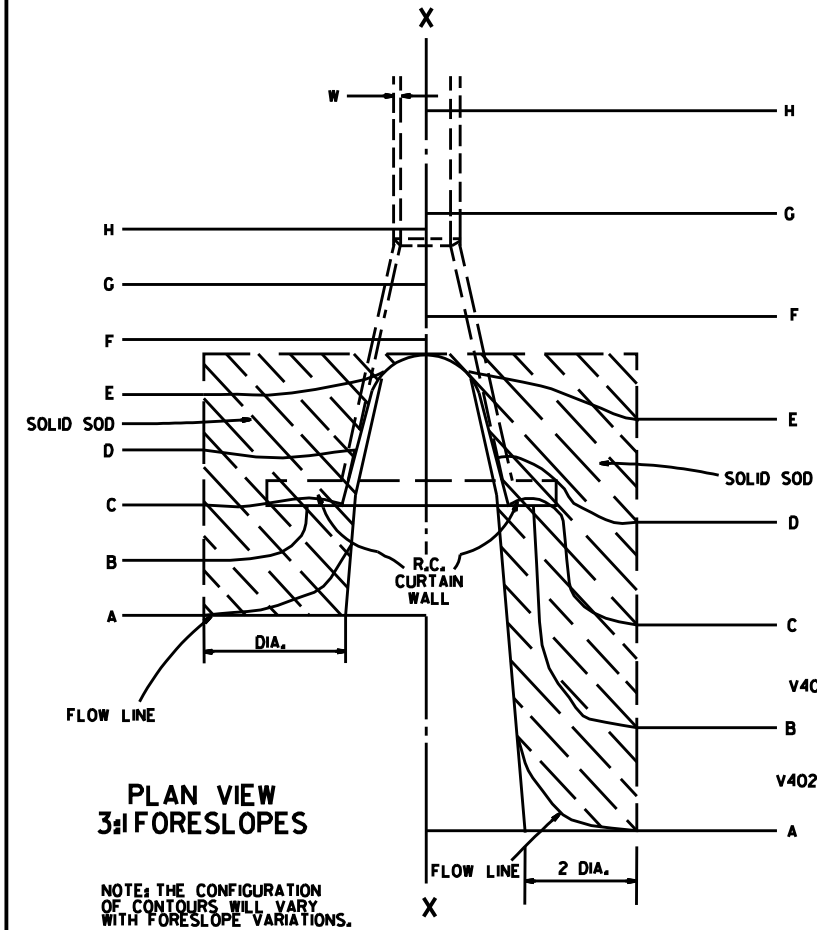
TYPICAL PIPE CULVERT WITH FLARED END SECTION & 3/4:1 FORESLOPE



TYPICAL PIPE CULVERT WITH FLARED END SECTION & FLATTENED ADJACENT SLOPES

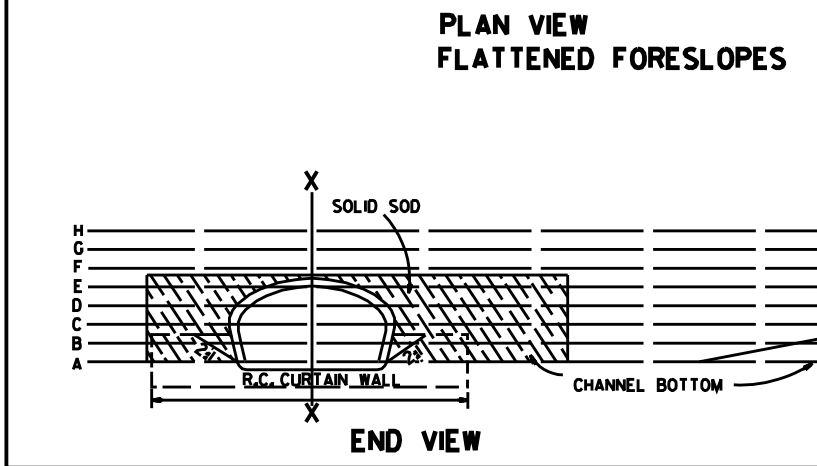


TYPICAL MULTIPLE PIPE CULVERT WITH FLARED END SECTIONS & FLATTENED ADJACENT SLOPES



PLAN VIEW 3/4:1 FORESLOPES

PLAN VIEW FLATTENED FORESLOPES

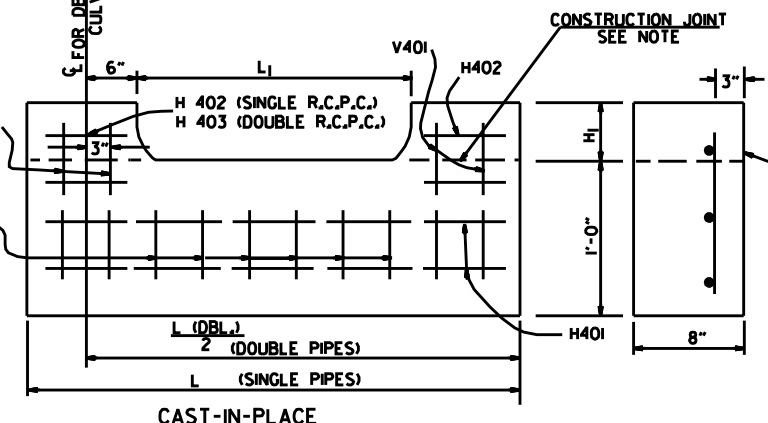


END VIEW

R.C. CURTAIN WALL DIMENSIONS & QUANTITIES

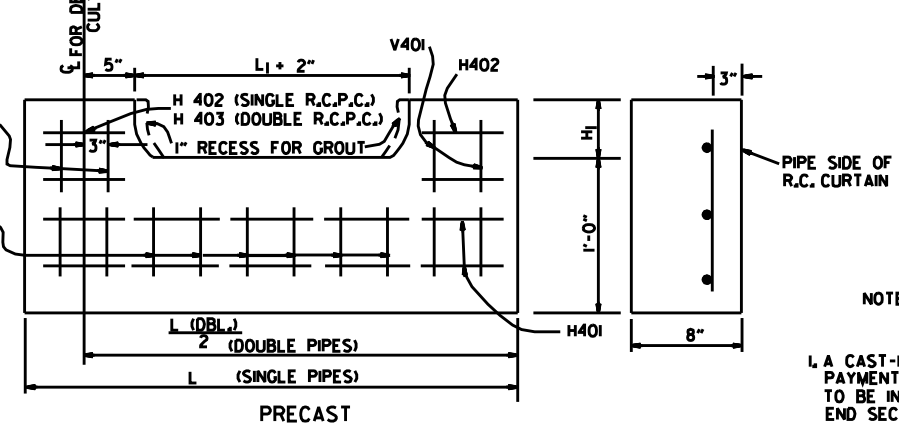
PIPE DIA.	H ₁	L ₁	L	L (DBL.) 2	SINGLE R.C.P.C.		DOUBLE R.C.P.C.	
					CONC.	REINF. STEEL	CONC.	REINF. STEEL
					CU. YDS.	LBS.	CU. YDS.	LBS.
18"	11/2"	3'-5"	8'-0"	6'-3"	0.31	27.7	0.45	39.5
24"	1'-0 1/2"	4'-6"	9'-6"	7'-6"	0.37	33.4	0.53	48.0
30"	1'-3 1/2"	5'-7"	11'-0"	9'-0"	0.45	39.0	0.67	59.0
36"	1'-7"	6'-8"	13'-0"	10'-6"	0.58	52.6	0.83	73.9
42"	2'-1 1/2"	7'-3"	15'-6"	12'-0"	0.82	77.1	1.10	100.7
48"	2'-5"	7'-10"	17'-0"	13'-0"	0.98	94.9	1.27	120.4
54"	2'-9 1/2"	8'-5"	18'-6"	14'-0"	1.16	115.8	1.47	143.7
60"	3'-4"	9'-0"	20'-6"	15'-6"	1.47	149.7	1.84	180.3
72"	4'-5"	10'-2"	25'-6"	18'-6"	2.31	232.6	2.73	271.0

NOTE: QUANTITIES SHOWN ARE FOR ONE (1) CURTAIN WALL.



NOTE: THE PORTION OF THE R.C. CURTAIN WALL BENEATH THE FLARED END SECTION (LOWER 1'-0") SHALL BE PLACED MONOLITHICALLY. THE FLARED END SECTION SHALL THEN BE SET IN PLACE & THE REMAINING PORTIONS OF THE R.C. CURTAIN WALL PLACED.

R.C. CURTAIN WALL DETAILS



NOTE: THE PRECAST CURTAIN WALL WILL BE SET AND BACKFILLED WITH COMPACTED MATERIAL. THE FLARED END SECTION SHALL THEN BE SET IN PLACE AND THE 1" RECESS FILLED WITH GROUT. WHERE "L" EXCEEDS 11' THE CURTAIN WALL MAY BE CAST IN TWO (2) OR MORE SECTIONS. THE METHOD OF JOINING THE SECTIONS FOR INSTALLATION SHALL BE APPROVED BY THE ENGINEER.

REINFORCING STEEL SCHEDULE

PIPE DIA.	SINGLE R.C. PIPE CULVERT								DOUBLE R.C. PIPE CULVERT									
	H401		H402		V401		V402		H401		H402		H403		V401		V402	
	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.	L	NO.
18"	7'-8"	2	1'-11/2"	4	1'-7 1/2"	8	8"	8	12'-2"	2	1'-11/2"	4	8"	2	1'-7 1/2"	10	8"	14
24"	9'-2"	2	2'-2"	4	1'-8 1/2"	10	8"	9	14'-8"	2	2'-2"	4	8"	2	1'-8 1/2"	12	8"	18
30"	10'-8"	2	2'-4 1/2"	4	1'-11/2"	10	8"	12	17'-8"	2	2'-4 1/2"	4	8"	2	1'-11/2"	14	8"	22
36"	12'-8"	2	2'-10"	6	2'-3"	12	8"	14	20'-8"	2	2'-10"	6	8"	3	2'-3"	14	8"	28
42"	15'-2"	2	3'-9 1/2"	8	2'-9 1/2"	16	8"	15	23'-8"	2	3'-9 1/2"	8	8"	4	2'-9 1/2"	18	8"	30
48"	16'-8"	2	4'-3"	10	3'-1"	18	8"	16	25'-8"	2	4'-3"	10	8"	5	3'-1"	20	8"	32
54"	18'-2"	2	4'-8 1/2"	12	3'-5 1/2"	20	8"	17	27'-8"	2	4'-9"	12	8"	6	3'-5 1/2"	22	8"	34
60"	20'-2"	2	5'-5"	14	4'-0"	24	8"	18	30'-8"	2	5'-5"	14	8"	7	4'-0"	26	8"	36
72"	25'-2"	2	7'-4"	18	5'-1"	30	8"	20	36'-8"	2	7'-4"	18	8"	9	5'-1"	33	8"	40

ALL REINFORCING STEEL #4 BARS @ 6" O.C.

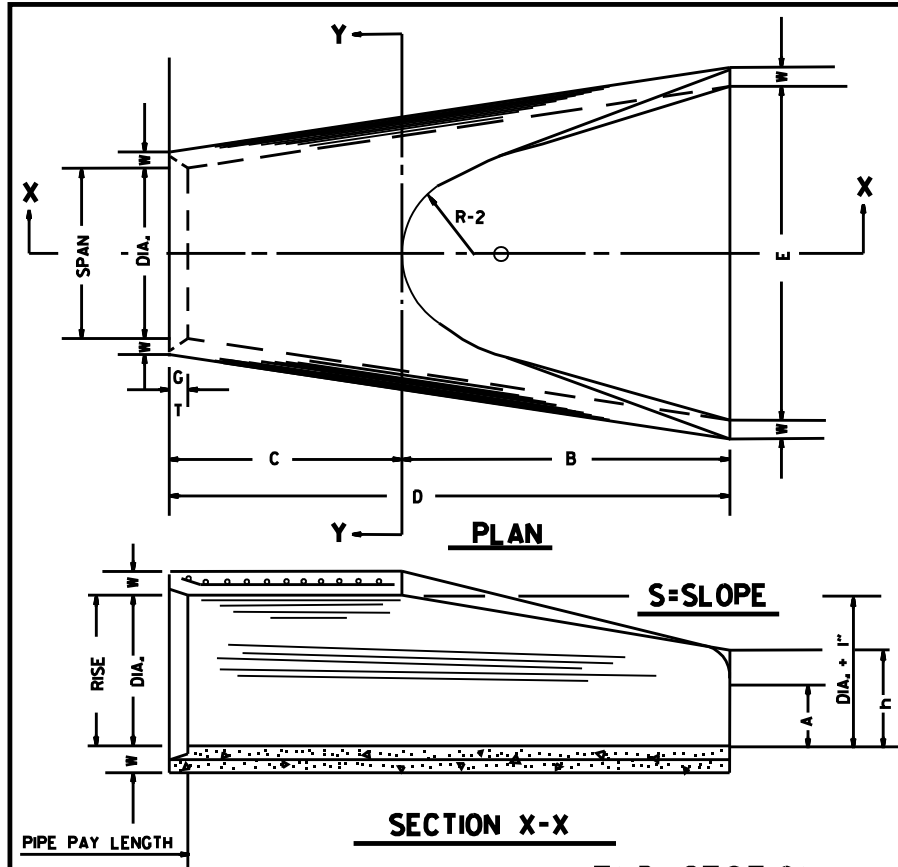
SOLID SODDING

PIPE DIA.	SINGLE R.C.P.C.						DOUBLE R.C.P.C.					
	3:1		4:1		6:1		3:1		4:1		6:1	
	SO. YDS.						SO. YDS.					
18"	5	7	12	6	8	13	5	7	12	6	8	13
24"	8	12	19	9	13	20	8	12	19	9	13	20
30"	13	18	29	14	19	30	13	18	29	14	19	30
36"	17	26	41	18	28	43	17	26	41	18	28	43
42"	23	35	55	25	37	57	23	35	55	25	37	57
48"	29	46	68	31	48	70	29	46	68	31	48	70
54"	35	57	85	37	59	87	35	57	85	37	59	87
60"	45	62	104	48	65	107	45	62	104	48	65	107
72"	64	92	156	67	95	159	64	92	156	67	95	159

NOTE: QUANTITIES SHOWN ABOVE ARE FOR ONE (1) END OF F.E.S.

- GENERAL NOTES
1. A CAST-IN-PLACE OR PRECAST CURTAIN WALL MAY BE USED. PAYMENT FOR THE CURTAIN WALL SHALL BE CONSIDERED TO BE INCLUDED IN THE UNIT PRICE BID EACH FOR FLARED END SECTIONS OF THE SEVERAL SIZES, WHICH PRICE SHALL BE FULL COMPENSATION FOR FURNISHING ALL MATERIALS INCLUDING REINFORCING STEEL AND CONCRETE; FOR FORMS, MIXING AND PLACING; FOR EXCAVATION AND BACKFILL, AND FOR ALL LABOR, TOOLS, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE WORK.
 2. ALL EXPOSED EDGES SHALL BE CHAMFERED 3/4".
 3. CONCRETE FOR CURTAIN WALL SHALL MEET THE REQUIREMENTS FOR CLASS A OR S CONCRETE AS PROVIDED IN SECTION 802 OF THE STANDARD SPECIFICATIONS OR FOR PAVING CONCRETE AS PROVIDED IN SECTION 501 OF THE STANDARD SPECIFICATIONS.
 4. WELDED WIRE MESH 3 x 3 W/10 x W/10 MAY BE USED IN LIEU OF REINFORCING BARS.

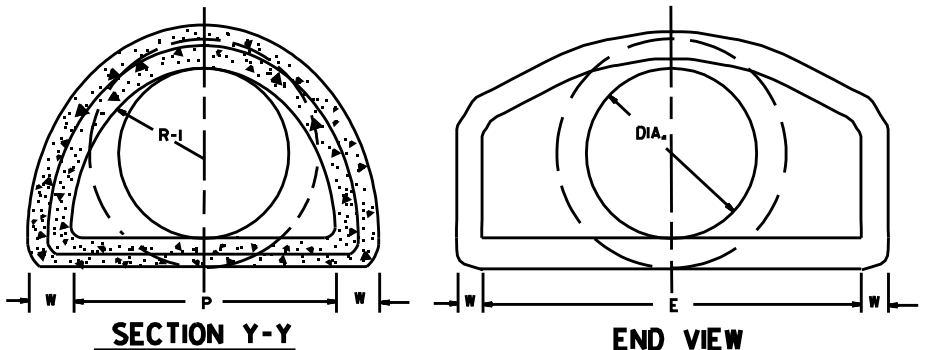
10-18-98	ADDED NOTE TO SOLID SODDING		ARKANSAS STATE HIGHWAY COMMISSION
10-12-98	CORRECTED SPELLING		
11-3-94	ADDED GENERAL NOTE NO. 4		
8-15-91	REV. CURTAIN WALL QUANT., STEEL SCH. & SOLID SOD QUANT.		
3-2-81	ADDED PRECAST IN 2 OR MORE PIECES CHAMFER EDGES		
5-15-80	ADDED PRECAST WALL & GENERAL NOTES		
10-2-72	REVISED AND REDRAWN		
DATE	REVISION	FILMED	STANDARD DRAWING FES-179



END SECTION FOR REINFORCED CONCRETE PIPE CULVERTS

TABLE OF DIMENSIONS

DIA.	WALL	A	B	C	D	E	S	DIA. + 1"	P	R-1	R-2	G-T	WT.	h
18"	2 1/2"	9"	2'-3"	3'-10"	6'-1"	3'-0"	3#1	19"	29"	15 1/2"	12"	2"	1000	1'-0 1/2"
24"	3"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	3#1	25"	33 3/8"	16 1/4"	14"	2 1/2"	1600	1'-1 1/2"
30"	3 1/2"	1'-0"	4'-6"	1'-7 1/2"	6'-1 3/4"	5'-0"	3#1	31"	37"	18 1/2"	15"	3 1/4"	1940	1'-4 5/8"
36"	4"	1'-3"	5'-3"	2'-10 1/4"	8'-1 1/2"	6'-0"	3#1	37"	47 1/4"	24 1/4"	20"	3 1/2"	4100	1'-8"
42"	4 1/2"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	3#1	43"	53 1/2"	27 1/2"	22"	3 1/2"	5380	2'-2 1/2"
48"	5"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	3#1	49"	56 1/2"	28 1/2"	22"	3 1/2"	6550	2'-6"
54"	5 1/2"	2'-4"	6'-6"	1'-10"	8'-4"	7'-6"	3#1	55"	65 1/2"	33 1/8"	24"	4"	8750	2'-10 1/2"
60"	6"	2'-10"	6'-6"	1'-10"	8'-4"	8'-0"	3#1	61"	72 1/2"	36 1/4"	24"	4"	9270	3'-5"
72"	7"	3'-10"	6'-6"	1'-10"	8'-4"	9'-0"	3#1	73"	77 1/4"	38 3/4"	24"	5"	13250	4'-6"



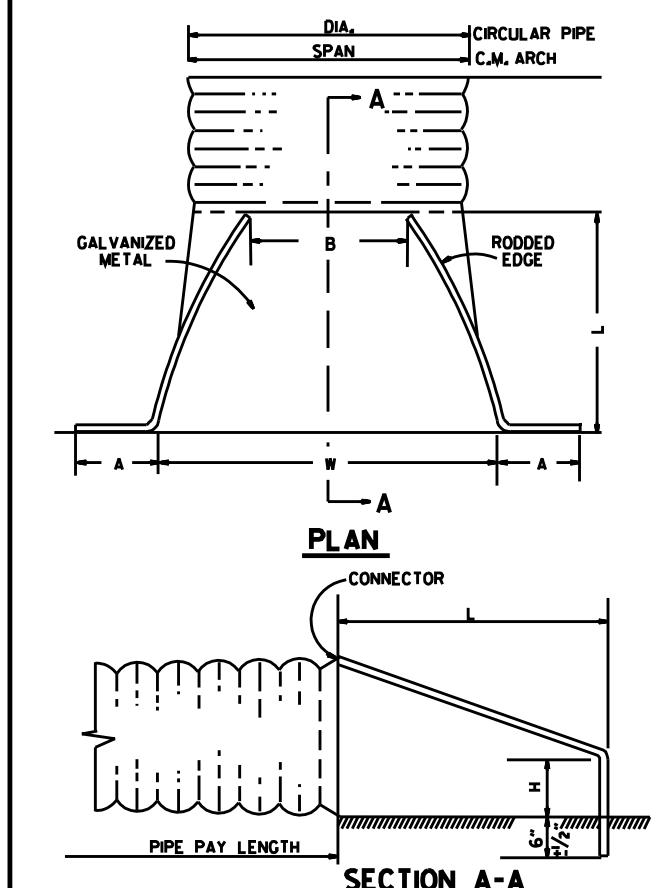
END VIEW CONCRETE ARCH PIPE

NOTE: TONGUE END ON UPSTREAM SECTION
GROOVE END ON DOWNSTREAM SECTION

ARCH PIPE

EQUIV. DIA.	SPAN		RISE		W	A	B	C	D	E	P	R2	G-T	S
	AASHTO M 206	AHD NOMINAL	AASHTO M 206	AHD NOMINAL										
15	18	18	11	11	2"	4"	2'-0"	4'-0"	6'-0"	3'-0"	29"	12"	1 1/2"	2 1/2#1
18	22	22	13 1/2	14	2 1/2"	5"	2'-0"	4'-1"	6'-1"	3'-6"	32 1/8"	13"	2 1/2"	2 1/2#1
21	26	26	15 1/2	16	3"	7"	2'-3"	3'-10"	6'-1"	4'-0"	34 1/8"	14"	2 1/2"	2 1/2#1
24	28 1/2	29	18	18	3"	9"	2'-3"	3'-10"	6'-1"	5'-0"	36 1/8"	15"	2 1/2"	2 1/2#1
30	36 1/4	36	22 1/2	23	3 1/2"	10"	3'-1"	3'-0 1/2"	6'-1 1/2"	6'-0"	47 1/4"	20"	3"	2 1/2#1
36	43 1/4	44	26 1/2	27	4"	10 1/2"	4'-0"	2'-1 1/2"	6'-1 1/2"	6'-6"	54 1/4"	22"	3 1/2"	2 1/2#1
42	51 1/8	51	31 1/2	31	4 1/2"	11 1/2"	4'-7"	1'-10 1/4"	6'-5 1/4"	7'-2"	59 1/2"	23"	3 3/4"	2 1/2#1
48	58 1/2	59	36	36	5"	1'-3"	5'-3"	2'-10 1/4"	8'-1 1/4"	7'-10"	70 1/4"	24"	4 1/4"	2 1/2#1
54	65	65	40	40	5 1/2"	1'-7"	5'-3"	2'-11"	8'-2"	8'-6"	72 1/4"	24"	4 1/4"	2 1/2#1
60	73	73	45	45	6"	1'-10"	5'-6"	2'-8"	8'-2"	9'-0"	77 1/4"	24"	5"	2 1/2#1

* THE MEASURED SPAN AND RISE SHALL NOT VARY MORE THAN ± 2 PER CENT FROM THE VALUES SPECIFIED BY AASHTO M 206.

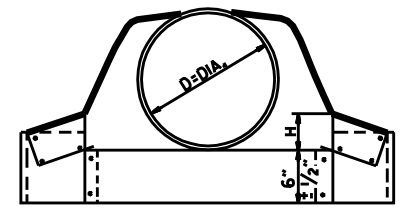


END SECTIONS FOR CORRUGATED METAL PIPE CULVERTS

NOTE: ALTERNATE CONNECTIONS TO THE PIPE CULVERTS, IN ACCORDANCE WITH MANUFACTURER'S STANDARD PRACTICES, MAY BE MADE SUBJECT TO THE APPROVAL OF THE ENGINEER.

CIRCULAR PIPE

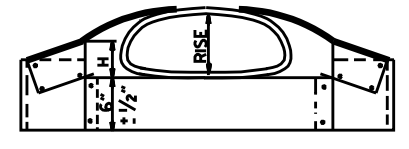
D. DIA.	GAUGE	A	B. MAX.	H	L	W	S
12	16	6	6	6	21	24	2 1/2#1
15	16	7	8	6	26	30	2 1/2#1
18	16	8	10	6	31	36	2 1/2#1
21	16	9	12	6	36	42	2 1/2#1
24	16	10	13	6	41	48	2 1/2#1
30	14	12	16	8	51	60	2 1/2#1
36	14	14	19	9	60	72	2 1/2#1
42	12	16	22	11	69	84	2 1/2#1
48	12	18	27	12	78	90	2 1/2#1
54	12	18	30	12	84	102	2#1
60	12	18	33	12	87	114	1 1/2#1
66	12	18	36	12	87	120	1 1/2#1
72	12	18	39	12	87	126	1 1/3#1



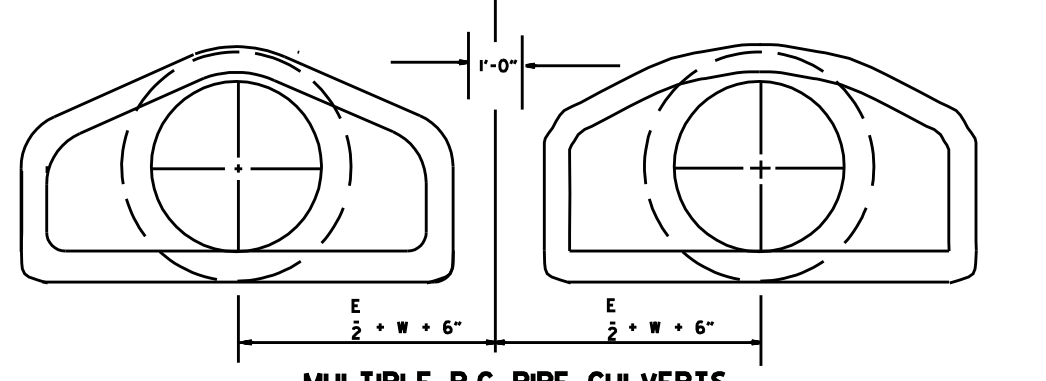
CIRCULAR PIPE

C.M. ARCH PIPE

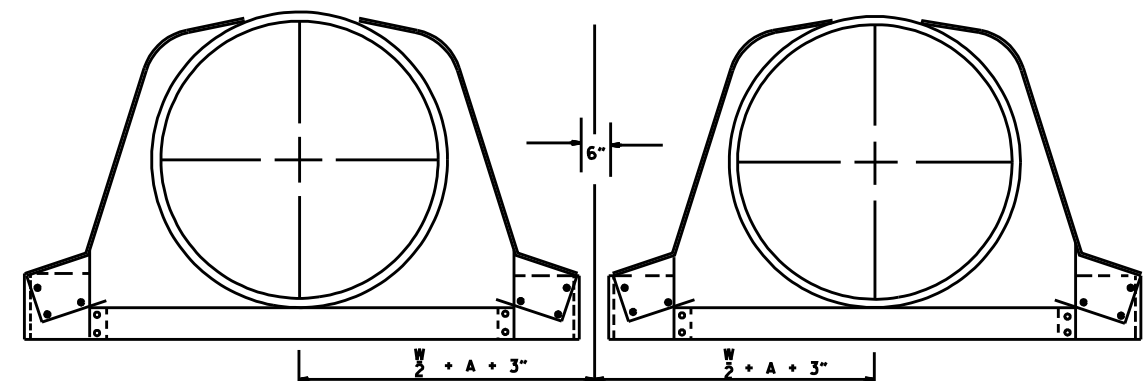
EQUIV. DIA.	SPAN	RISE	A	B	H	L	W	S	GAUGE
15"	17	13	7	9	6	19	30	2 1/2#1	16
18"	21	15	7	10	6	23	36	2 1/2#1	16
21"	24	18	8	12	6	28	42	2 1/2#1	16
24"	28	20	9	14	6	32	48	2 1/2#1	16
30"	35	24	10	16	6	39	60	2 1/2#1	14
36"	42	29	12	18	8	46	75	2 1/2#1	14
42"	49	33	13	21	9	53	85	2 1/2#1	12
48"	57	38	18	26	12	63	90	2 1/2#1	12
54"	64	43	18	30	12	70	102	2 1/2#1	12
60"	71	47	18	33	12	77	114	2 1/2#1	12



C.M. ARCH PIPE



MULTIPLE R.C. PIPE CULVERTS



MULTIPLE C.M. PIPE CULVERTS

10-18-96	REVISED ASTM REF. TO AASHTO		
5-15-80	REVISED DISTANCE BETWEEN MULTIPLE R.C.P. F.E.S.	664-5-15-80	ARKANSAS STATE HIGHWAY COMMISSION
7-14-78	C.M. ARCH SIZES TO CONFORM WITH AASHTO SIZES	752-7-14-78	
8-22-75	ADDED MULTIPLE PIPE CULVERTS	517-8-22-75	FLARED END SECTION
12-5-74	REMOVED NOTE RE REINF. FOR R.C. F.E.S.	500-12-5-74	
5-24-73	CMP END SECTION, SHOW PIPE PAY LENGTH	627-5-24-73	
10-2-72	REVISED AND REDRAWN	760-10-2-72	STANDARD DRAWING FES-2
DATE	REVISION	FIG. NO.	180

4'-0" LENGTH DROP INLET DROP INLET EXTENSION

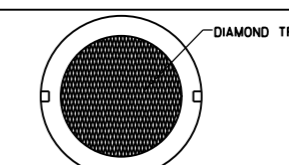
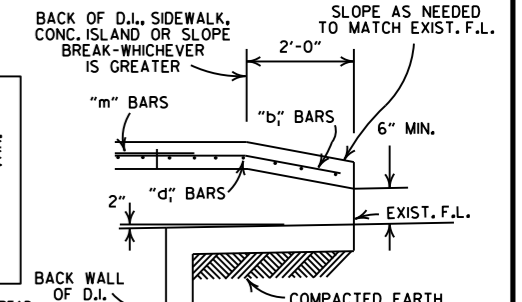
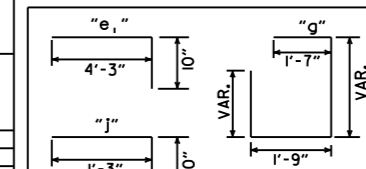
PIPE SIZE	MIN. WIDTH	HEIGHT 5'-0"		PLUS OR MINUS PER LIN. FT. OF HEIGHT		4'-0"		8'-0"	
		CLASS A CONC.	REINF. STEEL	CLASS A CONC.	REINF. STEEL	CLASS A CONC.	REINF. STEEL	CLASS A CONC.	REINF. STEEL
		CU. YDS.	POUNDS	CU. YDS.	POUNDS	CU. YDS.	POUNDS	CU. YDS.	POUNDS
18"	2'-6"	1.77	156	0.28	22	0.58	38	0.87	72
24"	2'-6"	1.79	156	0.28	22				
30"	3'-2"	2.39	205	0.30	26				
36"	3'-8"	2.63	236	0.32	28				
42"	4'-4"	2.95	250	0.34	30				
48"	4'-10"	3.21	265	0.36	32				
						DEDUCT FROM QUANTITY COMPUTED FOR EACH EXTENSION ADDED.			
						0.04	3		

NOTE: QUANTITIES ARE APPROXIMATE AND ARE SHOWN FOR BIDDER INFORMATION ONLY.

DEDUCT FROM QUANTITY COMPUTED FOR EACH PIPE ENTERING INLET

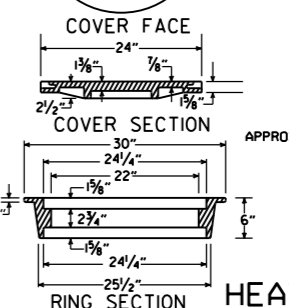
INSIDE DIA. PIPE	CLASS A CONC.	REINF. STEEL
INCHES	CU. YDS.	POUNDS
18	0.05	2
24	0.09	3
30	0.13	4
42	0.24	8

BAR DIAGRAM



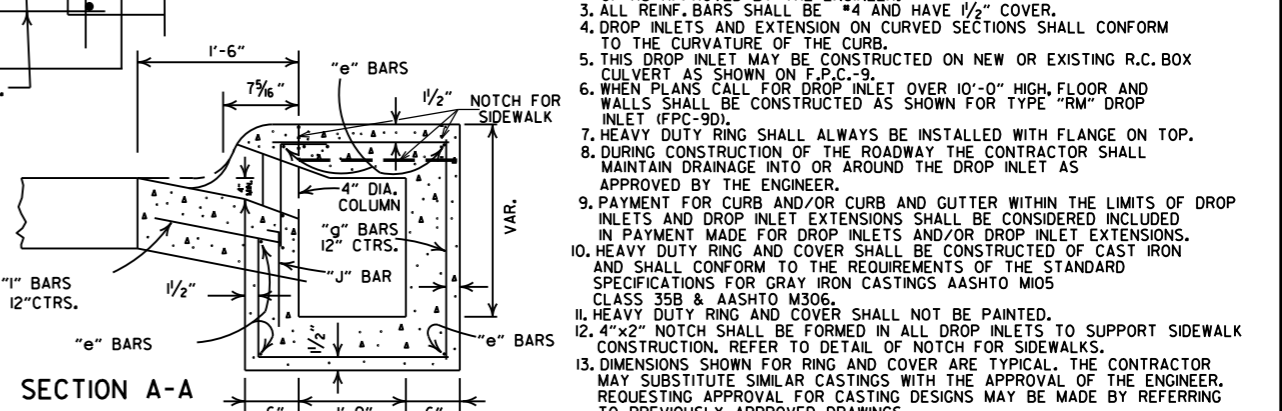
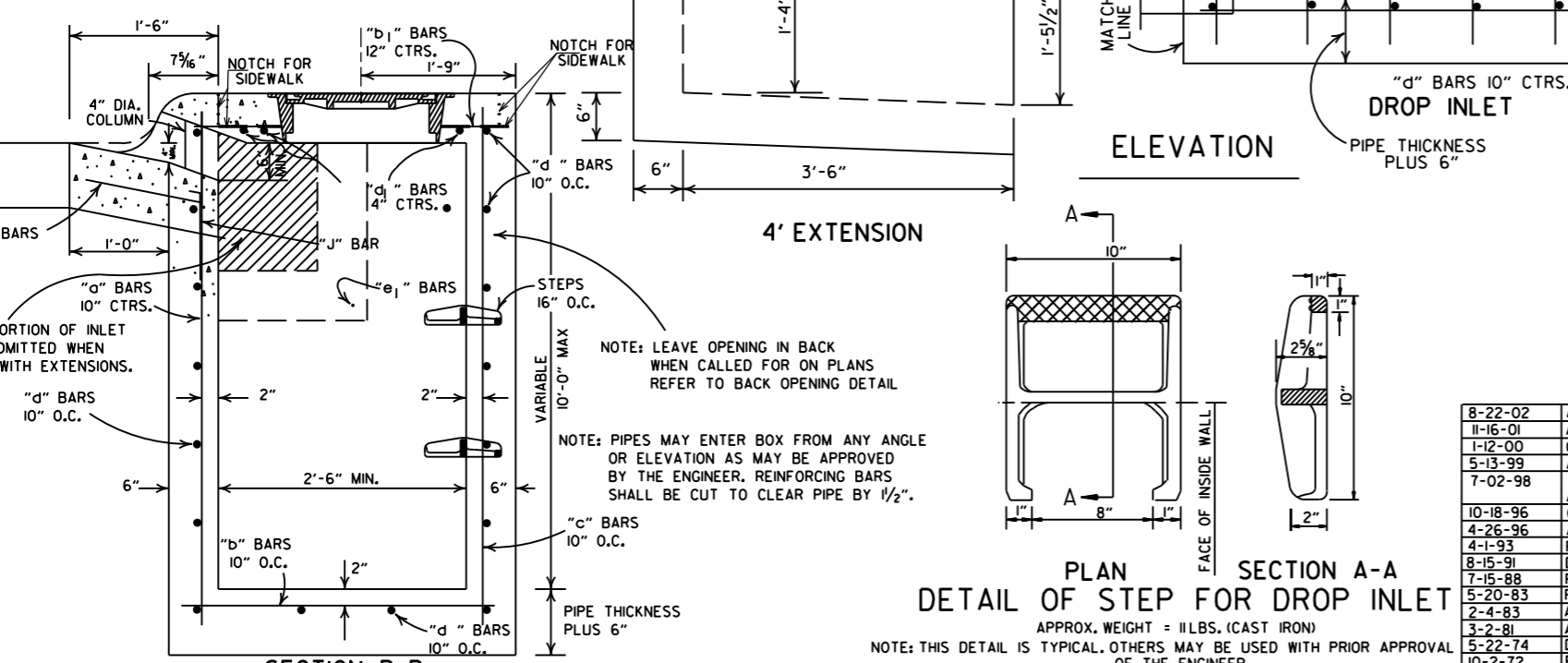
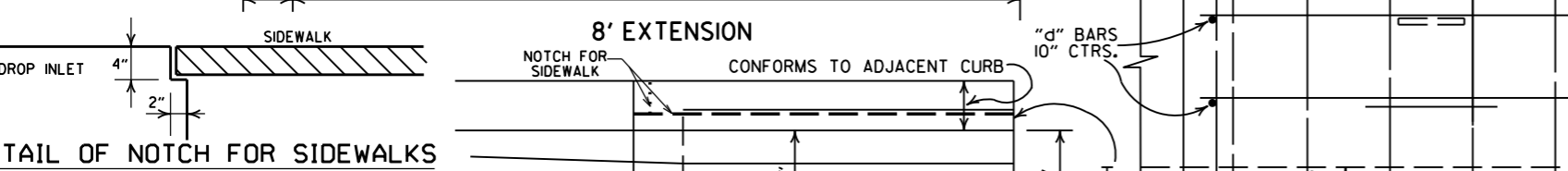
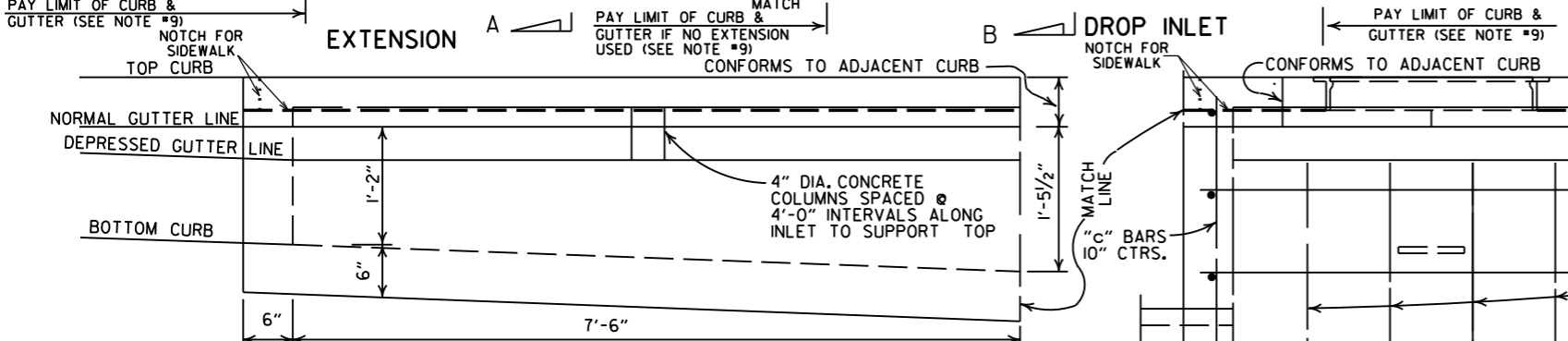
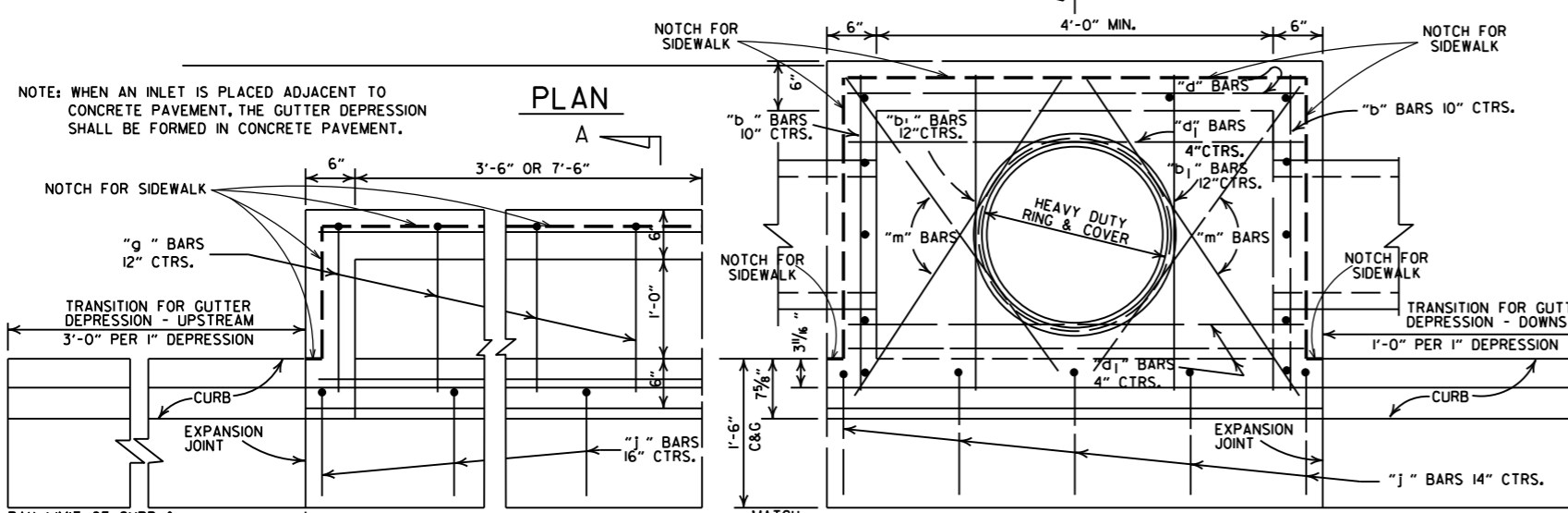
BACK OPENING

WHEN OPENING IN BACK IS CALLED FOR ON PLANS EXTEND OPENING AS SHOWN IN DETAIL. PAYMENT TO BE INCLUDED IN PRICE BID FOR DROP INLET (TYPE C).



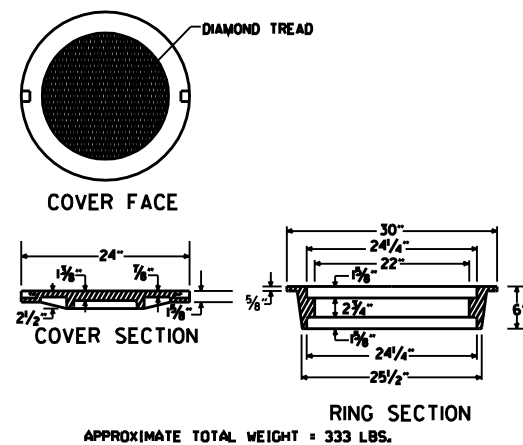
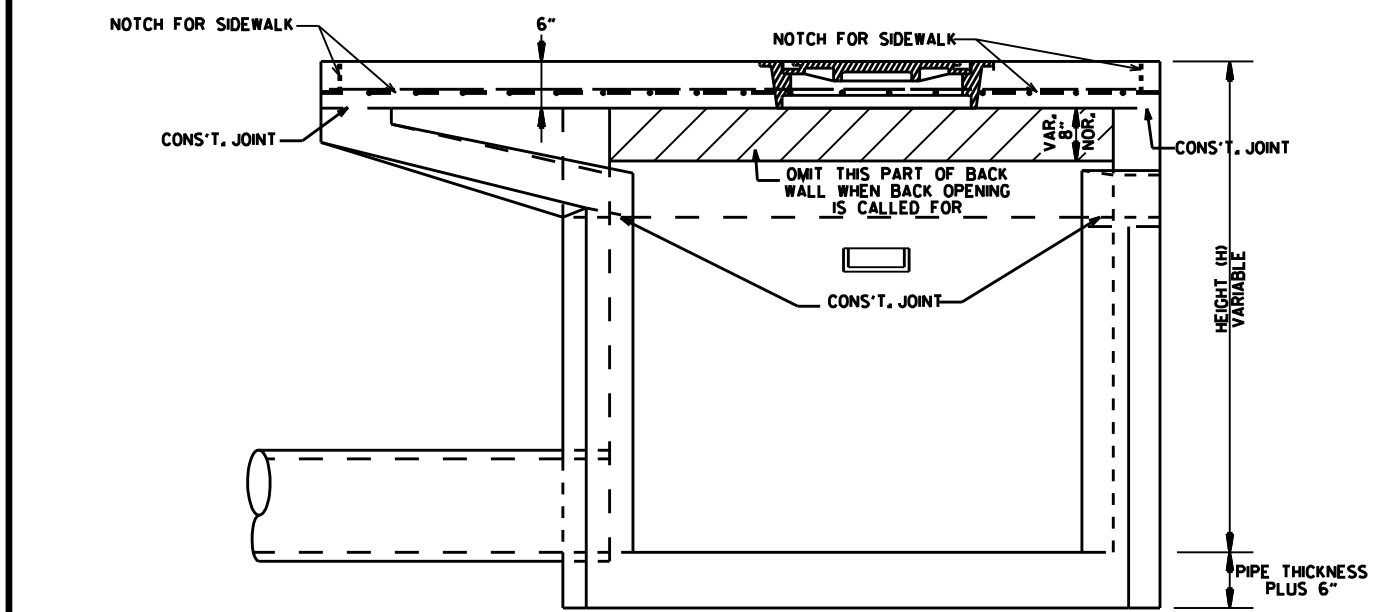
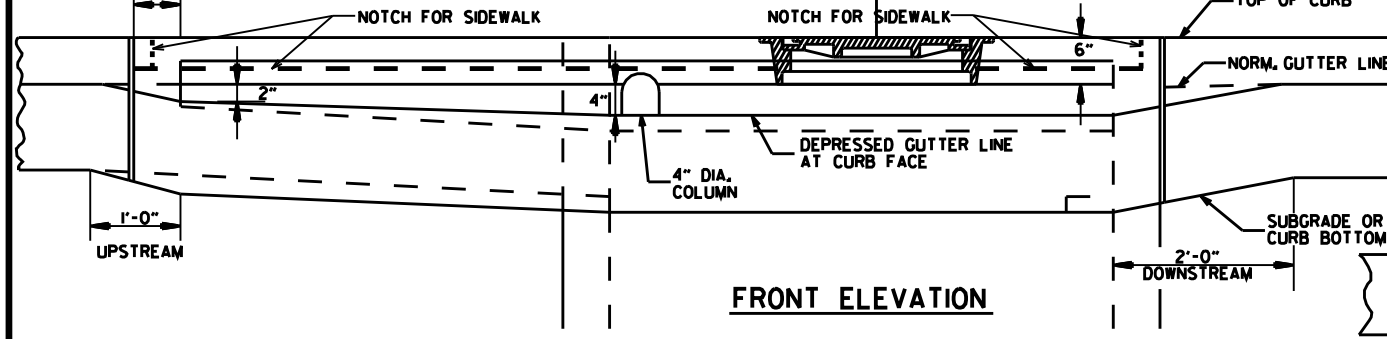
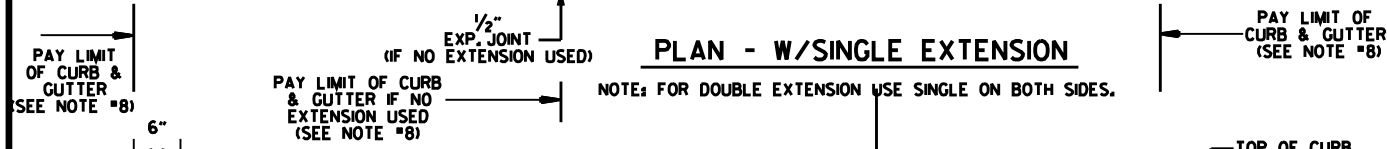
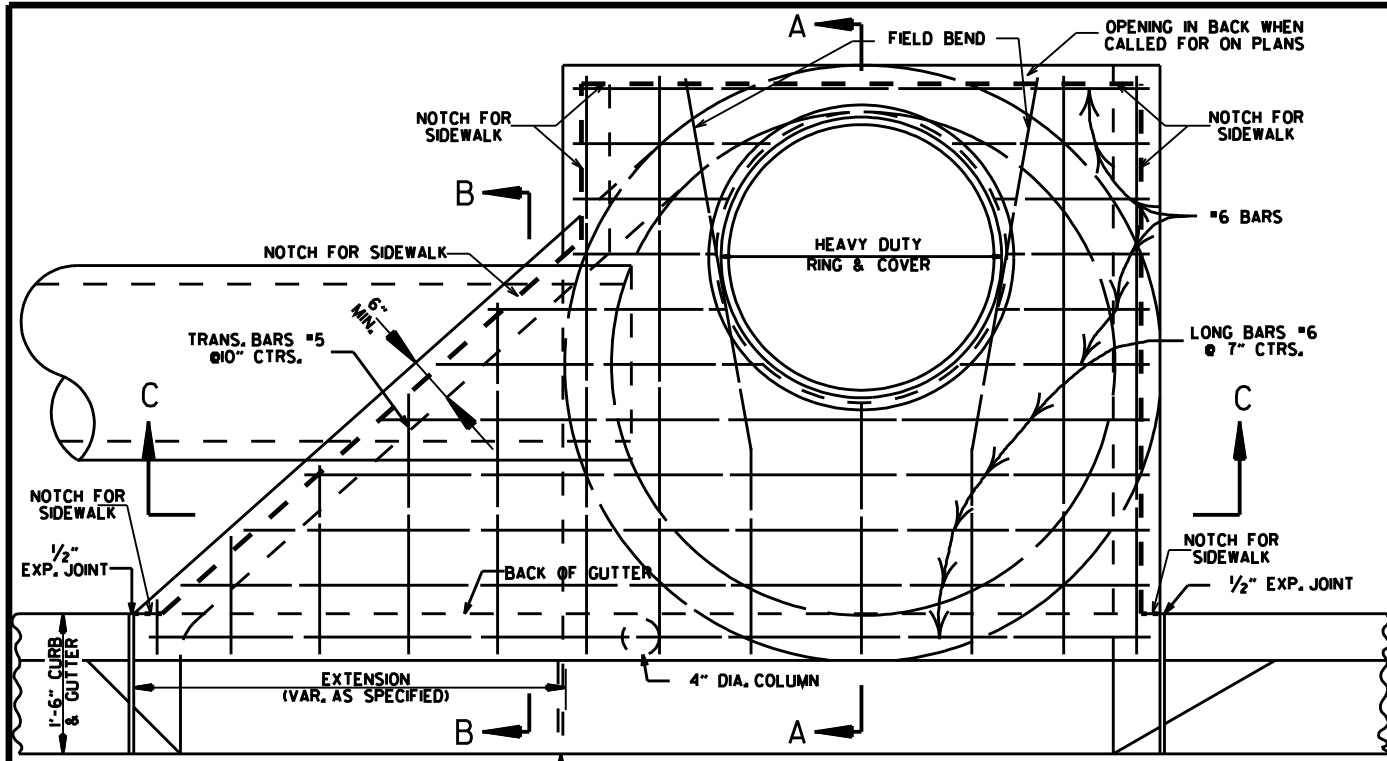
HEAVY DUTY RING & COVER

- APPROXIMATE TOTAL WEIGHT = 333 LBS.
- GENERAL NOTES:
- ALL EXPOSED CORNERS TO HAVE 3/4" CHAMFER.
 - STEPS SHALL BE INSTALLED IN ALL INLETS 4'-0" HIGH AND OVER OF AS APPROVED BY THE ENGINEER.
 - ALL REINF. BARS SHALL BE #4 AND HAVE 1/2" COVER.
 - DROP INLETS AND EXTENSION ON CURVED SECTIONS SHALL CONFORM TO THE CURVATURE OF THE CURB.
 - THIS DROP INLET MAY BE CONSTRUCTED ON NEW OR EXISTING R.C. BOX CULVERT AS SHOWN ON F.P.C.-9.
 - WHEN PLANS CALL FOR DROP INLET OVER 10'-0" HIGH, FLOOR AND WALLS SHALL BE CONSTRUCTED AS SHOWN FOR TYPE "RM" DROP INLET (FPC-9D).
 - HEAVY DUTY RING SHALL ALWAYS BE INSTALLED WITH FLANGE ON TOP.
 - DURING CONSTRUCTION OF THE ROADWAY THE CONTRACTOR SHALL MAINTAIN DRAINAGE INTO OR AROUND THE DROP INLET AS APPROVED BY THE ENGINEER.
 - PAYMENT FOR CURB AND/OR CURB AND GUTTER WITHIN THE LIMITS OF DROP INLETS AND DROP INLET EXTENSIONS SHALL BE CONSIDERED INCLUDED IN PAYMENT MADE FOR DROP INLETS AND/OR DROP INLET EXTENSIONS.
 - HEAVY DUTY RING AND COVER SHALL BE CONSTRUCTED OF CAST IRON AND SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR GRAY IRON CASTINGS AASHTO M105 CLASS 35B & AASHTO M306.
 - HEAVY DUTY RING AND COVER SHALL NOT BE PAINTED.
 - 4"x2" NOTCH SHALL BE FORMED IN ALL DROP INLETS TO SUPPORT SIDEWALK CONSTRUCTION. REFER TO DETAIL OF NOTCH FOR SIDEWALKS.
 - DIMENSIONS SHOWN FOR RING AND COVER ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR CASTINGS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR CASTING DESIGNS MAY BE MADE BY REFERRING TO PREVIOUSLY APPROVED DRAWINGS.



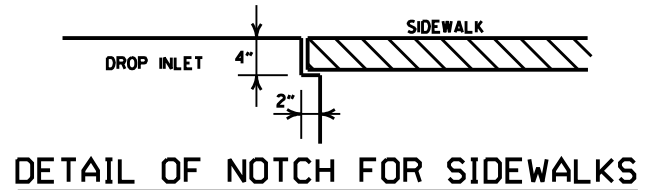
DATE	REV.	REVISION	DATE FILMED
8-22-02		ADDED PAY LIMIT CURB NOTES TO SECTIONS A-A & B-B	
11-16-01		ADDED NOTE 13; REVISED SECTION B-B	
1-12-00		CORRECTED DIMENSION ON SECTION B-B & REVISED RING & COVER	
5-13-99		ADDED DETAIL OF NOTCH FOR SIDEWALKS	
7-02-98		REPLACED RING & COVER W/HEAVY DUTY RING & COVER	
10-18-96		ADDED NOTES 9,10,&11	
4-26-96		CORRECTED SPELLING	
4-1-95		ADDED NOTE 8 & REVISED (4'x8') EXTENSION TITLES	10-18-96
8-15-91		REVISED BACK OPENING & NOTE	
7-15-88		DELETE TYPE IV GRATE	
5-20-83		REVISED STEP DETAIL	
2-4-83		REVISED DETAILS OF GRATES (TYPE IV & IV-A)	
3-2-81		ADDED GENERAL NOTE NO. 4	
10-2-72		ADDED TYPE IV-A GRATE	
		DELETED INLET (TYPE F) & GRATE (TYPE III)	
		REVISED AND REDRAWN	

ARKANSAS STATE HIGHWAY COMMISSION
 DETAILS OF DROP INLETS
 (TYPE C)
 STANDARD DRAWING FPC-9E

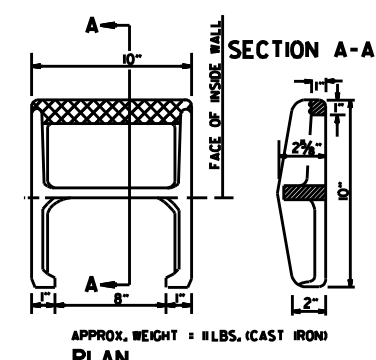


HEAVY DUTY RING & COVER

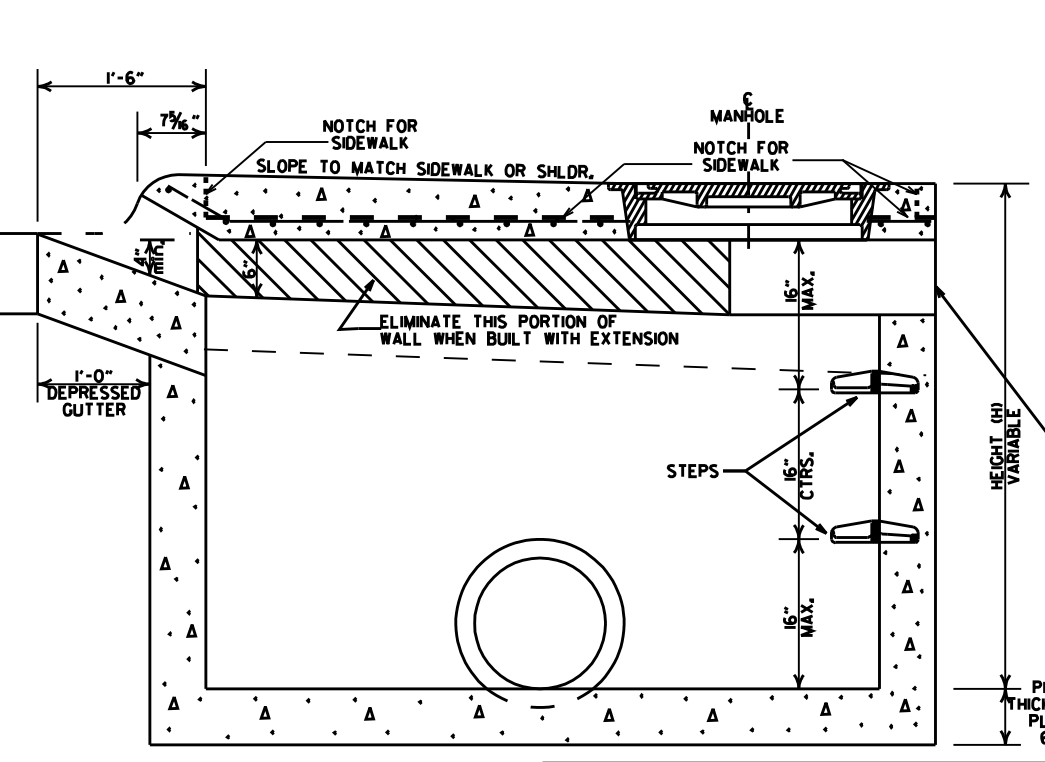
1. HEAVY DUTY RING AND COVER SHALL BE CONSTRUCTED OF CAST IRON AND SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR GRAY IRON CASTINGS AASHTO M105 CLASS 35B & AASHTO M306.
2. HEAVY DUTY RING AND COVER SHALL NOT BE PAINTED.
3. HEAVY DUTY RING SHALL ALWAYS BE INSTALLED WITH FLANGE ON TOP.



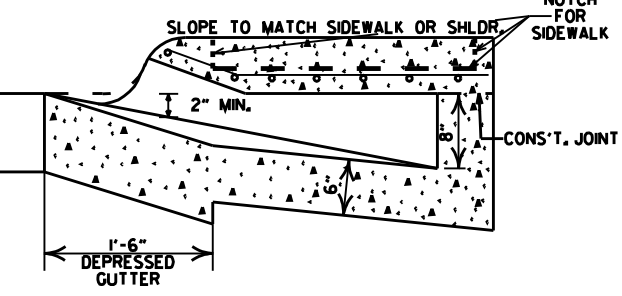
DETAIL OF NOTCH FOR SIDEWALKS



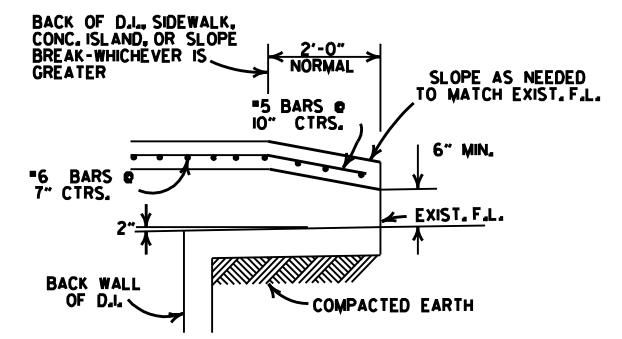
DETAIL OF STEP FOR DROP INLET



SECTION A-A



SECTION B-B



BACK OPENING

WHEN OPENING IN BACK IS CALLED FOR ON PLANS EXTEND OPENING AS SHOWN IN DETAIL. PAYMENT TO BE INCLUDED IN PRICE BID FOR DROP INLET (TYPE MO).

- GENERAL NOTES:
1. ALL EXPOSED CORNERS TO HAVE 3/4" CHAMFER.
 2. STEPS SHALL BE INSTALLED IN ALL INLETS 4'-0" HIGH AND OVER OR AS DIRECTED BY THE ENGINEER.
 3. ALL REINFORCING BARS SHALL BE GRADE 60 AND HAVE MIN. 1/2" COVER.
 4. DROP INLETS AND EXTENSION ON CURVED SECTIONS SHALL CONFORM TO THE CURVATURE OF THE CURB.
 5. 4" DIA. COLUMNS SPACED AT MAX. 4'-0" INTERVALS SHALL BE INSTALLED ALONG INLET AND EXTENSION TO SUPPORT TOP.
 6. BASE AND INLET WALLS SHALL BE CAST MONOLITHICALLY.
 7. THE THROAT SHALL BE CAST INTEGRALLY WITH THE GUTTER.
 8. PAYMENT FOR CURB AND/OR CURB AND GUTTER WITHIN THE LIMITS OF DROP INLETS AND DROP INLET EXTENSIONS SHALL BE CONSIDERED INCLUDED IN PAYMENT MADE FOR DROP INLETS AND/OR DROP INLET EXTENSIONS.
 9. PIPES MAY ENTER DROP INLET FROM ANY ANGLE OR ELEVATION AS MAY BE APPROVED BY THE ENGINEER.
 10. APPROPRIATE SIZE TYPE C DROP INLETS MAY BE SUBSTITUTED FOR TYPE MO DROP INLETS AS APPROVED BY THE ENGINEER. PAYMENT TO BE AS DROP INLET (TYPE MO).
 11. DURING CONSTRUCTION OF THE ROADWAY THE CONTRACTOR SHALL MAINTAIN DRAINAGE INTO OR AROUND THE DROP INLET AS APPROVED BY THE ENGINEER.
 12. 4"x2" NOTCH SHALL BE FORMED IN ALL DROP INLETS TO SUPPORT SIDEWALK CONSTRUCTION. REFER TO DETAIL OF NOTCH FOR SIDEWALKS.
 13. DIMENSIONS SHOWN FOR RING AND COVER ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR CASTINGS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR CASTING DESIGNS MAY BE MADE BY REFERRING TO PREVIOUSLY APPROVED DRAWINGS.

LEAVE OPENING IN BACK WHEN CALLED FOR ON PLANS REFER TO BACK OPENING DETAIL

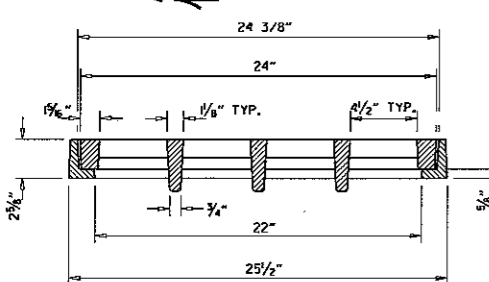
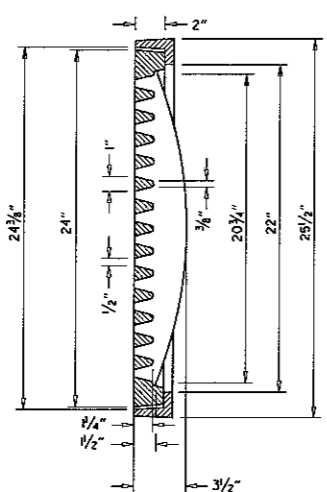
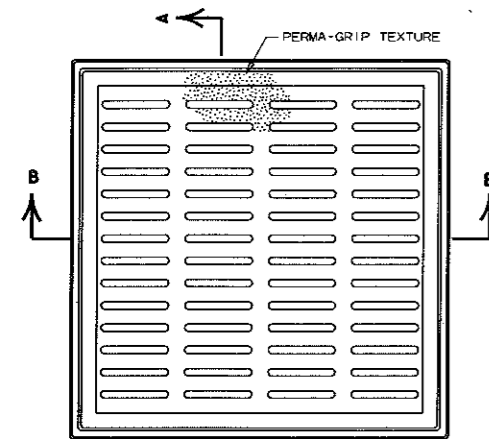
MINIMUM WALL THICKNESS			
DIA. OF D.I.	DIA. OF OUTLET PIPE	CAST IN PLACE	PRECAST
4" LD.	12" THRU 27"	6"	5"
5" LD.	30" THRU 42"	8"	6"
6" LD.	48" THRU 54"	8"	7"

DATE	REVISIONS	DATE FILED
11-22-02	ADDED PAY LIMIT CURB NOTES TO SECTIONS A-A & B-B	
11-16-01	ADDED NOTE 13	
11-12-00	REVISED HEAVY DUTY RING & COVER	
5-13-99	ADDED NOTCH DETAIL FOR SIDEWALKS	
7-02-98	REP. NOTE B, RING PLAN DET., REV. PICTURE FOR COVER AND DETAIL OF STEP FOR DROP INLET	
10-12-96	ADDED NOTE 11 BY ADD. OPENING DIMENSION	
11-20-95	CORRECTED #5 BAR SPACING	
12-2-95	CORRECTED DIAMETER OF D.I. IN BOX	
12-2-95	TYPE C TO MO (OPEN BACK DETAIL)	
11-15-94	REVISED GENERAL NOTES	11-15-94
11-15-94	REVISED DETAIL A NOTE	11-15-94
11-15-94	REVISED NOTES 11/2 A ADDED BACK OPEN DETAIL	11-15-94
11-15-94	ADDED NOTE NO. 12	11-15-94
11-15-94	ADDED NOTE A MINIMUM WALL THICKNESS	11-15-94
11-15-94	ADDED EXTENT NOTE TO SECTION A-A	11-15-94
11-15-94	MODIFIED WALL THICKNESS	11-15-94
11-15-94	ISSUED	11-15-94

ARKANSAS STATE HIGHWAY COMMISSION

DETAILS OF DROP INLET (TYPE MO)

STANDARD DRAWING FPC-9M 182

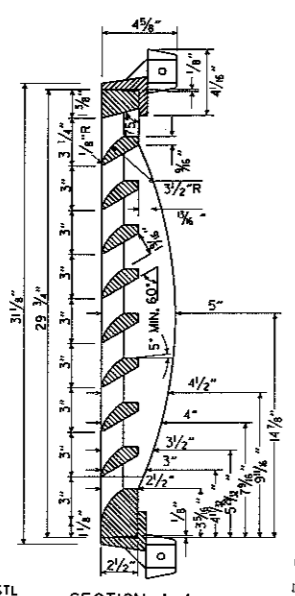
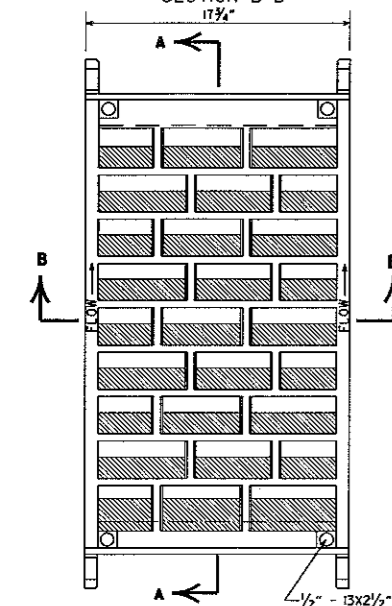
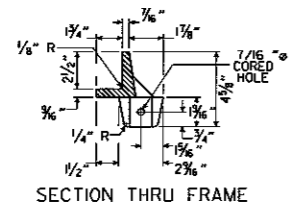
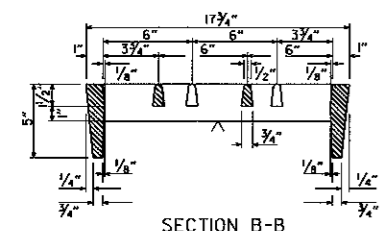


SECTION A-A

GENERAL NOTES (PEDESTRIAN GRATE & FRAME)

1. THE PEDESTRIAN GRATE SHALL BE ORIENTED IN THE TOP OF THE DROP INLET SO THAT THE 1/2" OPENINGS ARE PERPENDICULAR TO THE PATH OF PEDESTRIAN TRAVEL.
2. THE PEDESTRIAN GRATE AND FRAME SHALL BE CONSTRUCTED OF CAST IRON AND SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR GRAY IRON CASTINGS AASHTO M 105, CLASS 35B, & AASHTO M 306.
3. THE GRATE AND FRAME SHALL NOT BE PAINTED.
4. THE GRATE AND FRAME SHALL BE INSTALLED IN THE DROP INLET IN THE ASSEMBLED POSITION.
5. THE APPROXIMATE WEIGHT OF THE GRATE AND FRAME SHALL BE 21 LBS.
6. THE MINIMUM WATERWAY OPENING SHALL BE 122 SQ. IN.

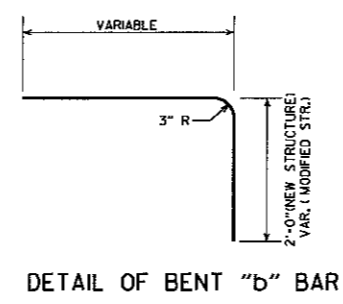
SECTION B-B
DETAILS OF PEDESTRIAN GRATE AND FRAME



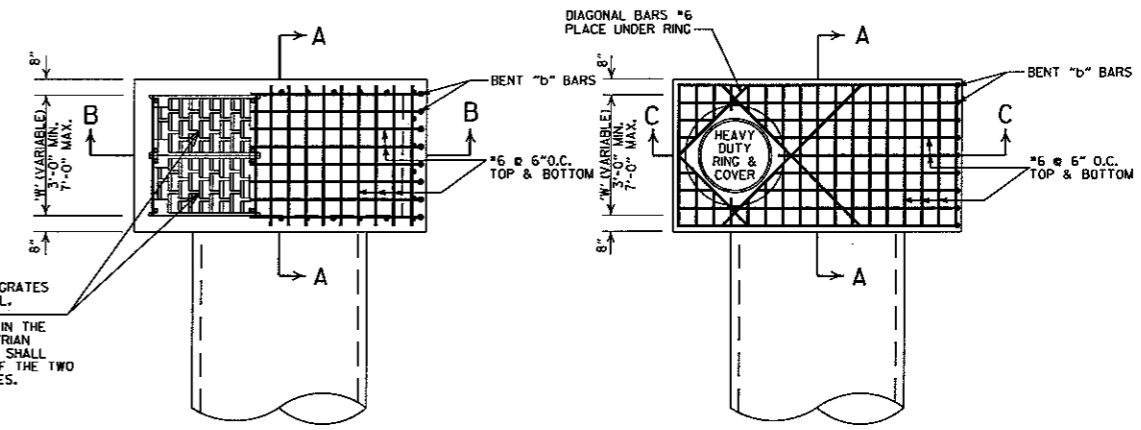
GENERAL NOTES (RIBBED VANE GRATE & FRAME)

1. RIBBED VANE GRATE AND FRAME SHALL BE CONSTRUCTED OF CAST IRON AND SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR GRAY IRON CASTINGS AASHTO M 105, CLASS 35B, & AASHTO M 306.
2. GRATE AND FRAME SHALL NOT BE PAINTED.
3. GRATE AND FRAME SHALL BE INSTALLED IN DROP INLET IN ASSEMBLED POSITION.
4. APPROXIMATE WEIGHT OF GRATE SHALL BE 170 LBS.

SECTION A-A
DETAILS OF RIBBED VANE GRATE AND FRAME



DETAIL OF BENT "b" BAR



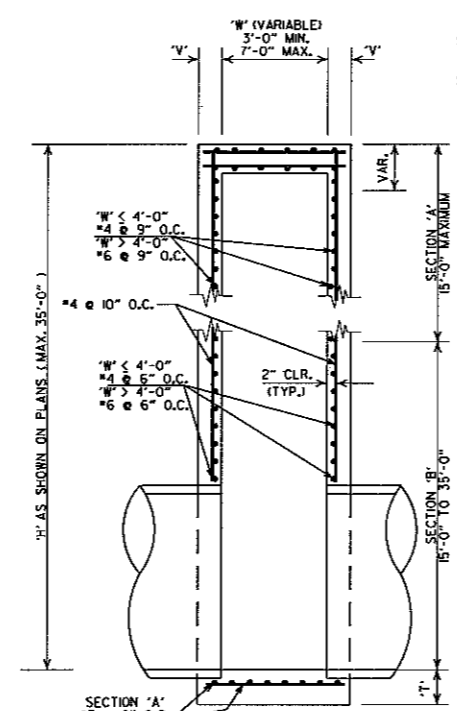
TWO RIBBED VANE GRATES WITH FRAME NORMAL.

WHEN CALLED FOR IN THE PLANS, ONE PEDESTRIAN GRATE WITH FRAME SHALL BE USED IN LIEU OF THE TWO RIBBED VANE GRATES.

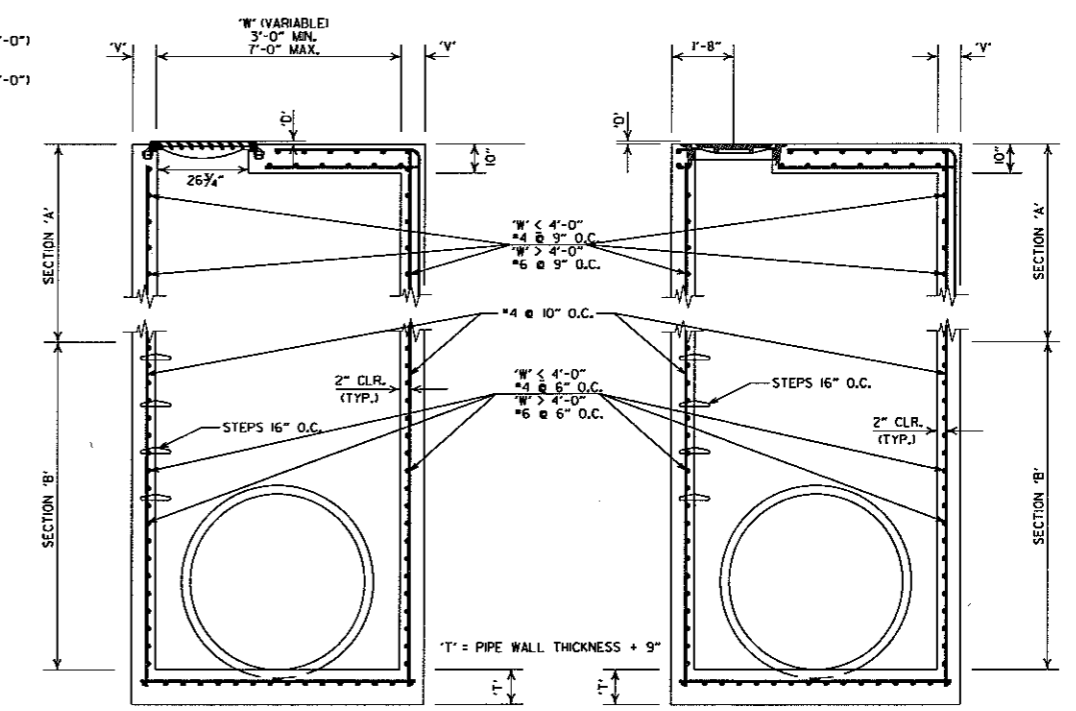
SECTION 'A'
"V" = 8"

SECTION 'B' (W < 4'-0")
"V" = 8"

SECTION 'B' (W > 4'-0")
"V" = 10"



SECTION A-A
DETAILS OF DROP INLET (TYPE ST)



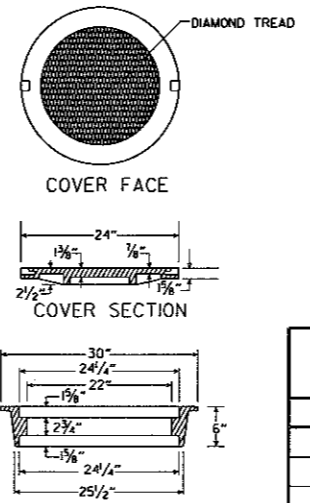
SECTION B-B
SECTION C-C
DETAILS OF JUNCTION BOX (TYPE ST)

GENERAL NOTES (TYPE ST DROP INLET & JUNCTION BOX)

1. THE 'D' DIMENSION SHALL MATCH THE FINAL LIFT OF ACQU SURFACE COURSE SHOWN IN THE PLANS WHEN ASPHALT PAVING SURROUNDS THE GRATE OR RING COVER, AND SHALL BE 0" AT OTHER INSTALLATIONS.
2. THE STEPS SHALL BE OMITTED WHERE 'H' IS LESS THAN 4'-0".
3. ALL EXPOSED CORNERS ARE TO HAVE A 3/4" CHAMFER.

GENERAL NOTES (HEAVY DUTY RING & COVER):

1. HEAVY DUTY RING AND COVER SHALL BE CONSTRUCTED OF CAST IRON AND SHALL CONFORM TO THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR GRAY IRON CASTINGS AASHTO M 105, CLASS 35B, & AASHTO M 306.
2. HEAVY DUTY RING AND COVER SHALL NOT BE PAINTED.
3. HEAVY DUTY RING SHALL ALWAYS BE INSTALLED WITH FLANGE ON TOP.
4. DIMENSIONS SHOWN FOR RING AND COVER ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR CASTINGS WITH THE APPROVAL OF THE ENGINEER, REQUESTING APPROVAL FOR CASTING DESIGNS MAY BE MADE BY REFERRING TO PREVIOUSLY APPROVED DRAWINGS.



RING SECTION
HEAVY DUTY RING & COVER
APPROXIMATE TOTAL WEIGHT = 333 LBS.

DATE REVISED	DATE FILMED	DESCRIPTION
7-26-12		REMOVED NOTE 4, REVISED 'T', REVISED BOTTOM SLAB REBAR FOR SECTION 'A', SHOWED REBAR CLEARANCE IN SECTIONS
11-16-01		ADDED NOTE 4
1-12-00		REVISED HEAVY DUTY RING & COVER
5-13-99		ADDED PEDESTRIAN FRAME & GRATE
7-02-98		REMOVED NOTE 5, REV. DIMENSIONS, ADDED HEAVY DUTY RING & COVER, ADDED AASHTO REF, REVISED GRATE
10-18-96		REVISED ASTM REF. TO AASHTO
10-1-92		REVISED & REISSUED
8-15-91	8-15-91	REVISED & REISSUED

ARKANSAS STATE HIGHWAY COMMISSION
DETAILS OF DROP INLET & JUNCTION BOX (TYPE ST)
STANDARD DRAWING FPC-95

REINFORCED CONCRETE ARCH PIPE DIMENSIONS

EQUIV. DIA.	SPAN		RISE	
	AASHTO M 206	ARDDOT NOMINAL	AASHTO M 206	ARDDOT NOMINAL
INCHES	INCHES			
15	18	18	11	11
18	22	22	13½	14
21	26	26	15½	16
24	28½	29	18	18
30	36¼	36	22½	23
36	43¾	44	26¾	27
42	51½	51	31¾	31
48	58½	59	36	36
54	65	65	40	40
60	73	73	45	45
72	88	88	54	54
84	102	102	62	62
90	115	115	72	72
96	122	122	77½	77
108	138	138	87½	87
120	154	154	96¾	97
132	168¾	169	106½	107

THE MEASURED SPAN AND RISE SHALL NOT VARY MORE THAN ± 2 PERCENT FROM THE VALUES SPECIFIED BY AASHTO M206.

REINFORCED CONCRETE HORIZONTAL ELLIPTICAL PIPE DIMENSIONS

EQUIV. DIA.	AASHTO M 207	
	SPAN	RISE
INCHES	INCHES	
18	23	14
24	30	19
27	34	22
30	38	24
33	42	27
36	45	29
39	49	32
42	53	34
48	60	38
54	68	43
60	76	48
66	83	53
72	91	58
78	98	63
84	106	68

THE MEASURED SPAN AND RISE SHALL NOT VARY MORE THAN ± 2 PERCENT FROM THE VALUES SPECIFIED BY AASHTO M207.

CONSTRUCTION SEQUENCE

1. PLACE STRUCTURAL BEDDING MATERIAL TO GRADE. DO NOT COMPACT.
2. INSTALL PIPE TO GRADE.
3. COMPACT STRUCTURAL BEDDING OUTSIDE THE MIDDLE THIRD OF THE PIPE.
4. PLACE AND COMPACT THE HAUNCH AREA UP TO THE MIDDLE OF THE PIPE.
5. COMPLETE BACKFILL ACCORDING TO SUBSECTION 606.03.(f)(1).

NOTE: HAUNCH AND STRUCTURAL BEDDING MATERIAL WILL NOT BE PAID FOR SEPARATELY, BUT COMPENSATION WILL BE CONSIDERED TO BE INCLUDED IN THE PRICE BID PER LINEAR FOOT OF CONCRETE PIPE.

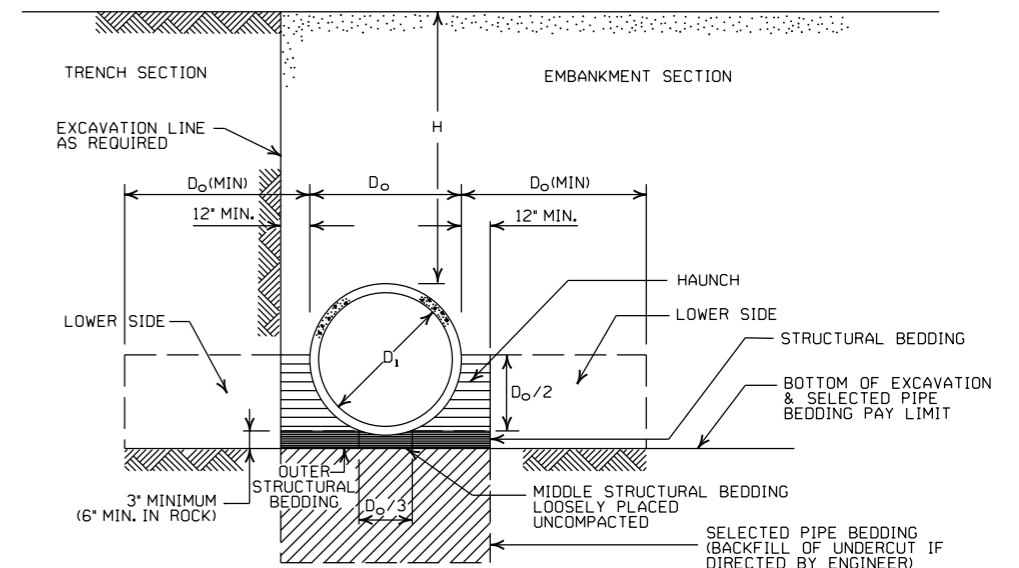
- LEGEND -

- D_i = NORMAL INSIDE DIAMETER OF PIPE
- D_o = OUTSIDE DIAMETER OF PIPE
- H = FILL COVER HEIGHT OVER PIPE (FEET)
- MIN. = MINIMUM
- UNDISTURBED SOIL

INSTALLATION TYPE	MATERIAL REQUIREMENTS FOR HAUNCH AND STRUCTURAL BEDDING
TYPE 1	AGGREGATE BASE COURSE (CLASS 5 OR CLASS 7)
TYPE 2	SELECTED MATERIALS (CLASS SM-1, SM-2, OR SM-4) OR TYPE 1 INSTALLATION MATERIAL*
TYPE 3**	AASHTO CLASSIFICATION A-1 THRU A-6 SOIL OR TYPE 1 OR 2 INSTALLATION MATERIAL

* SM-3 WILL NOT BE ALLOWED.

** MATERIALS SHALL NOT INCLUDE ORGANIC MATERIALS OR STONES LARGER THAN 3 INCHES.



EMBANKMENT AND TRENCH INSTALLATIONS

1. MATERIAL IN THE HAUNCH AND OUTER STRUCTURAL BEDDING SHALL BE COMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OR CLASS OF MATERIAL USED.
2. FOR TRENCHES WITH WALLS OF NATURAL SOIL, THE DENSITY OF THE SOIL IN THE LOWER SIDE ZONE SHALL BE AS FIRM AS THE 95% DENSITY REQUIRED FOR THE HAUNCH. IF THE EXISTING SOIL DOES NOT MEET THIS CRITERIA, IT SHALL BE REMOVED AND RECOMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OF MATERIAL USED.
3. FOR EMBANKMENTS, THE MATERIAL IN THE LOWER SIDE ZONE SHALL BE COMPACTED TO 95% OF THE MAXIMUM DENSITY ACCORDING TO THE TYPE OR CLASS OF MATERIAL USED.

GENERAL NOTES

1. CONCRETE PIPE CULVERT CONSTRUCTION SHALL CONFORM TO ARKANSAS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION), WITH APPLICABLE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS. UNLESS OTHERWISE NOTED IN THE PLANS, SECTION AND SUBSECTION REFER TO THE STANDARD CONSTRUCTION SPECIFICATIONS.
2. CONCRETE PIPE CULVERT DESIGN SHALL CONFORM TO AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, FIFTH EDITION (2010) WITH 2010 INTERIMS.
3. ALL PIPE SHALL CONFORM TO SECTION 606. CIRCULAR R.C. PIPE CULVERTS SHALL CONFORM TO AASHTO M170. R.C. ARCH PIPE CULVERTS SHALL CONFORM TO AASHTO M206 AND HORIZONTAL ELLIPTICAL PIPE CULVERTS SHALL CONFORM TO AASHTO M207.
4. ALL PIPE SHALL BE PROTECTED DURING CONSTRUCTION BY A COVER SUFFICIENT TO PREVENT DAMAGE FROM PASSAGE OF EQUIPMENT.
5. THE MINIMUM TRENCH WIDTH SHALL BE THE OUTSIDE DIAMETER OF THE PIPE PLUS 24 INCHES. THE MAXIMUM ALLOWABLE TRENCH WIDTH SHALL BE THE MINIMUM WIDTH PRACTICABLE FOR WORKING CONDITIONS.
6. MULTIPLE PIPE CULVERTS SHALL BE INSTALLED WITH A MINIMUM CLEARANCE OF 24 INCHES BETWEEN STRINGS OF PIPE. REFER TO STD. DWG. FES-2 FOR MINIMUM CLEARANCE WHERE FLARED END SECTIONS ARE USED.
7. IMPERVIOUS MATERIAL SHOULD BE PLACED AS DIRECTED BY THE ENGINEER AT THE ENDS OF THE CULVERT TO PREVENT LOSS OF STRUCTURAL BEDDING WHEN PERVIOUS MATERIAL IS USED FOR STRUCTURAL BEDDING AND/OR BACKFILL.
8. NOT MORE THAN ONE LIFTING HOLE MAY BE PROVIDED IN CONCRETE PIPE TO FACILITATE HANDLING. HOLE MAY BE CAST IN PLACE, CUT INTO THE FRESH CONCRETE AFTER FORMS ARE REMOVED, OR DRILLED. THE HOLE SHALL NOT BE MORE THAN TWO INCHES IN DIAMETER OR TWO INCHES SQUARE. CUTTING OR DISPLACEMENT OF REINFORCEMENT WILL NOT BE PERMITTED. SPALLED AREAS AROUND THE HOLE SHALL BE REPAIRED IN A WORKMANLIKE MANNER. LIFTING HOLE SHALL BE FILLED WITH MORTAR, CONCRETE, OR OTHER METHOD AS APPROVED BY THE ENGINEER.
9. WHEN DIRECTED BY THE ENGINEER, UNSUITABLE MATERIAL THAT IS ENCOUNTERED AT THE BOTTOM OF THE EXCAVATED TRENCH (BELOW THE AREA IDENTIFIED AS "STRUCTURAL BEDDING" ABOVE) WILL BE EXCAVATED AND REPLACED WITH SELECTED PIPE BEDDING. THE QUANTITY OF MATERIAL REQUIRED TO BACKFILL THE UNDERCUT AREA UP TO THE SELECTED PIPE BEDDING PAY LIMIT DESIGNATED ABOVE WILL BE MEASURED AND PAID FOR AS "SELECTED PIPE BEDDING."
10. WHEN THE EXISTING MATERIAL EXCAVATED FOR THE PIPE TRENCH IS DETERMINED BY THE ENGINEER TO BE UNSUITABLE FOR BACKFILLING THE PIPE (ABOVE THE AREA IDENTIFIED ABOVE AS THE HAUNCH), BORROW MATERIAL OR MATERIAL FROM THE ROADWAY EXCAVATION WILL BE USED TO BACKFILL THE PIPE. IF SUITABLE MATERIAL IS NOT AVAILABLE, THE ENGINEER MAY AUTHORIZE THE USE OF "SELECTED PIPE BACKFILL."

MINIMUM HEIGHT OF FILL "H" OVER CIRCULAR R.C. PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE			
	CLASS III	CLASS IV	CLASS V	CLASS V
PIPE ID (IN.)	FEET			
12-15	2	2.5	2	1
18-24	2.5	3	2	1
27-33	3	4	2	1
36-42	3.5	5	2	1
48	4.5	5.5	2	1
54-60	5	7	2	1
66-78	6	8	2	1
84-108	7.5	8	2	1

NOTE: FOR MINIMUM COVER VALUES, "H" SHALL INCLUDE A MINIMUM OF 12" OF PAVEMENT AND/OR BASE.

MAXIMUM HEIGHT OF FILL "H" OVER CIRCULAR R.C. PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE		
	CLASS III	CLASS IV	CLASS V
TYPE 1	21	32	50
TYPE 2	16	25	39
TYPE 3	12	20	30

NOTE: IF FILL HEIGHT EXCEEDS 50 FEET, A SPECIAL DESIGN CONCRETE PIPE WILL BE REQUIRED USING TYPE 1 INSTALLATION.

MINIMUM HEIGHT OF FILL "H" OVER R.C. ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE	
	CLASS III	CLASS IV
TYPE 2 OR TYPE 3	FEET	
	2.5	1.5

NOTE: TYPE 1 INSTALLATION WILL NOT BE ALLOWED FOR ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS.

NOTE: FOR MINIMUM COVER VALUES, "H" SHALL INCLUDE A MINIMUM OF 12" OF PAVEMENT AND/OR BASE.

MAXIMUM HEIGHT OF FILL "H" OVER R.C. ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS

INSTALLATION TYPE	CLASS OF PIPE	
	CLASS III	CLASS IV
TYPE 2	13	21
TYPE 3	10	16

NOTE: TYPE 1 INSTALLATION WILL NOT BE ALLOWED FOR ARCH & HORIZONTAL ELLIPTICAL PIPE CULVERTS.

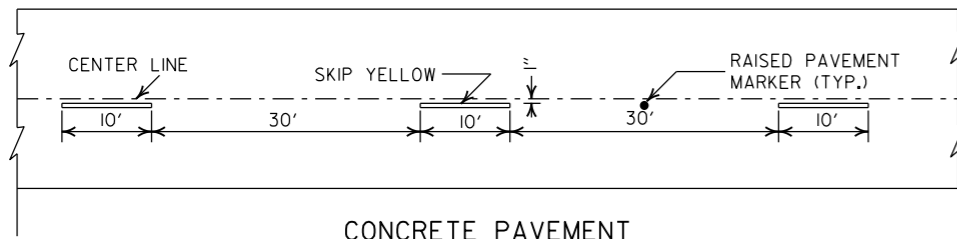
DATE	REVISION	DATE FILMED
2-27-14	REVISED GENERAL NOTE 1.	
12-15-11	REVISED FOR LRFD DESIGN SPECIFICATIONS	
5-18-00	REVISED TYPE 3 BEDDING & ADDED NOTE	
3-30-00	REVISED INSTALLATIONS	
11-06-97	ISSUED	

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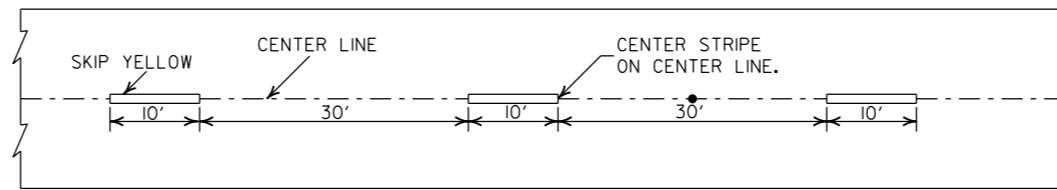
CONCRETE PIPE CULVERT FILL HEIGHTS & BEDDING

STANDARD DRAWING PCC-1



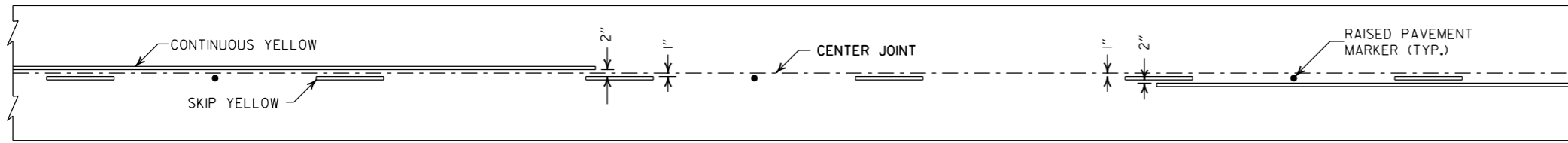


CONCRETE PAVEMENT

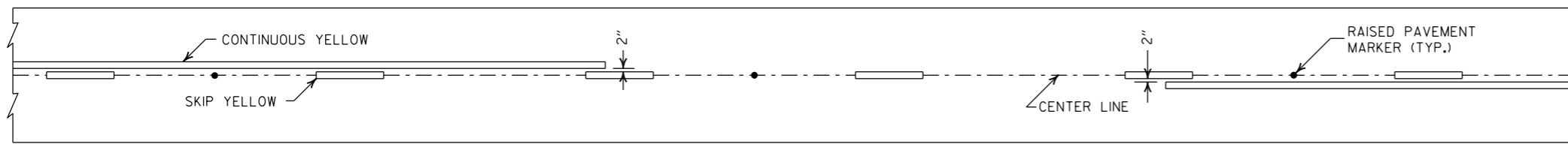


ASPHALT PAVEMENT

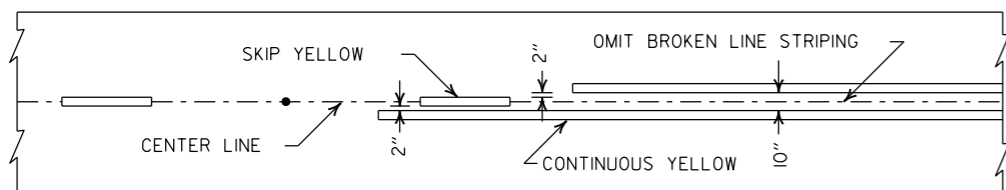
BROKEN LINE STRIPING



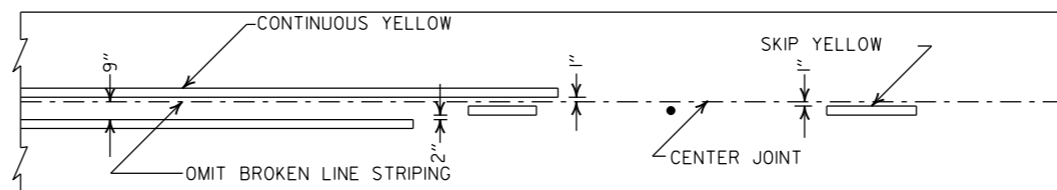
SOLID LINE STRIPING ON CONCRETE PAVEMENT



SOLID LINE STRIPING ON ASPHALT PAVEMENT

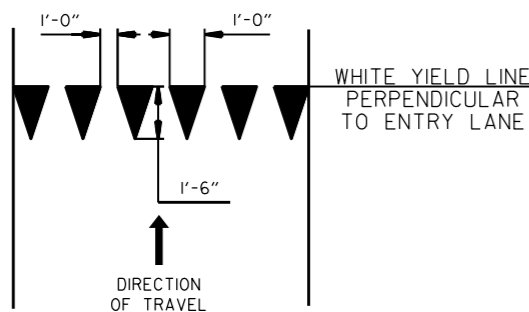


ASPHALT PAVEMENT

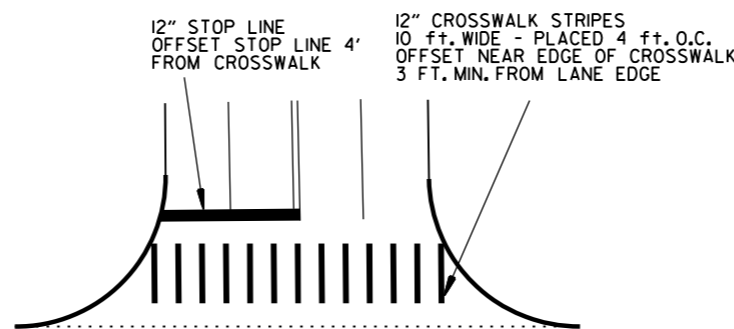


CONCRETE PAVEMENT

STRIPING AT ADJACENT NO PASSING LANES



YIELD LINE DETAIL

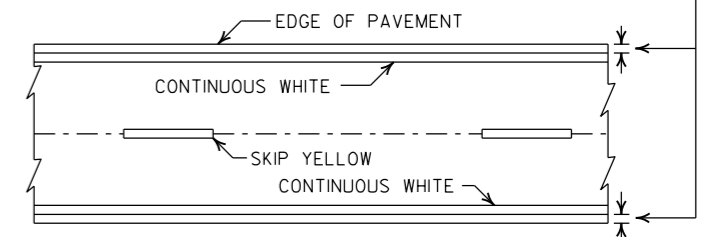


CROSSWALK AND STOP LINE DETAILS

NOTES:

1. REFER TO THE STRIPING DETAILS FOR PAVEMENT MARKING LINE WIDTHS.
2. THIS DRAWING SHALL BE USED IN CONJUNCTION WITH THE LATEST REVISED ADDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES."
3. RAISED PAVEMENT MARKERS SHALL BE PLACED ON AN 80 FEET SPACING UNLESS OTHERWISE SHOWN IN THE PLANS.

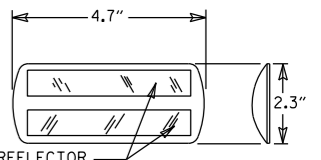
2" FOR ASPHALT OR CONCRETE PAVEMENT
6" FOR BITUMINOUS SURFACE TREATMENT



PAVEMENT EDGE LINE MARKING

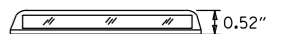
NOTE:
THE RED LENS OF THE TYPE II R.P.M. SHALL FACE THE INCORRECT TRAFFIC MOVEMENT.

TYPE II
RED/CLEAR OR
YELLOW/YELLOW



PRISMATIC REFLECTOR

NOTE:
DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ARDOT QUALIFIED PRODUCTS LIST.



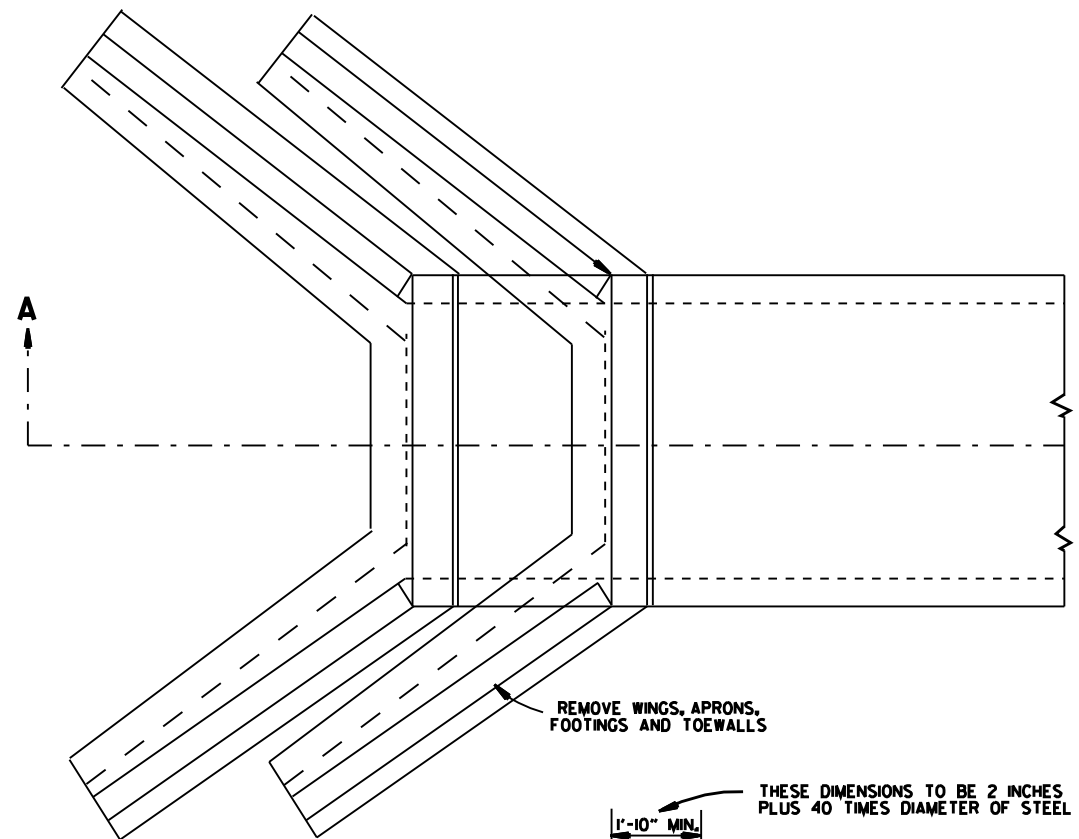
DETAIL OF STANDARD RAISED PAVEMENT MARKERS

DATE	REVISION	FILMED
2-27-20	REVISED STOP LINE DETAILS	
6-1-17	ADDED YIELD LINE DETAIL	
5-12-16	REVISED LINE WIDTHS, SPACING, & NOTES	
9-12-13	REVISED DETAIL OF STANDARD RAISED PAVEMENT MARKERS	
11-17-10	REVISED GENERAL NOTES & REMOVED PLOWABLE PVMT MRKRS	
11-18-04	REVISED NOTE 2 & GENERAL NOTES	
8-22-02	ADDED CROSSWALK & STOPBAR DTL.	
7-02-98	ADDED DETAILS OF STD. RAISED PAV'T. MARKERS	
4-26-96	REV. NOTES 3&4; ADDED R.P.M.	
9-30-80	DRAWN	1-9-30-80

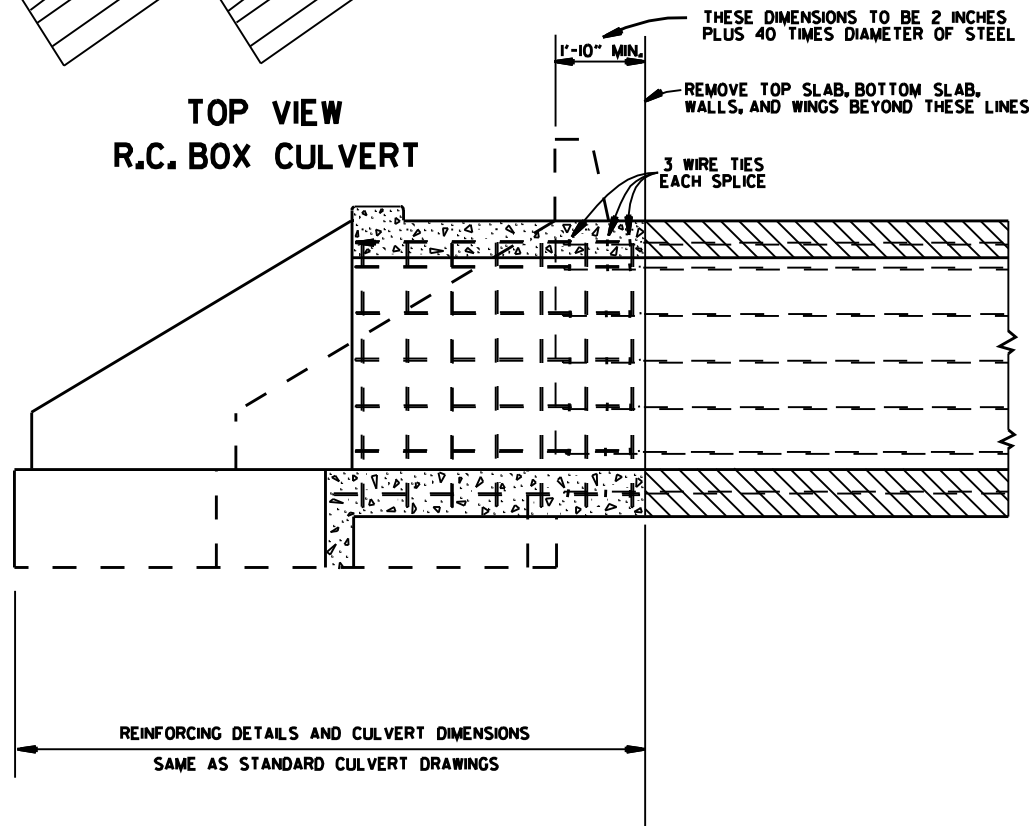
ARKANSAS STATE HIGHWAY COMMISSION

PAVEMENT MARKING DETAILS

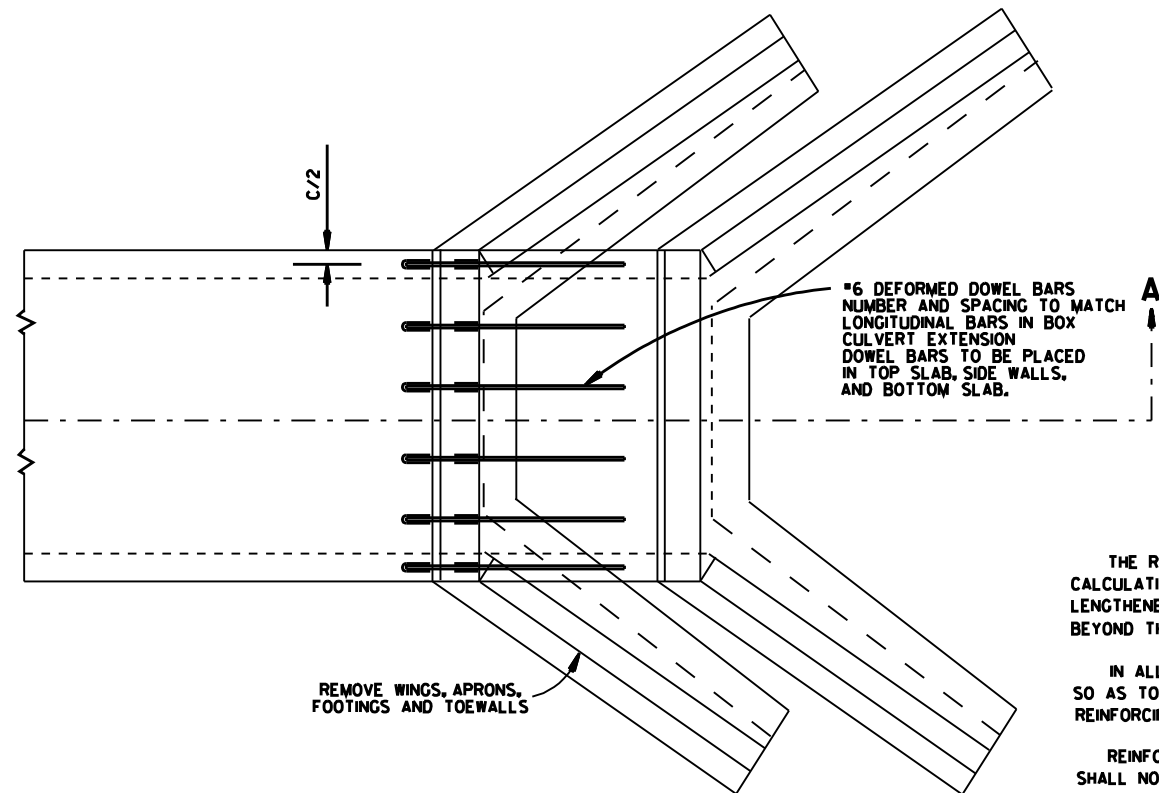
STANDARD DRAWING PM-1



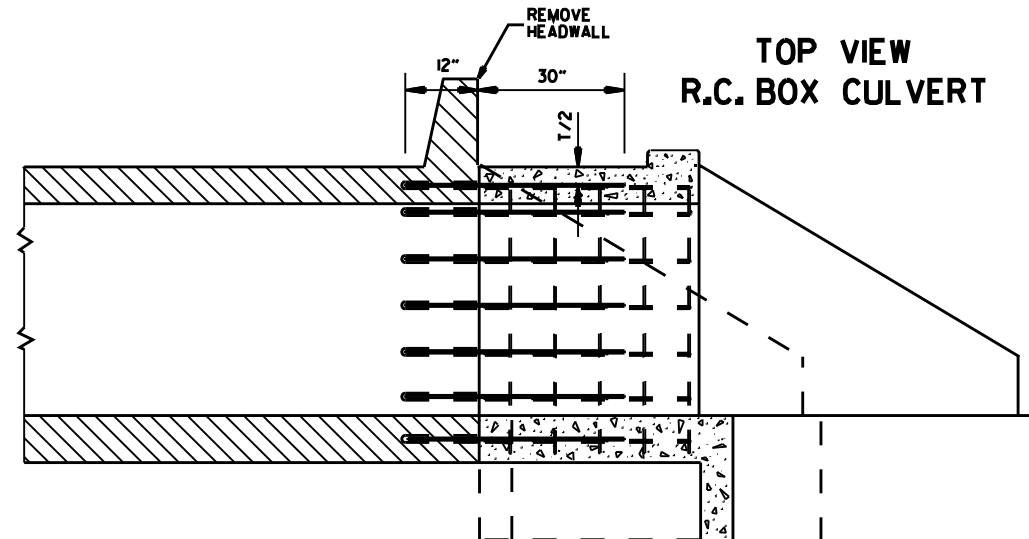
TOP VIEW
R.C. BOX CULVERT



SECTION A-A
METHOD 1



#6 DEFORMED DOWEL BARS
NUMBER AND SPACING TO MATCH
LONGITUDINAL BARS IN BOX
CULVERT EXTENSION
DOWEL BARS TO BE PLACED
IN TOP SLAB, SIDE WALLS,
AND BOTTOM SLAB.



SECTION A-A
METHOD 2

- GENERAL NOTES
- | | |
|---|-----|
| THE RESIDENT ENGINEER WILL MAKE INDIVIDUAL CALCULATIONS OF QUANTITIES FOR EACH STRUCTURE LENGTHENED, MAKING NO ALLOWANCE FOR OVERBREAKAGE BEYOND THE LINES INDICATED. | 1 |
| IN ALL INSTANCES CONCRETE SHALL BE REMOVED SO AS TO PERMIT FULL 40 DIAMETER SPLICE OF REINFORCING STEEL. | 1 |
| REINFORCING STEEL REMOVED FROM EXISTING STRUCTURE SHALL NOT BE REUSED IN CONSTRUCTING EXTENSION. | 1&2 |
| ON R.C. BOX CULVERTS THAT HAVE AN EXISTING CONCRETE APRON, THE CONCRETE APRON SHALL BE REMOVED WITH THE WINGS, THE COST OF REMOVING ALL OLD CONCRETE WILL BE INCLUDED IN THE PRICE BID PER CUBIC YARD FOR NEW CONCRETE OF THE CLASS SPECIFIED AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. | 1&2 |
| MATERIALS FOR SECURING DOWEL BARS SHALL MEET THE REQUIREMENTS OF SECTION 507.02 OF THE STANDARD SPECIFICATIONS. | 2 |
| DOWEL BARS SHALL BE INSTALLED AS FOLLOWS: THE DRILLING PROCEDURE SHALL BE APPROVED BY THE ENGINEER, THE FILLING SYSTEM SHALL BE APPROVED BY THE ENGINEER, AND SHALL BE AN INJECTION-TYPE SYSTEM WHICH WILL INSURE THAT SUFFICIENT MATERIAL IS INJECTED SO IT COMPLETELY SURROUNDS THE BARS AND FILLS THE HOLES. | 2 |
| THE CONTRACTOR SHALL HAVE THE OPTION OF USING EITHER METHOD 1 OR METHOD 2, REGARDLESS OF WHICH METHOD IS USED, PAY QUANTITIES WILL BE CALCULATED BASED ON METHOD 1. | 1&2 |

NOTE:
NO PART OF THIS STANDARD IS TO BE USED FOR ANY DETAILS RELATIVE TO NEW CONSTRUCTION.
SEE STANDARD DRAWING LISTED IN TABULATION OF STRUCTURES FOR ALL NEW CONSTRUCTION DETAILS.

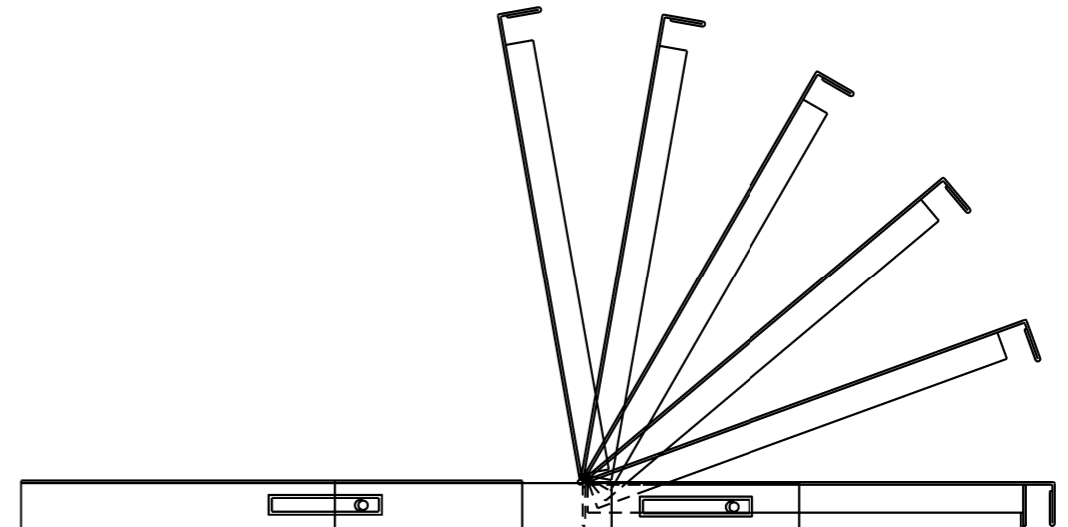
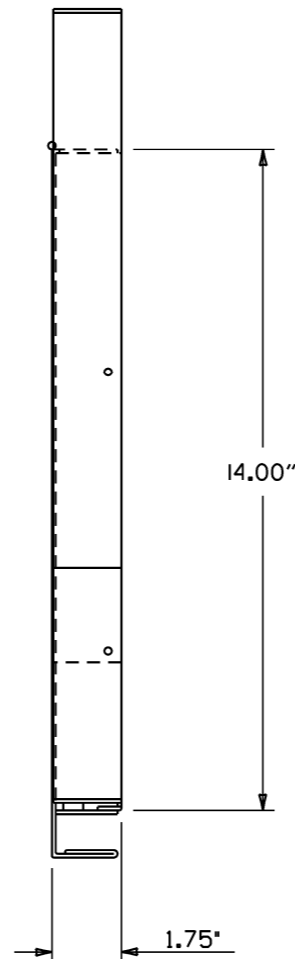
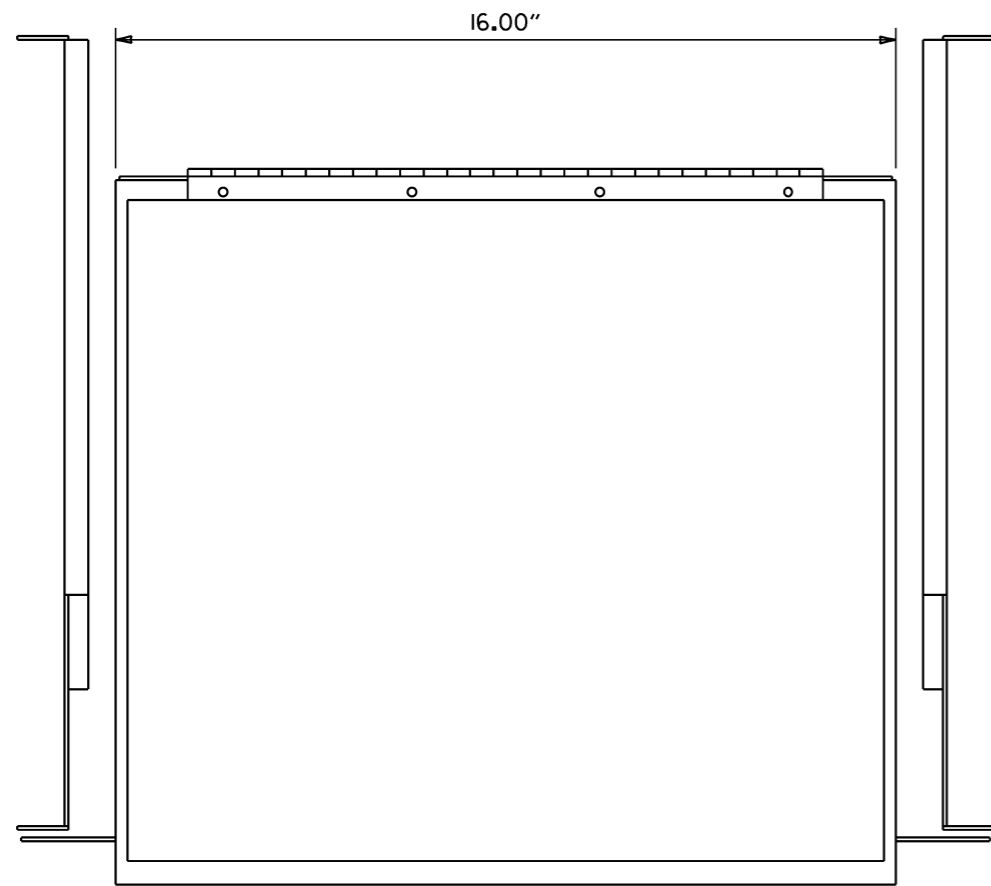
DATE	REVISION	DATE FILED
10-12-95	CHANGED DRAWING * FROM 144-A	
4-1-93	ADDED GENERAL NOTE	
10-1-92	ADDED ALT. METHOD OF EXTENSION	
11-30-89	REDRAWN	
1-4-83	ELIMINATED CONCRETE CLASS	
12-20-56	RETRACED	

ARKANSAS STATE HIGHWAY COMMISSION

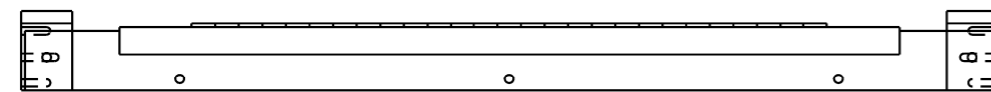
METHOD OF EXTENDING
EXISTING R.C. BOX CULVERTS

STANDARD DRAWING RCB-3

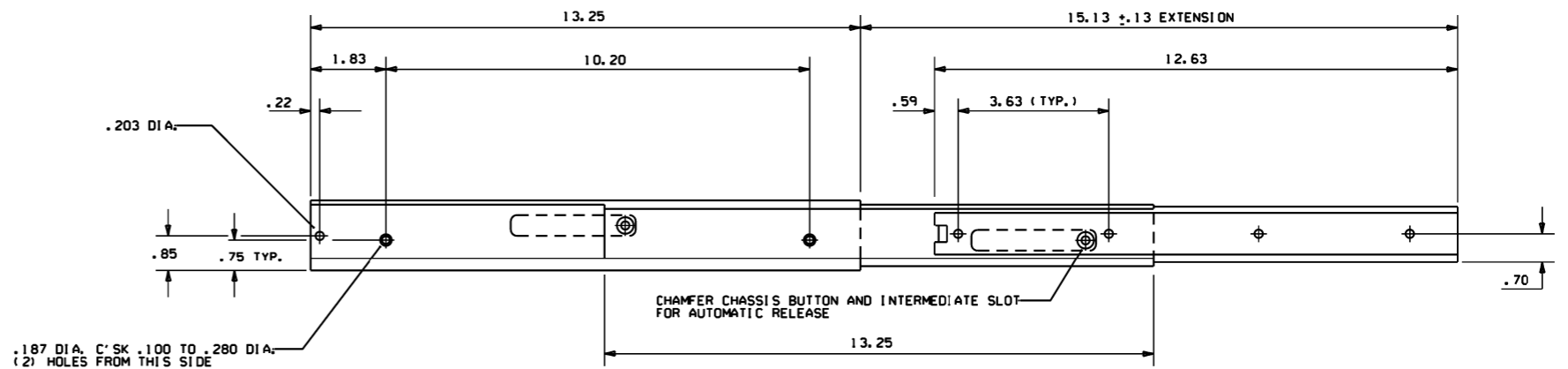
DRAWER PLAN VIEW



- NOTES:
 1. RIGHT HAND SLIDE SHOWN, LEFT SLIDE OPPOSITE.
 2. GENERAL DEVICES (CC3002-99-0102) OR EQUAL AND CONTAINS (1) RIGHT HAND SLIDE ASSEMBLY, (1) LEFT HAND SLIDE ASSEMBLY.
 3. ALL HARDWARE NECESSARY TO FASTEN SLIDE ASSEMBLY TO UNDERSIDE OF CONTROLLER SHELF SHALL BE INCLUDED.



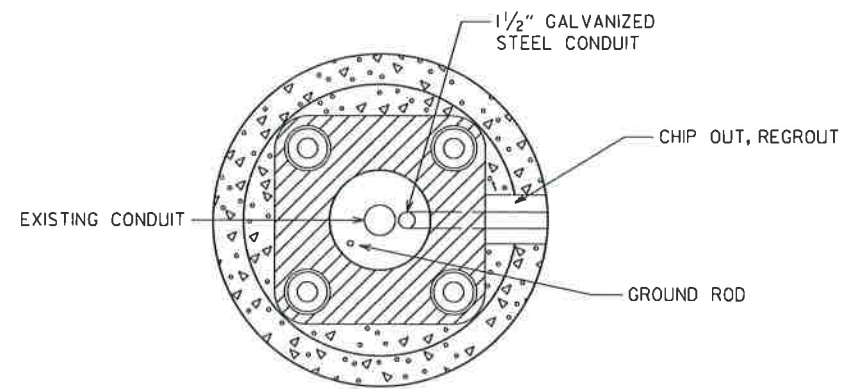
FRONT VIEW



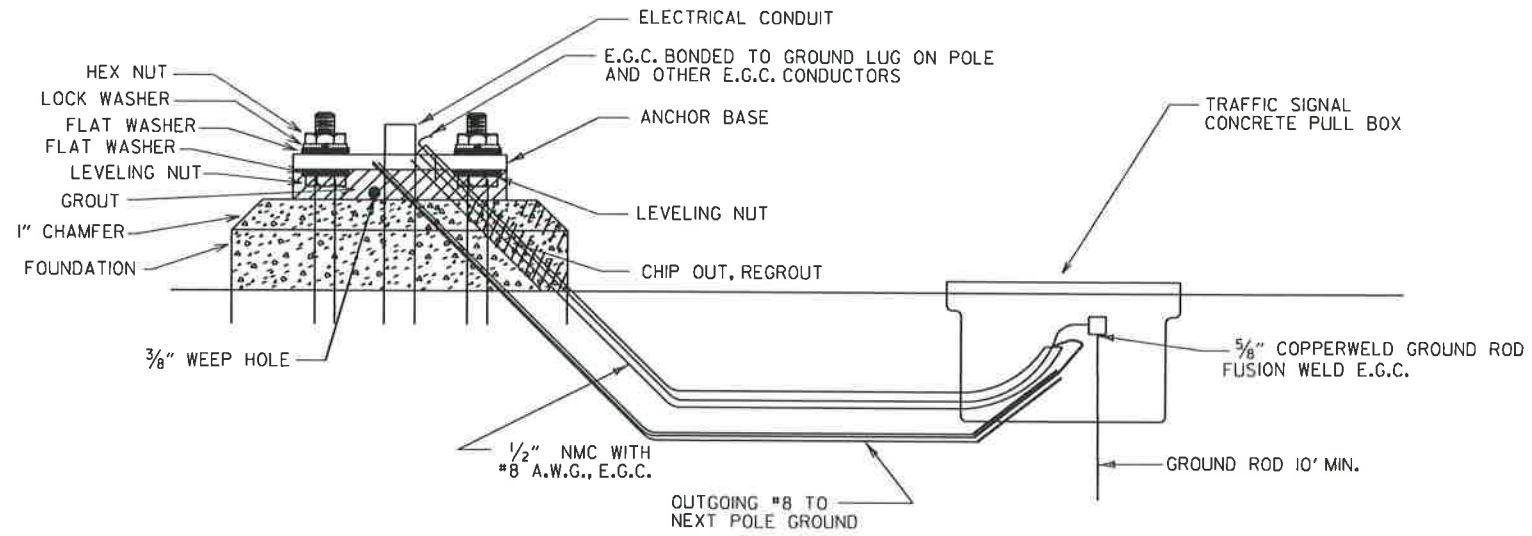
RIGHT SIDE ASSEMBLY

			ARKANSAS STATE HIGHWAY COMMISSION
			CONTROLLER CABINET UTILITY DRAWER
9-12-13	ISSUED AS STANDARD DRAWING		
6-15-05	ISSUED		
DATE	REVISION	DATE FILED	STANDARD DRAWING SD-5 187

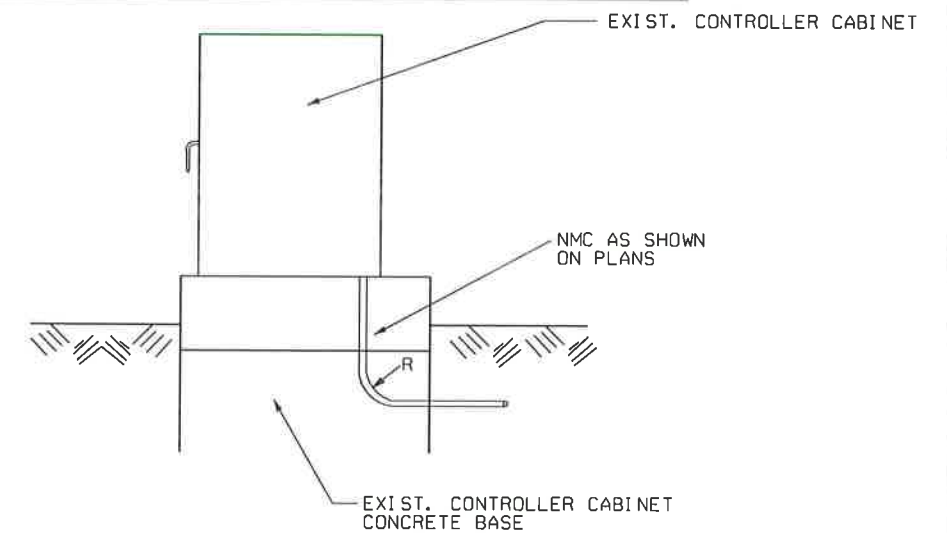
CONDUIT ENTRY TO EXISTING POLE BASE



ANCHOR BASE

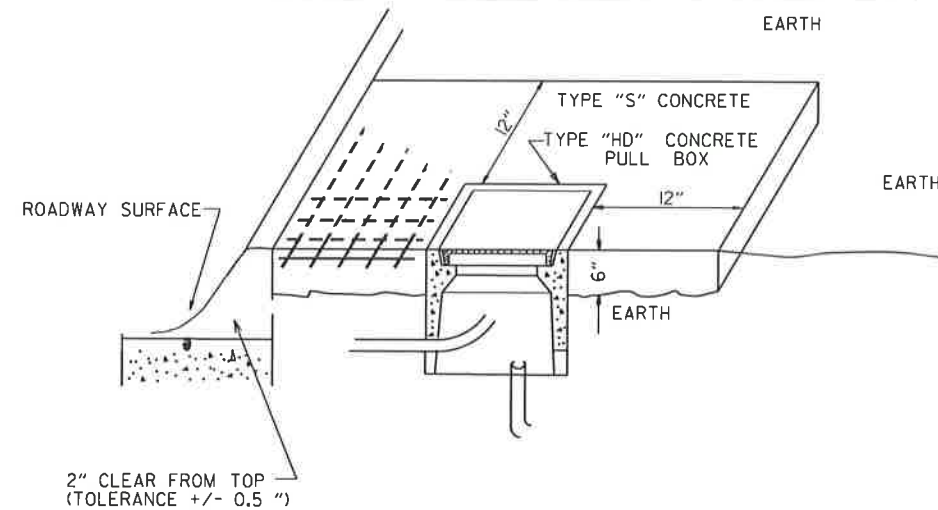


CONDUIT ENTRY TO EXISTING CONTROLLER CABINET

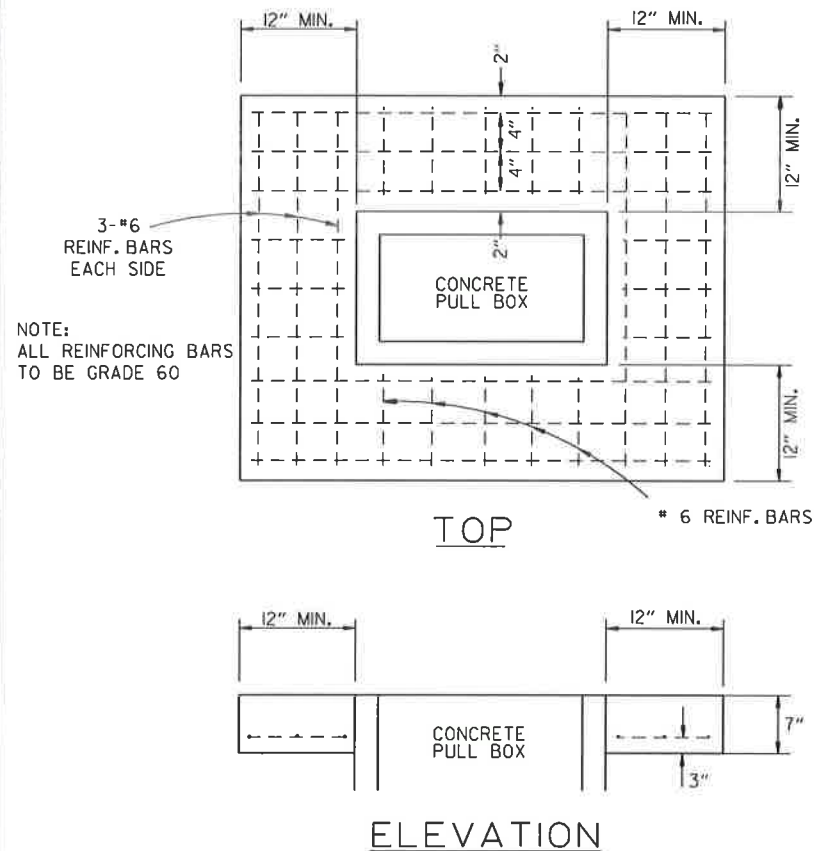


NOTE: ENTRY TO CABINET SHALL BE THROUGH A CUT IN THE BASE SUFFICIENT TO PROVIDE ADEQUATE CONDUIT RADIUS FOR ITEM.

TYPE "HD" CONCRETE PULL BOX DETAIL



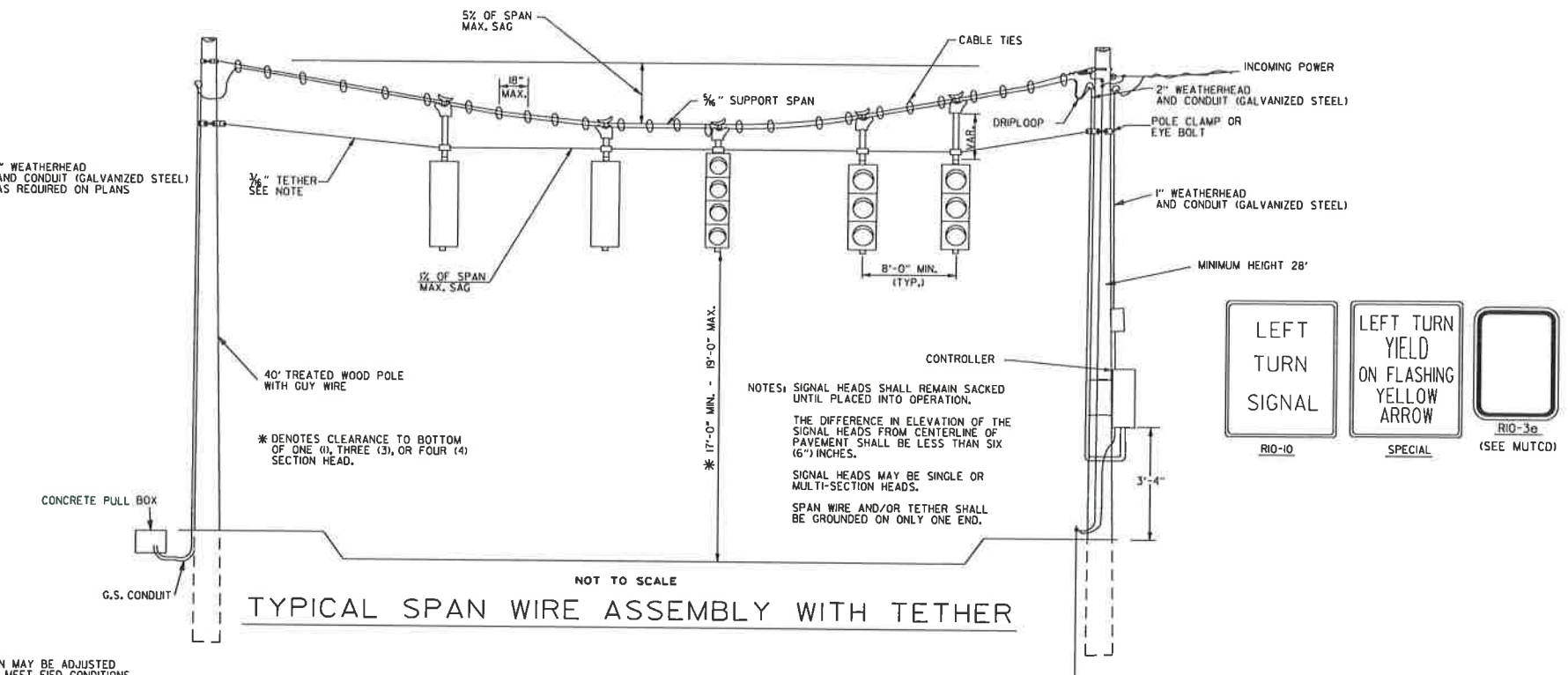
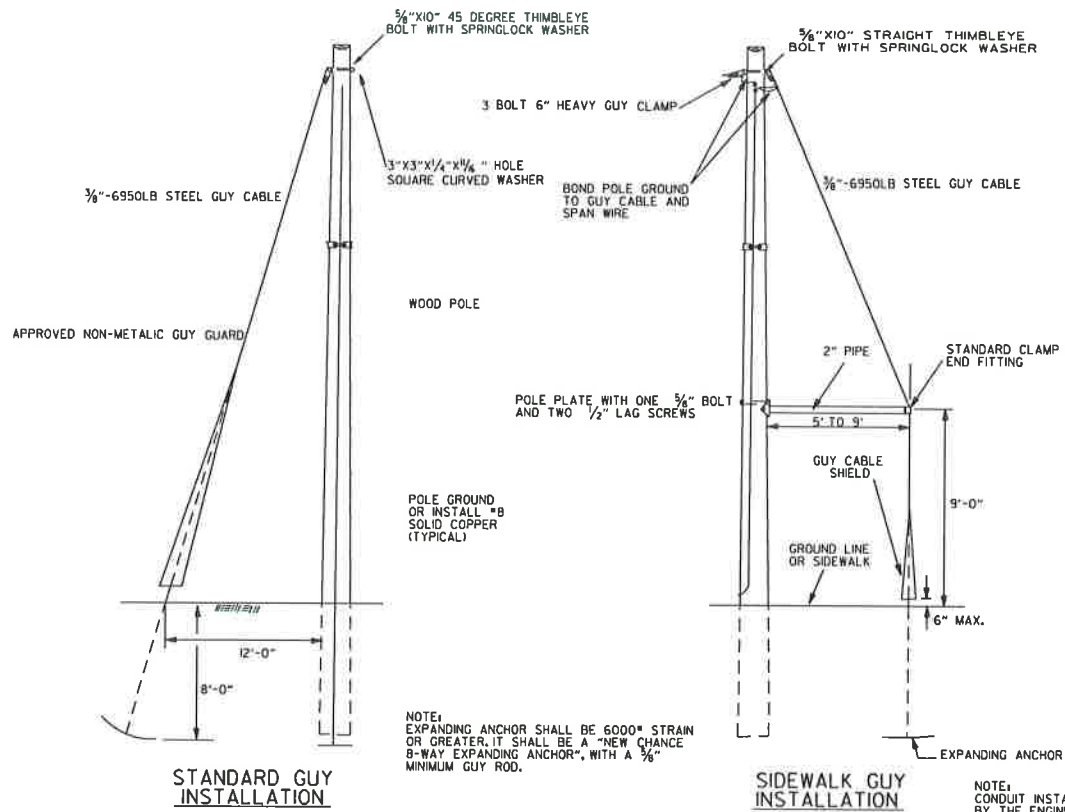
NOTE: ALL TYPE 1 AND TYPE 2 HD CONCRETE PULL BOXES ARE INSTALLED WITH AN APRON OF CONCRETE 12" WIDE AND 7" IN DEPTH. ALL PAYMENT SHALL BE INCLUDED IN THE PRICE OF THE TYPE HD CONCRETE PULL BOX. THE CONCRETE PULL BOX SHALL BE INSTALLED FLUSH TO SURROUNDING GRADE UNLESS OTHERWISE INSTRUCTED BY THE ENGINEER. THE CONCRETE SHALL BE CLASS "S". THREE #6 REINFORCING BARS IN THE APRON ON ALL SIDES OF THE CONCRETE PULL BOX IS REQUIRED IN CONCRETE.



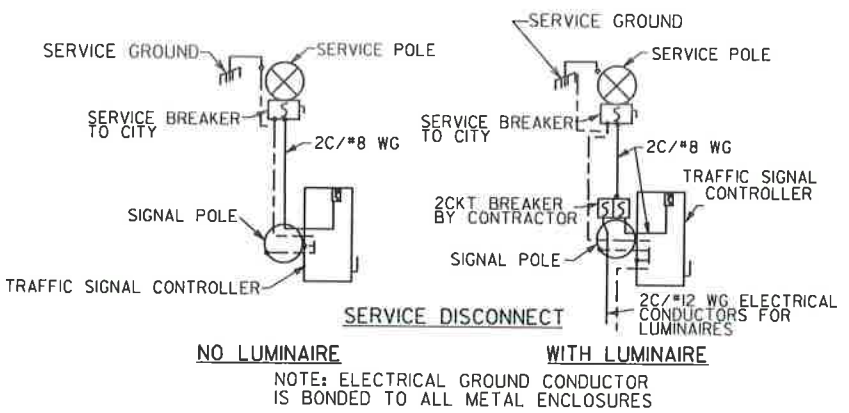
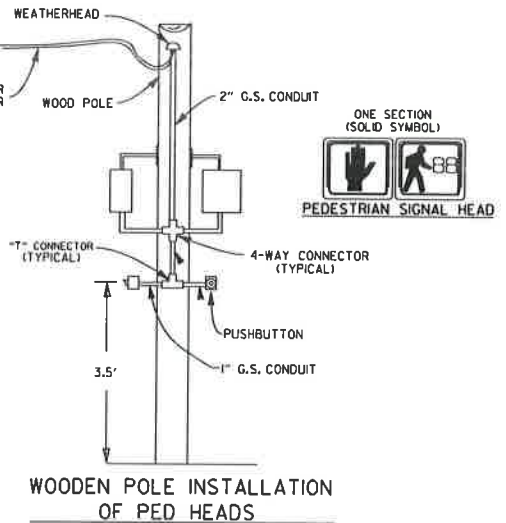
NOTE: ALL REINFORCING BARS TO BE GRADE 60

DATE	ISSUED	REVISION	FILMED
11-16-17	REVISED NOTES		
09-02-15	REVISED PULL BOX DEPTH		
09-12-13	ISSUED AS STANDARD DRAWING		
05-21-09	REVISED DRAWING		
07-31-08	ADDED & REVISED CONDUIT ENTRY		
06-23-04	REVISED CLEARANCE AT CURB ENTRY		
01-04-02	ADDED REINFORCING TO BOX APRON		
07-02-01	REVISED		
12-27-99	REVISED NOTES		
11-18-98	ISSUED		

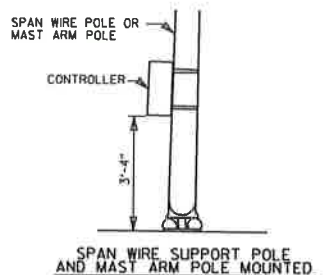
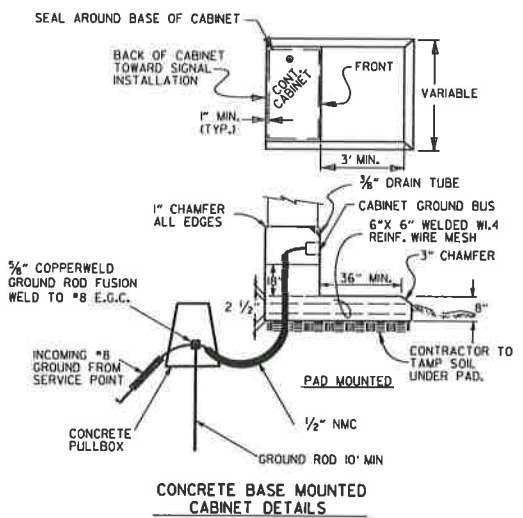
ARKANSAS STATE HIGHWAY COMMISSION
HEAVY DUTY PULL BOX
STANDARD DRAWING SD-6



NOTES:
SPAN WIRE POLES SHALL BE MOUNTED A MINIMUM OF FOUR (4') FEET BEHIND CURB OR SHOULDER.
SPAN WIRE ASSEMBLIES WILL REQUIRE TETHER UNLESS OTHERWISE NOTED ON PLAN SHEETS.
CABLE TIES SHALL BE SUITABLE FOR OUTSIDE USE (BLACK).
THE CONTROLLER POWER SUPPLY GROUND BUSS SHALL BE BONDED TO THE FOUNDATION GROUND ROD WITH A #8 A.W.G. SOLID COPPER WIRE, ON EXISTING FOUNDATIONS WITH NO GROUND ROD, CONTRACTOR SHALL INSTALL A 10' X 3/8" COPPERWELD GROUND ROD.



NOTES:
EACH ITEM "TRAFFIC SIGNAL HEAD (4 SEC., I-WAY)" SHALL INCLUDE A SPECIAL SIGN AS SHOWN, ATTACHED TO THE MAST ARM OR SPAN ASSEMBLY 12" TO THE RIGHT OF THE SIGNAL HEAD UNLESS REMOVED WITHIN SIGNAL PLAN NOTES.
EACH ITEM "TRAFFIC SIGNAL HEAD (3 SEC., I-WAY)", TO BE USED AS A LEFT TURN INDICATION ONLY, SHALL INCLUDE A SIGN (RIO-10) AS SHOWN, ATTACHED TO THE MAST ARM OR SPAN ASSEMBLY 12" TO THE RIGHT OF THE SIGNAL HEAD.
ALL SIGN BLANK SHALL BE CONSTRUCTED OF ALUMINUM ALLOY (ASTM DESIGNATION B-209, ALLOY 5052-H38) WITH A THICKNESS OF 0.100 INCH.
ALL SIGN FACE SHALL BE CONSTRUCTED OF HIGH INTENSITY SHEETING (TYPE III) WITH SILKSCREEN LEGEND AND BORDER.
SIGNAL OPERATION NOTES:
FLASHING OPERATION - PRIOR TO NORMAL OPERATION, SIGNAL SHALL BE FLASHED FOR A PERIOD OF 3 TO 5 WORK DAYS. SIGNAL SHALL BE PLACED IN OPERATION ONLY ON A REGULAR WORK DAY, EXCEPT FRIDAY.
THE CONTRACTOR MAY BE REQUIRED TO ALTER THE FLASHING DISPLAY DURING THE TEMPORARY FLASH PERIOD. AT THE TIME THE INTERSECTION IS PLACED IN PERMANENT OPERATION, THE FLASH SEQUENCE SHALL THEN BE RETURNED TO THAT INDICATED ON THE PLAN SHEETS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THESE ALTERATIONS IN FLASH SEQUENCE.

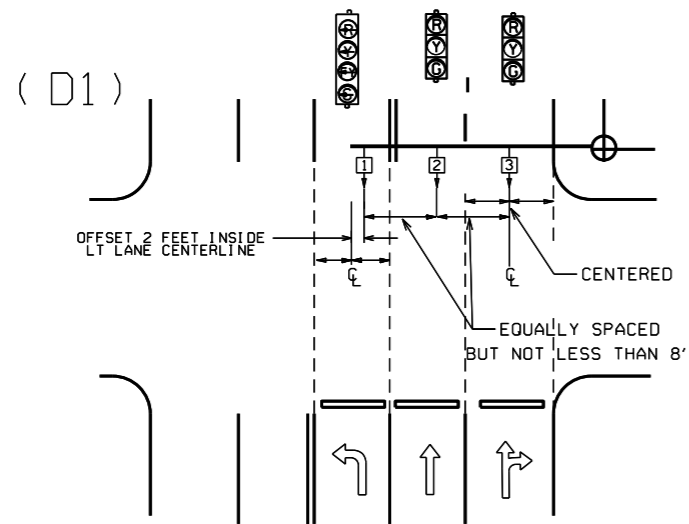
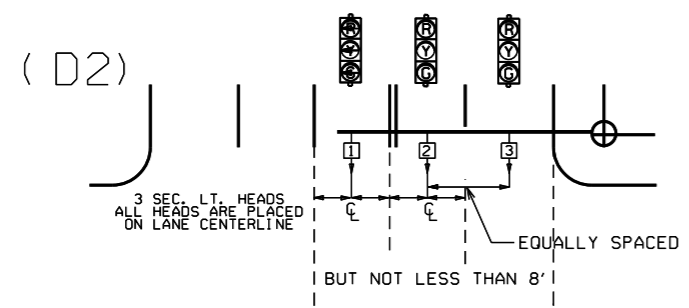
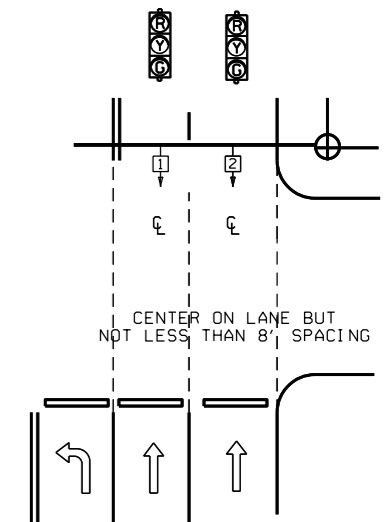
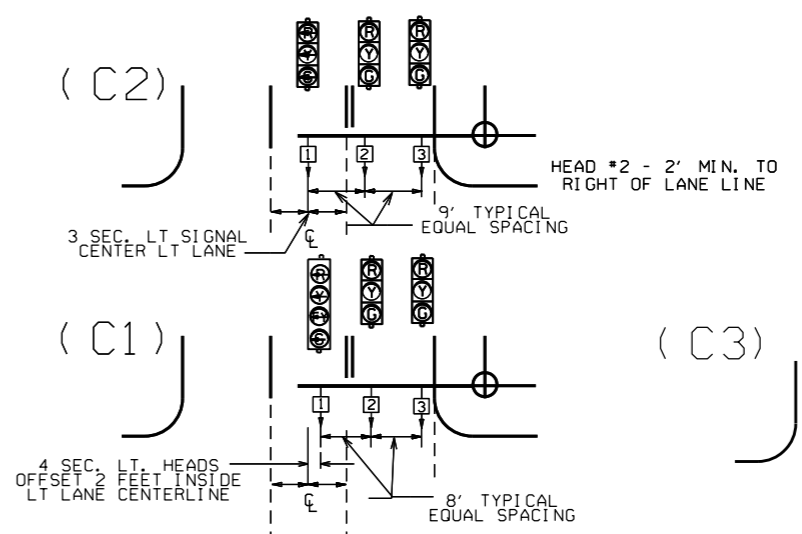
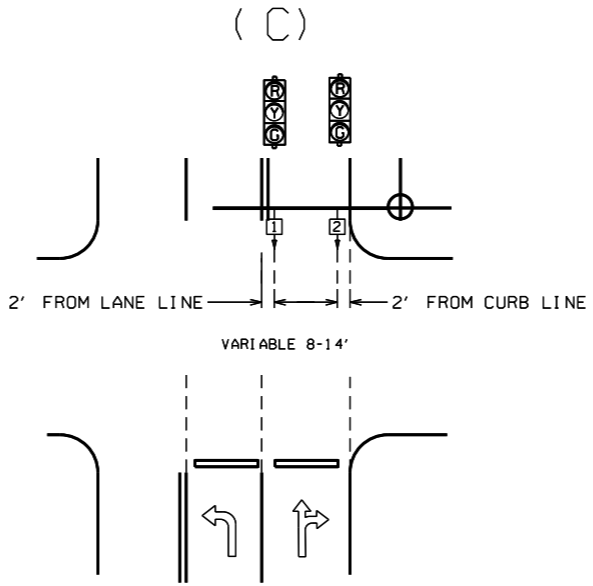
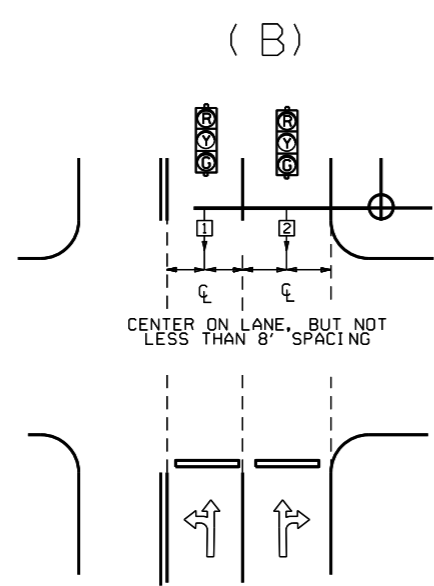
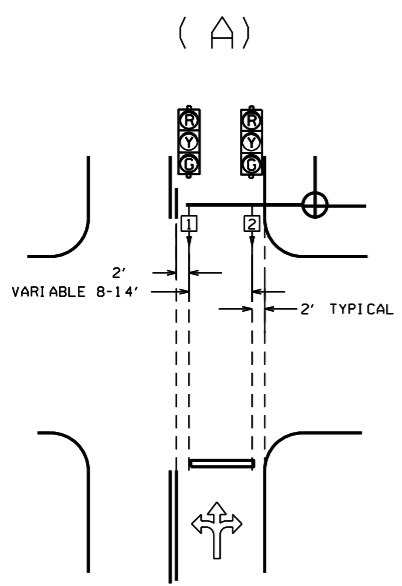


MINIMUM STRUCTURAL REQUIREMENTS:
DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4TH EDITION (2001) WITH 2003 AND 2006 INTERIMS.
CONSTRUCTION SPECIFICATIONS: STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION) WITH APPLICABLE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.
BASE WIND SPEED: 90 MPH
STEEL MEMBERS CONSIDERED MAIN LOAD CARRYING MEMBERS WITH THICKNESS GREATER THAN 1/2" SHALL MEET THE LONGITUDINAL CHARPY V-NOTCH TEST SPECIFIED IN SUBSECTION 807.05 OF THE STANDARD SPECIFICATIONS.

DATE	REVISION	FILMED
11-16-17	REVISED NOTES, ADDED SPAN WIRE SUPPORT POLE DETAIL, ADDED PEDESTRIAN SIGNAL HEAD DETAIL	
02-27-14	REVISED NOTES	
09-12-13	ISSUED AS STANDARD DRAWING	
07-21-8	REVISED PEDESTRIAN SIGN & GROUNDING	
04-17-08	REVISED TO 2001 AASHTO STANDARDS	
10-12-04	REV. CABINET ORIENTATION & SIGNAL OPERATION	
05-22-02	REV. TYP. SPAN WIRE ASSEMBLY	
12-27-99	REVISED	
11-18-98	REVISION TO NOTES	
11-21-95	ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION
SPAN WIRE ASSEMBLY
WOOD POLE
STANDARD DRAWING SD-7

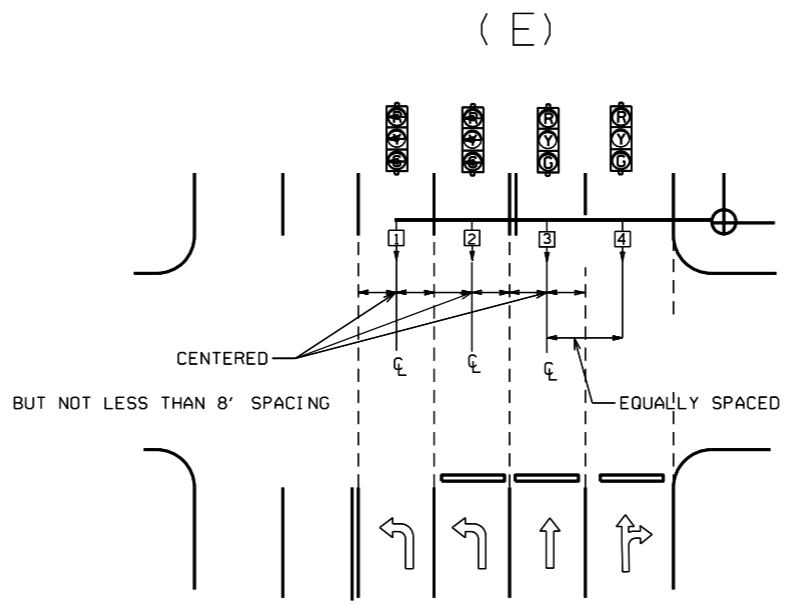
CABINET NOTE:
UNLESS OTHERWISE DIRECTED BY THE ENGINEER, CABINET ORIENTATION SHALL BE SUCH THAT THE BACK OF THE CABINET IS PARALLEL TO THE STREET AND POSITIONED TO ALLOW VISIBILITY OF THE SIGNAL DISPLAY WHILE OBSERVING THE CONTROLLER FRONT PANEL.



NOTE: WHERE LEFT TURN HEAD (HEAD 1 ON D1 AND D2) IS NOT CALLED FOR ON PLANS, MAST ARM LENGTH MAY STILL BE ALLOWED FOR FUTURE INSTALLATION. HEADS FOR THROUGH MOVEMENTS SHALL STILL BE ALIGNED WITH THROUGH LANES AS SHOWN ON DETAILS.

GENERAL NOTES:

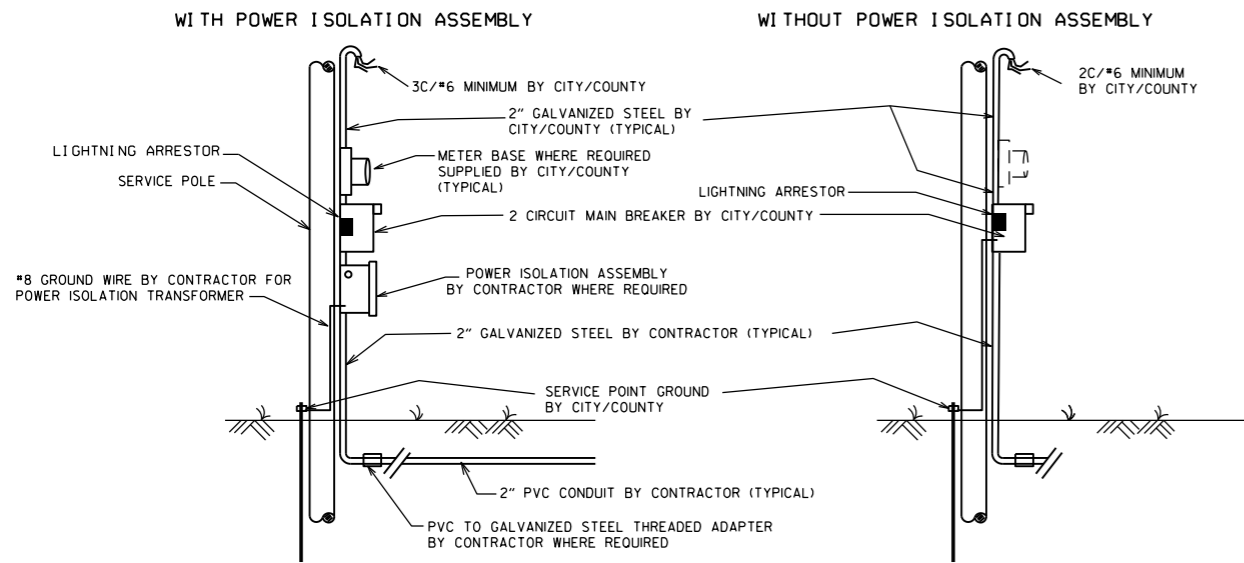
- FOUR SECTION "PROTECTED/PERMISSIVE" LEFT TURN HEADS SHOULD BE PLACED A MINIMUM OF TWO (2') FEET TO THE RIGHT OF THE CENTERLINE OF THE APPROACHING LEFT TURN LANE.
- THREE SECTION "PROTECTED" LEFT TURN HEADS SHOULD BE PLACED ON THE CENTERLINE OF THE APPROACHING LEFT TURN LANE.
- WHEN IT IS NECESSARY TO PLACE POLES OTHER THAN AS SHOWN ON PLAN SHEET(S) RESULTING IN MAST ARM EXTENDING MORE THAN TWO FEET PAST (TO THE LEFT OF) THE CENTERLINE OF THE APPROACHING LEFT TURN LANE, MAST ARM SHALL BE CUT TO APPROPRIATE LENGTH AS DETERMINED BY THE ENGINEER, AND A NEW END CAP PROVIDED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THIS PRIOR TO INSTALLING THE MAST ARM IF ADDITIONAL COMPENSATION IS REQUIRED.
- SIGNAL HEAD SPACING SHALL, IN NO CASE, BE LESS THAN EIGHT (8') FEET BETWEEN HEADS ON CENTER, MEASURED HORIZONTALLY PERPENDICULAR TO THE APPROACH.
- ALL SIGNAL HEADS SHOWN ON THIS DETAIL SHEET SHALL BE LOCATED ACCORDING TO THE DIMENSIONS SHOWN IN RELATION TO THE APPROACH SIDE OF THE INTERSECTION.
- MAXIMUM MOUNTING HEIGHT OF SIGNAL FACES LOCATED BETWEEN 40 FEET AND 53 FEET FROM STOP BAR SHALL BE IN ACCORDANCE WITH FIGURE 4D-5 OF 2009 MUTCD.



℄ = CENTER OF LANE FROM APPROACH SIDE

12-8-16	REVISED NOTE 6		ARKANSAS STATE HIGHWAY COMMISSION
9-12-13	ISSUED AS STANDARD DRAWING		SIGNAL HEAD PLACEMENT
3-11-10	2009 MUTCD		
12-9-99	ISSUED		
DATE	REVISION	DATE FILM	STANDARD DRAWING SD-8 190

MAIN BREAKER NOT NEAR CONTROLLER CABINET SECONDARY REQUIRED



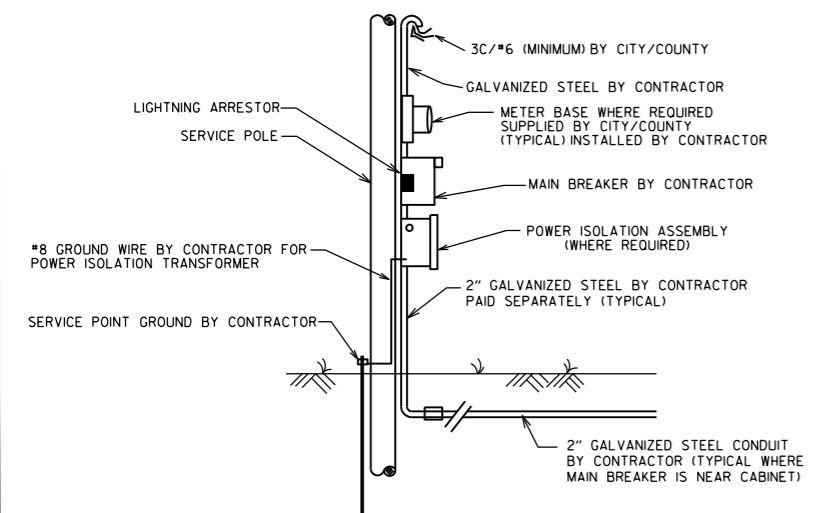
NOTES TO CONTRACTOR AND AGENCY RESPONSIBLE FOR MAINTENANCE OF THE INTERSECTION (CITY/COUNTY):

ELECTRICAL SERVICE TYPICALLY FALLS INTO TWO CATEGORIES: MAIN BREAKER NEAR CONTROLLER CABINET; AND MAIN BREAKER NOT NEAR CONTROLLER CABINET. THE CONTRACTOR'S AND THE CITY'S/COUNTY'S RESPONSIBILITY VARIES ACCORDINGLY AS INDICATED ON THESE DETAILS.

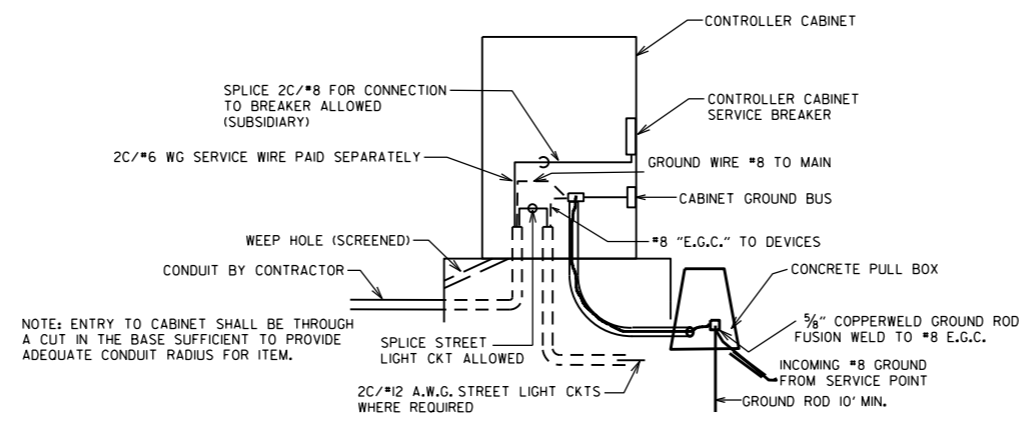
ALL SITUATIONS: ELECTRICAL SERVICE SHALL BE PROVIDED BY THE CITY/COUNTY TO A SERVICE POLE WITH EXTERNAL RAIN-TIGHT BREAKER (MAIN BREAKER) AT A MUTUALLY ACCEPTABLE POINT WITHIN THE RIGHT-OF-WAY. SERVICE POINT INCLUDES GALVANIZED STEEL CONDUIT TO A POINT 18" BELOW GROUND LINE, TWO CIRCUIT MAIN BREAKER, LIGHTNING ARRESTOR, POWER ISOLATION ASSEMBLY WHERE REQUIRED, METER LOOP IF REQUIRED BY LOCAL UTILITY COMPANY, ELECTRICAL CONDUCTORS AND WEATHERHEAD. WHERE STREET LIGHTING IS INCLUDED AS PART OF SIGNAL INSTALLATION STREET LIGHTING CIRCUIT (2C/#12 A.W.G. UF RATED, TYPICAL) SHALL BE KEPT SEPARATE FROM THE CIRCUIT SERVING TRAFFIC SIGNAL. SERVICE WIRE AND WIRING FROM THE CONTROLLER TO MAIN BREAKER IS PROVIDED BY THE CONTRACTOR AS A PART OF THIS CONTRACT. WIRE AND WIRING FROM MAIN BREAKER, AND CONNECTION TO THE UTILITY IS THE RESPONSIBILITY OF THE CITY/COUNTY.

MAIN BREAKER NOT NEAR CONTROLLER CABINET: THE MAIN BREAKER ASSEMBLY, GALVANIZED STEEL CONDUIT, WEATHERHEAD AND WIRE ABOVE MAIN BREAKER AND CONNECTION TO THE UTILITY SHALL BE PROVIDED BY CITY/COUNTY. CONTRACTOR SHALL PROVIDE AS PART OF CONTRACT SECONDARY BREAKER, CONDUIT, WIRE AND WIRING TO THE MAIN BREAKER.

MAIN BREAKER NEAR CONTROLLER CABINET: ALL COMPONENTS OF THE SERVICE POINT WITH THE EXCEPTION OF THE WIRE AND WIRING ABOVE THE MAIN BREAKER IS FURNISHED AND INSTALLED BY THE CONTRACTOR. WIRING FROM MAIN BREAKER INCLUDING CONNECTION TO THE UTILITY, IS THE RESPONSIBILITY OF THE CITY/COUNTY. IF METER LOOP IS REQUIRED, METER BASE AND HARDWARE IS PROVIDED BY THE CITY/COUNTY AND INSTALLED BY THE CONTRACTOR.



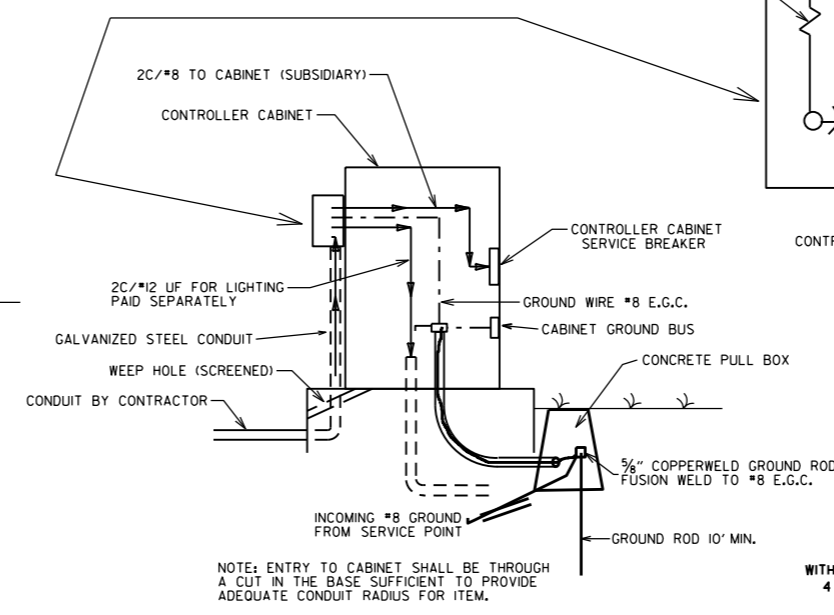
MAIN BREAKER NEAR CONTROLLER CABINET SECONDARY NOT REQUIRED



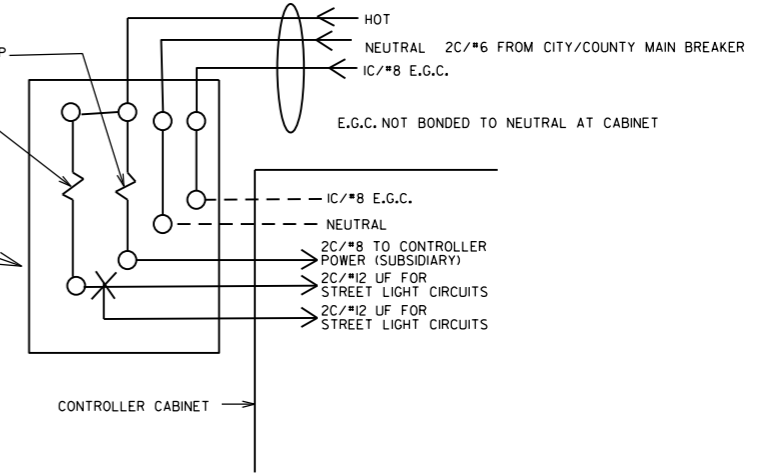
NOTE: ENTRY TO CABINET SHALL BE THROUGH A CUT IN THE BASE SUFFICIENT TO PROVIDE ADEQUATE CONDUIT RADIUS FOR ITEM.

GROUND ROD - A 10' X 5/8" GROUND ROD SHALL BE INSTALLED IN THE CONCRETE PULL BOX FOR EACH POLE AND THE CONTROLLER. PAYMENT FOR THE GROUND ROD AND 1/2" NMC SHALL BE INCLUDED IN ITEM 70L. THE CONCRETE PULL BOX AND CONDUCTOR BOX SHALL BE PAID FOR SEPARATELY.

SECONDARY BREAKER BY CONTRACTOR (SUBSIDIARY)



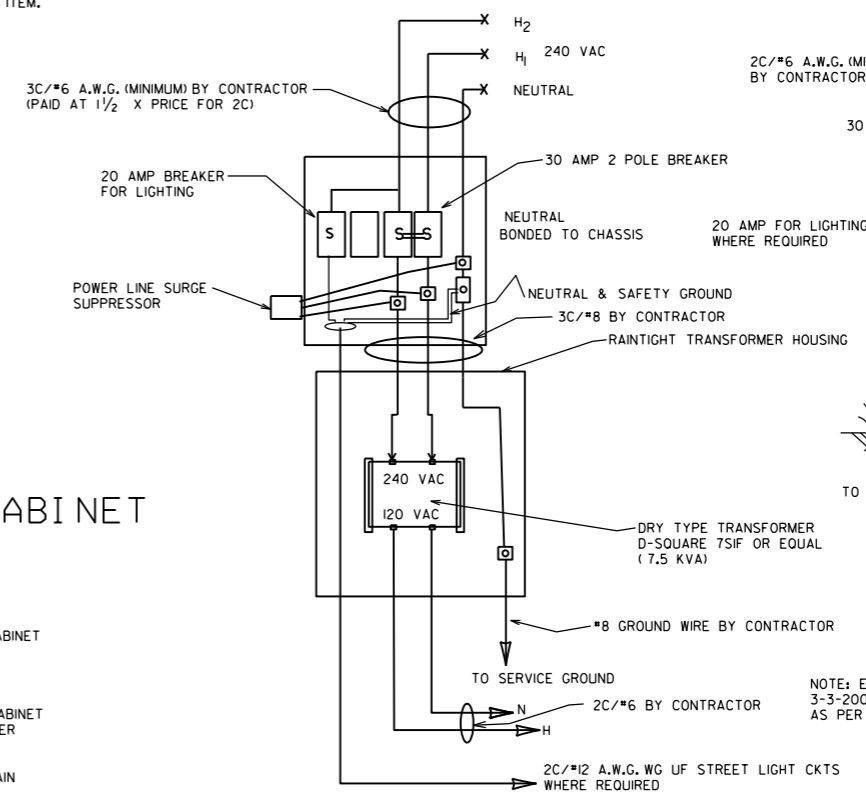
NOTE: ENTRY TO CABINET SHALL BE THROUGH A CUT IN THE BASE SUFFICIENT TO PROVIDE ADEQUATE CONDUIT RADIUS FOR ITEM.



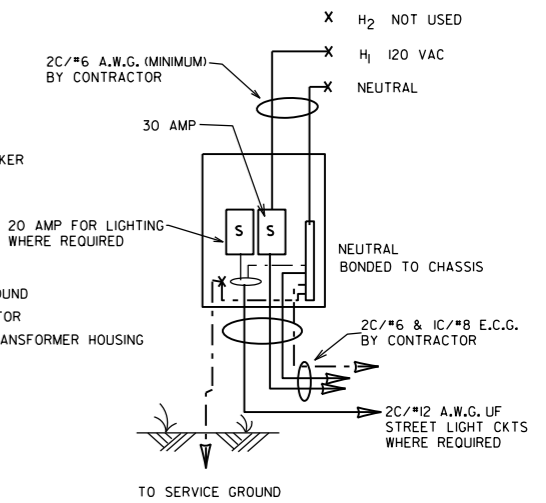
MAIN BREAKER WIRING (TYPICAL)

SERVICE GROUND IS TYPICALLY TIED TO NEUTRAL AT THE MAIN BREAKER. AS SUCH, CONTROLLER GROUND IS NOT TIED TO NEUTRAL AT SECONDARY BREAKER OR IN CONTROLLER CABINET.

WITH POWER ISOLATION ASSEMBLY 4 CIRCUIT MAIN BREAKER



WITHOUT POWER ISOLATION ASSEMBLY 2 CIRCUIT MAIN BREAKER



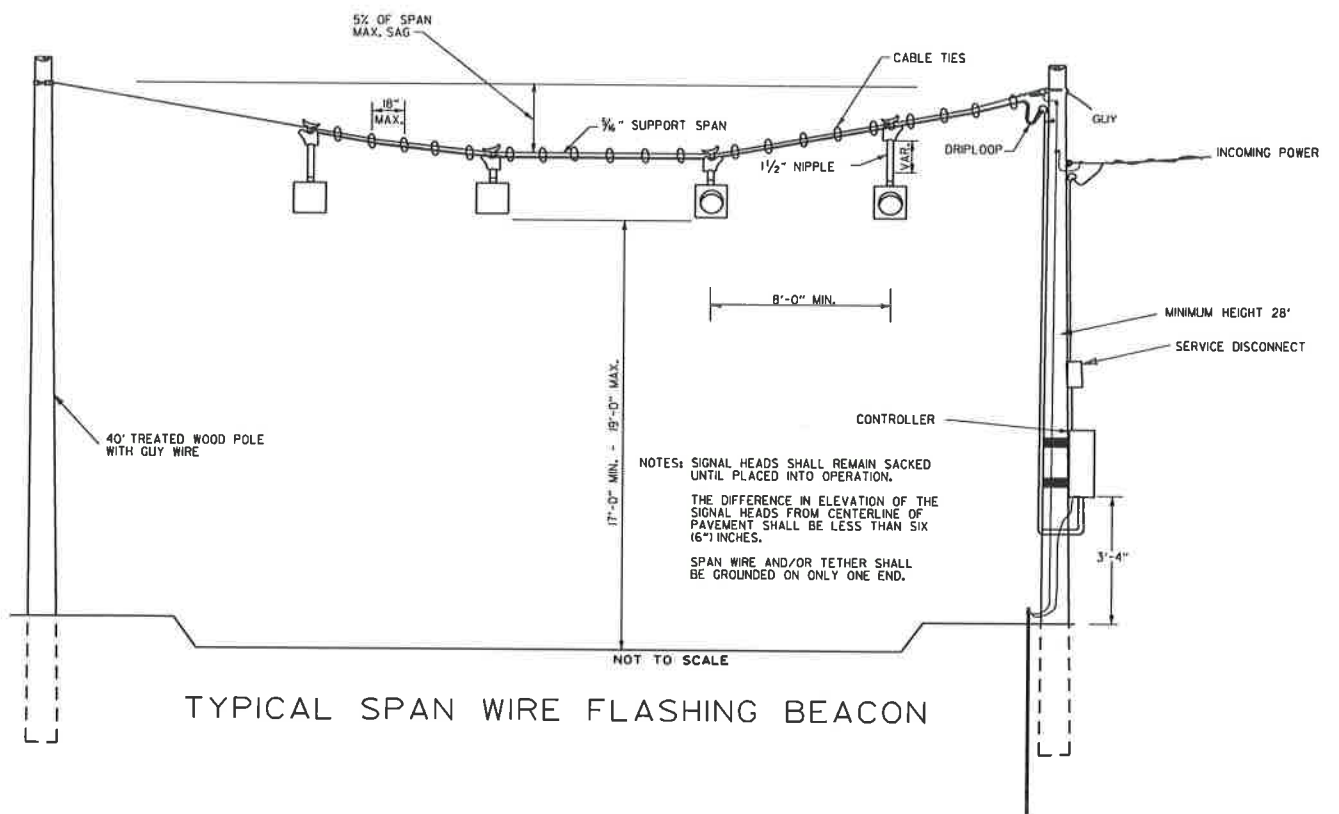
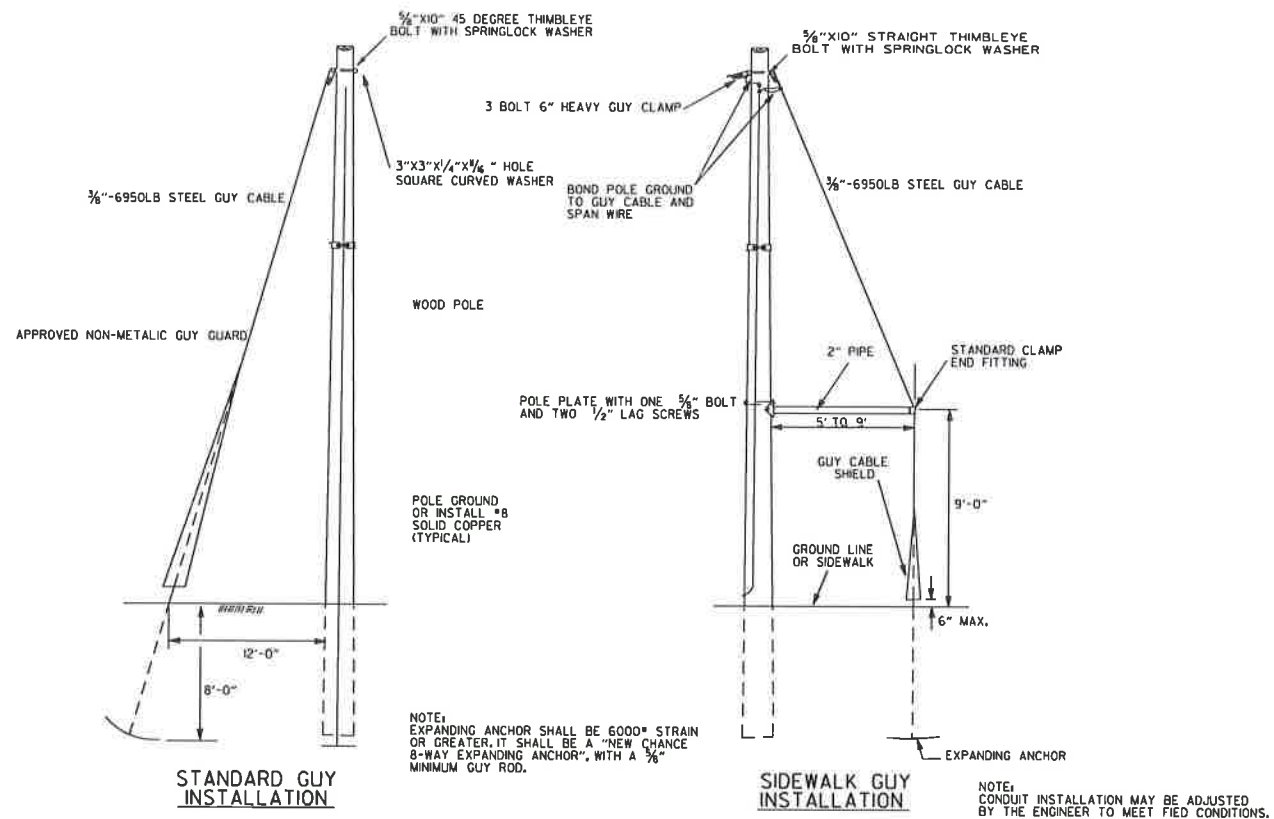
NOTE: ELECTRICAL GROUND CONDUCTOR (E.G.C.) ADDED 3-3-2003, CONSISTING OF A 1C/#8 A.W.G. CU GREEN WIRE AS PER NATIONAL ELECT. CODES.

DATE	REVISION	FILMED
11-07-19	REVISED	
11-16-17	REVISED NOTES	
09-12-13	ISSUED AS STANDARD DRAWING	
04-18-13	ADDED LIGHTNING ARRESTOR	
05-21-09	REVISED GROUNDING	
07-31-08	REVISED GROUNDING	
03-03-03	ADDED EGC NOTE	
09-26-01	REVISED	
12-27-99	REVISED	
07-28-99	REVISED	
02-05-99	ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION

SERVICE POINT

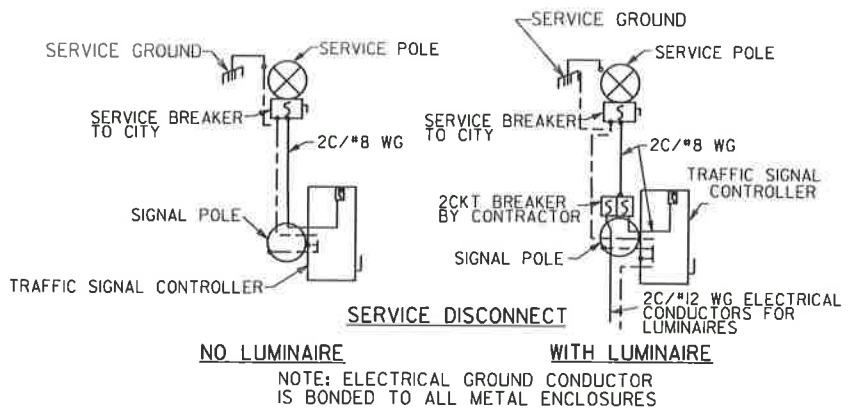
STANDARD DRAWING SD-9



NOTES:
 SPAN WIRE POLES SHALL BE MOUNTED A MINIMUM OF FOUR (4) FEET BEHIND CURB OR SHOULDER.
 SPAN WIRE ASSEMBLIES WILL REQUIRE TETHER UNLESS OTHERWISE NOTED ON PLAN SHEETS.
 CABLE TIES SHALL BE SUITABLE FOR OUTSIDE USE (BLACK).
 THE CONTROLLER POWER SUPPLY GROUND BUSS SHALL BE BONDED TO THE GROUND ROD WITH A #8 A.W.G. SOLID COPPER WIRE. ON EXISTING FOUNDATIONS WITH NO GROUND ROD, CONTRACTOR SHALL INSTALL A 10' X 5/8\"/>

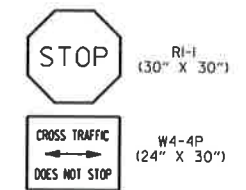
GENERAL NOTES:
 DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4TH EDITION (2001) WITH 2003 AND 2006 INTERIMS.
 CONSTRUCTION SPECIFICATIONS: STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION) WITH APPLICABLE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.
 THE FLASHING BEACON ASSEMBLY SHALL INCLUDE LIGHTNING AND R.F.I. SUPPRESSORS, GALVANIZED STEEL CONDUIT, TRAFFIC SIGNAL CABLE, 12\"/>

THE CITY/COUNTY SHALL BE RESPONSIBLE FOR PROVIDING, THROUGH A LOCAL UTILITY COMPANY, A SERVICE POINT AND UNDERGROUND/AERIAL POWER TO THE FLASHING BEACON CONTROLLER.



NOTE: ELECTRICAL GROUND CONDUCTOR IS BONDED TO ALL METAL ENCLOSURES

AT INTERSECTIONS WITH FLASHING RED ON ONE APPROACH AND FLASHING YELLOW ON OTHER, SUPPLEMENTAL "CROSS TRAFFIC DOES NOT STOP" SHALL BE INSTALLED ON THE SUPPORT FOR THE STOP SIGN PRIOR TO ACTIVATION OF BEACON.



TRAFFIC SIGNAL LEGEND

SYMBOL	DEFINITION
[Symbol: rectangle with dashed line]	LOOP DETECTOR
[Symbol: dashed line]	LOOP WIRING
[Symbol: double line]	CONDUIT
[Symbol: circle with A]	PHASE A IN PHASING DIAGRAM
[Symbol: circle with 2\"/>	

SIGNAL OPERATION NOTES:
 FLASHING OPERATION - PRIOR TO NORMAL OPERATION, SIGNAL SHALL BE FLASHED FOR A PERIOD OF 3 TO 5 WORK DAYS. SIGNAL SHALL BE PLACED IN OPERATION ONLY ON A REGULAR WORK DAY, EXCEPT FRIDAY.

ARKANSAS STATE HIGHWAY COMMISSION		
WOOD POLE SPAN WIRE INSTALLATION		
STANDARD DRAWING SD-10		
DATE	REVISION	FILMED

11-16-17	REVISED NOTES
09-12-13	ISSUED AS STANDARD DRAWING
05-11-04	REV. GROUND CONDUCTORS & SIGNING
12-27-99	REVISED NOTES
11-17-98	REVISED NOTES
11-21-95	ISSUED

NOTES:
PEDESTRIAN AND TRAFFIC SIGNAL HEAD SIGNS:
 EACH ITEM "TRAFFIC SIGNAL HEAD (4 SEC., 1-WAY)" SHALL INCLUDE A SPECIAL SIGN AS SHOWN, ATTACHED TO THE MAST ARM OR SPAN ASSEMBLY 12" TO THE RIGHT OF THE SIGNAL HEAD UNLESS REMOVED WITHIN THE SIGNAL PLAN NOTES.

EACH ITEM "TRAFFIC SIGNAL HEAD (3 SEC., 1-WAY)" TO BE USED AS A LEFT TURN INDICATION ONLY SHALL INCLUDE A SIGN (RIO-10) AS SHOWN, ATTACHED TO THE MAST ARM OR SPAN ASSEMBLY 12" TO THE RIGHT OF THE SIGNAL HEAD.

EACH PEDESTRIAN PUSHBUTTON SHALL HAVE ONE RIO-3E SIGN ATTACHED TO THE POLE ABOVE THE BUTTON. ALL SIGNS SHALL BE MANUFACTURED IN ACCORDANCE WITH SECTION 723 OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

ALL SIGN BLANKS SHALL BE CONSTRUCTED OF ALUMINUM ALLOY (ASTM DESIGNATION B-209, ALLOY 5052-H38) WITH THICKNESS OF 0.100 INCH.

GENERAL NOTES:
 1. MAST ARM POLES SHALL BE MOUNTED A MINIMUM OF FOUR (4') FEET BEHIND CURB OR SHOULDER.

2. OCTAGONAL POLES AND ARMS MEETING THE REQUIREMENTS OF THE PLANS SPECIFICATIONS CAN BE INSTALLED IN LIEU OF ROUND. ALL POLES AND ARMS IN A JOB MUST BE THE SAME SHAPE.

3. MINIMUM STRUCTURAL REQUIREMENTS: DESIGN SPECIFICATIONS; AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS, 4TH EDITION (2001) WITH 2003 AND 2006 INTERIMS.

USE FATIGUE CATEGORY I FOR ALL STRUCTURES ON ROUTES WHERE THE SPEED LIMIT IS 65 MPH AND GREATER AT THE STRUCTURE LOCATION AND ON ROUTES WHERE THE SPEED LIMIT IS GREATER THAN 45 MPH WITH AN MAST ARM OF 60' OR LONGER.

USE FATIGUE CATEGORY II FOR ALL STRUCTURES ON ROUTES WHERE THE SPEED LIMIT IS LESS THAN 65 MPH AND GREATER THAN 45 MPH WITH MAST ARMS LESS THAN 60' AND ON ROUTES WHERE THE SPEED LIMITS OF 45 MPH AND LESS WITH AN MAST ARM OF 60' OR LONGER.

USE FATIGUE CATEGORY III FOR ALL STRUCTURES WHERE THE SPEED LIMIT IS 45 MPH AND LESS AND MAST ARMS LESS THAN 60'.

CONSTRUCTION SPECIFICATIONS:
 STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION (CURRENT EDITION) WITH APPLICABLE SUPPLEMENTAL SPECIFICATIONS AND SPECIAL PROVISIONS.

BASE WIND SPEED: 90 MPH.

STEEL MEMBERS CONSIDERED MAIN LOAD CARRYING MEMBERS WITH A THICKNESS GREATER THAN 1/2" SHALL MEET THE LONGITUDINAL CHARPY V-NOTCH TEST SPECIFIED IN SUBSECTION 807.05 OF THE STANDARD SPECIFICATIONS.

DEAD LOAD: AS A MINIMUM, DESIGN SHALL BE BASED ON THE FIXED ATTACHMENTS SHOWN BELOW OR AS MODIFIED IN THE PLANS.

ALL SIGNAL HEADS TO BE ONE WAY, TWELVE (12") INCH AND HAVE FIVE (5") INCH BACK PLATES:

SIGNAL HEADS AT THE END OF MAST ARM - ONE 4 SEC., 85 LB., 14.5 SQ. FT., ONE SIGN MOUNTED 3 FEET FROM SIGNAL HEAD (2'-0" X 2'-6" 20 LB.) REMAINING SIGNAL HEADS SPACED AT 8 FT. (13 SEC., 56 LB., 8.3 SQ. FT.); DESIGN TO ACCOMMODATE:
 2 SIGNAL HEADS FOR MAST ARMS 10 FT. TO 16 FT.
 3 SIGNAL HEADS FOR MAST ARMS 18 FT. TO 24 FT.
 4 SIGNAL HEADS FOR MAST ARMS OVER 26 FT.

STREET NAME SIGN - 72" X 18", 36 LB., MOUNTED SUCH THAT OUTSIDE EDGE IS NOT GREATER THAN 12 FT. FROM POLE, DEPENDING UPON POSITION OF SIGNAL HEAD ADJACENT TO POLE, SIGN MAY OVERLAP POLE SHAFT.
 ROADWAY LUMINAIRES (WHERE REQUIRED ON PLAN SHEET) - VARIABLE ARM LENGTH (MAX. WT. 75 LB., 3.3 SQ. FT.)
 PEDESTRIAN SIGNALS - TWO 1 SEC., 12 INCH MOUNTED 8 FT. FROM BASE OF POLE, POST MOUNTED 3 SEC. SIGNAL HEAD AT 10 FT. ON SIDE OF POLE.

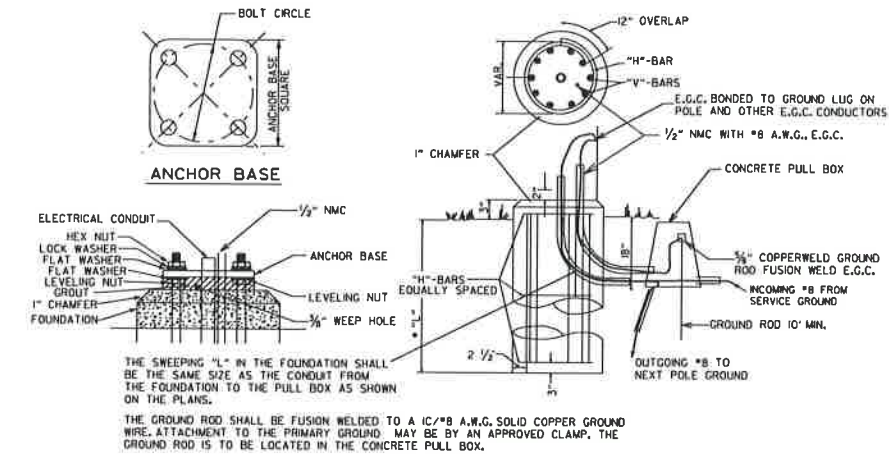
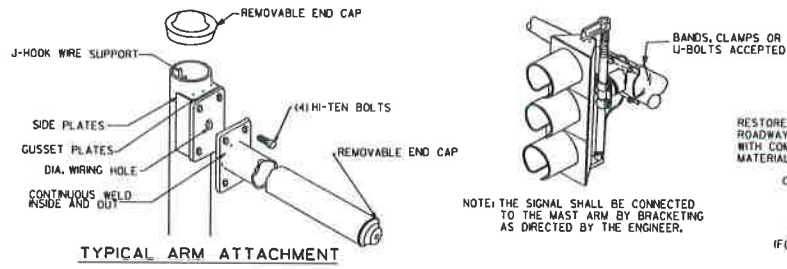
4. POLE/MAST ARM CAP - POLE AND MAST ARM CAPS SHALL BE PROVIDED, FABRICATED OF EITHER STEEL OR CAST ALUMINUM.

5. HAND HOLE - HAND HOLES SHALL BE 4 IN. X 6 IN. FOR STANDARD, AND 3 IN. X 5 IN. FOR PED POLES, MINIMUM PLACED APPROXIMATELY 12 INCHES FROM BASE, AND SHALL BE FIXED WITH A BOLT DOWN COVER. A VACUUM FORMED ABS COVER IS AN ACCEPTABLE ALTERNATE TO STEEL. POLES GREATER THAN 21 FT. IN HEIGHT (FOR ROADWAY LUMINAIRE ATTACHMENT) SHALL INCLUDE A HAND HOLE WITHIN 12 INCHES OF MAST ARM(S) ATTACHMENT(S).

6. POLE/MAST ARM TAPER SLOPE - AVERAGE TAPER OF SIGNAL MAST ARMS AND POLE SHAFT SHALL BE 0.25 TO 0.15 INCHES PER FOOT.

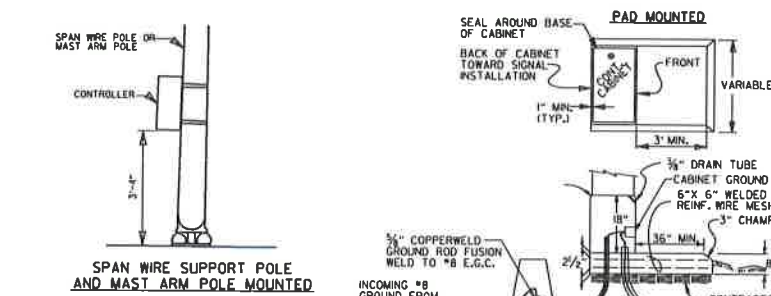
MAST ARM CENTERLINE ANGLE AT ATTACHMENT POINT WITH POLE SHALL MAINTAIN NOT LESS THAN 0.5 DEGREES OR MORE THAN 4 DEGREES POSITIVE SLOPE WITH A LINE PERPENDICULAR TO THE POLE CENTERLINE. THE MAST ARM SHALL MAINTAIN A POSITIVE SLOPE AFTER IT IS PLACED UNDER LOAD.

7. NUT COVERS - EACH POLE SHALL INCLUDE A BOLT DOWN NUT COVER FOR EACH ANCHOR BOLT.



TYPICAL FOUNDATION DETAILS
 POLE FOUNDATION MINIMUM DIMENSIONS AND STEEL REINFORCING. ALL REINFORCING STEEL SHALL BE GRADE 40 MIN.

ARM LENGTH	FOUNDATION DIAMETER	DEPTH "L"*	STEEL		
			VERTICAL	HORIZONTAL	O.C.
PED	30"	7'-0"	12-#7 (6'-6")	10-#4	8.44"
2' TO 12'	30"	10'-6"	12-#7 (10'-0")	15-#4	8.42"
OVER 12' TO 20'	30"	11'-6"	12-#7 (11'-0")	16-#4	8.66"
OVER 20' TO 35'	36"	12'-6"	13-#8 (12'-0")	17-#4	8.88"
OVER 35' TO 50'	36"	13'-6"	13-#8 (13'-0")	19-#4	8.56"
OVER 50' TO 72'	42"	14'-6"	18-#8 (14'-0")	20-#4	8.74"
TWINS TO 20'	30"	16'-0"	12-#6 (15'-6")	22-#4	8.76"
TWINS OVER 20' TO 44'	36"	16'-0"	13-#8 (15'-6")	22-#4	8.76"
TWINS OVER 44' TO 50'	42"	16'-0"	18-#8 (15'-6")	22-#4	8.76"
TWINS OVER 50' TO 72'	42"	16'-6"	18-#8 (16'-0")	23-#4	8.64"



CONTROLLER CABINET MOUNTING DETAILS

NOTE:
 UNLESS OTHERWISE DIRECTED BY THE ENGINEER, CABINET ORIENTATION SHALL BE SUCH THAT THE BACK OF THE CABINET IS PARALLEL TO THE STREET AND POSITIONED TO ALLOW VISIBILITY OF THE SIGNAL DISPLAY WHILE OBSERVING THE CONTROLLER FRONT PANEL.

8. GROUND ROD - A 10' X 3/4" GROUND ROD SHALL BE INSTALLED IN THE CONCRETE PULL BOX FOR EACH POLE AND THE CONTROLLER. PAYMENT FOR THE GROUND ROD AND 1/2" NMC SHALL BE INCLUDED IN ITEM 714 FOR SIGNAL POLES AND ITEM 701 FOR THE CONTROLLER. THE CONCRETE PULL BOX AND CONDUCTOR BOX SHALL BE PAID SEPARATELY.

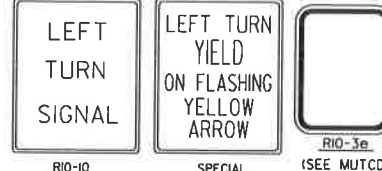
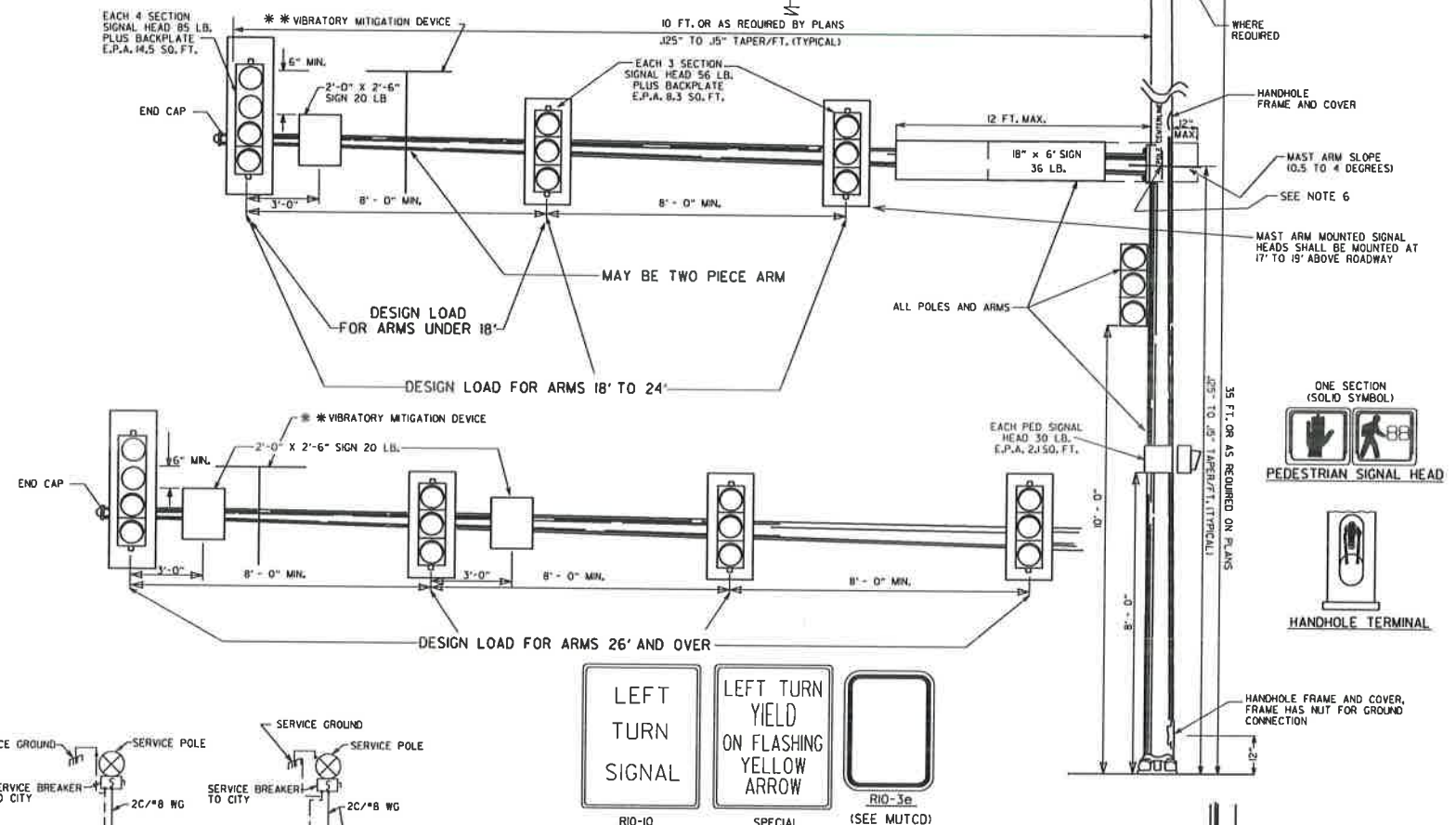
9. POLE BASE/FOUNDATION - ANCHOR BOLTS SHALL INCLUDE AS A MINIMUM, ONE LEVELING NUT, TWO FLAT WASHERS, ONE LOCK WASHER, AND ONE HEX NUT. PERIMETER OF ANCHOR BASE SHALL BE GROUDED WITH A 1/4" WEEP HOLE. ALL CONCRETE SHALL BE CLASS "S" OR GREATER.

10. CONCRETE - ALL CONCRETE FOR CONTROLLER CABINET AND POLE FOUNDATIONS SHALL BE CLASS "S" OR GREATER.

TRENCHING DETAIL
 (FOR SAW CUT TRENCH IN ROADWAY)
 * WHEN THE GROUND ELEVATION AT THE POLE IS LOWER THAN THE ROADWAY ELEVATION, THE LENGTH OF FOUNDATION ABOVE THE ROADWAY MAY BE INCREASED TO PROVIDE THE REQUIRED SIGNAL HEAD CLEARANCE ABOVE THE ROADWAY. WHEN THE REQUIRED LENGTH OF FOUNDATION ABOVE THE GROUND IS 18" OR LESS, NO INCREASE IN DEPTH "L" WILL BE REQUIRED. WHEN THE REQUIRED LENGTH OF FOUNDATION ABOVE THE GROUND IS 5'-6" OR LESS, INCREASE DEPTH "L" BY 1'-0". FOR LENGTHS GREATER THAN 5'-6", DEPTH "L" SHALL BE ADJUSTED AS DIRECTED BY THE ENGINEER. LONGITUDINAL REINFORCING, AS SHOWN IN THE TABLE, SHALL BE PROVIDED FOR THE LENGTH OF THE EXTENDED SHAFT AND #4 TIES SHALL BE PROVIDED AT A SPACING NOT TO EXCEED 9" ON CENTERS. PAYMENT WILL BE IN ACCORDANCE WITH SECTION 714 TRAFFIC SIGNAL MAST ARM AND POLE WITH FOUNDATION OF THE STANDARD SPECIFICATIONS.

** IN LIEU OF DESIGNING THE STRUCTURE TO RESIST PERIODIC GALLOPING, A VIBRATORY MITIGATION DEVICE MAY BE PROVIDED BY THE POLE MANUFACTURER. THE VIBRATORY MITIGATION DEVICE SHALL BE AN ANTI-GALLOPING PANEL CONSISTING OF A 60" X 16" X 0.125" SIGN BLANK MOUNTED NEAR THE END OF THE MAST ARM NOT TO EXCEED ONE QUARTER OF THE LENGTH OF THE MAST ARM FROM THE END OF THE MAST ARM WITH THE LONG AXIS OF THE PANEL COLLINEAR WITH THE LONG AXIS OF THE MAST ARM. THE PANEL SHOULD BE MOUNTED AT SUCH THE HEIGHT AS TO PROVIDE AT LEAST 6" CLEAR FROM THE TOP OF ANY SIGNAL ASSEMBLY OF SIGN PANEL LOCATED ON THE MAST ARM WITHIN THE LENGTH OF THE ANTI-GALLOPING PANEL.

TRUCK-INDUCED GUST LOADS SHALL BE EXCLUDED FOR FATIGUE DESIGN FOR ALL STRUCTURES EXCEPT MAST ARMS MOUNTED OVER FACILITIES WITH POSTED SPEEDS OF 65 MPH OR GREATER AT THE LOCATION OF THE STRUCTURE.



DATE	REVISION	FILMED
11-16-17	REVISED NOTES, ADDED PEDESTRIAN SIGNAL HEAD DETAIL, ADDED HANDHOLE TERMINAL DETAIL, ADDED TRENCHING DETAIL	
02-27-14	REVISED NOTES	
09-12-13	ISSUED AS STANDARD DRAWING	
12-08-16	REVISED NOTES	
02-27-14	REVISED NOTES	
09-12-13	ISSUED AS STANDARD DRAWING	
07-21-11	REVISED VMD, SIGNAL HEADS	
05-21-09	REVISED GROUNDING	
07-31-08	REVISED GROUNDING	
04-25-08	ADDED VIBRATORY MITIGATION DEVICE & NOTES	
04-18-08	REVISED AASHTO NOTES	
04-17-08	REVISED TO 2001 AASHTO STANDARDS	
10-12-04	REVISED CABINET ORIENTATION	
06-23-04	REVISED	
05-11-04	REV. NOTE 3/AASHTO REQUIREMENTS	
06-11-01	REV. NOTES & POLE MAST ARM SLOPE	
04-11-01	REVISED POLE TAPERS	
04-25-00	REV. NOTES & SIGNAL HEAD PLACEMENT	
11-22-99	REVISED FOUNDATION DETAILS	
11-17-98	REVISED DETAILS AND NOTES	
11-21-95	ISSUED	

ARKANSAS STATE HIGHWAY COMMISSION



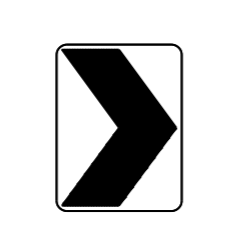



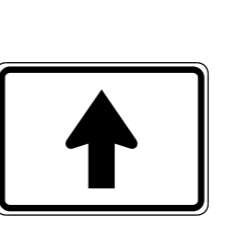
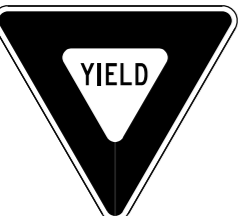

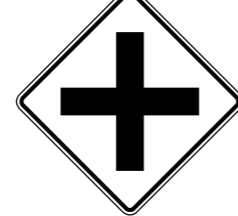



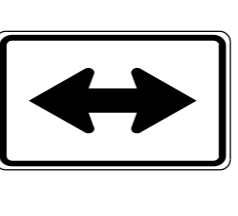


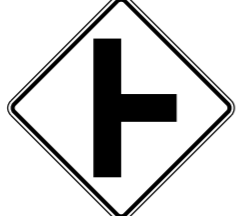



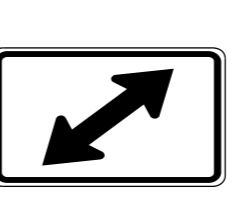

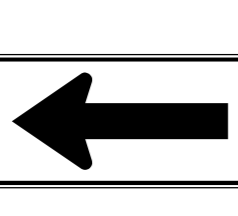
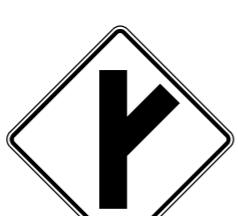

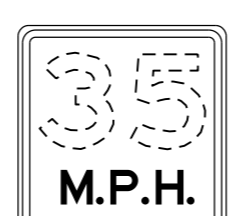
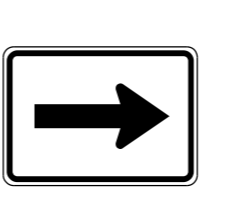
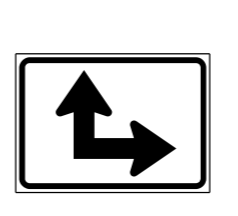
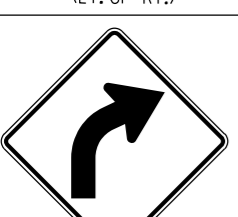
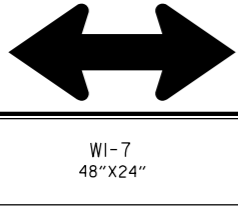
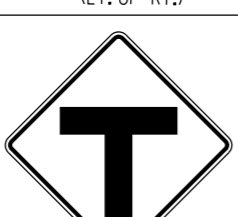

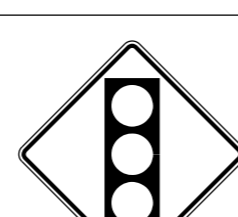

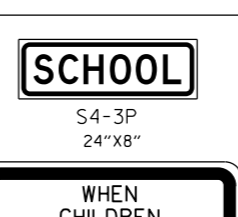

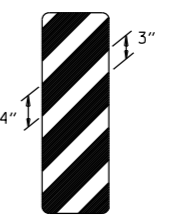
STEEL POLE WITH MAST ARM

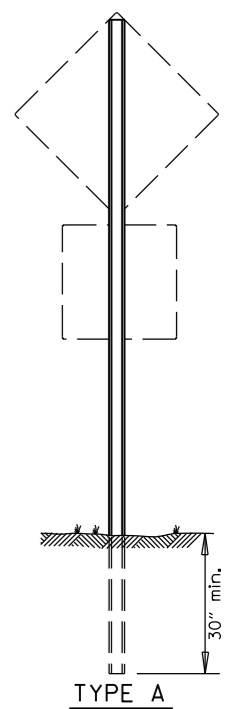
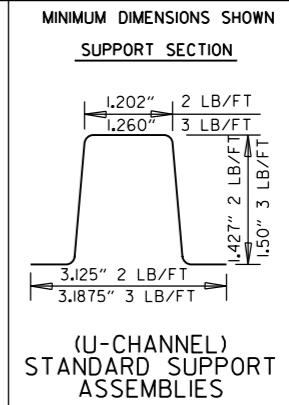
STANDARD DRAWING SD-11

SIGNAL OPERATION NOTES:
 FLASHING OPERATION - PRIOR TO NORMAL OPERATION, SIGNAL SHALL BE FLASHED FOR A PERIOD OF 3 TO 5 WORK DAYS OR AS DIRECTED BY THE ENGINEER. SIGNAL SHALL BE PLACED IN OPERATION ONLY ON A REGULAR WORK DAY, EXCEPT FRIDAY.

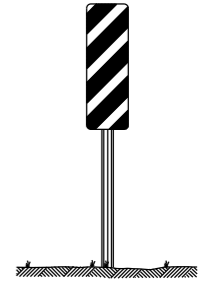
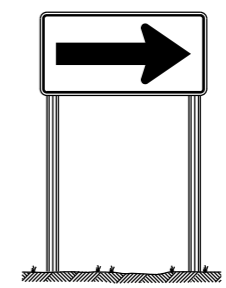
THE CONTRACTOR MAY BE REQUIRED TO ALTER THE FLASHING DISPLAY DURING THE TEMPORARY FLASH PERIOD, AT THE TIME THE INTERSECTION IS PLACED IN PERMANENT OPERATION, THE FLASH SEQUENCE SHALL THEN BE RETURNED TO THAT INDICATED ON THE PLAN SHEETS. NO ADDITIONAL COMPENSATION SHALL BE ALLOWED FOR THESE ALTERATION IN FLASH SEQUENCE.

SPECIAL NOTE: 90 MPH WIND ZONE DESIGN, SEE NOTE 3. MINIMUM STRUCTURAL REQUIREMENTS.

 RI-1 30"x30"	 W1-3 30"x30" (LT. OR RT.)	 W1-8 18"x24"	 W2-5 30"x30"	 W3-1 36"x36"	 W5-1 36"x36"	 M6-3 21"x15"
 RI-2 36"x36"x36"	 W1-4 30"x30" (LT. OR RT.)	 W2-1 30"x30"	 SI-1 36"x36"	 W3-2 36"x36"	 LASSEN 16 COUNTY County Route Marker MI-6 24"x24"	 M6-4 21"x15"
 R2-1 24"x30"	 W1-5 30"x30" (LT. OR RT.)	 W2-2 30"x30"	 NARROW BRIDGE W5-2 36"x36"	 PAVEMENT ENDS W8-3 36"x36"	 NOTE: REFLECTORIZED YELLOW LEGEND (COUNTY NAME, ROUTE LETTER & NUMBER) & BORDER ON A BLUE BACKGROUND. ALL WAY RI-3P 18"x6"	 M6-5 21"x15"
 W1-1 30"x30" (LT. OR RT.)	 W1-6 48"x24"	 W2-3 30"x30" (LT. OR RT.)	 ONE LANE BRIDGE W5-3 36"x36"	 35 M.P.H. W13-IP 18"x18"	 M6-1 21"x15"	 M6-6 21"x15"
 W1-2 30"x30" (LT. OR RT.)	 W1-7 48"x24"	 T W2-4 30"x30"	 R X R W10-1 36" DIAMETER	 W3-3 36"x36"	 M6-2 21"x15"	 SCHOOL S4-3P 24"x8"
					 WHEN CHILDREN ARE PRESENT S4-2P 24"x10"	 OM-3 12"x36" (LT. OR RT.)



NOTE: LENGTH OF SIGN POSTS SHALL BE DETERMINED SO AS TO PROVIDE FOR MINIMUM VERTICAL CLEARANCES AS CALLED FOR IN THE SPECIFICATIONS PLUS A MINIMUM VERTICAL PENETRATION OF 30" IN THE SOIL.

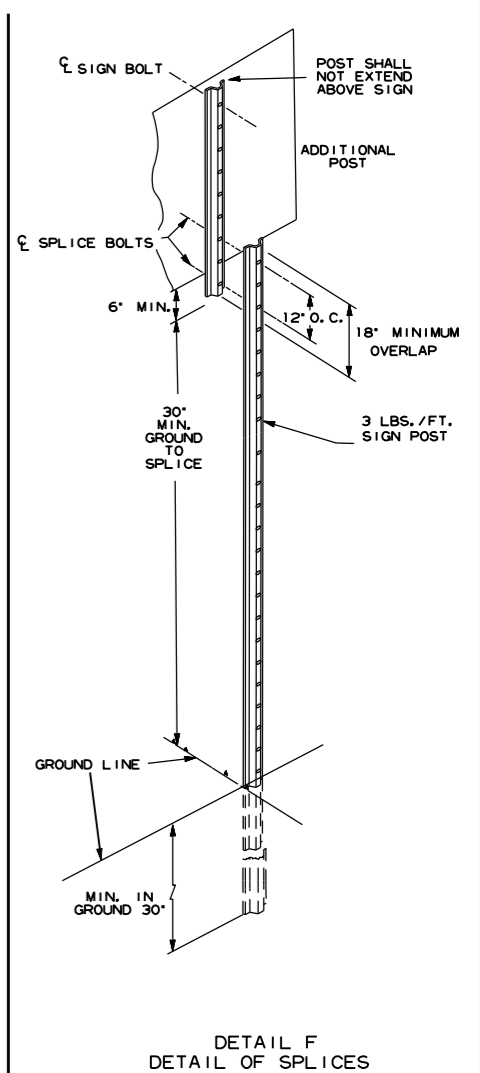
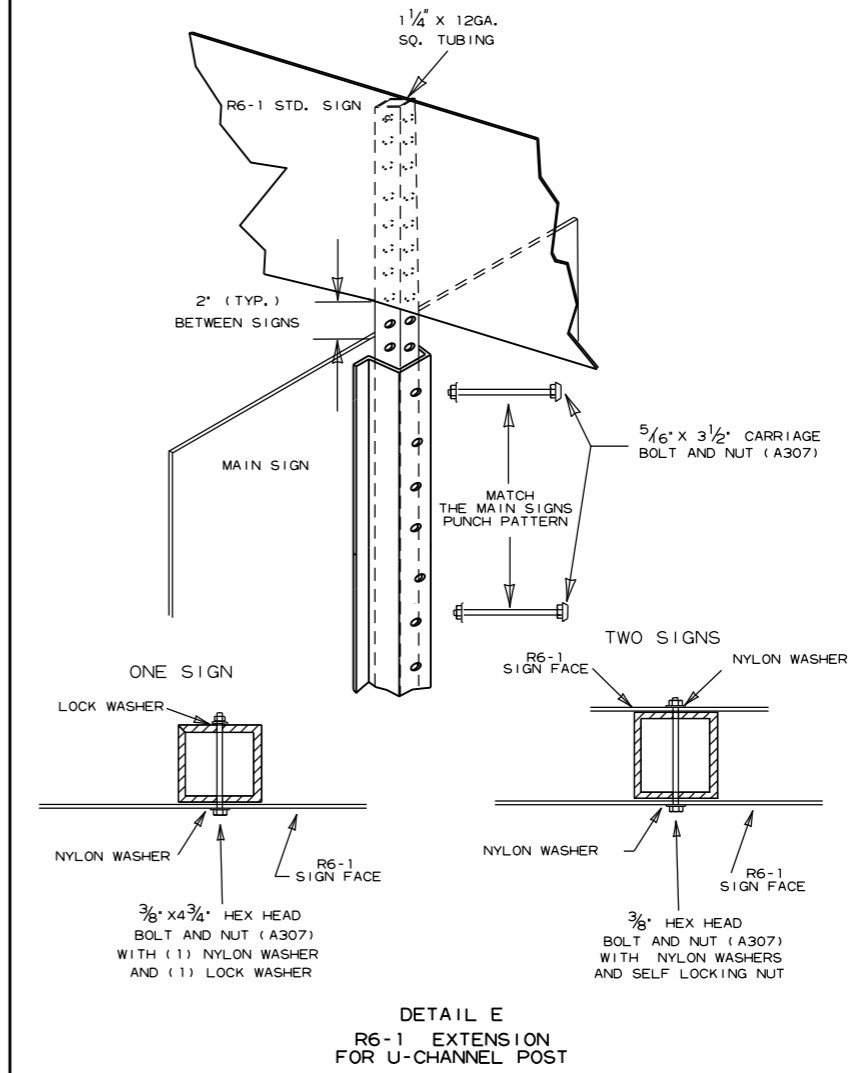
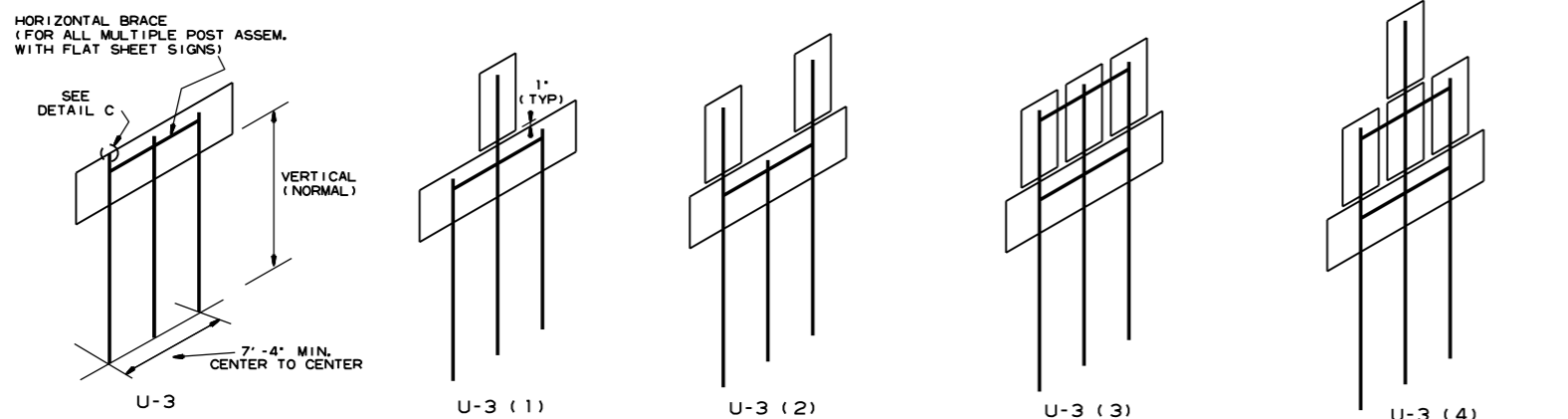
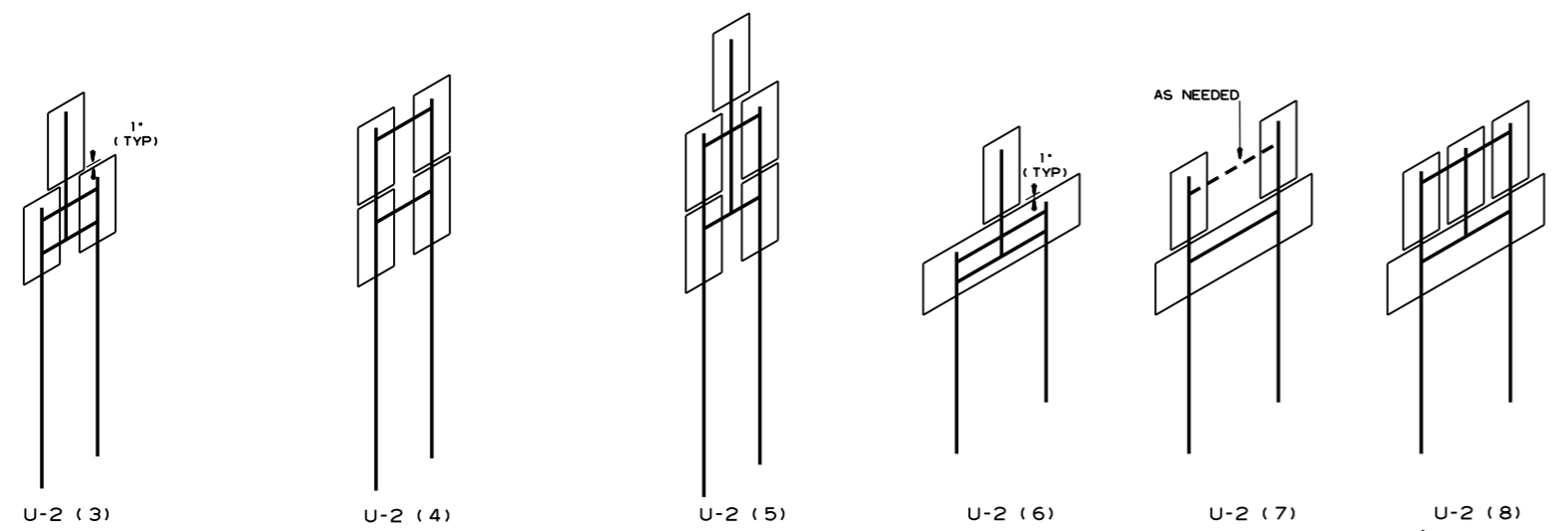
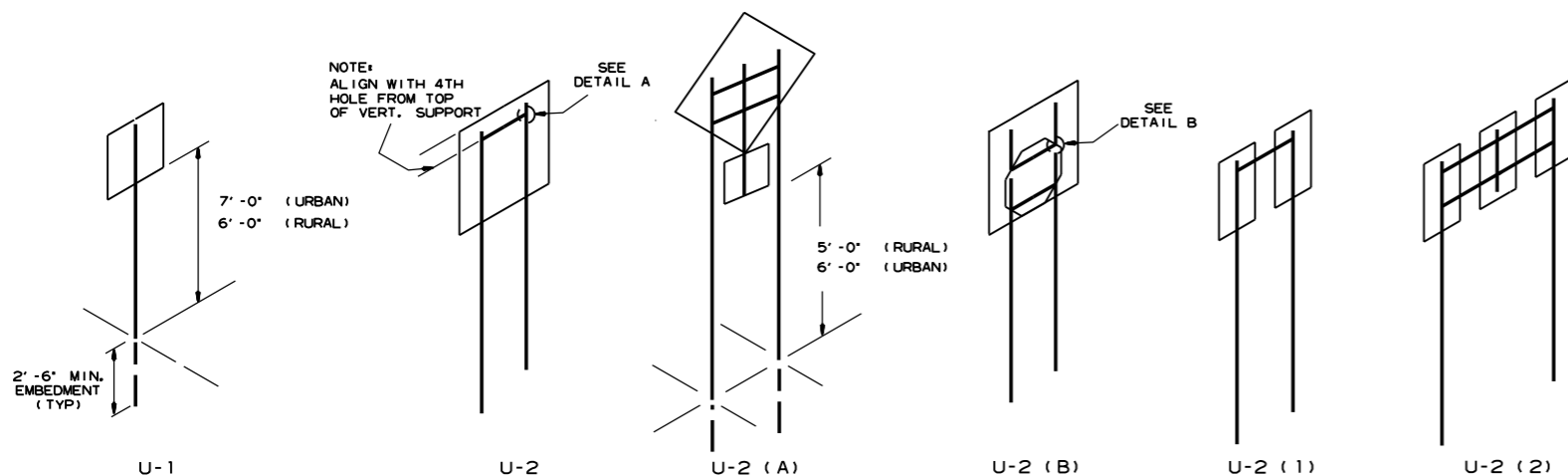


MINIMUM WEIGHT
TYPE A & B = 3 LBS./FT.
TYPE C = 2 LBS./FT.

STANDARD HIGHWAY SIGNS

9-12-13	DELETED JOB NO. BLOCK; REVISED RI-3 TO RI-3P	
4-17-08	REVISED SIGN DESIGNATION - W3-1 & W3-2	
4-10-03	REVISED W5-2, W8-3, OM-3; ADDED W1-8	
1-5-81	REDRAWN	960-1-15-81
9-15-78	ADDED W14-3	877-9-15-78
9-2-76	POST WT.	623-9-3-76
5-3-76	STEEL POST WT. FROM 2"-3"; ADDED S4-2 & S4-3	504-5-3-76
8-12-74	REV. HT. TYPE "C" ASSEMBLY	500-8-21-74
12-21-72	ADDED M6-2,3,4,5,6	500-12-21-72
12-1-72	ISSUED	562-12-1-72
DATE	REVISION	DATE FILMED

SUPPORT ASSEMBLIES
ARKANSAS STATE HIGHWAY COMMISSION
STANDARD HIGHWAY SIGNS
AND SUPPORT ASSEMBLIES
STANDARD DRAWING SHS-1



NOTES:

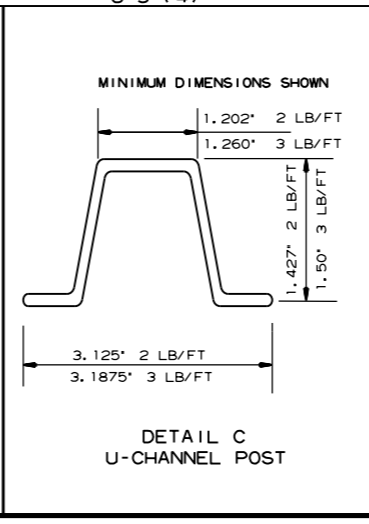
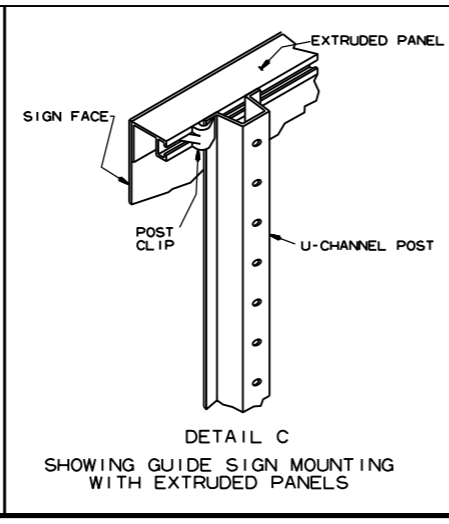
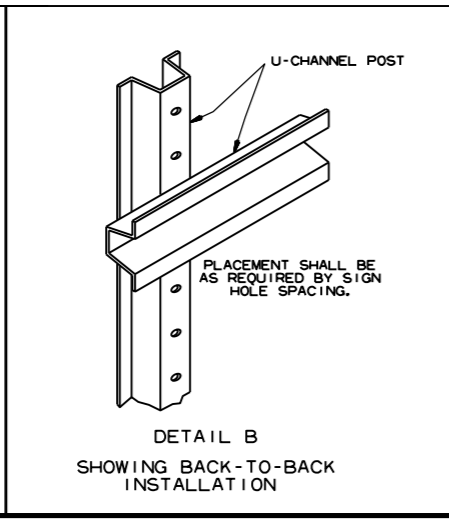
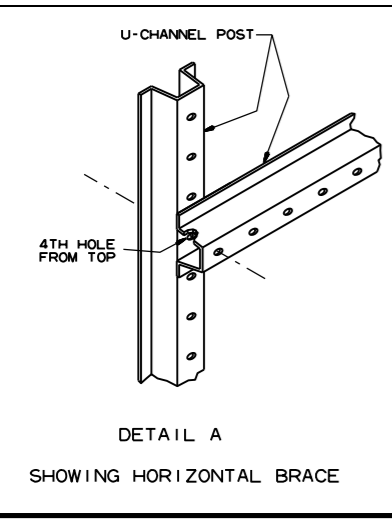
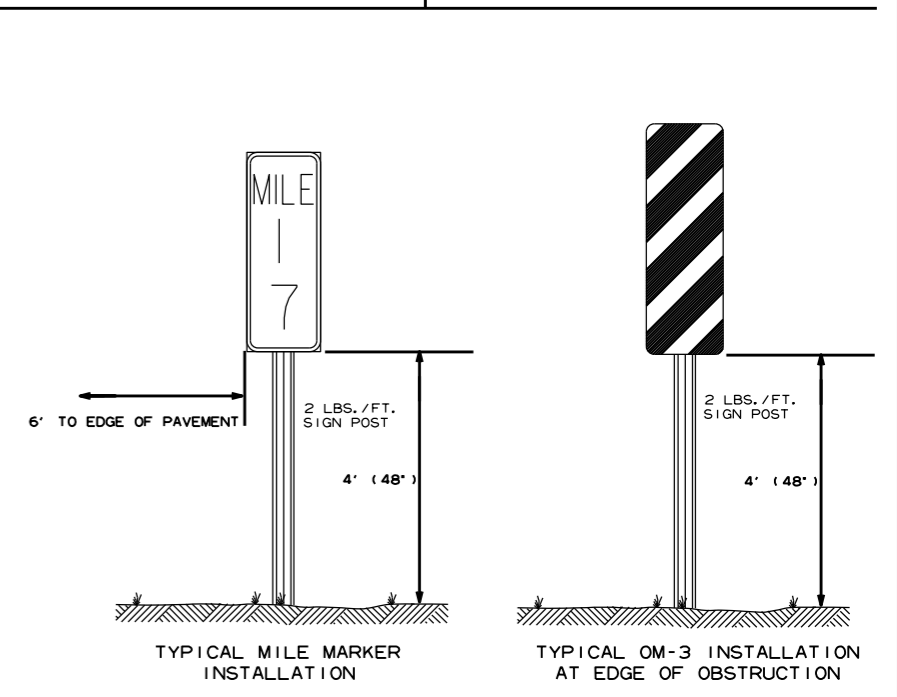
SIGNS AT LEAST 8' IN LENGTH MAY BE INSTALLED ON THREE 3 LB. POST. IN NO CASE SHALL THERE BE MORE THAN TWO 3 LB. POSTS WITHIN A 7' PATH.

SPLICES NECESSARY TO ATTAIN PROPER MOUNTING HEIGHT SHALL BE AS SHOWN IN DETAIL (F).

NORMAL INSTALLATIONS WILL REQUIRE 3/8" DIA. CARRIAGE BOLTS TO MOUNT SIGNS TO POST AND TO ASSEMBLE THE VARIOUS POST SUPPORTS.

ALL SIGN POSTS SHALL BE PLUMB.

THE POST FOR *TYPE U* SUPPORTS SHALL BE HOT DIP GALVANIZED.

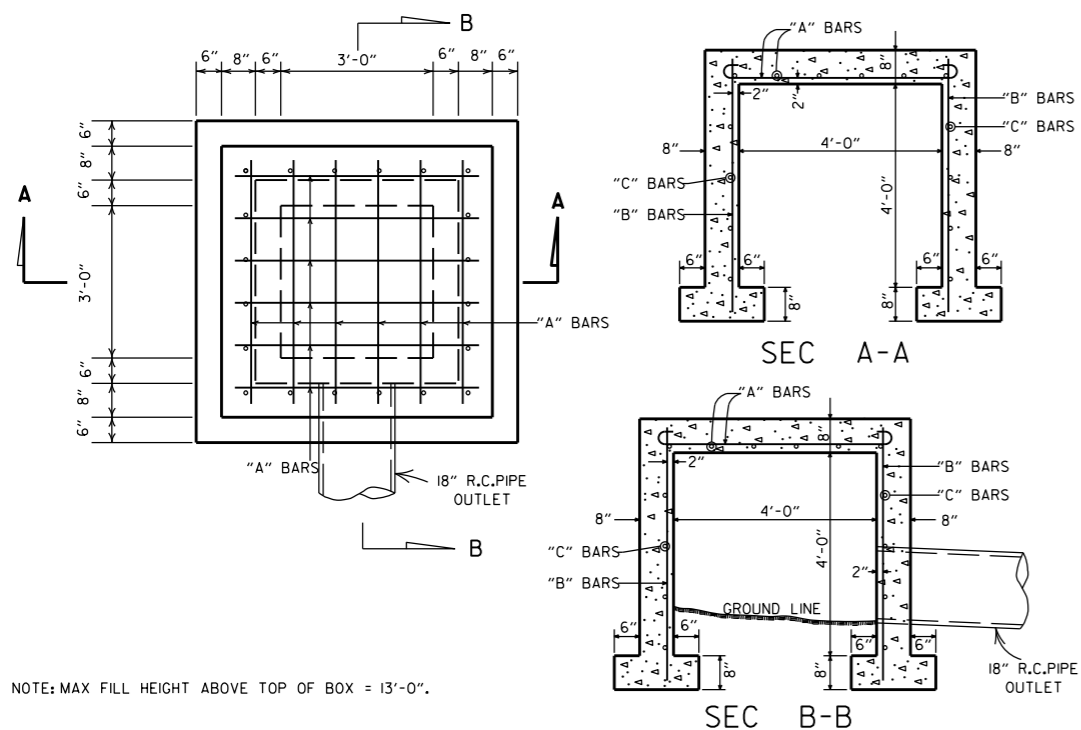


7-25-19	REVISED CARRIAGE BOLT WITH MATERIAL REQUIREMENT	
2-27-14	REVISED NOTES.	
9-12-13	REVISED U-2(3), U-2(6), U-3(1), DETAIL D; ADDED DETAILS E & F; ADDED TYPICAL MARKERS	
10-9-03	REMOVED ROUND POST & REVISED SPACING	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL	6-8-95
2-2-95	REDRAWN	2-2-95
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION

U-CHANNEL POST ASSEMBLIES

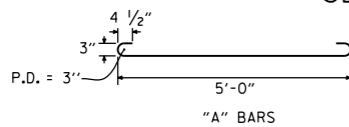
STANDARD DRAWING SHS-195



NOTE: MAX FILL HEIGHT ABOVE TOP OF BOX = 13'-0".

STEEL SCHEDULE			
BARS	NUMBER	LENGTH	SPACING
"A"	12	6'-0"	10"
"B"	20	5'-0"	10 1/2"
"C"	16	5'-0"	12"

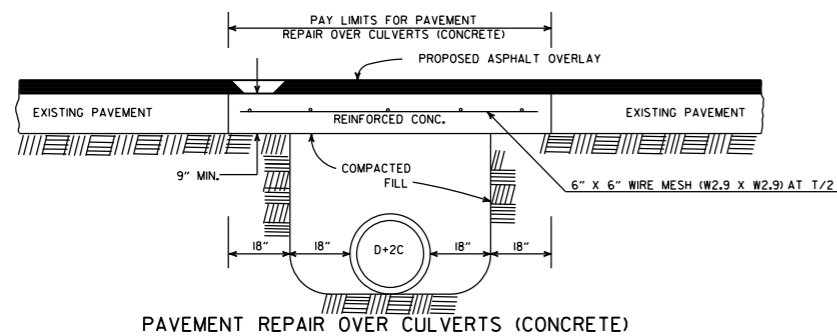
ALL STEEL TO BE #4 BARS



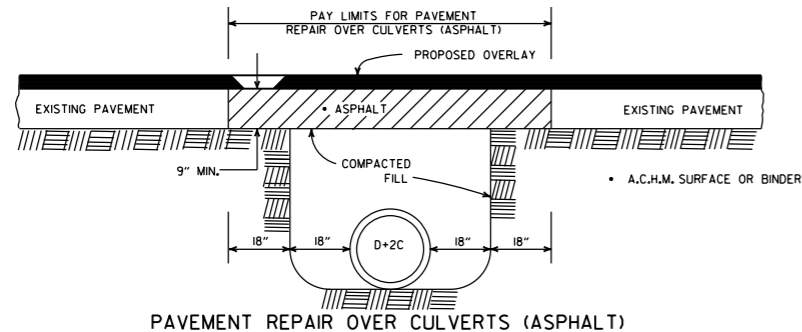
QUANTITIES
 "A" BARS
 CONCRETE 3.31 CU. YDS.
 REINFORCING STEEL 168 LB.

GENERAL NOTE:
 THE PAY ITEMS FOR REINFORCED CONCRETE SPRING BOXES SHALL BE FOR THE QUANTITIES OF CONCRETE OF THE CLASS SPECIFIED, REINFORCING STEEL, EXCAVATION FOR STRUCTURES AND 18" R.C. PIPE CULVERT.

REINFORCED CONCRETE SPRING BOX

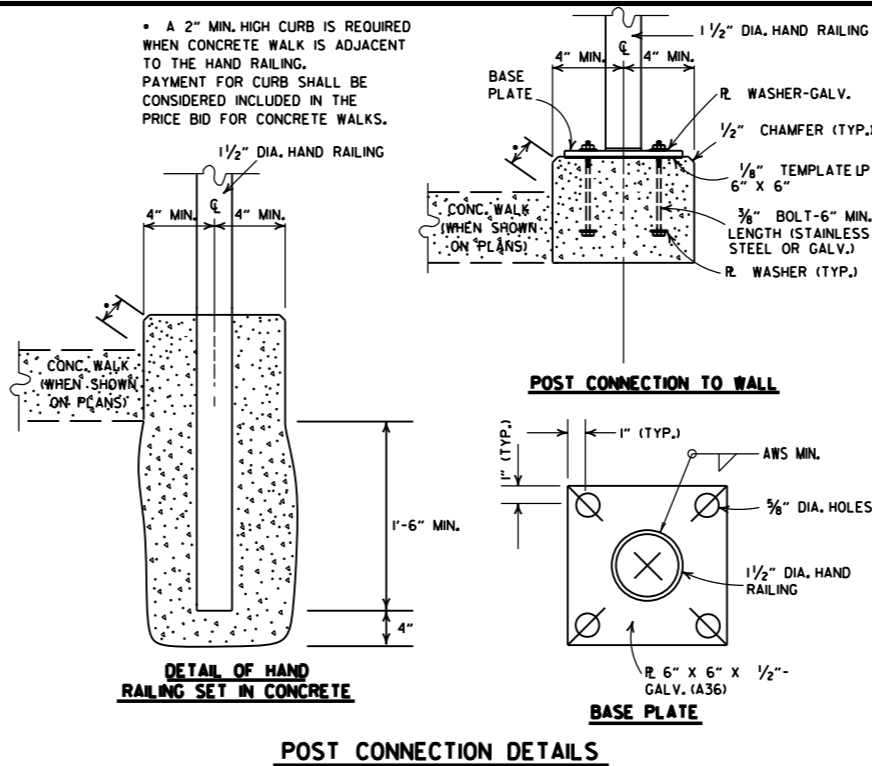


PAVEMENT REPAIR OVER CULVERTS (CONCRETE)



PAVEMENT REPAIR OVER CULVERTS (ASPHALT)

DETAIL SHOWING REPAIR OF EXISTING PAVEMENT AT CULVERT INSTALLATIONS

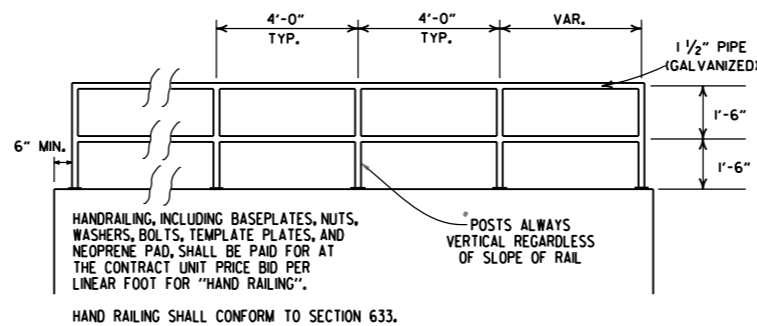


DETAIL OF HAND RAILING SET IN CONCRETE

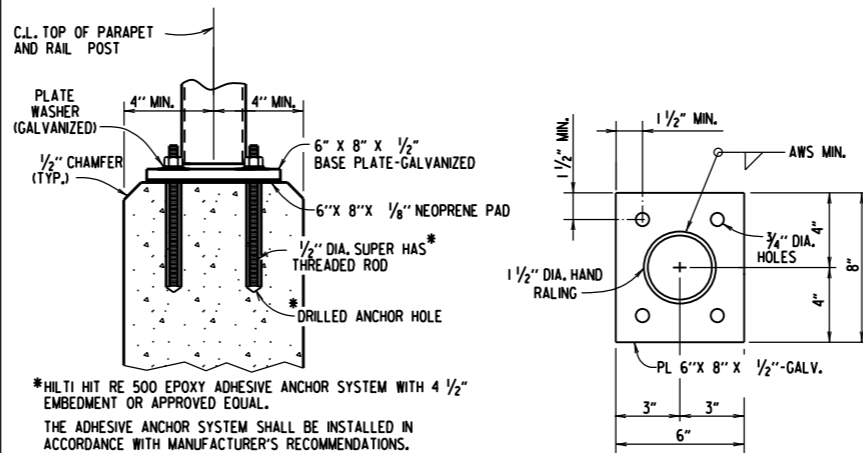
POST CONNECTION TO WALL

BASE PLATE

POST CONNECTION DETAILS



HAND RAILING SHALL CONFORM TO SECTION 633.

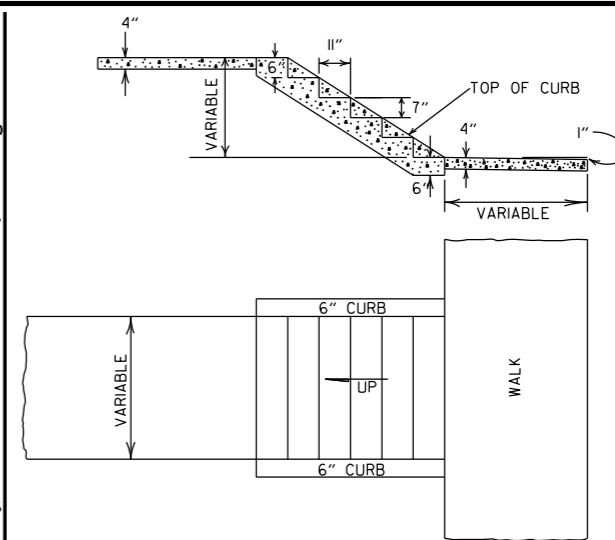


POST CONNECTION TO WALL

BASE PLATE

DETAILS OF ALTERNATE POST ANCHOR SYSTEM (EPOXY ADHESIVE ANCHORS)

HAND RAILING DETAILS



DETAILS OF CONCRETE STEPS & WALKS


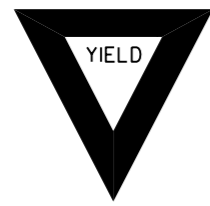







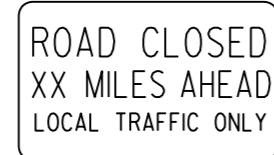
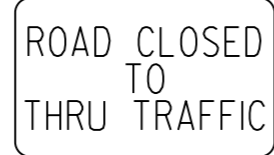

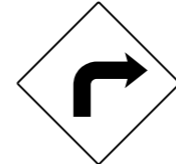



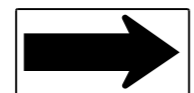

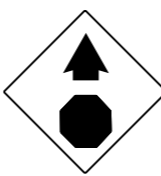
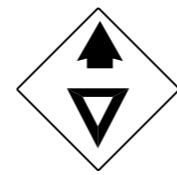
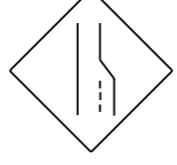

















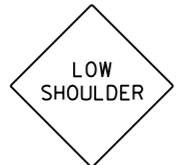
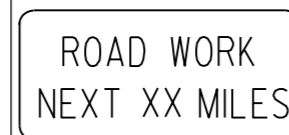
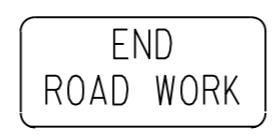
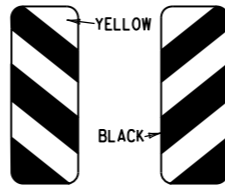


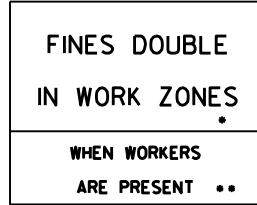
GENERAL NOTES
 1. RISE AND TREAD DIMENSIONS OF STEPS MAY BE VARIED AS DIRECTED BY THE ENGINEER, HOWEVER, TREAD WIDTHS SHALL BE 11" MIN. ALL STEPS IN A FLIGHT SHALL HAVE CONSISTENT TREAD & RISER DIMENSIONS.
 2. 1" TRANSVERSE EXPANSION JOINTS SHALL BE PLACED IN CONCRETE WALKS AT 45' INTERVALS.

DATE	REVISION	DATE FILMED
10-25-18	REVISED DETAIL SHOWING REPAIR OF EXISTING PAVEMENT AT CULVERT INSTALLATIONS	
9-12-13	REVISED REINFORCED CONCRETE SPRING BOX	
7-26-12	REMOVED RETAINING WALL DETAILS & REVISED HAND RAILING DETAILS	
4-17-08	REV. JOINT & FOOTING STEP DETAILS	
11-29-07	REVISED RETAINING WALL DRAINAGE	
5-25-06	REVISED PVMT REPAIR OVER CULVERTS (CONC); REVISED REINFORCED CONG SPRING BOX	
10-9-03	REVISED PIPE RAILING DETAILS TO HAND RAILING DETAILS	
4-10-03	REVISED RETAINING WALL DRAWING	
8-22-02	ADDED HAND RAILING DETAIL	
11-16-01	REVISED PVMT REPAIR OVER CULVERTS (CONC); CORRECTED SPELLING IN GENERAL NOTES	
11-18-98	ADDED GENERAL NOTES TO CONCRETE STEPS & WALKS	
7-02-98	ENLARGED PIPE	
4-03-97	ADDED NOTE TO STEEL BAR SCHED.	
10-18-96	CORRECTED SPELLING	
4-26-96	ADD WEEP HOLE; REV. JOINT SPACING IN RET. WALL	
6-2-94	CHANGED CONST. TO CONTRACTION JOINT	
10-1-92	CHANGED MESH FABRIC TO WIRE MESH	10-1-92
8-15-91	DELETED HDWL MODIFICATION DETAIL	8-15-91
11-8-90	DELETED COLD MIX FROM CULV'T. REPAIR	11-8-90
11-30-89	REV. RETAINING WALL STEEL SCHEDULE	11-30-89
11-17-88	V. BARS BEHIND ARROW	665-11-17-88
7-15-88	REV. PAVEMENT REPAIR	649-7-15-88
11-1-84	ADDED HDWL. MODS, DEL. PIPE UNDERDRAINS	
1-4-83	REV. TRENCH FOR PIPE UNDERDRAIN	510-11-1-84
	ELIMINATED CONG. CLASS & ADDED CHAMFER NOTE	682-1-4-83
3-2-81	SPELLING OF "UNDERDRAIN"	721-3-2-81
4-20-79	REV. UNDERDRAIN DET & PAVEMENT REPAIR	674-4-20-79
2-2-76	12" MIN. GRAN. MAT'L. OVER PIPE	919-2-2-76
4-10-75	REM. SPECS. FOR GRAN. MAT'L.	568-4-10-75-853
5-22-74	GRANULAR MAT'L. TO BE SB-3	567-5-22-74-740
10-2-72	REVISED AND REDRAWN	564-10-16-72

ARKANSAS STATE HIGHWAY COMMISSION

DETAILS OF SPECIAL ITEMS

STANDARD DRAWING SI - 1

<p>RI-1</p>  <p>STANDARD 30"x30" EXPRESSWAY 36"x36" SPECIAL 48"x48"</p>	<p>RI-2</p>  <p>STD. 36"x36"x36" EXPWY. 48"x48"x48" FWY. 60"x60"x60"</p>	<p>R2-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>W3-5</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>W3-5a</p>  <p>STD. 36"x36" EXPWY. 48"x48" FWY. 48"x48"</p>	<p>R4-1</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	<p>R4-2</p>  <p>STD. 24"x30" EXPWY. 36"x48" FWY. 48"x60"</p>	
<p>R5-1</p>  <p>STD. 30"x30" EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>R11-2</p>  <p>48"x30"</p>	<p>R11-3A</p>  <p>60"x30"</p>	<p>R11-4</p>  <p>60"x30"</p>	<p>W21-5a</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-1</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>WI-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>WI-3</p>  <p>STD. 48"x48"</p>	<p>WI-4</p>  <p>STD. 48"x48"</p>	<p>WI-6</p>  <p>STD. 48"x24" SPECIAL 60"x30"</p>	<p>WI-8</p>  <p>STD. 18"x24" SPECIAL 24"x30" EXPWY. 30"x36" FWY. 36"x48"</p>	<p>W3-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W3-2</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W4-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	
<p>W5-1</p>  <p>STD. 36"x36" SPECIAL 48"x48"</p>	<p>W6-3</p>  <p>EXPWY. 36"x36" SPECIAL 48"x48"</p>	<p>W8-7</p>  <p>EXPWY. 36"x36" FWY. 48"x48"</p>	<p>W9-2</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W13-1</p>  <p>STD. 24"x24"</p>	<p>W20-1</p>  <p>STD. 48"x48"</p>	<p>W20-2</p>  <p>STD. 48"x48"</p>	<p>W20-3</p>  <p>STD. 48"x48"</p>
<p>W20-4</p>  <p>STD. 48"x48"</p>	<p>W20-5</p>  <p>STD. 48"x48"</p>	<p>W20-7a</p>  <p>18" 500 FEET 24" W16-2</p> <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W21-2</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W21-5</p>  <p>STD. 30"x30" SPECIAL 36"x36"</p>	<p>W24-1</p>  <p>STD. 36"x36"</p>	<p>WI-4b</p>  <p>STD. 48"x48"</p>	<p>R56-1</p>  <p>STD. 18"x18"</p>
<p>W8-11</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>W8-9</p>  <p>STD. 36"x36" FWY. 48"x48"</p>	<p>G20-1</p>  <p>60"x24"</p>	<p>G20-2</p>  <p>48"x24"</p>	<p>OM-3L OM-3R</p>  <p>12"x36"</p>	<p>M4-9</p>  <p>STD. 30"x24" SPECIAL 48"x36" SPECIAL 60"x48"</p>	<p>M4-10</p>  <p>48"x18"</p>	<p>R55-1</p>  <p>36"x60"</p> <p>• USE 6" C LETTERS •• USE 4" D LETTERS</p>

ADVANCE DISTANCES
(XXXX)

500 FT	1/2 MILE
1000 FT	3/4 MILE
1500 FT	1 MILE AHEAD

GENERAL NOTES:

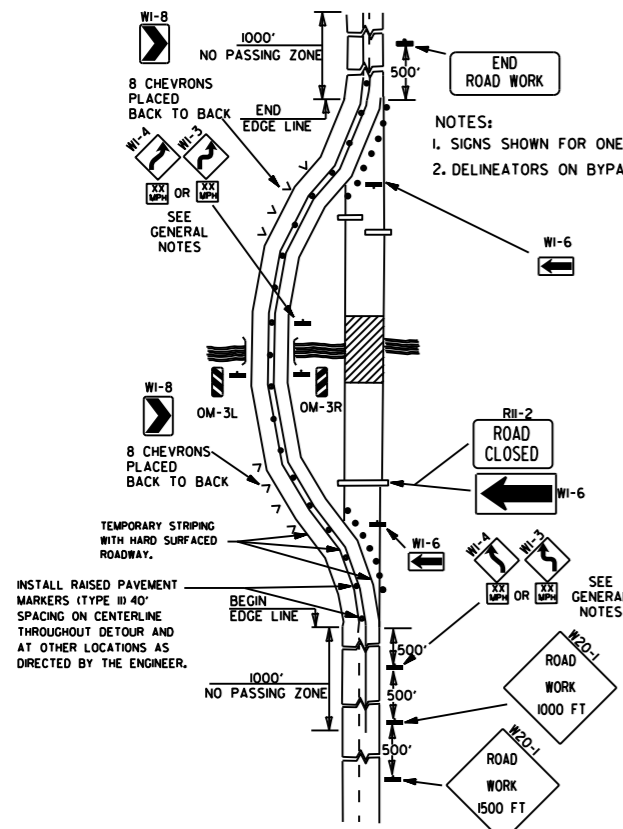
- ALL TRAFFIC CONTROL DEVICES USED ON ROAD CONSTRUCTION SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION, AND TO THE STANDARD HIGHWAY SIGNS, LATEST EDITION, OR AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION.
- TRAFFIC CONTROL DEVICES SHALL BE SET UP JUST BEFORE THE START OF CONSTRUCTION OPERATIONS AND SHALL BE PROPERLY MAINTAINED DURING THE TIME SUCH CONDITIONS EXIST. THEY SHALL REMAIN IN PLACE ONLY AS LONG AS NEEDED AND REMOVED THEREAFTER.
- EXISTING SIGNS AND CONSTRUCTION SIGNS SHALL BE KEPT IN PROPER POSITION, AND BE CLEAN AND LEGIBLE AT ALL TIMES. SIGNS THAT DO NOT APPLY TO EXISTING CONDITIONS SHALL BE REMOVED. SIGNS THAT ARE DAMAGED, DEFACED, OR THAT ACCUMULATE DIRT DURING CONSTRUCTION SHALL BE CLEANED, REPAIRED, OR REPLACED.
- SIGNS ARE USUALLY MOUNTED ON A SINGLE POST, ALTHOUGH THOSE WIDER THAN 36" OR LARGER THAN 10 SQ. FT. SHALL BE MOUNTED ON TWO POSTS OR ABOVE A TYPE III BARRICADE.
- SIGN POSTS DIRECT BURIED IN SOIL SHALL BE 2 LB. MINIMUM CHANNEL POST OR 4"x4" WOOD POSTS. CHANNEL POSTS SHALL BE PAINTED GREEN. WOOD POSTS SHALL BE PAINTED WHITE. ALL POSTS SHALL BE NEATLY CONSTRUCTED, AND SHALL BE REPLUMBED, CLEANED, OR REPAIRED AS NEEDED FOR THE DURATION OF THE JOB. THERE SHALL NOT BE MORE THAN 2 POSTS IN A 7' PATH FOR WOOD OR CHANNEL POSTS. ANY CHANNEL POST SPLICE SHALL BE IN ACCORDANCE WITH STANDARD DRAWING TC-3.
- POST MOUNTED SIGNS IN RURAL AREAS SHALL BE CONSTRUCTED WITH THE NEAR EDGE OF THE SIGN FROM 6 TO 12 FEET FROM THE PAVEMENT EDGE. SIGNS IN URBAN AREAS AND BARRICADE MOUNTED SIGNS SHALL BE MOUNTED A MINIMUM OF 2 FEET FROM THE PAVEMENT EDGE.
- ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN URBAN AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE. ALL POST AND BARRICADE MOUNTED SIGNS MOUNTED IN RURAL AREAS SHALL BE MOUNTED A MINIMUM DISTANCE OF 7' FROM THE BOTTOM OF THE SIGN TO THE ROADWAY SURFACE, EXCEPT A MINIMUM OF 6' SHALL BE USED WHEN MOUNTING AN ADVISORY SIGN BELOW A WARNING SIGN. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR INTERMEDIATE TERM STATIONARY WORK CONDITIONS. THE SIGNS MINIMUM MOUNTING HEIGHT SHALL BE 5'. RETROREFLECTIVE DEVICES SHALL BE USED. TEMPORARY SIGNS MAY BE MOUNTED ON PORTABLE SUPPORTS FOR SHORT-TERM, SHORT DURATION, AND MOBILE CONDITIONS. THEY SHALL BE NO LESS THAN ONE (1) FOOT ABOVE THE TRAVELED WAY. LONG-TERM STATIONARY SIGNS SHALL BE DIRECT BURIED IN SOIL, UNLESS CONDITIONS NECESSITATE THE USE OF PORTABLE SIGNS, OR AS APPROVED BY THE ENGINEER. CONCRETE PADS, CONCRETE OR ROCK BALLAST, OR OTHER SOLID MATERIALS SHALL NOT BE UTILIZED WITH PORTABLE SIGN SUPPORTS.
- FLAGGERS SHALL USE REFLECTORIZED STOP-SLOW PADDLES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
- MOST OF THE SIGNS SHOWN ARE ORIENTED TO THE RIGHT. HOWEVER, THIS DOES NOT PRECLUDE THE USE OF MIRROR IMAGES OF THESE SIGNS WHERE THE REVERSE ORIENTATION MIGHT BETTER CONVEY TO MOTORISTS THE PROPER DIRECTION OF MOVEMENT.
- R55-1 SIGNS SHALL BE PLACED AT LEAST 1500' BUT NOT MORE THAN 1 MILE IN ADVANCE OF THE WORK ZONE. IF A SPEED LIMIT REDUCTION IS IN EFFECT, THE SIGN SHALL BE PLACED A MINIMUM OF 500' IN ADVANCE OF THE "REDUCED SPEED AHEAD" SIGN.

• NOTE: SUPPORTS FOR SIGNS, BARRICADES, AND VERTICAL PANELS THAT ARE DIFFERENT FROM THE REQUIREMENTS SHOWN IN NOTES 4 & 5, BUT MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), WILL BE ACCEPTED. COMPLIANCE WITH THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) IS REQUIRED FOR ALL PROJECTS.

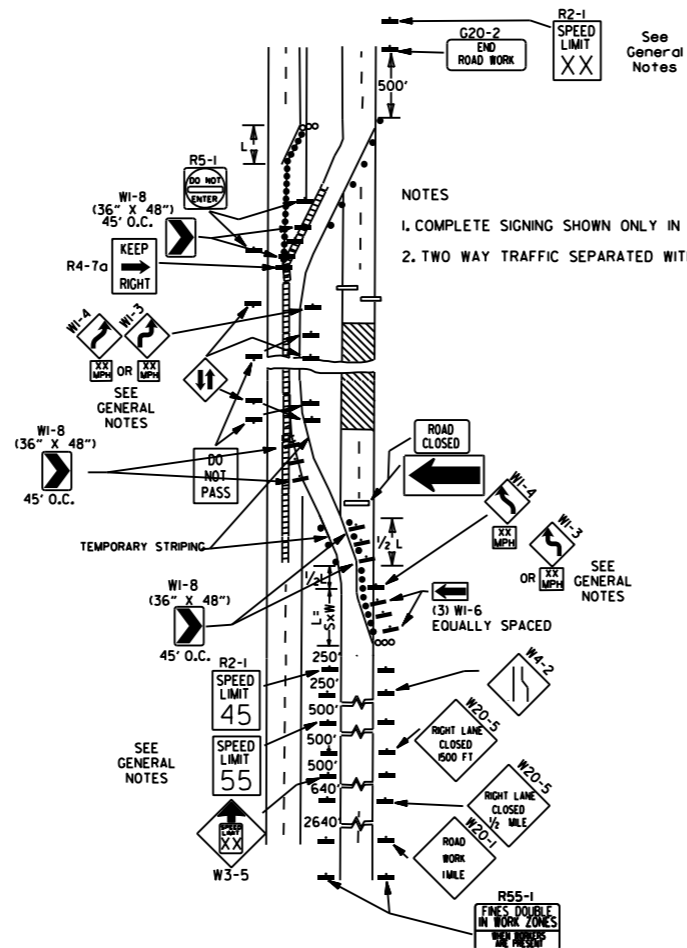
DATE	REVISION	FILMED
11-07-19	REVISED FOR MASH	
4-13-17	DELETED RSP-1 & ADDED W21-5a	
9-2-15	REVISED REDUCED SPEED LIMIT AHEAD SIGNS REVISED ROAD WORK NEXT XX MILES	
12-15-11	REVISED W24-1	
11-17-10	DELETED W8-9a & ADDED W8-9	
10-15-09	ADDED REFERENCE TO MASH & ADDED SIGN W24-1	
4-17-08	REVISED SIGN DESIGNATIONS	
11-18-04	REVISED NOTES	
10-9-03	REVISED NOTE 1	
11-16-01	REVISED NOTE 7	
9-28-00	REVISED NOTE	
11-18-98	ADDED NOTE	
6-26-97	REVISED NOTE 5	
4-03-97	REVISED NOTE 5	
10-18-96	ADDED CONTROLLED ACCESS HWY. SIGN & TO NOTE 7	
10-12-95	ADDED R55-1	
6-8-95	REVISED TO CORRECT SIGN ILLUSTRATIONS	6-8-95
2-2-95	REVISED PER PART VI, MUTCD SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

ARKANSAS STATE HIGHWAY COMMISSION
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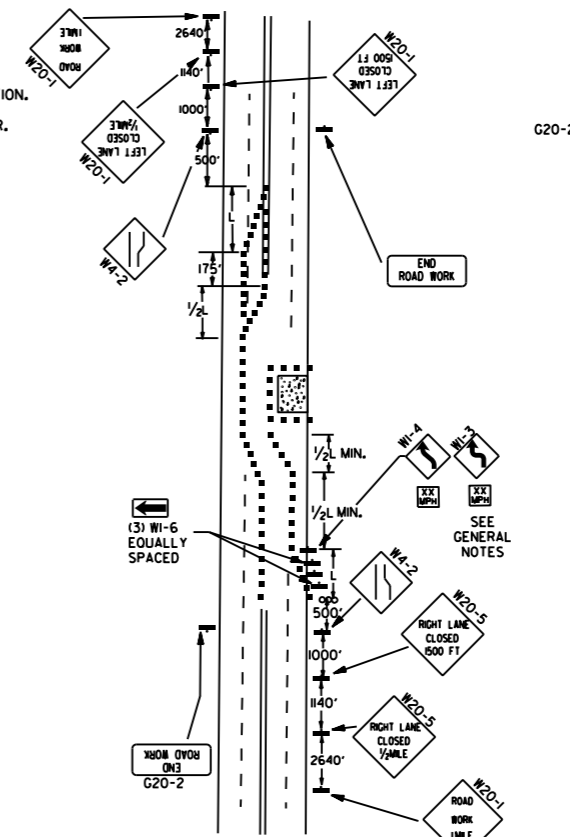
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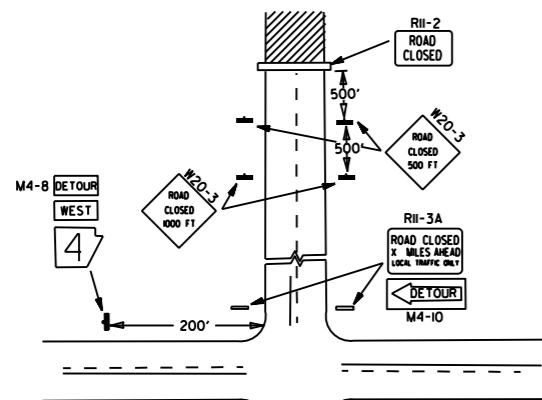
(A) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON A 2-LANE HIGHWAY WHERE THE ENTIRE ROADWAY IS CLOSED AND A BYPASS DETOUR IS PROVIDED.



(B) TYPICAL APPLICATION - 4-LANE DIVIDED ROADWAY WHERE ONE ROADWAY IS CLOSED.

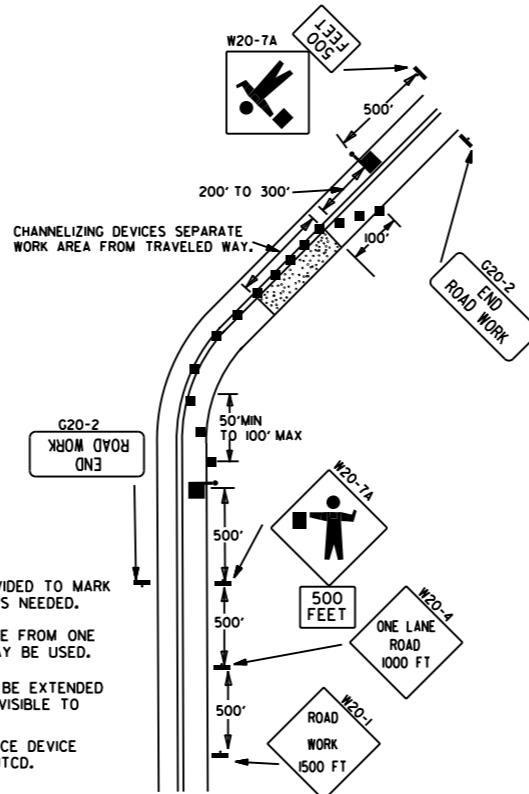


(C) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.



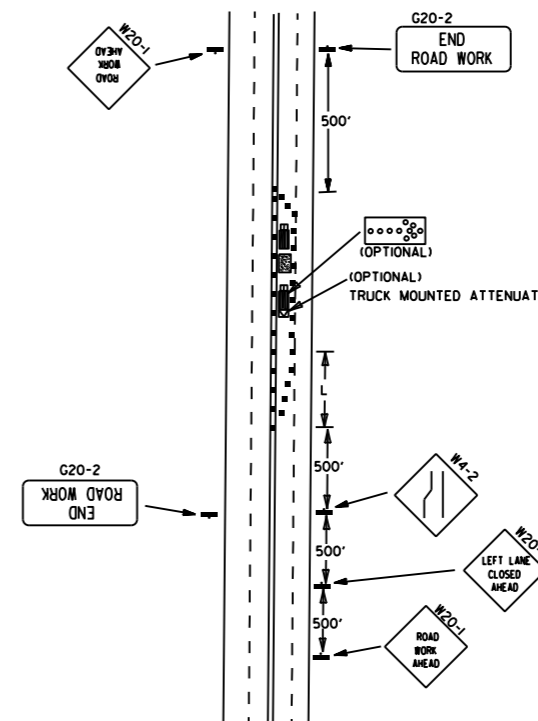
NOTES:
 1. REGULATORY TRAFFIC CONTROL DEVICES TO BE MODIFIED AS NEEDED FOR THE DURATION OF THE DETOUR.
 2. STREET NAMES MAY BE USED WHEN DESIRABLE FOR DIRECTING DETOURED TRAFFIC.

(D) TYPICAL APPLICATION - ROADWAY CLOSED BEYOND DETOUR POINT.



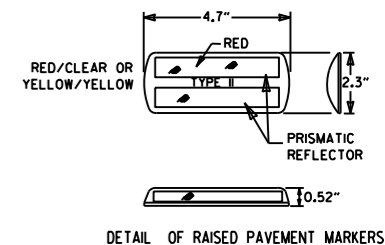
NOTES:
 1. FLOOD LIGHTS SHOULD BE PROVIDED TO MARK FLAGGER STATIONS AT NIGHT AS NEEDED.
 2. IF ENTIRE WORK AREA IS VISIBLE FROM ONE STATION, A SINGLE FLAGGER MAY BE USED.
 3. CHANNELIZING DEVICES ARE TO BE EXTENDED TO A POINT WHERE THEY ARE VISIBLE TO APPROACHING TRAFFIC.
 4. AUTOMATED FLAGGER ASSISTANCE DEVICE (AFAD) OPTIONAL. REFER TO MUTCD.

(E) TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES ON 2-LANE HIGHWAY WHERE ONE LANE IS CLOSED AND FLAGGING IS PROVIDED.



(F) TYPICAL APPLICATION - 4-LANE UNDIVIDED ROADWAY WITH INSIDE LANE CLOSED.

- KEY:
- FLAGGER
 - POSITIVE BARRIER
 - ARROW PANEL (IF REQUIRED)
 - TYPE III BARRICADE
 - CHANNELIZING DEVICE
 - TRAFFIC DRUM
 - RAISED PAVEMENT MARKER



TYPICAL ADVANCE WARNING SIGN PLACEMENT

TAPER FORMULAE:

$L = S \times W$ FOR SPEEDS OF 45MPH OR MORE.

$L = \frac{W \times S^2}{60}$ FOR SPEEDS OF 40MPH OR LESS.

WHERE:
 L = MINIMUM LENGTH OF TAPER.

S = NUMERICAL VALUE OF POSTED SPEED LIMIT PRIOR TO WORK OR 85TH PERCENTILE SPEED.

W = WIDTH OF OFFSET.

- GENERAL NOTES:
1. THE MAINTENANCE DIVISION SHALL CONDUCT A BALL BANK STUDY TO DETERMINE THE ADVISORY SPEED LIMIT PRIOR TO OPENING TO TRAFFIC. THE ADVISORY SPEED WILL BE POSTED ON W1-3 OR W1-4 CURVE WARNING SIGNS. USE W1-4 WHEN SPEED IS GREATER THAN 30MPH AND W1-3 WHEN 30MPH OR LESS.
 2. WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45) SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-1(55) SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
 4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT, OR AS DIRECTED BY THE ENGINEER.
 5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
 6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
 7. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER. WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.
 8. DIMENSIONS SHOWN FOR RAISED PAVEMENT MARKERS ARE TYPICAL. THE CONTRACTOR MAY SUBSTITUTE SIMILAR MARKERS WITH THE APPROVAL OF THE ENGINEER. REQUESTING APPROVAL FOR SIMILAR MARKERS MAY BE MADE BY REFERRING TO THE ARDOT QUALIFIED PRODUCTS LIST.
 9. ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

DATE	REVISION	FILED
11-07-19	REVISED NOTE 1, ADDED NOTE 9	
9-2-15	REVISED NOTE 2, ADDED NOTE 8, REVISED DRAWING (A) & REPLACED R2-5A WITH W3-5	
9-12-13	REVISED DETAIL OF RAISED PAVEMENT MARKERS	
3-11-10	ADDED (AFAD)	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED GENERAL NOTE	
10-18-96	ADDED R55-1	
4-26-96	CORRECTED (a) BEHIND G20-2	
6-8-95	CORRECTED SIGN IDENT. ON W1-4A	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

TRAFFIC CONTROL DEVICES

VERTICAL DIFFERENTIAL	LOCATION	TRAFFIC CONTROL	
		≤ 45 MPH	> 45 MPH
≤ 2"	CENTERLINE	W8-11 AND LANE STRIPING	W8-11 AND LANE STRIPING
> 2"	CENTERLINE	STANDARD LANE CLOSURE	STANDARD LANE CLOSURE
≤ 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9, EDGE LINE STRIPING, AND VERTICAL PANELS	W8-9, EDGE LINE STRIPING, AND VERTICAL PANELS
> 3"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND VERTICAL PANELS	W8-17, EDGE LINE STRIPING, AND VERTICAL PANELS
≤ 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽¹⁾	A STABILIZED WEDGE, W8-17, EDGE LINE STRIPING AND TRAFFIC DRUMS ⁽¹⁾
> 24"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER ⁽⁴⁾ & EDGE LINES	PRECAST CONCRETE BARRIER ⁽⁴⁾ & EDGE LINES

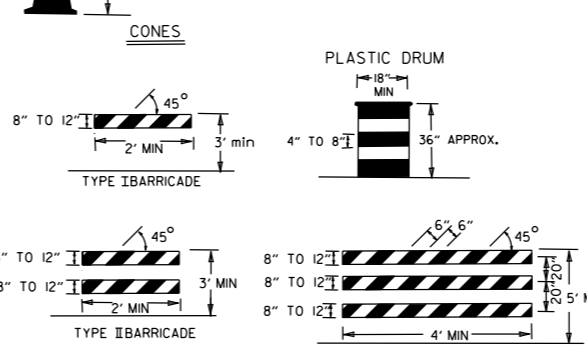
INTERSTATE		
VERTICAL DIFFERENTIAL	LOCATION	TRAFFIC CONTROL
≤ 2"	CENTERLINE	W8-11 AND LANE STRIPING
≤ 2"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-9, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 2"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	W8-17, EDGE LINE STRIPING, AND TRAFFIC DRUMS ⁽²⁾
> 6"	EDGE OF TRAVELED LANE OR EDGE OF SHOULDER	PRECAST CONCRETE BARRIER & EDGE LINES

INTERSTATE AND NON-INTERSTATE		
FORESLOPE	HEIGHT	TRAFFIC CONTROL
1:1	> 2 FT	PRECAST CONCRETE BARRIER
2:1	≤ 5 FT	TRAFFIC DRUMS
2:1	> 5 FT	PRECAST CONCRETE BARRIER
Flatter than 2:1	N/A	TRAFFIC DRUMS

- GENERAL NOTES:
1. WHEN THE SHOULDER AREA IS USED AS PART OF THE TRAVELED LANE AND THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, THEN VERTICAL PANELS SHALL BE USED.
 2. WHEN THERE IS INSUFFICIENT WIDTH TO PLACE TRAFFIC DRUMS ON THE REMAINING SHOULDER WIDTH, A STABILIZED WEDGE SHALL BE USED. PRECAST CONCRETE BARRIER WALL CAN BE USED IN LIEU OF A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS, IF AND WHERE DIRECTED BY THE ENGINEER. A STABILIZED WEDGE, W8-17 SIGN, EDGE LINE STRIPING, AND TRAFFIC DRUMS CAN BE USED IN LIEU OF PRECAST CONCRETE BARRIER WALL, IF AND WHERE DIRECTED BY THE ENGINEER.
 3. W21-5, W21-5a, AND/OR W21-5b SIGNS SHALL BE USED WHERE THE ROADWAY IS UNOBSTRUCTED IF AND WHERE DIRECTED BY THE ENGINEER.

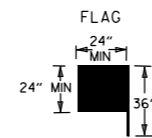
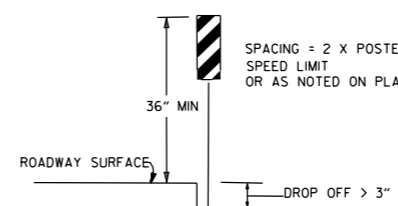
CHANNELIZING DEVICES

WHEN CONES ARE USED ON FREEWAYS AND MULTI-LANE HIGHWAYS, THEY SHALL BE 28" MIN. DURING HOURS OF DARKNESS, 28" CONES SHALL BE USED ON ALL ROADWAYS, AND SHALL BE REFLECTORIZED IN ACCORDANCE WITH THE M.U.T.C.D.

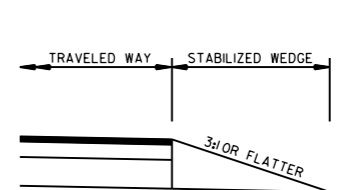


NOTE: FOR ALL ROAD CLOSURES, THE TYPE III BARRICADES SHALL BE OF SUFFICIENT LENGTH TO EXTEND ACROSS ENTIRE ROADWAY.

VERTICAL PANEL PLACEMENT



FLAG SHALL BE OF GOOD GRADE RED MATERIAL

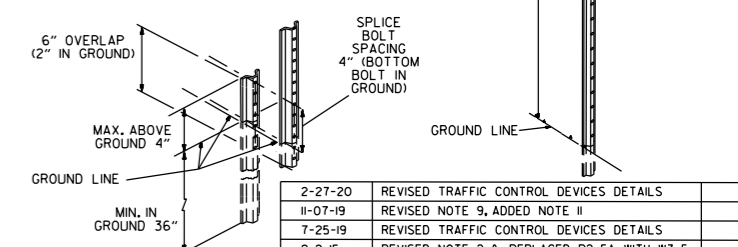


STABILIZED WEDGE

NOTE: MATERIALS FOR THE STABILIZED WEDGE SHALL MEET THE REQUIREMENTS PROVIDED IN SECTION 603.02 OF THE STANDARD SPECIFICATIONS.

DETAIL OF SPLICES: SIGN BOLT, POST SHALL NOT EXTEND ABOVE SIGN, ADDITIONAL POST, SPLICE BOLTS, 6 inch MIN. OVERLAP, 18 inch MINIMUM OVERLAP, SIGN POST, 30 inch MIN. GROUND TO SPLICE.

NOTES: USE SPLICES ONLY WHEN NECESSARY FOR INSTALLATION. TYPICAL INSTALLATION SHOULD HAVE NO SPLICES (SEE STD. DRAWING NO. SHS-2) NORMAL INSTALLATIONS WILL REQUIRE 1/4" DIA. BOLTS TO MOUNT SIGNS TO POST AND 5/16" DIA. BOLTS TO ASSEMBLE THE VARIOUS POST SUPPORTS. EACH OF THESE BOLTS SHALL BE CARRIAGE BOLTS. SIGN POSTS SHALL BE PAINTED GREEN; SIGNS SHALL NOT BE PAINTED, AND ALL SIGN POSTS SHALL BE PLUMB.



DATE	REVISION	FILMED
2-27-20	REVISED TRAFFIC CONTROL DEVICES DETAILS	
11-07-19	REVISED NOTE 9, ADDED NOTE II	
7-25-19	REVISED TRAFFIC CONTROL DEVICES DETAILS	
9-2-15	REVISED NOTE 2 & REPLACED R2-5A WITH W3-5	
10-15-09	ADDED REFERENCE TO MASH	
11-20-08	REVISED SIGN DESIGNATIONS	
11-18-04	ADDED NOTE	
10-1-98	ADDED NOTE	
4-03-97	ADDED (SP) TO W6-18 & REVISED TRAFFIC CONTROL DEVICES NOTE	
10-18-96	ADDED R55-1	
10-12-95	MOVED UPPER SPLICE	
6-8-95	REVISED SPLICE DETAIL, TEXT	6-8-95
2-2-95	REVISED PER PART VI, MUTCD, SEPT. 3, 1993	
8-15-91	DRAWN AND PLACED IN USE	

(A) TYPICAL APPLICATION - DAYTIME MAINTENANCE OPERATIONS OF SHORT DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

(B) TYPICAL APPLICATION - 3-LANE ONEWAY ROADWAY WHERE CENTER LANE IS CLOSED.

KEY:

- ○ ○ ○ ARROW PANEL (IF REQUIRED)
- CHANNELIZING DEVICE
- TRAFFIC DRUM

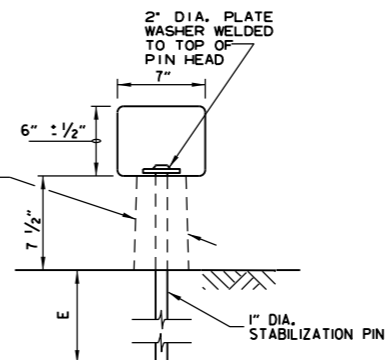
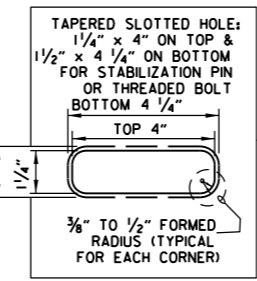
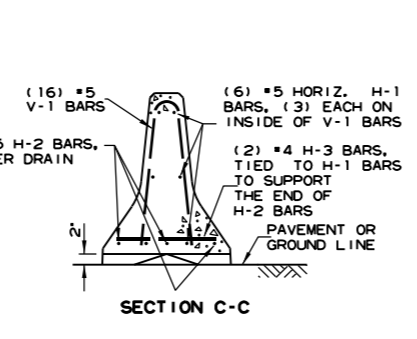
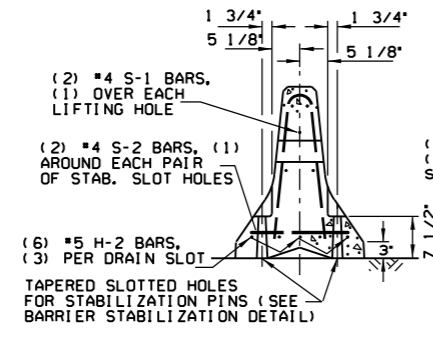
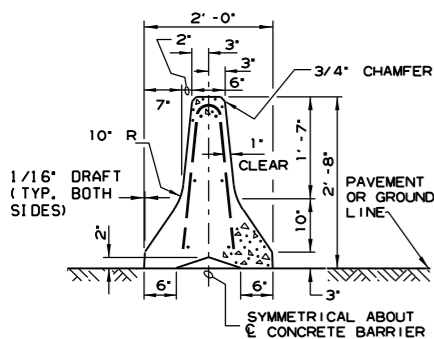
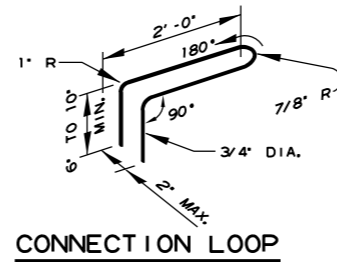
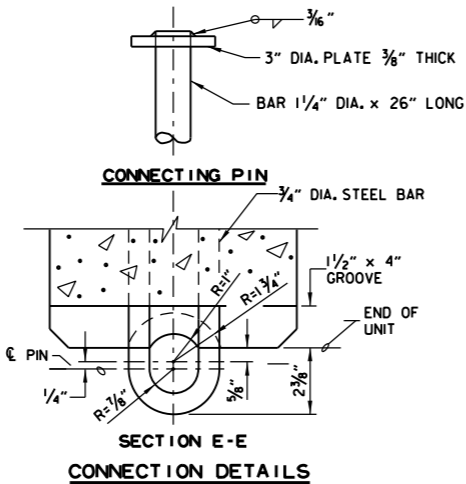
GENERAL NOTES:

1. A SPEED LIMIT REDUCTION MAY BE IMPLEMENTED ONLY WHEN DESIGNATED IN THE PLAN OR WHEN RECOMMENDED BY THE ROADWAY DESIGN DIVISION.
2. WHEN THE EXISTING SPEED LIMIT IS 55MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 45MPH, THE R2-1(55) SHALL BE OMITTED AND THE W3-5 SHALL BE INSTALLED AT THAT LOCATION. ADDITIONAL R2-1(45) SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
3. WHEN THE EXISTING SPEED LIMIT IS 65MPH AND THE PLANS REQUIRE A SPEED LIMIT OF 55MPH, THE R2-1(65) SHALL BE OMITTED. ADDITIONAL R2-1(55) SPEED LIMIT SIGNS SHALL BE INSTALLED AT A MAXIMUM OF 1MILE INTERVALS. AT THE END OF THE WORK AREA A R2-1(XX) SHALL BE INSTALLED TO MATCH ORIGINAL SPEED LIMIT.
4. THE MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT. BEYOND THE TAPER, MAXIMUM SPACING SHALL BE TWO TIMES THE SPEED LIMIT OR AS DIRECTED BY THE ENGINEER.
5. WARNING LIGHTS AND/OR FLAGS MAY BE MOUNTED TO SIGNS OR CHANNELIZING DEVICES AT NIGHT AS NEEDED.
6. PAVEMENT MARKINGS NO LONGER APPLICABLE WHICH MIGHT CREATE CONFUSION IN THE MINDS OF VEHICLE OPERATORS SHALL BE REMOVED OR OBLITERATED AS SOON AS PRACTICABLE.
7. THE G20-1 SIGN WILL BE REQUIRED ON JOBS OF OVER TWO MILES IN LENGTH. WHEN THE LANE CLOSURE IS NOT AT THE BEGINNING OF THE PROJECT, THE G20-1 SIGN SHALL BE ERRECTED 125' IN ADVANCE OF THE JOB LIMIT. ADDITIONAL W20-1(1MILE) SIGNS ARE NOT REQUIRED IN ADVANCE OF LANE CLOSURES THAT BEGIN INSIDE THE PROJECT LIMITS.
8. FLAGGERS SHALL USE STOP/SLOW PADDLES FOR CONTROLLING TRAFFIC THROUGH WORK ZONES. FLAGS MAY BE USED ONLY FOR EMERGENCY SITUATIONS.
9. ALL PLASTIC DRUMS AND CONES SHALL MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
10. TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE DELINEATED BY AFFIXING CONSPICUITY MATERIAL IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER, WHEN PLACED ON OR ADJACENT TO THE SHOULDER AND NOT BEHIND A POSITIVE BARRIER, THESE DEVICES SHALL BE DELINEATED BY PLACING FIVE (5) TRAFFIC DRUMS, EQUALLY SPACED ALONG THE TRAFFIC SIDE OF THE DEVICE.
11. ALL TRAILER MOUNTED DEVICES SUCH AS ARROW PANELS AND PORTABLE CHANGEABLE MESSAGE SIGNS SHALL MEET THE REQUIREMENTS OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).

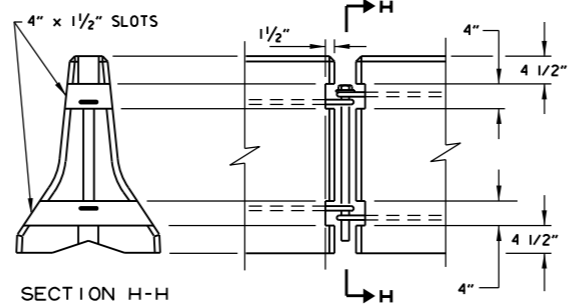
(C) TYPICAL APPLICATION - CONSTRUCTION OPERATIONS OF INTERMEDIATE TO LONG TERM DURATION ON A 4-LANE DIVIDED ROADWAY WHERE HALF OF THE ROADWAY IS CLOSED.

(D) TYPICAL APPLICATION - CLOSING MULTIPLE LANES OF A MULTILANE HIGHWAY.

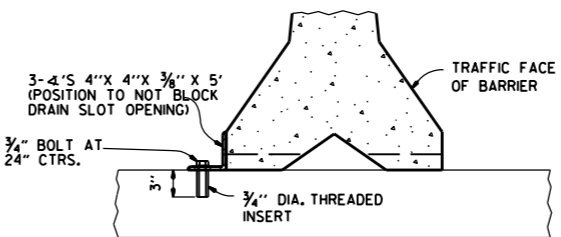
REINFORCING BAR TABLE PER BARRIER UNIT			
MARK	LOCATION	BAR SIZE (NO. BARS)	SKETCH
H-1	HORIZONTAL IN BARRIER TIED INSIDE V-1 BARS	#5 (6)	19'-3"
H-2	CENTERED ABOVE DRAIN SLOTS LONG. & TRANSVERSELY	#5 (6)	6'-6"
H-3	TIED ABOVE H-1 BARS TO SUPPORT H-2, TIED TO V-1	#4 (2)	1'-6"
S-1	OVER LIFT HOLES	#4 (2)	2'-5" LIFTING HOLE 3 3/8" R 90°
S-2	HORIZ. AROUND SLOTS BETWEEN V-1'S & DRAIN SLOTS	#4 (2)	1 1/2" R SLOTS 5'-1" BAR W/ (4) 1 1/2" R BENDS & MIN. 1'-0" OVERLAP
V-1	VERTICAL IN BARRIER (3) EACH END & (2) AT EACH DRAIN SLOTS	#5 (16)	TOTAL LENGTH 4'-9" 2 3/16" R 12° 4 3/8" 2'-1 3/8"



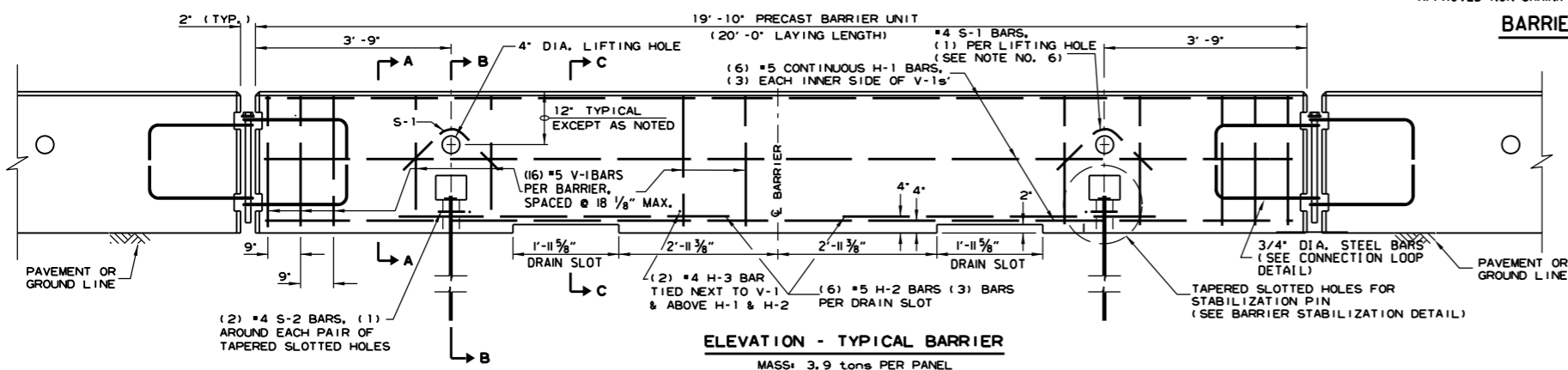
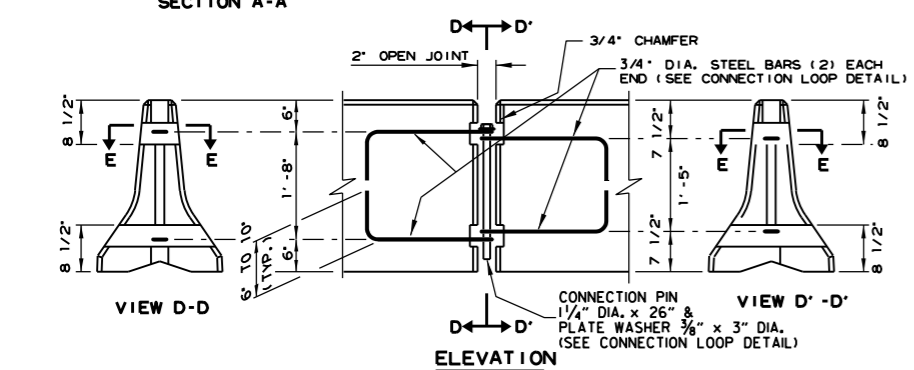
BARRIER STABILIZATION DETAIL
ROADWAY SECTION
E 4" - CONCRETE PAVEMENT
8" - ASPHALT PAVEMENT
12" - SHOULDER AREAS



BARRIER REMOVAL SLOT DETAILS



BARRIER STABILIZATION DETAIL
BRIDGE DECKS



ELEVATION - TYPICAL BARRIER
MASS: 3.9 tons PER PANEL

- GENERAL NOTES**
- THE CONTRACTOR SHALL FURNISH THE PRECAST CONCRETE BARRIER UNITS AND SHALL BE RESPONSIBLE FOR THE MANUFACTURE, SHIPMENT, STORAGE, PLACEMENT AND REMOVAL. AT THE COMPLETION OF THE PROJECT, THE PRECAST UNITS WILL REMAIN THE PROPERTY OF THE CONTRACTOR.
 - MATERIALS SHALL MEET THE FOLLOWING MINIMUM REQUIREMENTS:
CONCRETE: 2500 PSI COMPRESSIVE STRENGTH AT 28 DAYS.
REINFORCING STEEL: AASHTO M 31 OR M 53, GRADE 60
STRUCTURAL STEEL: AASHTO-M270 GRADE 36 SHALL BE USED FOR THE CONNECTION PIN, CONNECTION LOOPS, AND STABILIZATION PINS. A ONE PIECE PIN WITH A 3" ROUNDED TOP MAY BE USED IN PLACE OF THE DETAILED CONNECTION PIN.
DELINEATORS: DELINEATORS SHALL BE MOUNTED AT 10' SPACING ON TOP OF PRECAST BARRIER.

IN APPLICATIONS WHERE BARRIER WALL IS WITHIN 6 FEET OF A TRAFFIC LANE, ADDITIONAL DELINEATORS SHALL BE PLACED ON THE BARRIER AT 10' SPACING APPROXIMATELY ONE (1) FOOT FROM THE TOP OF THE BARRIER. DELINEATORS SHALL BE ON THE ARDOT QUALIFIED PRODUCTS LIST FOR CONSTRUCTION CONCRETE BARRIER MARKERS. DELINEATOR COLOR SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR DELINEATORS SHALL BE CONSIDERED INCLUDED IN THE PRICE BID PER LIN. FT. FOR "FURNISHING AND INSTALLING PRECAST CONCRETE BARRIER". THE CONTRACTOR SHALL CERTIFY TO THE ENGINEER THAT THE MATERIAL AND THE DESIGN USED IN THE PRECAST BARRIER UNITS MEETS THE REQUIREMENTS AS SHOWN ON THIS STANDARD DRAWING.
 - OTHER PRECAST CONCRETE BARRIERS THAT HAVE BEEN CRASH TESTED AND APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION TO MEET THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) WILL BE ACCEPTED OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL FURNISH A CERTIFICATION OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH) COMPLIANCE FOR ANY OTHER TYPES OF PRECAST BARRIER TO BE USED. THE CERTIFICATION SHALL STATE THAT THE PRECAST CONCRETE BARRIER MEETS THE REQUIREMENTS OF MANUAL FOR ASSESSING SAFETY HARDWARE (MASH). MIXING OF SHAPES WILL NOT BE ALLOWED IN A CONTINUOUS LINE OF UNITS.
 - DOWEL HOLES IN PAVEMENT OR BRIDGE SLABS THAT ARE TO REMAIN IN PLACE SHALL BE FILLED. HOLES IN CONCRETE PAVEMENT AND BRIDGE SLABS SHALL BE FILLED WITH AN APPROVED NON-SHRINK EPOXY GROUT. HOLES IN ASPHALT PAVEMENT SHALL BE FILLED WITH AN APPROVED ASPHALT JOINT FILLER. PAYMENT FOR DRILLING AND FILLING HOLES TO BE INCLUDED IN THE PRICE FOR VARIOUS BARRIER ITEMS.
 - ATTACH UNITS TO ROADWAY SURFACE WITH STABILIZATION PINS AND TO DECK SLABS USING BOLTS WHEN REQUIRED.
 - A 4" WHITE PVC SLEEVE MAY BE USED TO FORM THE LIFTING HOLE AND IF USED THE SLEEVE IS TO BE LEFT IN PLACE.

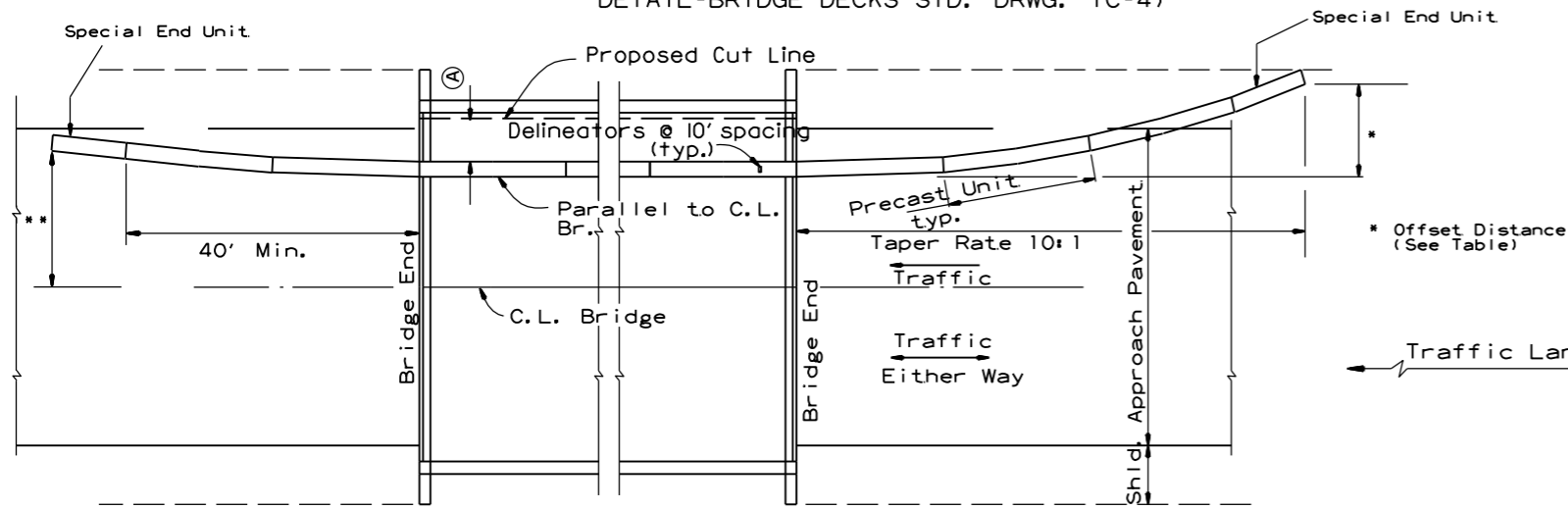
11-07-19	REVISED NOTE 3	
2-27-14	REVISED BARRIER STABILIZATION DETAIL	
10-15-09	ADDED REFERENCE TO MASH	
8-5-09	REV. NOTE 3 CONCERNING DRAIN SLOTS	
11-29-07	REVISED NOTE 3	
5-25-06	DELETED GENERAL NOTE 7	
11-18-04	REVISED BARRIER STABILIZATION DETAIL BRIDGE DECKS	
4-10-03	REVISED GENERAL NOTE 2	
8-22-02	ISSUED NEW DRAWING	
DATE	REVISION	FILMED

ARKANSAS STATE HIGHWAY COMMISSION

STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION -
TEMPORARY PRECAST BARRIER

STANDARD DRAWING TC-4

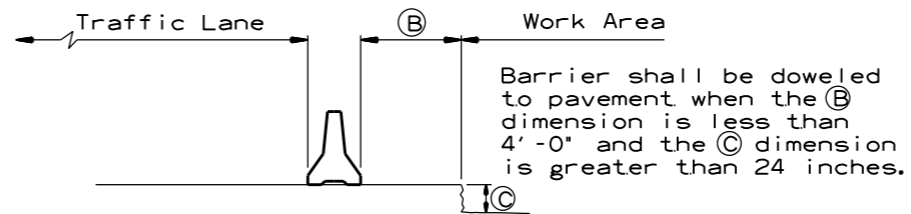
(A) 4 feet or greater preferred. If less than 4 feet, Precast Units shall be connected to slab (SEE BARRIER STABILIZATION DETAIL-BRIDGE DECKS STD. DRWG. TC-4)



BARRIER PLACEMENT ALONG BRIDGE WITH OFFSET

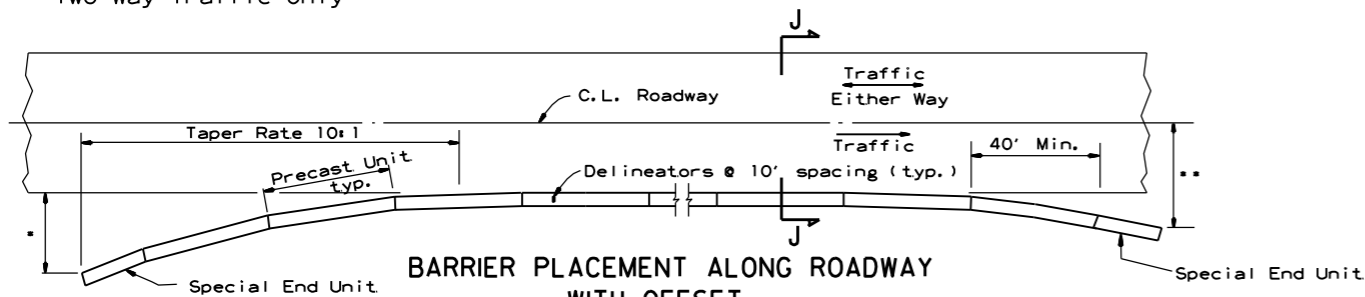
No Scale

** Offset Distance for Two Way Traffic Only



SECTION J-J

No Scale



BARRIER PLACEMENT ALONG ROADWAY WITH OFFSET

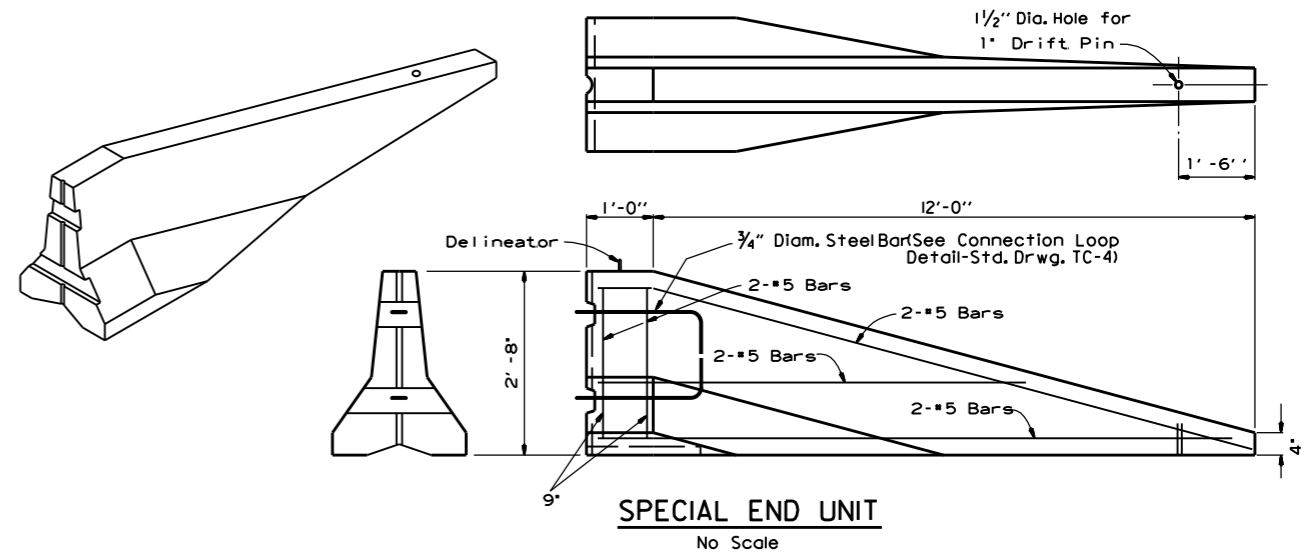
No Scale

* Offset Distance (See Table)

** Offset Distance For Two Way Traffic Only

Speed (MPH)	Offset Distance (FT.)
≤ 45	12
> 45	18

If offset distance is not attainable, then see 'Barrier Placement With Attenuator' Detail shown below.

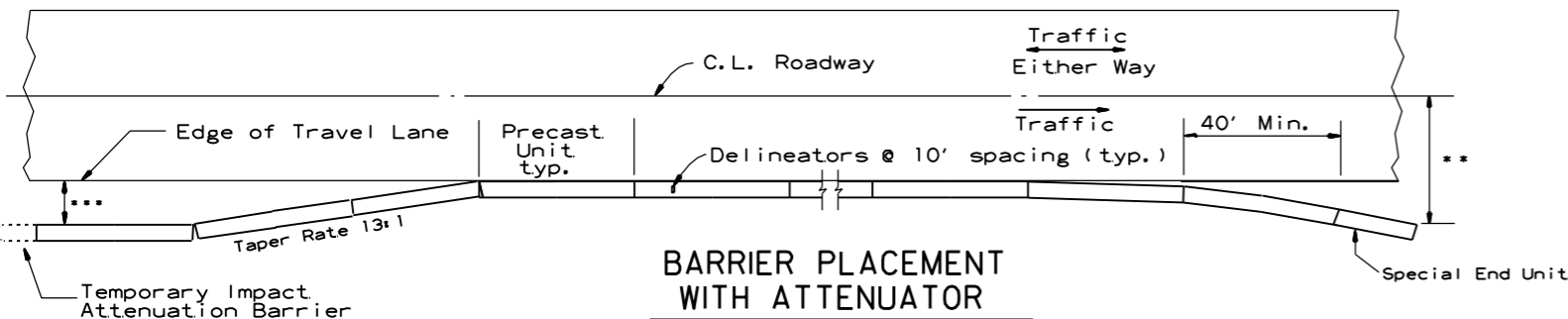


SPECIAL END UNIT

No Scale

General Notes

When shown on the Plans, the ends of the Temporary Precast Concrete Barrier shall be protected with a Manual For Assessing Safety Hardware (MASH) approved Crash Cushion. Payment for Crash Cushions shall be made under the item of "Temporary Impact Attenuation Barrier."



BARRIER PLACEMENT WITH ATTENUATOR

No Scale

** Offset Distance For Two Way Traffic Only

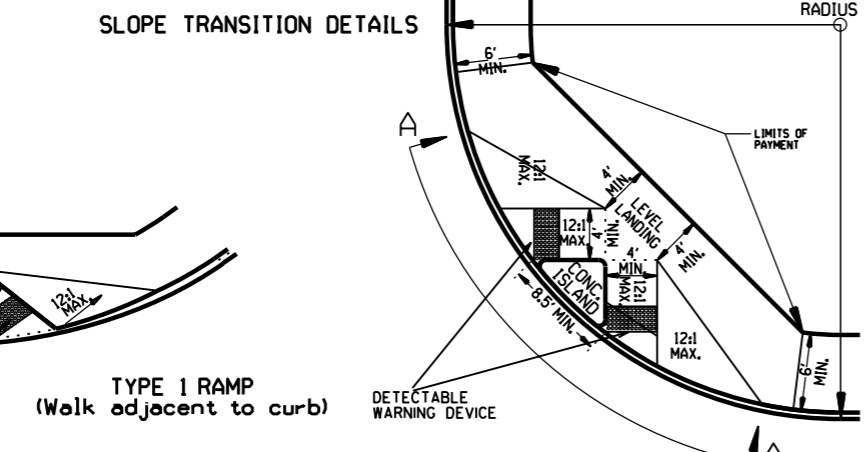
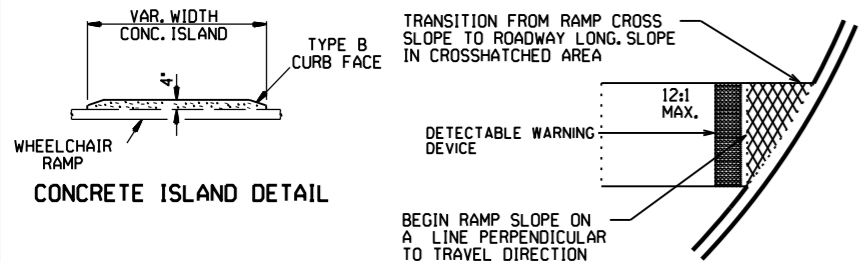
*** Min. 3'-0" From Edge of Travel Lane to Nearest Edge of Attenuator

DATE	REVISION	FILMED
11-07-19	REVISED NOTE	
10-15-09	ADDED REFERENCE TO MASH	
5-25-06	REVISED BARRIER PLACEMENT	
8-22-02	ISSUED NEW DRAWING	

ARKANSAS STATE HIGHWAY COMMISSION

**STANDARD TRAFFIC CONTROLS
FOR HIGHWAY CONSTRUCTION -
TEMPORARY PRECAST BARRIER**

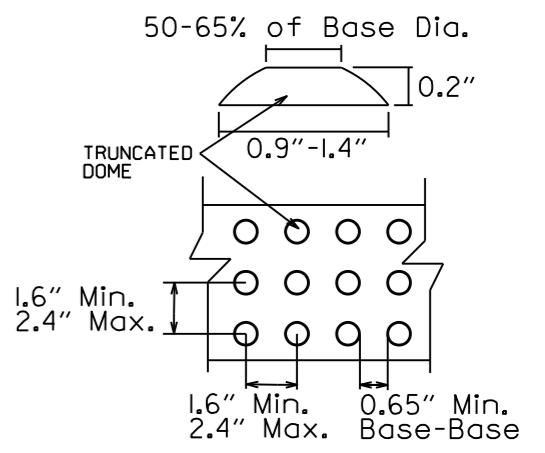
STANDARD DRAWING TC-5



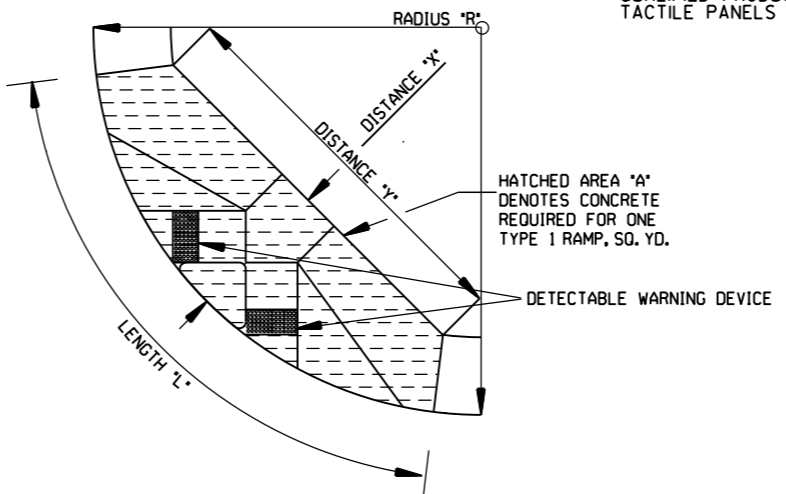
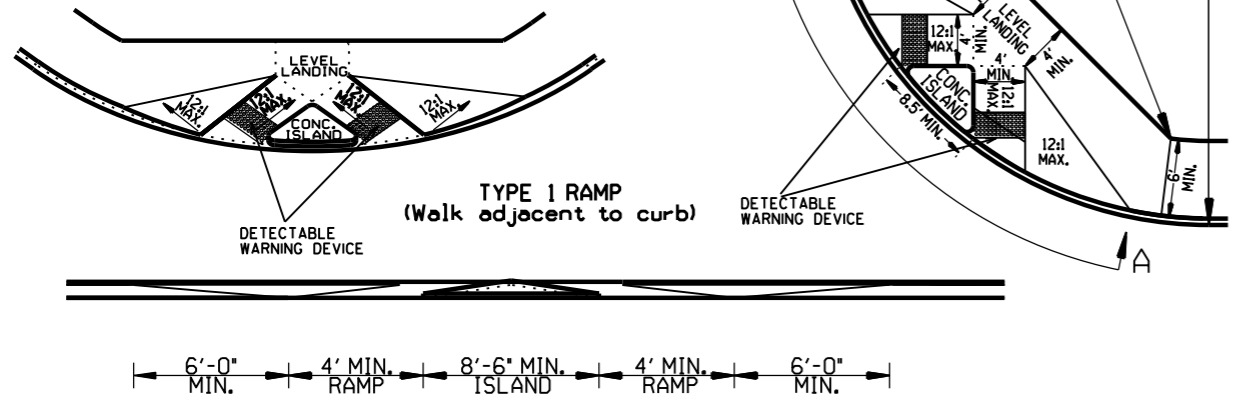
TYPE 1 RAMP DIMENSIONS AND QUANTITIES

RADIUS "R"	DISTANCE "X"	DISTANCE "Y"	LENGTH "L"	RAMP AREA "A"
FEET	FEET	FEET	FEET	SQ. YD.
15	11.67	18.82	32.18	26.21
20	11.52	22.28	35.46	30.07
25	11.43	25.60	38.77	33.80
30	11.37	30.26	40.93	36.90
35	11.33	33.51	43.11	39.77
40	11.30	36.45	45.26	42.45
45	11.27	39.16	47.34	44.97
50	11.25	41.69	49.36	47.35
55	11.24	44.07	51.31	49.63
60	11.22	46.33	53.21	51.80

GENERAL NOTES FOR DETECTABLE WARNING DEVICES
 THE DETECTABLE WARNING DEVICE SHALL BE LOCATED SO THAT THE NEAREST EDGE OF THE DEVICE IS 6 TO 8 INCHES FROM THE FACE OF THE CURB.
 TRUNCATED DOMES IN THE DETECTABLE WARNING SURFACE SHALL MEET THE REQUIREMENTS OF THE GEOMETRIC CONFIGURATION SHOWN.
 DOMES SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINANT DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES.
 DETECTABLE WARNING DEVICE SHALL BE 24 INCHES IN THE DIRECTION OF TRAVEL AND EXTEND THE FULL WIDTH OF THE CURB RAMP OR FLUSH SURFACE.
 DETECTABLE WARNING DEVICE SHALL BE ON THE ADOT QUALIFIED PRODUCTS LIST FOR CAST-IN-PLACE TACTILE PANELS (ADA DETECTABLE WARNING).



DETECTABLE WARNING DEVICE DETAIL



NOTE: THE CROSS SLOPE OF THE RAMPS, LEVEL LANDINGS, AND SIDEWALKS SHALL NOT EXCEED 2.0% UNLESS REQUIRED TO MATCH STREET LONGITUDINAL GRADE.

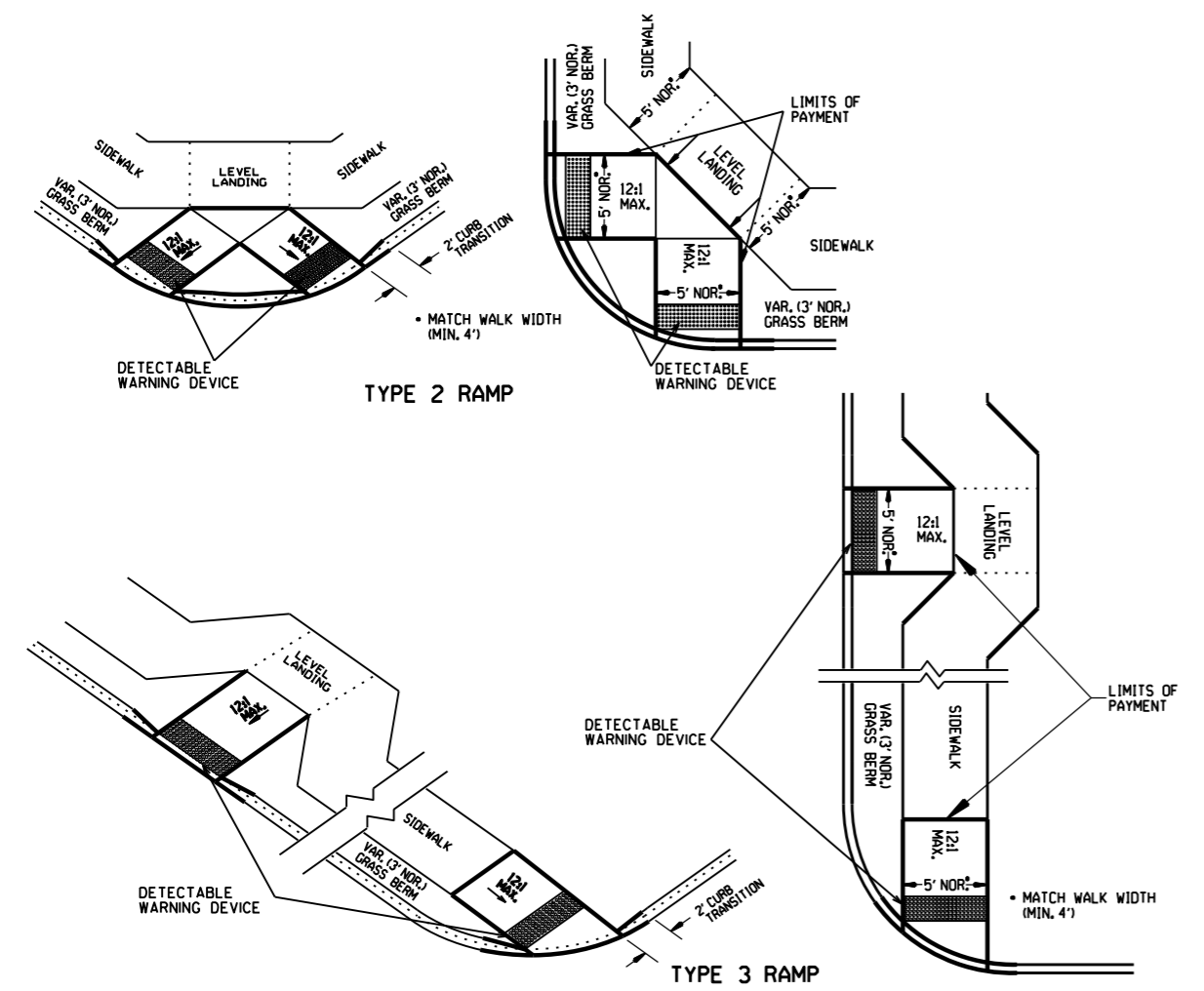
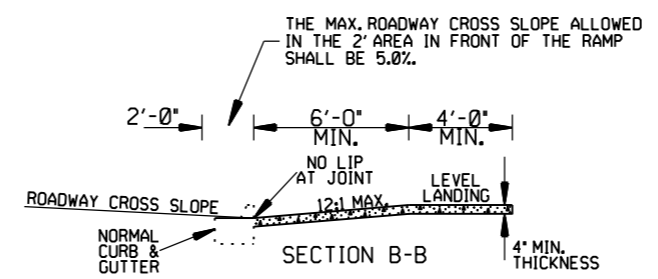
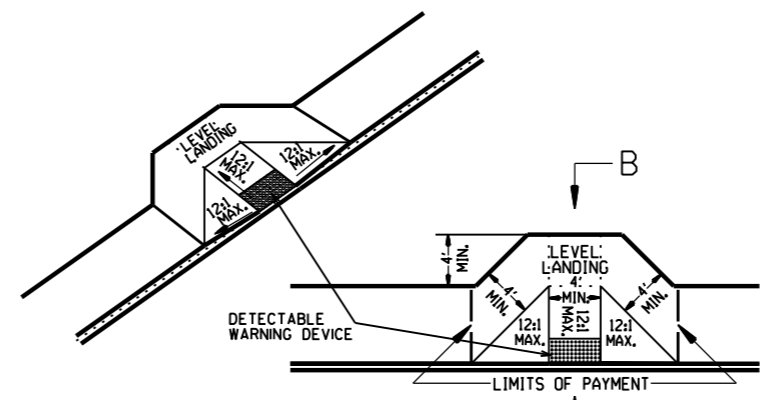
GENERAL NOTES:

IN NEW CONSTRUCTION, UNLESS OTHERWISE INDICATED ON THE PLANS, WHEELCHAIR RAMPS ARE TO BE PROVIDED AT ALL CORNERS OF CURBED STREET INTERSECTIONS AND MID-BLOCK CROSSWALK LOCATIONS.
 IN ALTERATIONS WHEELCHAIR RAMPS ARE TO BE PROVIDED AT CURBED STREET INTERSECTIONS WITH PEDESTRIAN TRAFFIC AND MID-BLOCK CROSSWALK LOCATIONS.
 THE LENGTH OF THE RAMP SHALL BE SUCH THAT THE SLOPE DOES NOT EXCEED 12:1. THE SURFACE TEXTURE OF THE RAMP SHALL CONFORM TO A CLASS 6 FINISH ACCORDING TO SECTION 802.19.
 THE NORMAL GUTTER GRADE SHALL BE MAINTAINED THROUGH THE AREA OF THE RAMP.
 ALL PAYMENT MARKINGS SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION.
 THE MINIMUM THICKNESS OF THE RAMP, WALK, & LANDING SHALL BE 4".
 THE MINIMUM WIDTH OF THE RAMPS SHALL BE THE WALK WIDTH OR 36", WHICHEVER IS GREATER.
 RAMPS SHALL BE MODIFIED AS NECESSARY TO INSURE THAT THEY ARE PARALLEL TO A LINE DRAWN FROM THE CENTER OF ONE RAMP TO THE CENTER OF THE RAMP ON THE OPPOSITE SIDE OF THE INTERSECTION.
 THE DIMENSIONS AND QUANTITIES SHOWN ON THIS DRAWING ARE FOR A 90° INTERSECTION ONLY. DIMENSIONS AND QUANTITIES FOR SKEWED INTERSECTIONS WILL VARY, AND ARE TO BE DETERMINED BY THE ENGINEER.

RAMP SELECTION CRITERIA

CHOICE	TYPE	DESCRIPTION
FIRST CHOICE	TYPE 1	CORNER LOCATIONS WITH THE WALK ADJACENT TO THE CURB (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 2	CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE INSUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE (BOTH NEW CONSTRUCTION AND ALTERATIONS).
	TYPE 3	CORNER LOCATIONS WITH THE WALK OFFSET FROM THE CURB A DISTANCE SUFFICIENT TO ALLOW THE REQUIRED RAMP SLOPE (BOTH NEW CONSTRUCTION AND ALTERATIONS).
SECOND CHOICE	TYPE 4	TANGENT LOCATIONS (BOTH NEW CONSTRUCTION AND ALTERATIONS).
THIRD CHOICE	TYPE 5	TANGENT LOCATIONS (ALTERATIONS ONLY).
FOURTH CHOICE	TYPE 6	CORNER LOCATIONS (ALTERATIONS ONLY). THIS RAMP MAY BE USED ONLY IF THE TYPE 5 RAMPS CANNOT BE PLACED AT THE ENDS OF THE RADIUS. IF SITE CONSTRAINTS PREVENT THE CONSTRUCTION OF ANY OF THE TYPES LISTED, THEN AND ONLY THEN CAN THE 12:1 MAX. SLOPE ON THE RAMP BE EXCEEDED TO PROVIDE ACCESS TO THE STREET LEVEL (ALTERATIONS ONLY). THE SLOPE CAN BE STEEPENED TO A 10:1 MAX. FOR A MAX. LENGTH OF 5' OR A 8:1 MAX. FOR A MAX. LENGTH OF 2'. SLOPES STEEPER THAN 8:1 ARE NOT ALLOWED UNDER ANY CIRCUMSTANCES.

NOTE: IN ALTERATIONS, THE SELECTION OF THE TYPE OF WHEELCHAIR RAMP TO BE CONSTRUCTED SHALL BE BASED ON THE AMOUNT OF RIGHT-OF-WAY AVAILABLE, AND ON THE PRESENCE OF OTHER SITE CONSTRAINTS (UTILITIES, BUILDINGS, ETC.). THE TABLE ABOVE LISTS THE ORDER IN WHICH THE RAMPS ARE TO BE CONSIDERED.
 AN ALTERATION IS DEFINED AS A PROJECT THAT CHANGES OR AFFECTS THE USE OF A PEDESTRIAN PATHWAY (OVERLAYS, SIGNALIZATION PROJECTS, ETC.) BUT DOES NOT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY. ALL PROJECTS THAT REQUIRE THE PURCHASE OF ADDITIONAL RIGHT-OF-WAY WILL USUALLY BE CONSIDERED NEW CONSTRUCTION FOR THE PURPOSES OF THE CHART ABOVE.



DATE	REVISION	DATE FILM
11-10-05	REVISED TO NEW SIDEWALK POLICY	
10-9-03	REVISED GEN. NOTES & ADDED NOTE	
4-10-03	REV. DETECTABLE WARNING DEVICES	
8-22-02	ADD DETECTABLE WARNING DEVICES	
3-30-00	ADD SLOPE, TRANS. & REV. ISL. DIMS.	
11-8-98	REVISED NOTES	
8-12-98	REVISED TEXTURE	
7-02-98	REDRAWN & REISSUED	
10-18-96	CORRECTED DIMENSIONS	10-18-96
5-24-90	FROM 10:1 MAX. SLOPES	5-24-90
7-15-88	ADJUSTED MAX. SLOPE	652-7-15-88
7-14-88	INCLUD. "CONC. ISLD." IN PAY ITEM	-----
6-02-76	ISSUED-P.H.D.	299-7-28-76