



**CONWAY**  
HISTORIC DISTRICT COMMISSION  
**OCTOBER**

**5:30pm • October 23, 2017 • City Hall**

# Robinson Historic District & Old Conway Design Overlay District

## **CONWAY HISTORIC DISTRICT COMMISSION MEETING**

OCTOBER 24 , 2017 • 5:30PM • CITY HALL • 1201 OAK STREET

### **AGENDA**

#### **MINUTES**

September 25, 2017

#### **PUBLIC HEARINGS** **ROBINSON HISTORIC DISTRICT**

None

#### **OLD CONWAY DESIGN OVERLAY DISTRICT**

A. Covered Carport - 611 Mitchell Street

#### **DISCUSSION**

B. Old Conway Village, LP - 725 and 819 Siebenmorgen Road, 1313 & 1323 Factory Street

C. Items as decided by the Commission

### **HISTORIC DISTRICT COMMISSION MEMBERS**

Steve Hurd, Chairman  
Marianne Black, Vice-Chairwoman  
Scott Zielstra, Secretary  
George Covington, Sr.  
Taylor Martin  
Betty Pickett  
Gerald Tosh

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**Conway Historic District  
Commission September 25, 2017  
Meeting Minutes**

City Hall - Downstairs Conference Room, 5:30 p.m.

**Roll Call**

Steve Hurd, Chairman - present  
Marianne Black, Vice Chairperson - present  
Scott Zielstra, Secretary - absent  
Taylor Martin - present  
George Covington, Sr. - present  
Betty Pickett - absent  
Gerald Tosh - present  
HDC City Staff - Bryan Patrick - present

**Finding of a Quorum.**

5 Commission members - Quorum present.

**Also in attendance:**

Bill Wilson, owner 1517 Caldwell Street 3600 Oakwood Rd, Little Rock, AR  
Frederick Simon, Conway 4H, 259 Hwy 64E, Conway  
Liz Hamilton, H+N Architects, Conway  
Joanna Nabholz, H+N Architects, Conway  
Richie Hambuchen, 86 Richland Hills, Conway  
Bob and Ann Cantrell, 44 Downwind Ave, Greenbrier  
Stanley Balmaz, 1210 Dogwood Trl, Conway

**Meeting Minutes**

August 28, 2017 minutes. Taylor Martin motioned for approval, seconded by Gerald Tosh. Minutes approved unanimously.

**Robinson Historic District Certificate of Appropriateness Review**

**Jan Clark Renovation, Remodel, and Demolition Review – 1517 Caldwell Street**

**Presentation:**

Ms. Clark would like to repair the front porch to be historically correct, do general maintenance, and replace the roof. She would also like to demolish non-historic parking cover and shed along with removal of the rear porch. The rear porch was added on as a screen porch and eventually enclosed. It is non-historic and in disrepair. A new parking lot will likely be wanted, but Ms. Clark stated that she would bring another request to the Commission to consider at a later date. The Planning Commission has recommended approval of a rezoning to allow an office use to the City Council. The Council will make the final decision tomorrow night. Mr. Wilson, owner of the property shared a photo of him as a child on the front porch to show the original

construction of the porch. It was noted that the current construction is similar.

**Neighborhood Discussion:**

None

**Commission Discussion:**

Mr. Hurd, as Chairman and HDC Architect, stated that he had visited the property with Ms. Clark to observe the condition of the structures. He felt that the request to demolish the non-historic structures and porch were appropriate. There was discussion about landscaping and the desire to screen any future parking area.

Motion made by Marianne Black to approve the renovation, remodel, and demolition with the following conditions:

- 1. Front porch renovations of "in-kind" repair approved.
- 2. Demolition of the non-historic shed and car cover shall be allowed.
- 3. Removal of the non-historic rear porch addition shall be allowed.

Motion seconded by Taylor Martin. Motion approved unanimously.

**Old Conway Design Overlay District Certificate of Appropriateness Reviews**

**Faulkner County 4-H Carport Cover**

**Presentation:**

Frederic Simon spoke on behalf of the Faulkner County 4-H requesting approval of a car cover for the 4-H passenger van. Mr. Simon stated that the van was recently repainted and is in good shape. They would like to place a metal car cover at the rear of the Faulkner County Extension Office parking lot (Old Presbyterian Church) where the van is currently parked.

**Neighborhood Discussion:**

None

**Commission Discussion:**

The Commission encouraged painting the cover or ordering one painted in a muted color to reduce daylight glare. Mr. Covington pointed out that the Conway Fire Department has a similar cover.

Motion made by George Covington to approve the car cover with the following condition:

- 1. The carport cover shall be constructed and placed as shown in submitted plans.

Motion seconded by Gerald Tosh. Motion approved unanimously.

**New Commercial Building Review – 931 Locust Avenue**

**Presentation:**

Ms. Liz Hamilton presented the project. The old structure was destroyed by fire earlier this year. The owner wants to place a new structure with the same footprint on the existing slab replacing the old walls and roof. The new structure will resemble the historic structure but will have additional doors and windows.

**Neighborhood Discussion:**

The adjoining property owners spoke concerning the lack of drainage and ponding of water in and around their buildings. There is concern about the lack of storm water infrastructure in the area. A building to the south was constructed many years ago that blocks the natural flow of water. The owners felt that the City needs to address the existing drainage problems and don't want any additional drainage problems. The height of the burned out strutters' slab was questioned. The height will remain the same.

**Commission Discussion:**

Mr. Hurd, as Chairman explained that the HDC did not review drainage. Mr. Patrick stated that as part of development review, the City Engineering Department would be encouraged to take a close look to possibly find solutions.

Motion made by George Covington to approve the new structure with the following conditions:

- 1. The commercial structure shall be constructed as shown in submitted plans.

Motion seconded by Marianne Black. Motion approved unanimously.

**H3 Home + Design Warehouse Addition Review – 801 1st Street**

**Presentation:**

Ms. Joann Nabholz presented the project. The owners would like to create a large addition on the existing warehouse. The existing structure is a large metal building with a concrete block wainscot that predates the creation of the Old Conway Design Overlay District. The owner would like to make the addition with similar materials and look. Mr. Richie Hambuchen asked if the curb cut could possibly be made wider to allow semi-truck deliveries better access. Currently, the property had one large continuous curb cut with exposed loading and dumpsters. The proposed structure would create a curb cut, provide landscaping, and screen the dumpsters. Mr. Patrick explained that the typical allowed curb cut width is 40 feet, but understood the need to serve the warehouse and that the project would be an improvement over the existing situation.

**Neighborhood Discussion:**

None.

**Commission Discussion:**

The Commission discussed the appropriate curb cut width and decided that a 60 foot curb cut would be acceptable.

Motion made by George Covington to approve the new structure with the following conditions:

- 1. The warehouse addition, parking, and loading area shall be constructed as shown in submitted plans.
- 2. A 60 foot wide curb cut to allow truck delivery is approved.
- 3. Landscaping including understory trees along First Street, shall meet zoning ordinance development review regulations.
- 4. Zoning ordinance development review regulations shall address lighting, dumpster pen size and materials, parking, etc.
- 5. A 5 foot sidewalk is required along First Street or an in-lieu fee of \$2025 must be paid. The sidewalk location should be coordinated with City Planning, Engineering, and Permits Departments.

Motion seconded by Taylor Martin. Motion approved unanimously.

**Discussion**

Upcoming 2018 HDC membership was discussed.

**Adjourn**

Marianne Black made the motion to adjourn. Seconded by George Covington. The meeting was adjourned.

**A EAVES COVERED CARPORT - 611 MITCHELL STREET**

**OLD CONWAY DESIGN OVERLAY DISTRICT  
CERTIFICATE OF APPROPRIATENESS REVIEW  
COVERED CARPORT ADDITION  
611 MITCHELL STREET**

**APPLICANT/OWNER**

Lindsey Eaves/Angela Longing  
611 Mitchell Street  
Conway, AR 72034

**SITE**

**Address.** 611 Mitchell Street 2 lots north of the intersection of Scott and Mitchell Streets on the west side of Mitchell Street.

**Lot Area.** ± 0.17 acres

**Present Zoning.** R-2A (Large Lot Duplex). The property is within the Old Conway Design Overlay Suburban District.

**Abutting Zoning.** R-2A (Large Lot Duplex) in the Old Conway Design Overlay Suburban District.

**Surrounding Area Structures.** The property is located in the Old Conway Design Overlay Suburban District. Area historic structures consist of single family residences in ranch, vernacular cottage, craftsman bungalow, and minimal traditional.

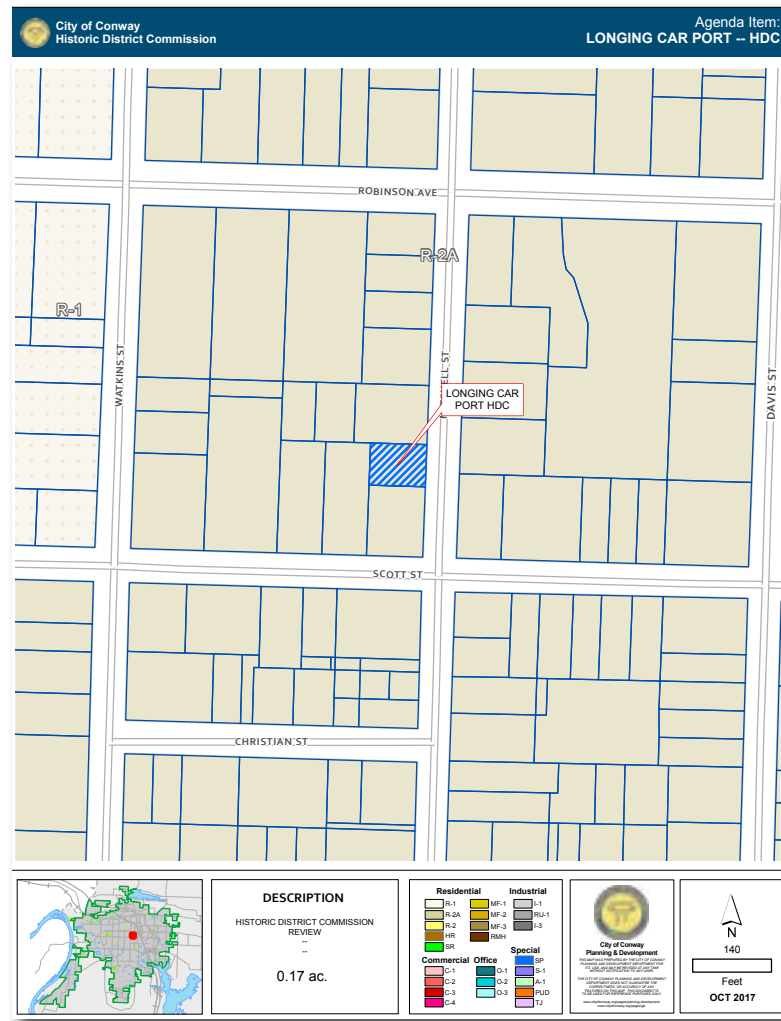
**General Description of Property and Proposed Development.** The submitted site plan and application describe a concrete slab with an arbor like carport cover 14 feet wide x 40 feet long on the north side of the residence.

**Setbacks.** Proposed setbacks should respect the predominant setbacks of area structures, especially the front setback. Front setbacks are allowed within a range of 80% to 110% of the average area front setback. From the submitted graphics, the front setback of the carport is unclear. However, it will be no closer to the street than the front of the residence. The north side of the carport is shown as 5 feet from the north property line. The 5 foot side setback is appropriate. Additional information concerning the front setback can be obtained at the meeting.

**Spacing.** Established spacing distance pattern between area structures. Spacing of the carport is similar to area garage/carport structures.

**Lot Coverage.** The Old Conway District allows up to 60% impervious lot coverage. The carport and concrete driveway are well under the maximum impervious percentage.

**Orientation.** The direction in which the front of a building faces. The carport faces Mitchell Street the front facade orientation of the residence is not affected.



**Alley.** There are no alleys in the property's block.

**Driveway/Parking/Carport.** There is an existing gravel ribbon driveway leading to the west side of the residence. The gravel strips are proposed to be changed to 2 foot wide concrete strips.

**Sidewalks.** There is a new sidewalk along Mitchell Street. **Fences.** There is an existing wooden privacy fence around the back yard.

**Fences.** No fencing is proposed.

**Tree preservation.** There are several large trees along the north property line. It would appear that the proposed concrete slab and carport will not affect these trees. The applicant can provide additional information at the meeting.

**MASSING**

**Scale.** The size of new construction in relation to neighboring structures and the proportion of structures to the human scale. The carport is in scale with the residence or other area structures.



Staff provided sketch showing massing and scale



**Height.** The average height of area structure’s eaves and cornices. Also, the first floor elevation / height relationship. The 10 foot height is nicely proportioned with the existing residence. The carport has a slightly sloping shed roof.

**Width.** New construction proportions shall respect the average widths of the majority of neighboring buildings in the area. The carport’s width is around 14 feet. The overall width seems appropriate with the existing residence and other area structures.

**Directional expression.** Measurement of the height to width ratio of a structure’s elevation. The height/vertical expression of the carport is in proportion to the existing home’s eave line.

**Footprint.** The area of land covered by a structure should be in relation to the majority of neighboring structures. The residence’s habitable footprint area is unaffected. The footprint of the carport structure is long in nature at 40 feet, however, the deep length will largely not be visible from the street. It is assumed that the 40 foot depth is desired to allow two cars to be placed under the carport.

**Complexity of form.** The level of detailing and breaks in wall planes of a structure. The carport’s design is similar to an open arbor with open rafter tails. This design is historically fitting with the residence and other area structures.

**Façade, wall area, rhythm.** Facades shall be compatible with surrounding historic structures in proportion of wall to opening area. The carport’s facade will feature an open rafter tail design.

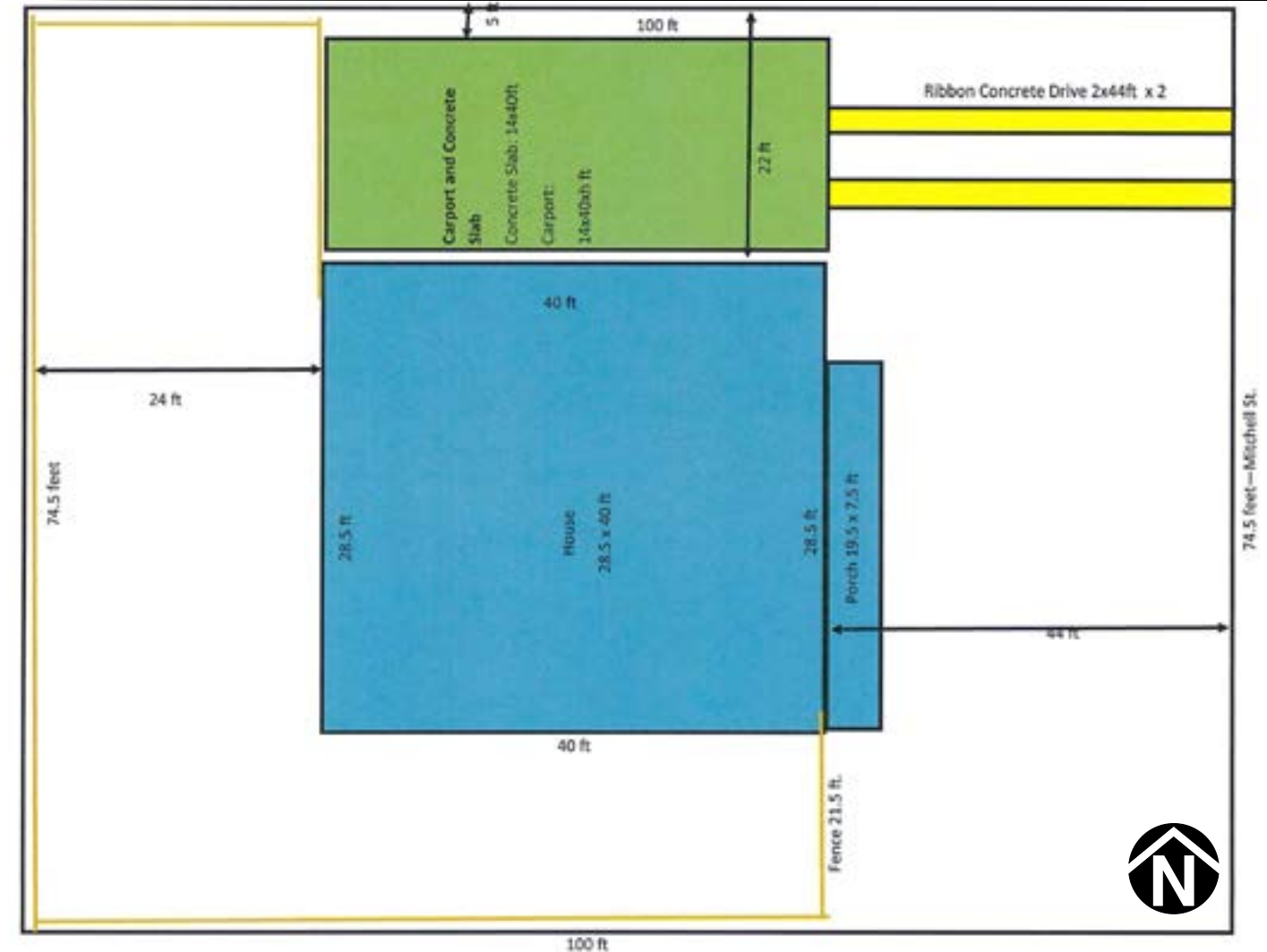
**DESIGN ELEMENTS**

**Style.** The existing residence is a cottage bungalow style. The style of the carport is arbor like with open rafter tails. This style blends with the existing residence.

**Entries, Porches, and Porticos.** Existing entries and porches are not affected by the carport.

**Doors and windows.** The rhythm, patterns, and ratio of voids to solids of new buildings should relate to and be compatible with adjacent facades; typically a higher proportion of voids to wall area. Not applicable. The carport is open on all sides.

Applicant provided sketch



**Awnings.** When new construction uses awnings, traditional awning designs, materials, and placement should be used. Not applicable.

**Lighting.** Any new lighting should be inward, downward, and shrouded so as to stay within the bounds of the property. No new lighting is proposed.

**MATERIALS & DETAILING**

**Architectural Details, Siding, and Bricks.** Synthetic materials such as vinyl, aluminum, and synthetic stucco (EIFS products) are not historic cladding materials and should not be used. The carport appears to be constructed with wood. Submitted photos show posts that are substantially sized.

**Shutters.** If used, shutters should be in proportion to their window opening. If they were closed, they should cover the window opening. Not applicable.

**Roof.** Submitted photos appear to show a metal roof. The roof will largely be unseen from the street. The applicant can provide roof details at the meeting.

**Decks/Plaza Space.** Not applicable.

**Skylights.** Not applicable.

**Mechanical Screening.** HVAC units should be visually located away from streets or screened by landscaping. Exterior HVAC ductwork should not be visible from the street. Not applicable

**RECOMMENDATIONS**

Staff recommends approval of the open carport, driveway, and parking slab with the following condition:

1. The 14’ wide x 40’ long x 10’ high carport, parking slab, and ribbon driveway shall be constructed as shown on the submitted plan.
2. The style of the carport shall be as shown in submitted photos with open rafter tails and substantially sized beams and joists.
3. Carport post shall be trimmed to be 8” x 8” minimum in size.
4. A corrugated metal roof shall be allowed.

**A EAVES COVERED CARPORT - 611 MITCHELL STREET**



Applicant provided picture indicating proposed style of carport



Applicant provided picture indicating proposed color of carport

**B OLD CONWAY VILLAGE, LP**

**OLD CONWAY DESIGN OVERLAY DISTRICT  
DISCUSSION AND PRE-REVIEW  
OLD CONWAY VILLAGE, LP  
675, 725, & 819 SIEBENMORGEN ROAD;  
1317 & 1323 FACTORY STREET**

**Northeast Old Conway Area Specific Plan Guidelines.** The Northeast Old Conway Area Specific Plan is a subset of the Old Conway Design Overlay District. Proposed structures within the NEOCA area should use a combination of design guidelines found in the NEOCA plan and the Old Conway Design Overlay District Pattern Book and ordinance. Specific structure forms; height, lot orientation, coverages, etc are found in the NEOCA plan. Design guidelines concerning architectural style, materials, windows, doors, detailing, etc are found in the Old Conway Design Guidelines.

Due to the large nature and of this development and its many details, the project will be presented to the Historic District Commission at 2 meetings. The October meeting will be for general discussion including public and Historic District Commission comments. Final drawings and details will be presented at the November meeting for HDC approval. Final drawings will incorporate comments and recommendations of the HDC.

**APPLICANT**  
Old Conway Village, LP  
1900 E Lark Lane  
Nixa, MO 65714

**OWNERS**  
City of Conway  
1201 Oak Street  
Conway, AR 72032

Melvin Williams  
4214 Hwy 60 E  
Bigelow, AR 72016

Hawks Family Team, LLC  
1720 Royal Drive  
Conway AR 72034

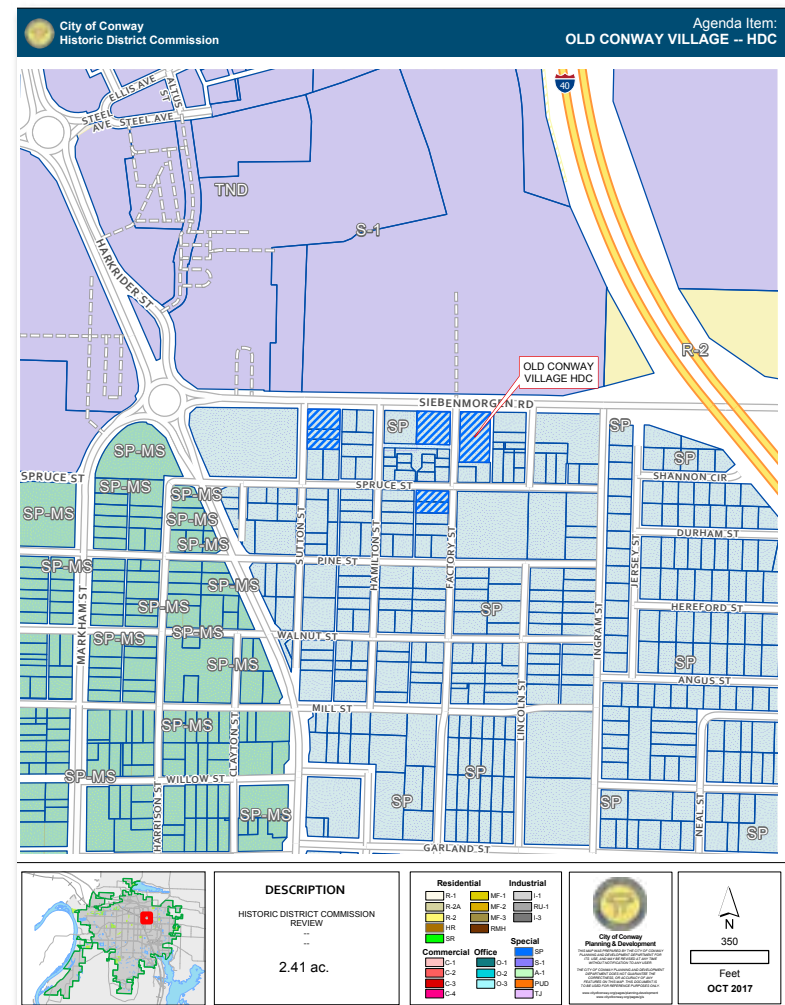
**SITE**.....

**Addresses.** 725 and 819 Siebenmorgen, 1317 and 1323 Factory Street

**Lot Area.** The project is spread across 4 separate tracts totaling 2.11 acres ±. An additional area at Lincoln and Siebenmorgen may be included.

**Present Zoning.** T3 (Suburban) and T4 (Urban Transition). The property is within the Northeast Old Conway Area Specific Plan.

**Abutting Zoning.** T3 (Suburban) and T4 (Urban Transition). The property is within the Northeast Old Conway Area Specific Plan.



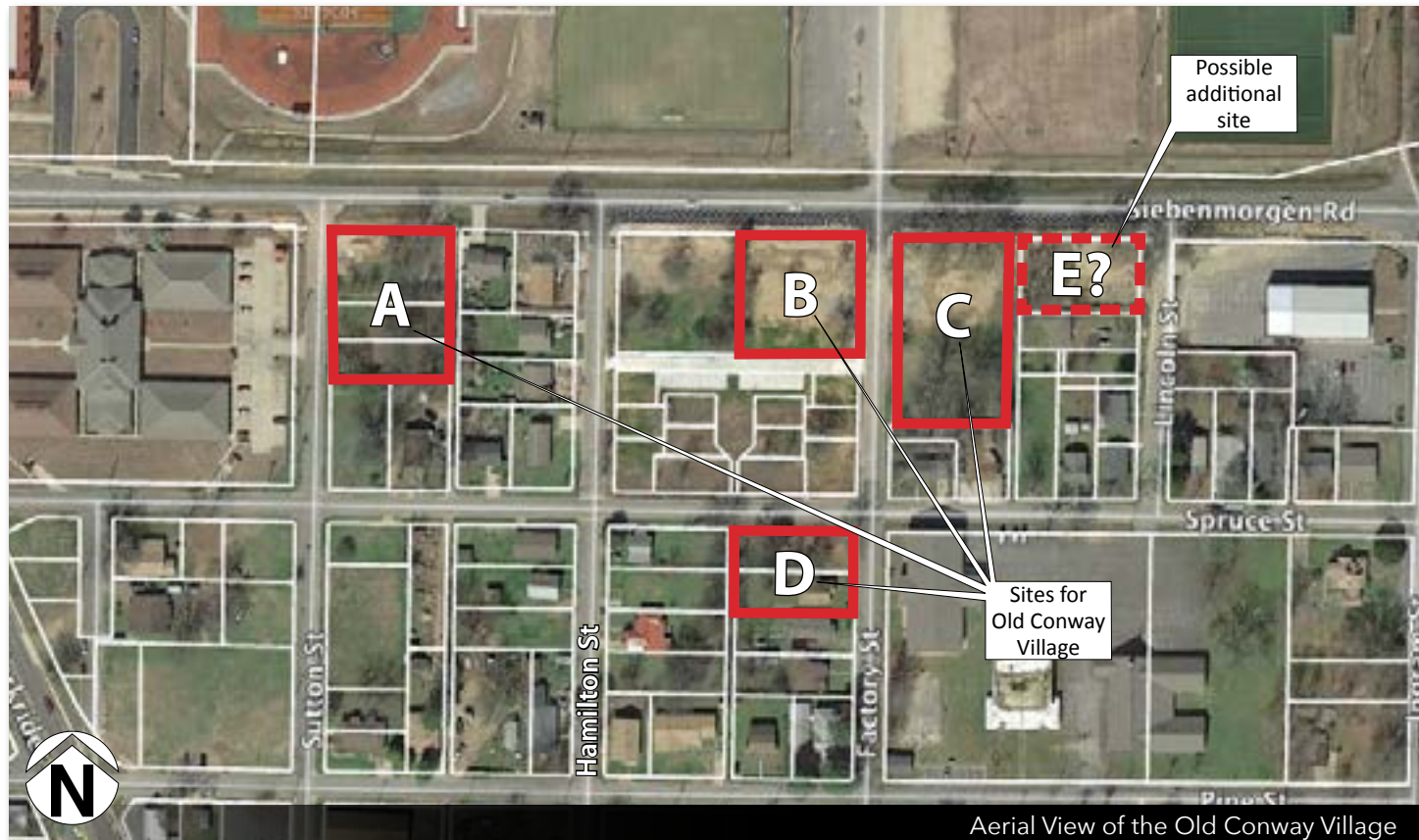
**Surrounding Area Structures.** The properties are located in the Northeast Old Conway Design Specific Plan. There are 4 separate tracts of land; the southeast corner of Sutton and Siebenmorgen (Labeled as A), the southwest and southeast corners of Factory and Siebenmorgen (Labeled as B and C), the southwest corner of Spruce and Factory (Labeled as D). Another tract of land (Labeled as E) at the southwest corner of Siebenmorgen and Lincoln may come into play.

Area structures mainly consists of ranch and minimal traditional residences. Nearby is the mid-century Pleasant Branch Baptist Church.

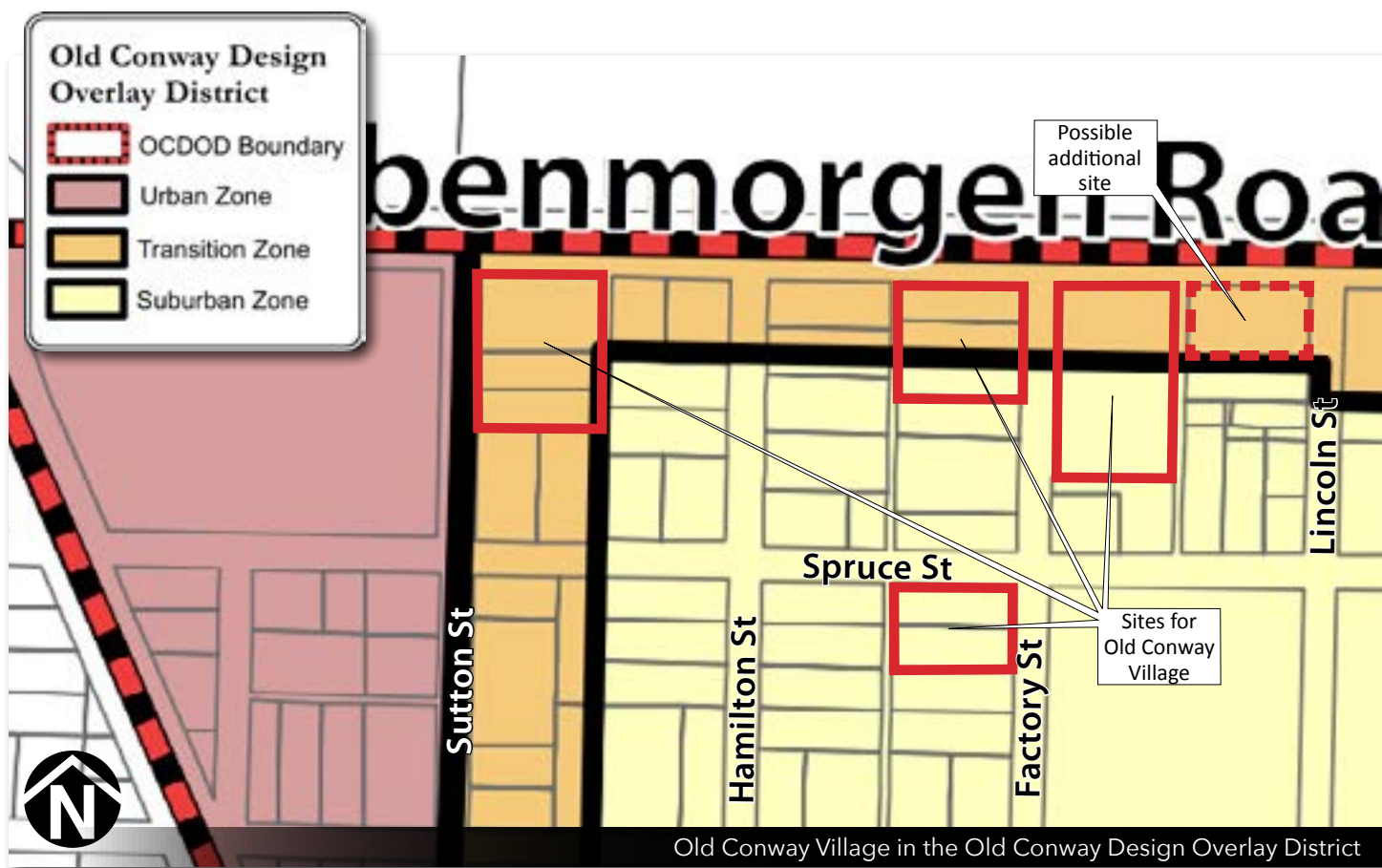
The Spruce Street Cottage project is planned to occupy the parcels central to this proposed development. The Spruce Street Cottages will be constructed by another developer. The HDC approved the design of the Cottages in January 2017.

**General Description of Property and Proposed Development.** The applicant is proposing to construct 7 (possibly 8) 3 bedroom, 2 bath, and 1 car garage townhome structures and possibly 3 single family detached homes. A clubhouse is also proposed on the south side of parcel A.

**B OLD CONWAY VILLAGE, LP - 675, 725, & 819 SIEBENMORGEN ROAD; 1317 & 1323 FACTORY STREET**



Aerial View of the Old Conway Village



Old Conway Village in the Old Conway Design Overlay District

All residential structures have rear loading single car garages.

All proposed townhome structures will be within the Northeast Old Conway Area Specific Plan T4 Transitional Zone. Townhomes are appropriate in the T4 Transitional Zone. 3 single family residences are possibly proposed in the T3 Suburban Zone on the south side of Spruce Street. If the property labeled E becomes available, the 3 single family structures will likely not be constructed as another townhome structure would be constructed instead.

**Setbacks.** Proposed setbacks should respect the predominant setbacks of area structures, especially the front setback.

NEOCA T4 setbacks:  
 Front: 6 feet minimum, 18 feet maximum  
 Side Street Side: 8 feet  
 Side Interior: 0 feet  
 Rear: 5 feet or 15 feet from centerline of alley, whichever is greater.

NEOCA T3 Setbacks:  
 Front: 17 feet minimum, 23 feet maximum  
 Side Street Side: 8 feet  
 Side Interior: 6 feet  
 Rear: 3 feet or 15 feet from centerline of alley, whichever is greater.

It is likely that some setback variances will be requested to construct as proposed. The NEOCA Plan area is a subset of the Old Conway Design Overlay District. As such, the HDC can grant variances to the above setbacks. The HDC should use the NEOCA setbacks as a guide to determine appropriate setbacks. The area is largely vacant making it difficult to use existing structures to determine an average area setback. The applicant will need to provide more detailed plans to establish suitable setbacks.

**Spacing.** Established spacing distance pattern between area structures. This area of the Pine Street neighborhood is largely vacant. Historically, homes would be spaced on typical residential lots in a gridded street neighborhood. This traditional area spacing has largely been lost due to the demolition of structures. The townhomes will be larger structures using a similar spacing pattern. The NEOCA Plan anticipated this pattern of development calling for townhomes along Siebenmorgen. Any proposed single family residential structures can more closely follow the traditional spacing pattern of the neighborhood.

**Lot Coverage.** The NEOCA Plan calls for up to 80% lot coverage in the T4 zone and 60% in the T3 zone. Lot coverage is defined as area impermeable to water. It appears that the proposed townhomes and parking areas will be in compliance as would any proposed single family structures.

**Orientation.** The direction in which the front of a building faces. Most townhomes are oriented to face abutting streets. However, 3 units are facing internally with the "front" facade facing abutting property (Parcels A, B and C. The rear of the units on A and C will be facing the abutting property. The front facade of the internal unit on Parcel B will be facing a newly constructed alley.

**Alley.** There are 3 unbuilt alleys and one newly constructed alley in the project area. An alley in area A and part of an alley in area C will need to be closed in order to construct as proposed. Alley closing documents are being prepared for City Council approval. The newly constructed alley will be used as the "front" of townhome units in Parcel B.

**Driveway/Parking/Carport.** Driveways and parking areas are planned for each townhome unit. The parking areas will be internally located between townhome units and will serve as the garage entry points. The townhome buildings will screen the parking areas and present a more pedestrian friendly facade to the public streets and alley. Proposed single family structures will have driveways accessed from an alley with rear garage entry.

**Sidewalks.** Sidewalk construction is required along all public streets.

**Fences.** No fencing is shown on the submitted plans. Tree preservation. No trees will be affected by the carport structure.

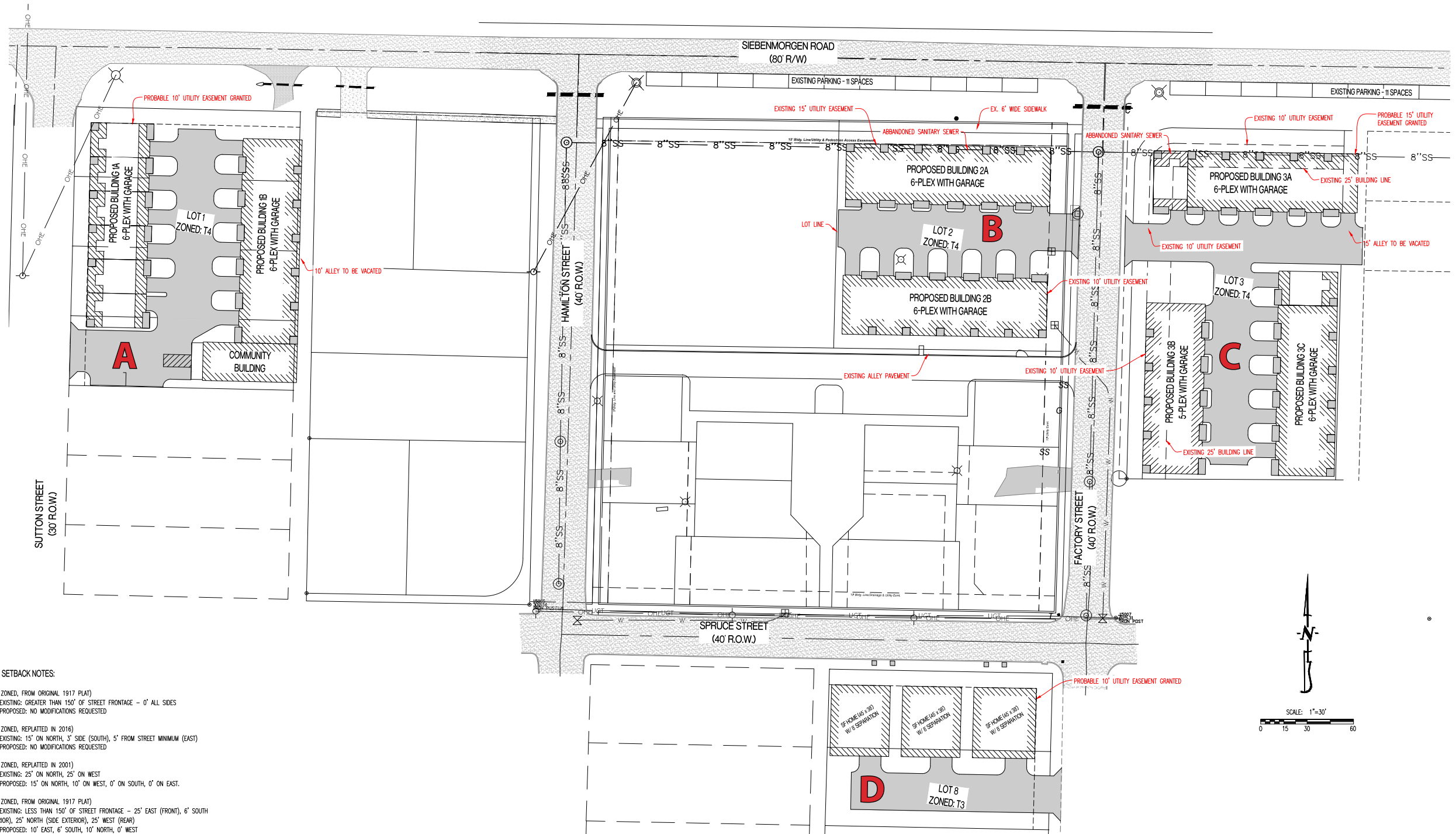
**Tree preservation.** There are several significant trees scattered throughout the project area. A more detailed landscaping plan will be needed to determine if any of these trees can be preserved. If tree removal is necessary remedial trees should be included in final landscape plans. Consideration for street trees should be given along public streets. However, the possibility for street trees will need to be coordinated local utilities and the City Engineering Department.

**MASSING**  
**Scale.** The size of new construction in relation to neighboring structures and the proportion of structures to the human scale. The townhomes are larger in overall scale than area residences. However, the NEOCA plan specifies townhomes as appropriate along Siebenmorgen. Townhomes will bring a new precedent in scale to the neighborhood. Structures should decrease in scale moving south into the Pine Street neighborhood creating a more typical single family residential scale.

**Height.** The average height of area structure's eaves and cornices. Also, the first floor elevation/height relationship. There are no dimensions on submitted building perspectives. An appropriate first floor height on the townhomes and single family residences is 9 feet minimum. A higher first floor height sets a more traditional vertical appearance to the structures. The elevations show a more traditional elevated entry on the front facade. The front entry height is highly encouraged.

**B OLD CONWAY VILLAGE, LP - 675, 725, & 819 SIEBENMORGEN ROAD; 1317 & 1323 FACTORY STREET**

Site Plan - Option #1



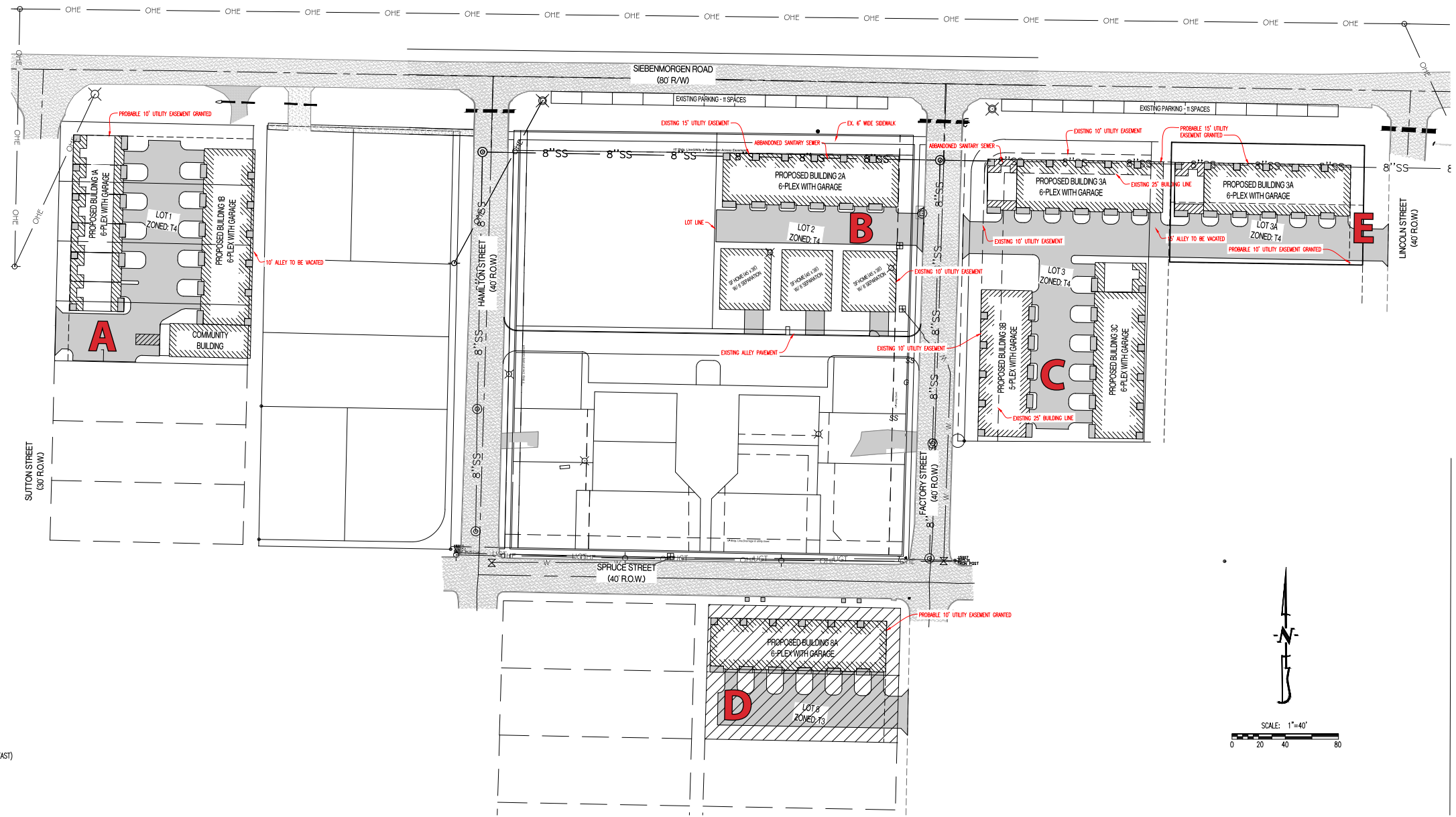
**BUILDING SETBACK NOTES:**

- LOT 1 (T4 ZONED, FROM ORIGINAL 1917 PLAT)  
EXISTING: GREATER THAN 150' OF STREET FRONTAGE - 0' ALL SIDES  
PROPOSED: NO MODIFICATIONS REQUESTED
- LOT 2 (T4 ZONED, REPLATTED IN 2016)  
EXISTING: 15' ON NORTH, 3' SIDE (SOUTH), 5' FROM STREET MINIMUM (EAST)  
PROPOSED: NO MODIFICATIONS REQUESTED
- LOT 3 (T4 ZONED, REPLATTED IN 2001)  
EXISTING: 25' ON NORTH, 25' ON WEST  
PROPOSED: 15' ON NORTH, 10' ON WEST, 0' ON SOUTH, 0' ON EAST.
- LOT 6 (T3 ZONED, FROM ORIGINAL 1917 PLAT)  
EXISTING: LESS THAN 150' OF STREET FRONTAGE - 25' EAST (FRONT), 6' SOUTH (SIDE INTERIOR), 25' NORTH (SIDE EXTERIOR), 25' WEST (REAR)  
PROPOSED: 10' EAST, 6' SOUTH, 10' NORTH, 0' WEST

REVISIONS:	
NO.	DATE
ORIGINAL	10/17/2017
THIS SHEET HAS BEEN SEaled, SEALED AND DATED ELECTRONICALLY	
TIMOTHY D. CROCKETT ARCHITECT - 19268	
 ENGINEERING CONSULTANTS 1000 W. Main Street, Suite 1 Conway, Arkansas 72034 www.crockettengineering.com Crockett Engineering Consultants, LLC Arkansas Certificate of Authority 0004	
OWNER:	NAME: ADDRESS: CITY, STATE:
OLD CONWAY VILLAGE SECTION 11 & 6, TOWNSHIP 6 NORTH, RANGE 13 WEST CONWAY, FAULKNER COUNTY, ARKANSAS	
DRAWING INCLUDES:	
SITE PLAN OPTION 1	
DESIGNED:	ATG
DRAWN:	ATG
PROJECT NO.:	170104
SHEET:	CE 1



Site Plan Option #2



**BUILDING SETBACK NOTES:**

- LOT 1 (T4 ZONED, FROM ORIGINAL 1917 PLAT)  
EXISTING: GREATER THAN 150' OF STREET FRONTAGE - 0' ALL SIDES  
PROPOSED: NO MODIFICATIONS REQUESTED
- LOT 2 (T4 ZONED, REPLATED IN 2016)  
EXISTING: 15' ON NORTH, 3' SIDE (SOUTH), 5' FROM STREET MINIMUM (EAST)  
PROPOSED: NO MODIFICATIONS REQUESTED
- LOT 3 (T4 ZONED, REPLATED IN 2001)  
EXISTING: 25' ON NORTH, 25' ON WEST  
PROPOSED: 15' ON NORTH, 10' ON WEST, 0' ON SOUTH, 0' ON EAST.
- LOT 3A (T4 ZONED, FROM ORIGINAL 1917 PLAT)  
EXISTING: LESS THAN 150' OF STREET FRONTAGE - 25' EAST (FRONT), 6' SOUTH (SIDE INTERIOR), 25' NORTH (SIDE EXTERIOR), 25' WEST (REAR)  
PROPOSED: 10' EAST, 6' SOUTH, 15' NORTH, 0' WEST

<b>REVISIONS:</b>	
NO.	DATE
ORIGINAL	10/17/2017
THE SHEET HAS BEEN SEEMED, SEALED AND DATED ELECTRONICALLY	
TIMOTHY D. CROCKETT ARCHITECT - 19268	
 ENGINEERING CONSULTANTS 1000 W. Main Street, Suite 1 Conway, Arkansas 72034 (501) 441-0292 www.crocketting.com Crockett Engineering Consultants, LLC Arkansas Certificate of Authority 0004	
OWNER:	NAME ADDRESS CITY, STATE
OLD CONWAY VILLAGE SECTION 11 & 6, TOWNSHIP 6 NORTH, RANGE 13 WEST CONWAY, FAULKNER COUNTY, ARKANSAS	
DRAWING INCLUDES:	
SITE PLAN OPTION 2	
DESIGNED:	ATG
DRAWN:	ATG
PROJECT NO.:	170104
SHEET:	CE 1

**Width.** New construction proportions shall respect the average widths of the majority of neighboring buildings in the area. The townhomes overall width is much larger than area homes. The townhome structures will set a new pattern of development along the Siebenmorgen area. This is congruent with the NEOCA plan. Proposed single family residences are similar in width to other area residences.

**Directional expression.** Measurement of the height to width ratio of a structure's elevation. The townhomes are taller and more vertical than area one story homes. A new precedent will be set in accordance with the NEOCA plan allowing townhomes along the Siebenmorgen area. The height to width of the proposed residences is similar to other area residences.

**Footprint.** The area of land covered by a structure should be in relation to the majority of neighboring structures. The townhome structure's footprints are larger in relation to other area structures. This larger footprint is congruent with the NEOCA plan. Proposed single family residence's footprints are similar to other area residences.

**Complexity of form.** The level of detailing and breaks in wall planes of a structure. The townhomes have a moderate level of detailing accomplished with breaks in wall planes, gables, and portico entries, and rear porch stoops. Paint colors are also used to differentiate. Additional breaks in wall planes, roof line, entry differentiation, etc, should be explored in order to create a more "individual" look to the townhome units. The proposed single family structures also have a moderate level of detailing. The side and rear facades are largely "flat". The front facade has a break in wall planes, gables, and an entry porch. The depth of the porch is unknown. Old Conway Design Guidelines require a 6 foot deep front porch in order to make the space usable. All 3 residences are proposed as the same design. Some variation to the facades should be considered.

**Facade, wall area, rhythm.** Facades shall be compatible with surrounding historic structures in proportion of wall to opening area. The townhomes appear to have an appropriate number of windows and doors with the exception of the north facing side elevations in Parcel area A. These elevations will be facing a major road and as such, could use additional windows and/or detailing. Possibly, the porches could be wrapped around the corner presenting more of a "front" facade along Siebenmorgen. The proposed single family residences seems to have the appropriate level of windows and doors.

**DESIGN ELEMENTS**

**Style.** The style should compliment the existing and area structures. The townhomes and residences are modern with craftsman like details such as wider window trim and tapered columns. The Pine Street neighborhood's residential styles are for the most part, not of a historic nature, mostly consisting of post WWII minimal traditional residences. The new structures set a precedent for future design.

**Entries, Porches, and Porticos.** Appropriate entry points are provided. Old Conway Design Regulations specify a 6 foot minimum porch depth. As shown, the townhome's front entries are more of a portico rather than a true porch. These could be increased in depth to create a usable porch if desired. There is a slightly larger porch/portico on the rear of the townhomes. This area is also in the transitional zoning district allowing a more "urban" form. This urban form could be more along a portico vs. a porch. The single family residences have a porch in the center of the front facade. The porch appears to be less than 6 feet in depth, but no dimensions are provided.

**Doors and windows.** Windows are shown with four over one dividers. Old Conway regulations discourage the use of false window dividers. One over one windows might be more appropriate. Front doors are shown as one half glass doors. This style door is more traditional and is encouraged.

**Awnings.** When new construction uses awnings, traditional awning designs, materials, and placement should be used. No awnings are proposed.

**Lighting.** Any new lighting should be inward, downward, and shrouded so as to stay within the bounds of the property. No lighting is shown on the plans. The project will be subject to Planning development review lighting guidelines.

**MATERIALS & DETAILING**

**Architectural Details, Siding, and Bricks.** Eaves, brackets, dentils, cornices, molding, columns, trim, etc. The are shown with a brick foundation wall and cement fiber board siding. Gables use a shake style siding to provide contrast to the horizontal siding. A horizontal banding board is shown at the second floor and top of first floor. It is assumed that vinyl will be used for fascia trim and gable siding. The applicant can provide details at the meeting. Wide trim in a more traditional style is proposed around windows and doors.

**Shutters.** Shutters are not shown on the submitted elevations. If used, shutters should be in proportion to their window opening. If they were closed, they should cover the window opening.

**Roof.** An asphalt composition roof is shown.

**Decks/Plaza Space.** No decks or patio space are shown.

**Skylights.** None are shown

**Mechanical Screening.** HVAC units should be visually located away from streets or screened by landscaping. Exterior HVAC ductwork should not be visible from the street.

**RECOMMENDATIONS**

The project is at a preliminary discussion phase. No Staff recommendations are being made at this time. As described in the text, areas that the HDC should possibly consider for discussion include:

1. Setbacks and any possible variances will need to be determined.
2. Tree preservation, remediation, possibility of street trees.
3. Landscaping details.
4. First floor heights and front entry heights.
5. Methods to create a more "individual" look for townhome and single family units.
6. North facing facades in Area A.
7. Porch depths/design to "individualize".
8. Siding and fascia materials.
9. Shutters - if used.

- Additional Points to keep in mind:
- Platting of the property is required. Additional street rights of way dedication may be required.
  - Planning Staff Development Review will be required. Development review includes project review by City Fire, Sanitation, Engineering, Planning and Conway Corporation.



